

**PLANNING COMMISSION
WEDNESDAY, FEBRUARY 10, 2016
6:00 PM**

AGENDA

I. 6:00 PM CALL TO ORDER - ROLL CALL

Jerry Greenfield, Chair Eric Postma, Vice Chair Peter Hurley
Al Levit Phyllis Millan
Simon Springall City Council Liaison Charlotte Lehan

II. 6:05 PM PLEDGE OF ALLEGIANCE

III. 6:10 PM CITIZEN'S INPUT

This is the time that citizens have the opportunity to address the Planning Commission regarding any item that is not already scheduled for a formal Public Hearing tonight. Therefore, if any member of the audience would like to speak about any Work Session item or any other matter of concern, please raise your hand so that we may hear from you now.

IV. 6:20 PM CITY COUNCIL LIAISON REPORT

V. 6:25 PM CONSIDERATION OF THE MINUTES

Consideration Of The January 13, 2016 Planning Commission Minutes

Documents: [WPC Jan 13 2016 DRAFT.pdf](#)

VI. 6:30 PM WORK SESSIONS

A. Transit Master Plan Public Involvement Strategy (Jen Massa Smith)

Documents: [Transit Master Plan Public Involvement Strategy.pdf](#)

Transit Master Plan PowerPoint Presentation

Documents: [Transit Master Plan PowerPoint Presentation.pdf](#)

VII. 7:15 PM INFORMATIONAL

A. Bike Wayfinding Signage Plan (Britney Buttacavoli)

Documents: [Bike Wayfinding Signage Plan - Small Pdf.pdf](#)

Bicycle Wayfinding PowerPoint Presentation

Documents: [Bicycle Wayfinding Powerpoint - Planning Commission.pdf](#)

VIII. 8:00 PM OTHER BUSINESS

IX. 8:05 PM ADJOURNMENT

Time frames for agenda items are not time certain.

Public Testimony

The Commission places great value on testimony from the public. People who want to testify are encouraged to:

- *Provide written summaries of their testimony*
- *Recognize that substance, not length, determines the value of testimony*
- *Endorse rather than repeat testimony of others*

Thank you for taking the time to present your views.

For further information on Agenda items, call Tami Bergeron, Planning Administrative Assistant, at (503) 570-1571 or e-mail her at bergeron@ci.wilsonville.or.us.

Assistive Listening Devices (ALD) are available for persons with impaired hearing and can be scheduled for this meeting.

The City will also endeavor to provide the following services, without cost, if requested at least 48 hours prior to the meeting:

- *Qualified sign language interpreters for persons with speech or hearing impairments
- *Qualified bilingual interpreters.

To obtain services, please call the Planning Administrative Assistant at (503) 682-4960

**PLANNING COMMISSION
WEDNESDAY, JANUARY 13, 2016
6:00 P.M.**

**Wilsonville City Hall
29799 SW Town Center Loop East
Wilsonville, Oregon**

DRAFT

Minutes

I. CALL TO ORDER - ROLL CALL

Acting Chair Greenfield called the meeting to order at 6:00 p.m. Those present:

Planning Commission: Jerry Greenfield, Eric Postma, Al Levit, Peter Hurley, Simon Springall, and Phyllis Millan.
City Councilor Charlotte Lehan was absent.

City Staff: Chris Neamtzu, Michael Kohlhoff, Miranda Bateschell

II. PLEDGE OF ALLEGIANCE

The Pledge of Allegiance was recited.

III. ELECTION OF CHAIR AND VICE CHAIR 2016

Al Levit nominated Jerry Greenfield as the 2016 Planning Commission Chair. Phyllis Millan seconded.

Peter Hurley moved to close nominations for Chair. Eric Postma seconded and the motion passed unanimously.

Jerry Greenfield was unanimously elected as the 2016 Planning Commission Chair.

Phyllis Millan nominated Eric Postma as the 2016 Planning Commission Vice Chair. Simon Springall seconded.

Peter Hurley moved to close nominations for Vice Chair. Al Levit seconded and the motion passed unanimously.

Eric Postma was unanimously elected as the 2016 Planning Commission Vice Chair.

IV. CITIZEN'S INPUT - This is an opportunity for visitors to address the Planning Commission on items not on the agenda. There was none.

V. CITY COUNCIL LIAISON REPORT

There was no City Council Liaison Report due to Councilor Lehan's absence.

Commissioner Millan asked Staff for a brief update on where things stood with regard to the City Council's vote on the Frog Pond Master Plan, and where the City was with that process.

Chris Neamtzu, Planning Director, said at the November 16, 2015 meeting, Council deliberated based upon several hours of testimony at the prior meeting and asked many questions of Staff. The item was continued at that time to ensure maximum City Councilor exposure. Staff proposed additional modifications to the Frog Pond Master Plan to enhance some of the recommendations from the Planning Commission hearing. Specific language was added to revisit the density of the attached row home product and the specific location of the retail center as part of master planning. Additionally, the Grange site was to have specific reference to arts

and culture rather than just environmental types of uses. The Council was comfortable adding more specificity and detail into the concept plan as placeholders. The text was very clear that this was not a unanimous position and that different points of view existed amongst the community members. Because the process was many years into the future, it made sense to try to codify that in specific language that anyone could pick up, read, understand, and then address when the time was more appropriate. The vote was 4 to 1 with Councilor Starr dissenting and the final document had been reposted on the project web page.

Commissioner Millan said she had read the information on the web page, but wanted to make sure she was reading it correctly, especially with regard to the density issue with which the Commission had struggled.

Chair Greenfield believed the changes made the recommendation stronger than the Commission had been comfortable with in its initial recommendation to City Council and he was pleased with the result.

VI. CONSIDERATION OF THE MINUTES

A. Consideration of the December 9, 2015 Planning Commission minutes

Chair Greenfield cited Robert's Rules of Order regarding the approval of minutes, noting that the minutes could be approved as distributed with any noted corrections without a formal motion. He confirmed the Commissioners did not object to implementing this change of procedure.

The December 9, 2015 Planning Commission minutes were approved as presented.

VII. WORK SESSION

A. Basalt Creek Concept Planning Update (Bateschell)

Miranda Bateschell, Long-Range Planning Manager, presented the update on the Basalt Creek Concept Plan Update and Next Steps via PowerPoint, noting that in December, the Joint City Council of the Cities of Tualatin and Wilsonville identified a preferred jurisdictional boundary for the Basalt Creek Planning Area.

Discussion and feedback from the Planning Commission was as follows with responses by Staff to various Commissioner questions, as noted:

- TDT was the acronym for Transportation Development Tax, and TLT was the Transit Lodging Tax; both were referenced in the Considerations for Success document (Page 4 of 5 of the Staff report). Mayor Knapp presented the Considerations for Success at December 16, 2015 Joint Council meeting and the Joint Council agreed to the document at that meeting. The Transit Lodging Tax was not specifically discussed at that meeting, but would likely be considered. Most of that tax was supposed to go to tourism.
- Item 5 in the Considerations for Success document stated that any substantial additional traffic loads from external locations would likely overload the system. A lot of regional traffic was expected from the development of 124th Ave in addition to the parkway. Traffic loads were projected in the study for the 124th Ave Extension, but the external locations mentioned in Item 5 regarded the issue of other regional facilities potentially connecting to 124th Ave, and the regional impacts that might create to this planning area.
 - The Joint Council wanted the Planning Commission to be attentive and aware of this in the event that those discussions took place because it would directly impact the planning area. The Joint Council wanted to be unified on those issues to ensure the planning area was protected moving forward.
 - A transportation model had been done for the planning area with the development projected in terms of the land uses. That study was available for the Commission. Traffic from the planning area was pretty well split between those traveling north and south. Impacts were definitely seen in the 20-year horizon from the 124th Ave Extension that would require upgrades to the City's intersections and to Grahams Ferry Rd, Boones Ferry Rd, and Day Rd.
 - The 124th Ave Extension would pass through the Sherwood industrial area. All the areas adjacent to Basalt Creek had master plans in place, including the Tonquin Employment Area, Southwest Tualatin

Concept Plan, and Coffee Creek Basalt Creek would be the last to be master planned. The entire area was approximately 2,000 cumulative acres.

- The study done was done in the spring of 2015, so to avoid the traffic congestion around the new Cabela's shopping area development.
- The 124th Ave Connector was a big concern for Wilsonville. They were intending to route traffic off the Tualatin/Sherwood Hwy south towards I-5 at the Elligsen junction. Including that in the model would be pertinent to the Basalt Creek development because the parkway and 124th Ave Extension went directly through the subject area. The 124th Ave Extension was to relieve traffic through the congested downtown area of Tualatin, not traffic on Tonquin Rd. Having a unified approach between Tualatin and Wilsonville was a challenge due to the Cities' opposing interests in the area with regard to traffic. It would be important to be specific with the traffic caps, although that might not be enough.
 - Washington County's role in the traffic routing was important because Tonquin Rd was a local road and the parkway would be a County road. The County is the lead on the 124th Ave Extension and Basalt Creek Parkway and had the modeling completed and shared the results with the City. The County sits on the Basalt Creek Technical Advisory Board.
- There was a long-term plan for an overpass, enabling the Basalt Creek Parkway to cross over I-5 at Greenhill Lane. The overpass was not shown on the Preferred Jurisdictional Boundary Map, as it was along the planning horizon at Greenhill on the current map's printed boundaries.
- The future Basalt Creek Parkway would split Frontage Road along Boones Ferry Rd and will serve as the dividing line, requiring two access points for the both sides of the residential community.
 - The residential area along Boones Ferry Rd south would remain Wilsonville residential. The City's long-term vision for the area was employment and industrial uses, so the area was anticipated to be some type of employment use. Residential property owners would remain in the County and when they wanted the opportunity to develop into something else that change would occur with a City of Wilsonville annexation.
 - The residential area east of Boones Ferry was pretty small. The full parcel appeared larger, but most of the back sides of the parcels were in the Basalt Creek Canyon and what would be the sensitive resource overlay. The frontage was about 10 acres.
- Staff clarified the locations of the prison, Coffee Creek Area, and Horizon School property, which was in the City of Tualatin on the Preferred Jurisdictional Boundary map (Slide 11). The teal color represented existing City of Wilsonville lands and the white between the teal areas indicated the Coffee Creek Area.
 - Staff reviewed the boundary options discussed to date, confirming the dividing line east of Boones Ferry Rd remained the same throughout the discussion, which was to follow Greenhill Ln, partly due to the anticipated overpass and because it seemed to be a clear dividing line.
- The elevation of the parkway coming over the railroad in the western portion of the planning area would be about 30 ft high. The parkway would return to grade shortly and then rise to about 5 to 10 feet above grade at the BPA easement. The parkway would be about 5 to 10 feet above grade, but would rise to about 20 or 25 feet above grade on a low grade property with drainage, and then return to grade when it hit the Basalt Creek Parkway/Grahams Ferry intersection.
 - Once the parkway was at full build-out, there would be vertical retaining walls on the west side of Grahams Ferry Rd, but for now, it would have side slopes. The parkway would affect the view from either side, almost like a massive berm. Envisioning the road profile in three dimensions and recognizing what a physical barrier the parkway would be in the interim and at final build-out was a turning point for both City Councils with regard to what made sense as a boundary. There was no relationship across it from one side of the other.
- Noting Consideration for Success Item 3, concern was expressed about the City finding some way to have a consistent industrial look on both sides of the parkway; otherwise it would be detrimental to the community. The assumption was that the Form-based Code planning being done on Day Rd would likely to shift to Basalt Creek, so coordinating with the City of Tualatin on look, design, feel, and a concept plan similar to the Form-based Code, even if Tualatin mimicked some of Wilsonville's concepts, would be beneficial to ensure a consistent look. . Even with similar aesthetics, each side of the road could have very different developments.

- With regard to SMART, there was no intention to combine efforts or integrate SMART into Tualatin; however, expanding SMART past the existing Wilsonville city limit was important to Wilsonville for the success of the Basalt Creek area. The City wanted to ensure employers in Basalt Creek received the services promised to other Wilsonville employers.
 - Expanding SMART would require a proposal to TriMet. The two Councils discussed the City of Tualatin willingness to support efforts to work with TriMet or any other entities to get SMART's service expanded in Wilsonville. The City of Tualatin would be responsible for determining TriMet or their own local service within their jurisdictional boundary. Stephan Lashbrook, SMART Director would discuss the process to expand SMART at the next City Council meeting.
 - Michael Kohlhoff, City Attorney, explained the petition and withdrawal process involved with TriMet being removed from a territory, which property owners had to support by State statute. TriMet could only be petitioned to be removed from a territory once every five years and this was the fifth year, so the horizon to do the petition fell into the latter part of this summer and fall. Mr. Lashbrook would be presenting that process and timing to City Council. As stated, if Tualatin supported the withdrawal, they would have to consider what that would mean for their city. For businesses in Basalt Creek, there would be a different rate structure because SMART's rate structure was less than TriMet. Although, TriMet would be able to compensate for that, TriMet would need to look at the service currently being provided, what service they could provide in the future, and the subsequent impacts on TriMet's rate base.
 - The prospect of TriMet providing service to SMART's central locations was one of many considerations being discussed in determining how the two transit entities could best cooperate and work toward a complete strategy and plan.
 - TriMet was trying to determine whether to have bus service into Portland, and Wilsonville already had a TriMet bus not coming to the transit center. The least expensive option seemed to be for TriMet to continue the route from Commerce Circle to the transit center, rather than SMART coming up with a new bus line.
 - Mr. Kohlhoff noted there were many issues to consider when trying to determine the best approach, such as the prison did not contribute to SMART, and yet TriMet was not serving the prison at all. Mr. Lashbrook was working diligently with respect to all the issues, legislation, etc.
- Considering that the parkway was projected to be dead ended for a while, all the traffic on the parkway would come down Boones Ferry Rd and possibly Grahams Ferry Rd resulting in a huge amount of traffic, especially with the southwest connector. Even if the parkway crossed I-5, there would be no reason to go over I-5 and people would still come south to get onto I-5. A few might use the parkway to access shopping if they could not get through the short way.
 - The Grahams Ferry Rd and Day Rd improvements outlined in the Basalt Creek Transportation Refinement Plan were connected to concerns about that extra traffic and were in the planning horizon.
 - The improvements were part of the concept plan's implementation, which was partly why the City was talking with its partners to the north about getting additional funds directed to Basalt Creek for those improvements. The City wanted to see the majority of the County's TDT directed to those specific projects in the Basalt Creek planning area.
- The extension to Tonquin Rd was omitted from the plan in favor of improvements to Grahams Ferry Rd. Staff was uncertain of the extent of the planned improvements and make the Basalt Creek report available to the Commission. Grahams Ferry Rd would become a major arterial to Day Rd.
- Improvements to the railroad underpass were also identified as projects in the concept plan.
- Staff confirmed the Preferred Boundary would not require the City to install a pumping station in Basalt Creek.
- Next steps included working with the consultant team to scope out how to get to a preferred or final concept plan. One of the first things to be done would be to get public input on the land use and concept plan around the Preferred Jurisdictional Boundary. Additionally, the project team would begin refining the ideas, tools, and implementation actions needed to address the ten considerations to ensure they were integrated into the final concept plan as well as the implementation actions for both cities and other potential parties, for example, if the City pursued using the County's TDT.

- Staff planned to return in the fall for the adoption of a single concept plan. Both cities would adopt the same Concept Plan, which was expected in the fall, and then each City would have its own implementing regulations since each city had different existing development codes, so the implementing ordinances and language in each city's code would be unique per city.
- The role of the Planning Commission was still being discussed as Staff worked with the City's partners on rescoping how the concept plan would be adopted. Wilsonville's City Staff envisioned that the Planning Commission would review the Concept Plan in a work session and hold a hearing on the draft Concept Plan prior to City Council's work sessions and hearings.
- Ground had already been broken on the first phase of the Basalt Creek Parkway, and the section from 124th Ave to Grahams Ferry Rd was envisioned being completed in the summer of 2018. Some additional improvements to Grahams Ferry Rd and Tonquin Rd would be part of that project. The County's goal was to finish the section between Grahams Ferry Rd and Boones Ferry Rd in 10 years, and they were currently working on a proposal for the environmental work on that phase.
- The City of Tualatin was the proposer for the project with Metro and received a substantial grant for the Basalt Creek planning, but an intergovernmental agreement was in place between Metro, both cities, and the County, that all the entities would work together on the concept planning and that both cities were responsible for the concept plan. Tualatin was also contributing Staff time.

VIII. INFORMATIONAL

A. Town Center Master Plan (Bateschell)

Miranda Bateschell, Long-Range Planning Manager, explained that considering a redevelopment plan for Town Center was identified as a project in the Urban Renewal Plan and had been set as a Council priority when City Council set its goals last spring. The City was awarded a \$320,000 Community Development Planning Grant (CDPG) to do the Town Center Master Plan with a City match of \$100,000 from the Urban Renewal Fund. The goal was to build on the vision established through the pilot project done by the City a few years ago with Portland State University. She read the vision from the Project Narrative, provided on Page 4 of 20 of the Staff report, and reviewed the diagram on Page 5 that identified the key outcomes of the Master Plan.

- Staff envisioned having a strong, innovative and highly engaged public involvement plan and wanted the master planning process to be exciting for the community. Everyone used Town Center and Staff wanted that tie to Town Center not only to continue, but be further enhanced and strengthened.
- An important piece resulting from the process would be an identified strategy and outline of action items to implement the Master Plan. To make Town Center a successful place, Staff envisioned having implementation actions for various interested parties in the community. As a first step, the City would likely have actions to implement, but actions were also envisioned for local businesses, and perhaps, a phase two demonstration project, but it all depended on how the master planning project took shape.
- The timeline for completing the project was expected to be about two years, perhaps longer.
- Staff would like to present the project to the public this summer to begin discussions about how Town Center functioned and what citizens wanted or would like to see, and get them excited and engaged on a regular basis with new communication tools.
- First steps included developing an RFP for consultant services to provide needed expertise and to develop a scope of work for the master planning project. The City had received a lot of interest about the project and hoped to get innovative ideas about how to engage the public and create the Master Plan.
- Staff anticipated returning to the Planning Commission in May to discuss and get input on the public involvement plan. Tonight, Staff sought ideas about events or methods to best engage Wilsonville's residents and business owners, as well as input about what results the Commission would like to see from project.

Comments from the Commission and discussion continued as follows:

- During the recent public meetings for Frog Pond, there was a lot of frustration because the public did not feel like they were being heard. The City needed to be careful to respect and acknowledge even opposing

views. The public input process seemed to fall apart because there were a couple meetings and then the process was over without any type of wrap up session in that forum to indicate what would happen next and provide closure, which was frustrating for many citizens who attended those meetings. There were some skeptical citizens now.

- It was important to acknowledge everyone's input and not just those on a task force because some citizens were shut down when making comments on Frog Pond because they were not on the task force. Keeping the lines of communication open was important.
 - One thing to consider was whether to have a task force or use different alternatives for public engagement and how you get advisory input. Being early in the process, choosing those methods was still open to conversation. With a task force, certain members were often perceived as being appointed, resulting in questions as to why they were chosen. Though voicing their own opinions, task force members are often expected in some ways to represent many other voices.
 - One alternative model focused on mass public engagement, which involved a lot more social media and web-based platforms that track people's comments so people could respond to one another's suggestions or project ideas. This model promoted more interaction and helped facilitate conversation about those ideas that would not occur otherwise.
 - Other models included focus groups of 10 to 15 people; hosting bigger, more charrette-based events for people to provide input; and setting up a storefront. For example, a tenant space in Town Center could be rented two or three times during the process where the consultants would be available so people could stop by when they could, such as on a lunch or coffee break, to get an update or provide input into the process.
 - Staff was exploring what public engagement options might be available and looking to get a lot of input and ideas from those responding to the RFP about what was working now. The world of communication and engagement was shifting drastically and the City sought input about how to best engage the community so the Master Plan would be successful, which would integrating input from the citizens and interested parties. The City needed to acknowledge all that input and figure out how to work in as much as possible in a balanced way to create a Master Plan about which everyone could be excited.
- The Memorial Park planning process seemed to go very well using mostly open houses to get grass roots input, and no task force. The process felt open and the mass meeting with the display boards seemed to be very engaging for people. The open houses were also a short term commitment, which was another issue with community involvement as no one had time to commit to weeks of planning. Providing opportunities for shorter time commitments would be better for a lot of Wilsonville's citizens.
- The Project Narrative mentioned getting input from a diversity of the community, which was extremely important but also a challenge, so it would be interesting to see what the consultants would suggest.
- Renting a storefront was a good idea and would draw in people who currently frequented Town Center.
- From the Chamber of Commerce's perspective, businesses were very interested in participating on a two-fold front. While the tendency was to focus on the land owners, many people had invested extensively in being long-term tenants, even well into the future, and they needed to be engaged as well as the land owners to have a successful process.
 - While diversity was certainly needed, one thing that was concerning about this approach from the business front was that business owners could not be as open as they wanted and may fear negative reactions from the citizenry who may have different interests or a different approach for Town Center. The City might want to consider opportunities for businesses to provide input in a different forum where they could speak openly. Businesses in the Town Center area were extremely interested in having a voice in the process, but they might not be overly honest about their business concerns if they believed it might negatively impact business.
 - There was no way to gauge or involve potential business tenants, but there must be a way to create excitement about what might happen in Town Center to attract potential businesses.
 - Citizens might want certain businesses in Town Center, but people did not realize the City could not force someone to open a particular business.

- Landlords of multi-tenant units tend to promote or work to attract certain businesses because the mix of business tenants was extremely important. Landlords spend a lot of money on consultants to figure out the right mix of businesses to have in a particular area.
- Given the overlay of Town Center, it was hard to imagine how achieve a grid pattern without massive reconstruction.
 - Per recent travel opportunities, it was witnessed that successful city centers had grid patterns that created a lot of vitality. A pedestrian-only section also seemed to work in successful places with parking toward the periphery.
 - Getting the right businesses with the right physical features to attract people was a consideration. For example, Kansas City had a strip mall with a beautiful walking area and water features but one restaurant had a dinosaur theme the emitted amplified dinosaur sounds, which were not appealing.
- Ms. Bateschell clarified that when the Project Narrative was written, the Regal Cinema structure was listed for sale, which was why Page 5 discussed it being recently vacated. She understood it had been taken off the market.
- Having Clackamas Community College involved in the process was important. The City should find a way to leverage the college, which was a great resource that many people were unaware of.
 - Clackamas Community College was definitely included on the list of people/entities the City would engage in this project, as well as OIT, especially with regard to the newer, more creative approaches using technology as part of the City's public engagement plan. The City would contact the college to determine how they wanted to be engaged, which also depended on the format used for public involvement. The formats might change based on who was engaged. For example, the tendency was to have smaller, more private focus groups for business owners.
- A task force format locked the City in, but not having a task force would enable the City to try a lot of different approaches for public involvement depending on the stage of the project.
 - Given the diverse uses of the land, the focus group approach would seem to be very appropriate and productive.
- If the storefront concept was utilized, architectural renderings, street plans, and other concepts could be displayed via video when the office was closed. A digital display could be changed as more information and public input was received. Providing flyers or a QR Code to enable cell phone users to be added to an email distribution list was also suggested.
 - During the Frog Pond process, people were required to attend meetings and make their comments at that time. In the end, when no closure was provided, people used social media to vent their frustrations.
 - Being able to insert an idea and have conversations about that specific idea without mixing or competing with other ideas for Town Center would be helpful.
 - Having tangible museum-like displays in a storefront would not necessarily require the storefront to be staffed by the consultants; even Planning Commissioners or other board members could volunteer to be at the storefront.
- The Parks and Recreation building could be an option for the storefront concept, but it would be out of the traffic loop and having the storefront in Town Center would be most beneficial. Perhaps, one of the vacant Town Center spaces could be donated.
- Ms. Bateschell added some of Staff's ideas for this summer included tapping into existing community events to spread the word and get people engaged, whether through some type of social media outlet, storefront, farmers market, etc. that would evolve over the time of the project.
 - The City also had the opportunity to use other events to launch the project unlike a standard hearing or public open house, such as tactical urbanism which put ideas and concepts of the master plan on the ground temporarily to give people a sense of what they might feel like and enable citizens to engage and respond to the concept. One example was creating a temporary greenway solely for pedestrians and bikes, where Astroturf was laid out to reflect potential designs. All kinds of events were planned on this temporary greenway with balloons and activities and people in the community were able to give real feedback about what felt good, what seemed to fit the environment, what they liked and would not like to see in the area. Such concepts provide the opportunity to be creative and play out the types of

potential changes in Town Center so people had the opportunity to engage with these potential concepts and respond accordingly.

- Providing people an opportunity to see how different features or ideas, like an amphitheater or meandering creek, might look virtually would also be beneficial. Examples would not have to be tactical, but providing a virtual rendition of how certain concepts and features might look would help because people may not be good at envisioning certain features.
 - Ms. Bateschell assured \$420,000 was a decent budget and visualization was discussed in the grant proposal, so with technology evolving so quickly, Staff would be able to execute some cool ideas at the right moments in the project, but obviously, it could not be done with every iteration.
- Portable digital terminals were to be used to get input for the Transit Master Plan which would enable the consultants to poll people immediately with specific questions no matter the location. This would provide another way to receive input from different locations.
 - Reaching out through social media would not reach everyone, but talking to people on the ground and collecting data would be very valuable, and would possibly get people involved in businesses at the ground level as well. Land owners and tenants had been mentioned, but employees would provide another perspective that had not really been discussed.
- No official tally had been taken of the landowners in this area, but the significant property owners in the 100-acre Town Center area included the community college, post office, shopping center, Fry's, and the apartment complex.
- The technical boundary for the Master Plan was Town Center Loop and Wilsonville Rd, but the grant application also discussed influence areas, so adjacent properties, such as those south of Wilsonville Rd and possibly farther, as well as the Art Tech School and City Community Center, were included to really understand the impacts. The City wanted to see more connections between both sides of Wilsonville Rd.
- The open land in the planning area was owned by Kaiser and the City.
- Portions of Town Center did fall within the urban renewal zone. The matching funds were coming from the East Side Urban Renewal District, which was scheduled to close in 2020, so the concept was that a new urban renewal area would likely be created if that was an appropriate funding mechanism to use.
- Having built new business areas like Fred Meyer and Argyle Square, the process should also include finding opportunities for businesses in Town Center to upgrade as well. The Development Code should be reviewed to ensure it included things that encourage redevelopment, not just on a macro level, but at micro levels as well to encourage tenant improvements and not just new developments with new buildings.
- Concern was expressed about development, like the three-story storage building being built opposite the post office, conflicting with the future vision of Town Center.
 - One way to prevent development outside the preferred scope would be to impose a planning moratorium for the area; however, the rationale for such a policy would have to be very solid. A moratorium would stop development during the master planning process, but given the past recessionary and new market times such a policy could be very difficult to implement and might not be practical.
 - Generally speaking, land owners who wanted to develop would probably want to wait and see how the master plan developed because it would affect the marketability of their land and the type of development they could bring in. On the other hand, people do make their own decisions on such things.

IX. OTHER BUSINESS

A. 2016 Planning Commission Work Program

Chris Neamtzu, Long-Range Planning Manager, noted a partial work program was included in the packet, but he and Ms. Bateschell had completed a 12-month work program that was quite robust. He noted an informational item regarding directional bike signage had been added to next month's agenda and that additional items were being added daily.

- He confirmed the Commission would begin work on the Frog Pond Area Plan in February, discussing different concepts, diagrams, and urban form with regard to developing the West Neighborhood. He was uncertain what the consultant would be presenting, but a presentation was scheduled for next month.

- The Citywide signage and wayfinding program was a high priority project on the work program, but it had not started partly because the City logo had not been finalized yet.
- With regard to the Old Town Bypass, Community Development Director Nancy Kraushaar had been talking with consultants about a corridor plan for that road. Determining which railroad crossing to use would be an important part of the corridor plan, but the project had not reached that point yet so the City did not know which crossing ODOT Rail preferred. The Old Town Bypass was listed on the yearlong work program.
- The Kinsman Road project between Boeckman Rd and Barber Rd was at about 90 percent design.

Michael Kohlhoff, City Attorney, added the Kinsman Road project was still on schedule to begin this summer. The City was currently considering a three-party utility provider agreement with Tualatin Valley Water District, the City of Hillsboro and ODOT to allow the Tualatin Valley/Hillsboro pipeline to be put in at the same time as the Kinsman Road project to avoid having to tear up a segment of Kinsman Rd in another couple years. That agreement was subject to some other cost and payment negotiations the City hoped to complete by May. The project was scheduled to go out to bid in April with construction to begin in May.

Staff had no further updates regarding the French Prairie Bridge but anticipated launching the public involvement plan in the summer, with an initial Planning Commission work session on the alternatives and a public open house likely in October.

X. ADJOURNMENT

Chair Greenfield adjourned the regular meeting of the Wilsonville Planning Commission at 7:50 p.m.

Respectfully submitted,

By Paula Pinyerd of ABC Transcription Services, Inc. for
Tami Bergeron, Administrative Assistant - Planning



PLANNING COMMISSION MEETING STAFF REPORT

Meeting Date: February 10, 2016	Subject: Transit Master Plan (TMP) update & Public Involvement Plan Staff Member: Jen Massa Smith Department: Transit	
Action Required	Advisory Board/Commission Recommendation	
<input type="checkbox"/> Motion <input type="checkbox"/> Public Hearing Date: <input type="checkbox"/> Ordinance 1 st Reading Date: <input type="checkbox"/> Ordinance 2 nd Reading Date: <input type="checkbox"/> Resolution <input checked="" type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input type="checkbox"/> Council Direction <input type="checkbox"/> Consent Agenda	<input type="checkbox"/> Approval <input type="checkbox"/> Denial <input type="checkbox"/> None Forwarded <input checked="" type="checkbox"/> Not Applicable	
Comments:		
Staff Recommendation:		
Recommended Language for Motion:		
Project / Issue Relates To: <i>[Identify which goal(s), master plans(s) your issue relates to.]</i>		
<input type="checkbox"/> Council Goals/Priorities	<input checked="" type="checkbox"/> Adopted Master Plan(s) Transit Master Plan (TMP)	<input type="checkbox"/> Not Applicable

ISSUE BEFORE COMMISSION:

Staff and consultants will provide an overview of SMART’s Transit Master Plan (TMP) public involvement strategy and the timeline for the TMP update. The current TMP was adopted in 2008, though most of the work for the plan was conducted in 2006 alongside the single advisory committee for the Bike/Ped and Parks & Rec Master Plans.

If you would like a quick refresher about SMART and the current TMP before your meeting, please visit www.ridesmart.com/126/About-SMART

Planning Commissioners are invited to participate in the first TMP Stakeholder Workshop being held on March 10, 2016, 5:30-8:30pm at City Hall. This key event will bring together 30-50

community representatives to work on transit issues in a hands-on way, and explore the particular trade-offs for the City of Wilsonville to consider in this plan.

The workshop purpose is to provide the necessary understanding of transit planning for well-informed decision-making and a community-supported Transit Master Plan for Wilsonville. More details will be provided at your work session meeting.

EXECUTIVE SUMMARY:

The Transit Master Plan is SMART’s guiding planning document and as such, will lay out the framework for what types of transportation options the City of Wilsonville wants to provide and at what level of transit service the community would like to see and support.

Current operations will be analyzed so that SMART can offer relevant and quality service that has the potential to retain and attract more riders of all ages and backgrounds.

Public participation is a crucial part of preparing a strong and successful Transit Master Plan. The citizens and the business community of Wilsonville will be asked to provide input and help shape the plan.

EXPECTED RESULTS:

TIMELINE:

Public Involvement Timeline:
January – October 2016

Phases:

The Public Involvement Plan’s approach represents four key phases:

- Phase 1: Identification of Issues and Opportunities
- Phase 2: Service and Options Development
- Phase 3: Service Options Review
- Phase 4: Plan Development

CURRENT YEAR BUDGET IMPACTS:

This project is mainly grant funded by the Federal Transit Administration (FTA) and ODOT. All activities associated with this project during this fiscal year are included in the current adopted budget.

FINANCIAL REVIEW / COMMENTS:

Reviewed by: _____ Date: _____

LEGAL REVIEW / COMMENT:

Reviewed by: _____ Date: _____

COMMUNITY INVOLVEMENT PROCESS:

Public engagement efforts will be tailored to reach people in practical and convenient ways so that the updated plan reflects learning from the diverse spectrum of existing system users and potential users.

Core activities include:

- Interviews with community stakeholders in SMART’s identified 11 service areas (businesses, residents, civic organizations, faith-based institutions, commercial and retail centers, schools and higher educational institutions, social services, etc.)
- Workshop and focus groups for targeted input
- Community-wide surveys (online, paper, and via mobile kiosks) to identify needs and opportunities and the service options to address them
- Speaking engagements to reach community organizations
- Event outreach to bring information to where Wilsonville gathers
- E-updates [the initial interested parties list currently represents over 400 community individuals, businesses, civic organizations, churches, schools, etc.]

POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY:

Some people use transit because they want to save money or reduce pollution. Some ride SMART because they have to in order to get around. SMART’s goals for this project all stem from the mission to provide high quality transportation options to employees and residents of Wilsonville.

Special attention will be given throughout the process to reach under-represented voices such as older adults, people with disabilities, youth, low-income, communities of color, and those with limited English proficiency.

ALTERNATIVES:

CITY MANAGER COMMENT:

ATTACHMENTS:

- A. Draft Public Involvement Plan



TRANSIT MASTER PLAN

PUBLIC INVOLVEMENT PLAN - DRAFT

December 2015



INTRODUCTION

South Metro Area Regional Transit (SMART) is updating the Wilsonville Transit Master Plan (TMP). Public input by residents, businesses, and employees about ways to improve service and future priorities for resources is essential to understanding and meeting community needs for transit and other transportation options.

Public engagement efforts will be tailored to reach people in practical and convenient ways so that the updated plan reflects learning from the diverse spectrum of existing system users and potential users. Gaining input from employee break rooms, neighborhood and community gatherings, and through mobile electronic kiosks are examples of the project's active public involvement approach resulting in system improvements that best suit identified needs and opportunities.

The following values will guide the TMP's public involvement:

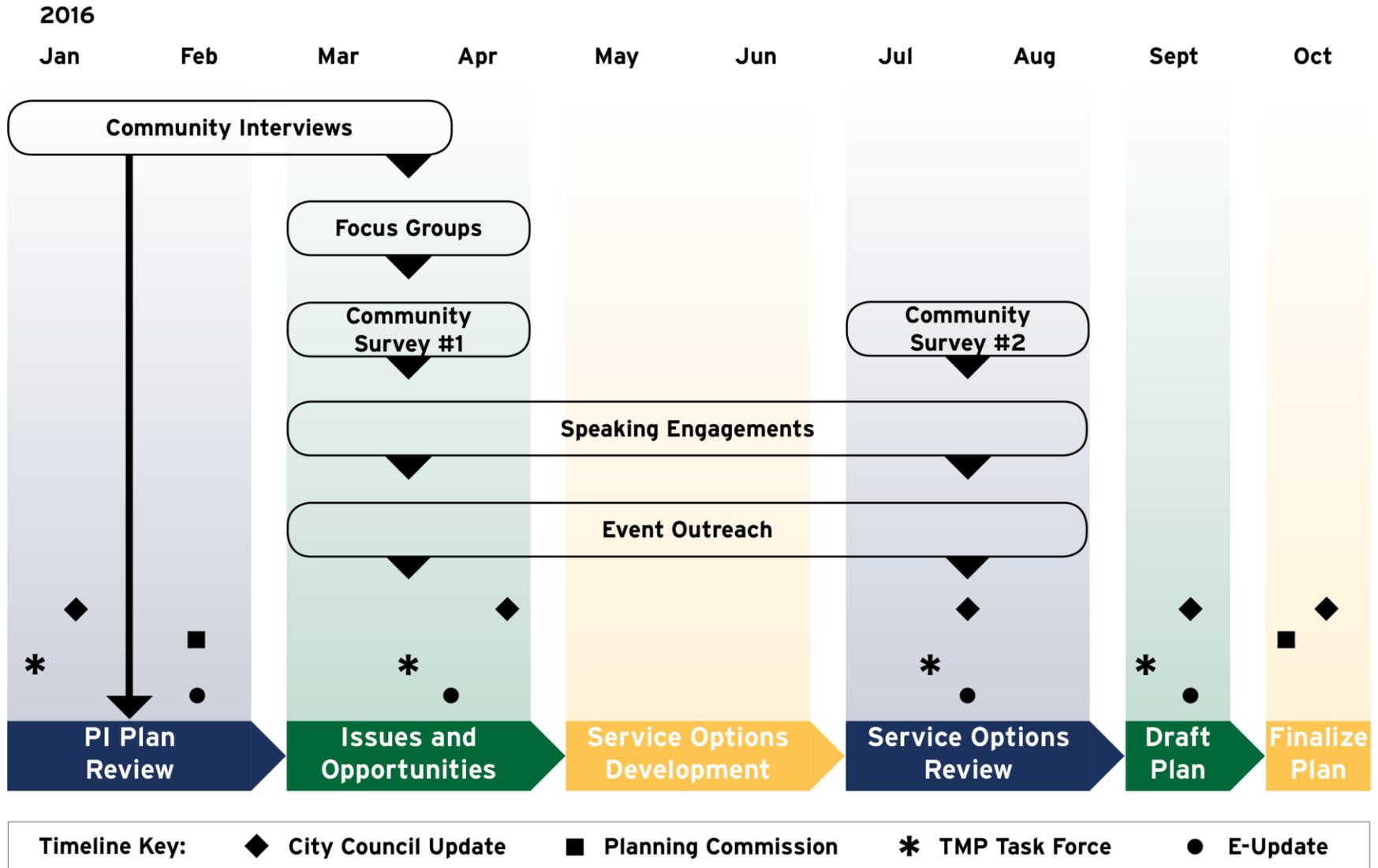
Realistic: be clear about the project constraints, objectives, and parameters

Inclusive: reach out to everyone, including those who don't use computers or face other barriers

Meaningful: provide timely information that is accurate and easily accessible

Transparent: make decisions public with key project materials available on the website and public City building

PUBLIC INVOLVEMENT PROCESS



KEY ELEMENTS OF PUBLIC INVOLVEMENT PLAN

- Consistent, reliable, accessible information and SMART contact person
- Thorough stakeholder analysis
- Vet Draft Public Involvement Plan with key stakeholders
- Representative stakeholder interviews from throughout Wilsonville
- Small issue-focused meetings, such as bicycle/pedestrian concerns
- Non-traditional outreach targeted to those with low-income, people with disabilities, young and elderly people
- E-updates to Interested Parties List
- Timely response to all submitted comments & questions
- Electronic kiosks with information and survey placed at high visibility locations
- Speaking engagements at civic organizations
- Updates to relevant committees and City Council
- Transit Master Plan website with comment and survey functions
- Media (print, radio, TV)

DECISIONS AND ROLES

The Wilsonville City Council is responsible for the adoption of the Transit Master Plan Update. The project will provide public briefings to Council during the engagement process and expects to offer the plan for adoption in autumn of 2016.

The Transit Master Plan Task Force is an advisory committee of residents, transit riders, businesses and community organizations who will serve as a sounding board during the public involvement and update process.

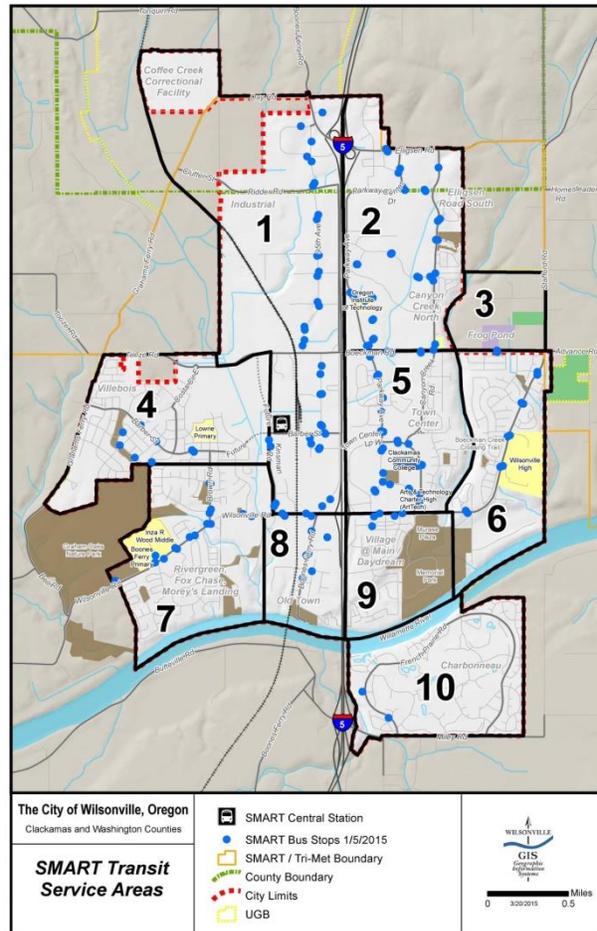
TARGETED COMMUNICATIONS STRATEGIES

Ten Identified Service Areas

Situated on I-5 between Portland and Salem, Wilsonville serves the area as a transportation hub and an important employment center. Large numbers of people commute to work in Wilsonville, and a large percentage of residents commute to other communities for their employment. SMART will tailor public involvement by neighborhood service areas, including residential, business and commercial zones, to optimize survey participation.

Actions:

- Conduct interviews to best understand methods to reach each of ten specific geographic service areas.
- *Add new ideas after interviews here.*
- Do outreach with display to explain the project and collect input at identified high volume venues such as shopping centers and community festivals.
- Place electronic kiosks to collect survey input at designated locations. Advertise locations in local media.
- Add contacts into Interested Parties List and send E-updates.
- Contact employers to engage them with the project, including potential communications with their workforce either through displays, electronic kiosks, and break room posters with surveys or email.
- Reach out to residents and property owners outside City but in planned growth areas.



Wilsonville Civic Leaders

Every community has leaders who connect with a variety of constituencies. Regularly updating those who serve on relevant Wilsonville committees, local service organizations, community non-profits, and business organizations is a good way to use social networks as conduits for accurate information and public input.

Actions:

- Network with Wilsonville organizations to place information in their newsletters and links to project website at major milestones.
- Arrange up to ten speaking engagements at civic committees and club meetings with the goals of sharing information, gathering input, and signing people up to receive future updates.
- Send e-Updates.

Title VI/ Environmental Justice (EJ) Populations

The goal of this outreach is to ensure that the impacts and benefits of the Wilsonville Transit Master Plan are equitably experienced regardless of race, national origin, gender, disabilities, English language proficiency or being low-income. Materials will be made available in appropriately accessible formats, direct outreach in Spanish will happen at specific, identified locations, and times of day and locations will be a consideration in effectively seeking all community viewpoints.

Actions:

- Network with organizations that serve Title VI/EJ populations to learn best opportunities to reach constituents, including outreach at events and schools.
- Arrange display outreach at faith-based venues serving Latinos.
- Arrange survey input opportunities at low-income service providers and identified shopping venues.
- Incorporate Title VI/EJ perspectives in Focus Groups.

Workshop and Issue-Based Focused Groups

SMART serves multiple constituencies with a variety of transportation needs. The project will organize discussions with specific types of users about the aspects most relevant to them to gain user insights on specific topics for the master plan.

Actions:

- Conduct a large community workshop with 30-50 community representatives to provide the necessary understanding of transit planning for well-informed decision-making and a community supportive Transit Master Plan for Wilsonville.
- Organize, recruit, and facilitate two meetings that include perspectives from bicycle and pedestrian users, people with disabilities, Title VI populations, youth and businesses.

Media

Media reaches the most people with information, yet it is not a way to gather input. The project will use media to inform the Wilsonville community about the project, and about specific opportunities to give input prior to key decisions and project milestones.

Actions:

- Release Media Advisories at key times with project information, including how to participate, to The Wilsonville Spokesman and the Boones Ferry Messenger.
- Schedule radio interviews.
- Reach out to Spanish-speaking media outlets.
- Build media outlets into the Interested Parties List and send e-updates.

PROJECT COMMUNICATIONS MATERIALS & TOOLS

- Project masthead
- Project description
- Process timeline
- Public Involvement & Communications Plan
- Service area map
- SMART contact name and number, business cards
- Website
- E-updates
- Electronic surveys
- Mobile kiosks
- Display boards, break room posters
- Leaflets for door-door outreach in some areas

Wilsonville

Transit Master Plan

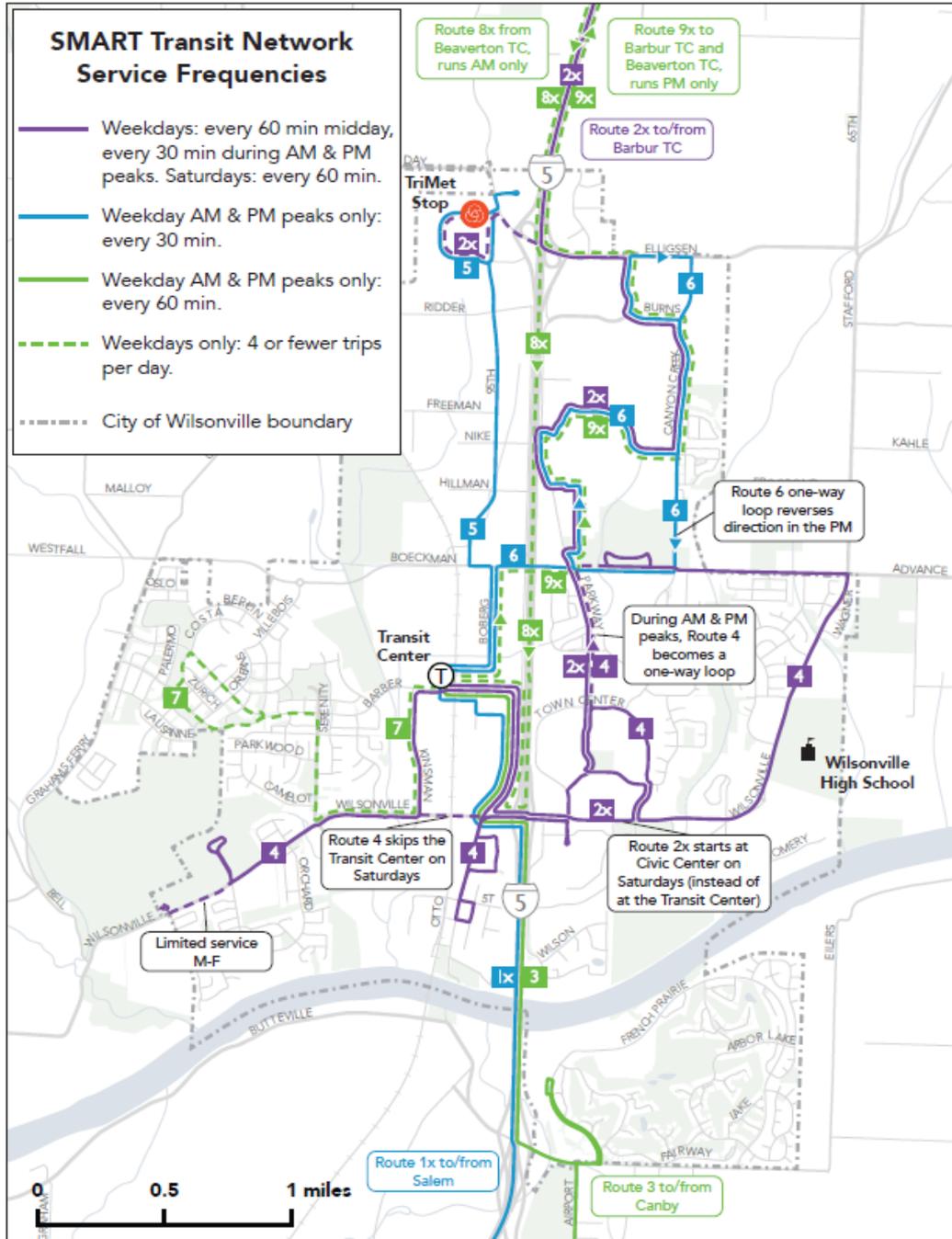


Think SMART. Ride SMART. Plan SMART.

www.plansmartwilsonville.com

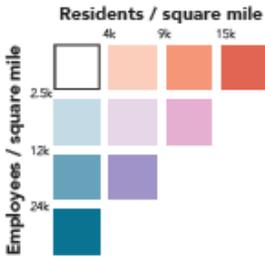
SMART Transit Network Service Frequencies

- Weekdays: every 60 min midday, every 30 min during AM & PM peaks. Saturdays: every 60 min.
- Weekday AM & PM peaks only: every 30 min.
- Weekday AM & PM peaks only: every 60 min.
- - - Weekdays only: 4 or fewer trips per day.
- - - - - City of Wilsonville boundary

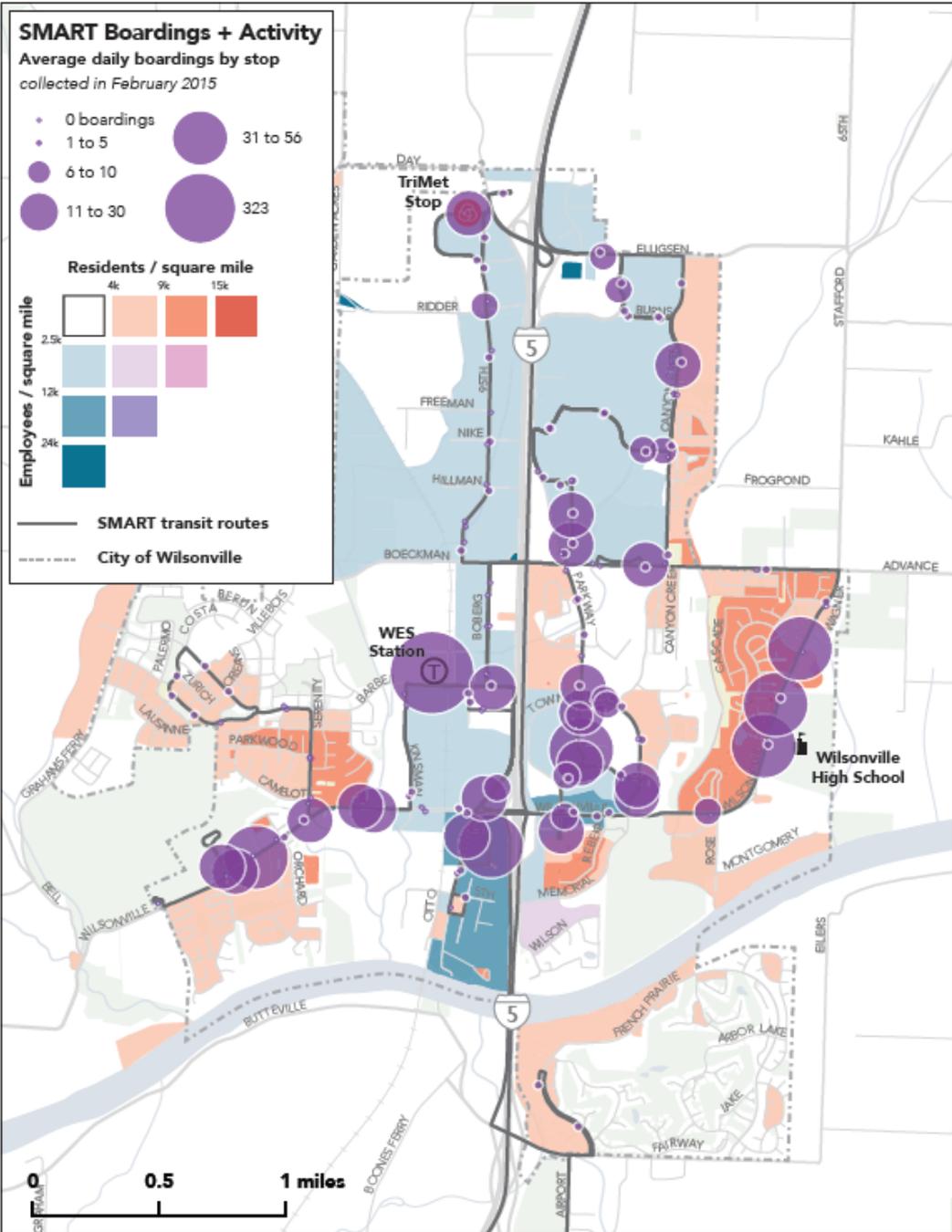


SMART Boardings + Activity

Average daily boardings by stop
collected in February 2015



— SMART transit routes
- - - City of Wilsonville



Wilsonville

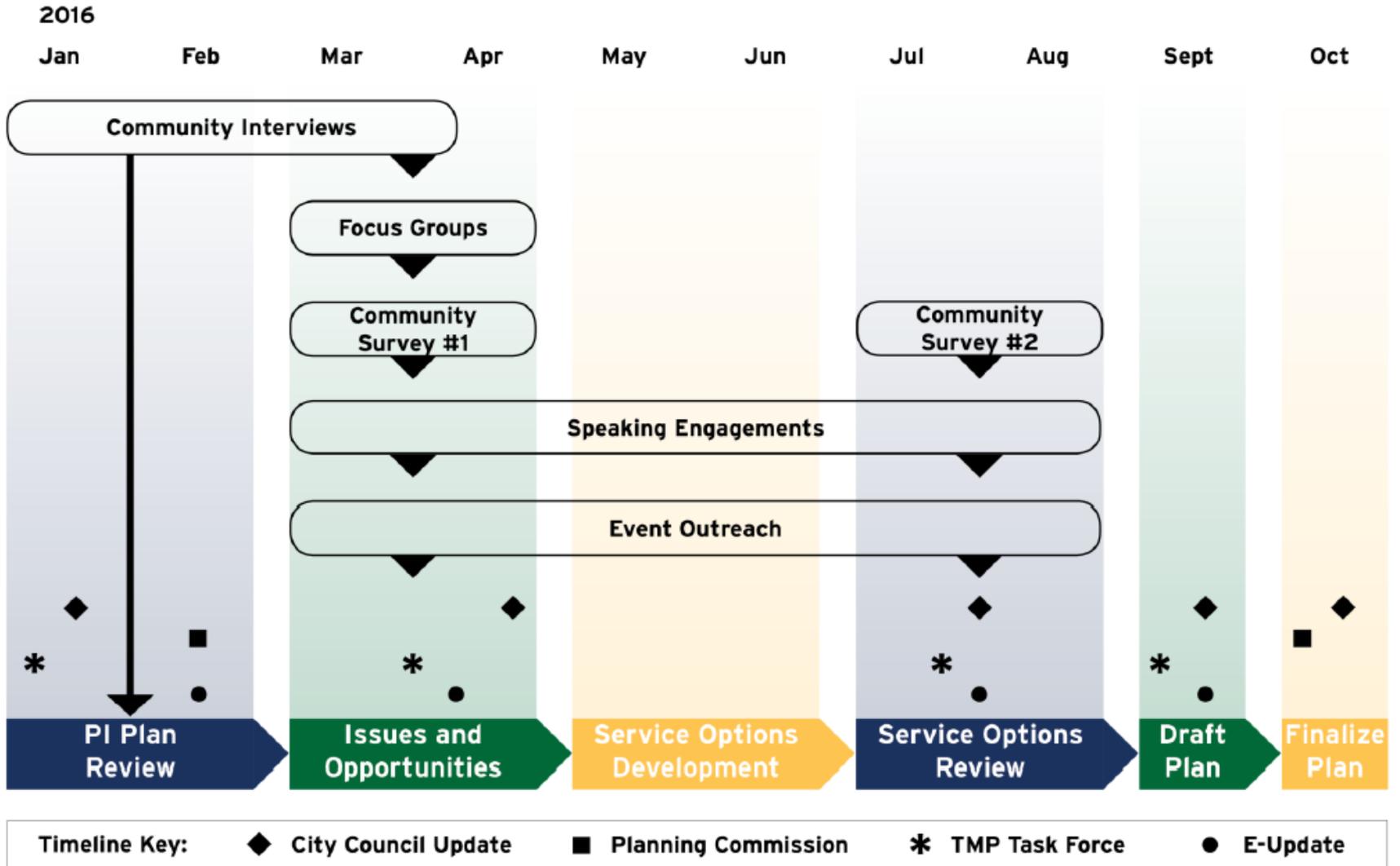
Transit Master Plan



Think SMART. Ride SMART. Plan SMART.

www.plansmartwilsonville.com

PUBLIC INVOLVEMENT PROCESS





THIRD FLOOR

Accounting

Building Division

City Attorney

City Clerk

City Manager

Community Development

Departmental Conference

Fire Prevention

Human Resources

Information Technology

Public Works

Revenue/Cashier

Utilities

Search

About

The City of Wilsonville is developing an update to their 2008 Wilsonville Transit Master Plan (TMP). The TMP is a broad look ahead to the type of transit system and supportive transportation options required to meet Wilsonville's mobility needs.

New Announcement

Please check this website and sign up for updates to stay informed about input opportunities as the TMP develops.

Sign Up For Updates

Name

Email

By submitting this form, you accept the [Mollom privacy policy](#).

Submit

Timeline



TMP Task Force

🕒 07/12/2016 - 6:00pm



City Council Work Session

🕒 07/25/2016 - 5:30pm



Phase 4: Plan Development & Review



TMP Task Force

🕒 09/13/2016 - 6:00pm



[? Support](#)

Events

Events Past events

- [PI Plan Review](#)
- [Phase 1: Identification of Issues](#)
- [TMP Task Force](#)
04/12/2016 - 6:00pm
- [City Council Work Session](#)
04/18/2016 - 5:30pm

Documents

Filter by Category or Keyword

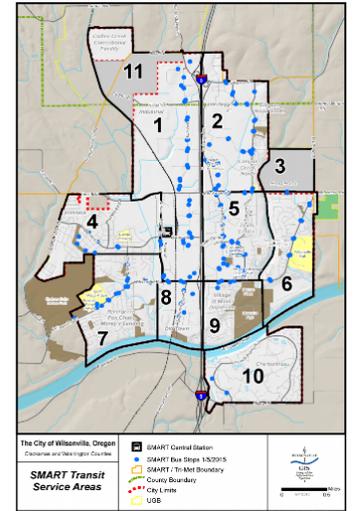
- [2008 Wilsonville Transit Master Plan](#) [More >>](#)
- [City of Wilsonville Climate Smart Communities](#) [More >>](#)
- [Draft Public Involvement Plan](#) [More >>](#)
- [Existing Conditions Report](#) [More >>](#)
- [Project Management Team](#) [More >>](#)
- [SMART Service Map](#) [More >>](#)
- [TMP Task Force](#) [More >>](#)

Overview

Much has changed in Wilsonville over the past nine years. This update is driven by the rapid growth of Wilsonville's population, an aging population, greater movement of people to jobs throughout the city and region, and opportunities to advance the [City of Wilsonville's Climate Smart Goals](#).

To best serve the current and future transit needs of Wilsonville requires thoughtful and deliberate community planning. The Transit Master Plan will accomplish this through broad community outreach and involvement to:

- Identify the key transit issues and opportunities for Wilsonville today, as well as tomorrow's new ridership markets.
- Review connections with TriMet, Canby Area Transit, and Salem-Keizer Transit (Cherriots) to maintain and enhance a user-friendly network of transit services.



Contact Us

Your Name

Your Email Address

Your Phone Number

Your Message

Sign Up For Updates

Name

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PLANNING COMMISSION STAFF REPORT

Meeting Date: February 10, 2016		Subject: Bicycle Wayfinding System	
		Staff Member: Britney Buttacavoli, Engineering Intern	
		Department: Engineering	
Action Required		Advisory Board/Commission Recommendation	
<input type="checkbox"/> Motion <input type="checkbox"/> Public Hearing Date: <input type="checkbox"/> Ordinance 1 st Reading Date: <input type="checkbox"/> Ordinance 2 nd Reading Date: <input type="checkbox"/> Resolution <input checked="" type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input type="checkbox"/> Council Direction <input type="checkbox"/> Consent Agenda		<input type="checkbox"/> Approval <input type="checkbox"/> Denial <input type="checkbox"/> None Forwarded <input checked="" type="checkbox"/> Not Applicable	
		Comments:	
Staff Recommendation:			
Planning Commission provides input regarding the implementation of wayfinding signage for bicyclists.			
Recommended Language for Motion: N/A			
Project / Issue Relates To:			
Bike and Pedestrian Master Plan specified a goal in 2006 to establish a wayfinding signage system to make navigation to destinations within Wilsonville more accessible to bikes.			
2013 Bike and Ped Connectivity Action Plan set a goal to encourage biking in Wilsonville through implementing wayfinding signage.			
In 2015, the City Council established a priority initiative to establish bikeway mile markers.			
<input checked="" type="checkbox"/> Council Goals/Priorities	<input checked="" type="checkbox"/> Adopted Master Plan(s)	<input type="checkbox"/> Not Applicable	

ISSUE BEFORE COMMISSION:

A bicycle wayfinding system has been developed to help promote biking within the City. Staff is seeking input and affirmation regarding the sign displays, and their locations, which will be used to create a safe and positive experience to cyclists of all experience levels.

EXECUTIVE SUMMARY:

Wilsonville's bicycle wayfinding system addresses the need documented in the Wilsonville Bicycle and Pedestrian Master Plan to establish directive signage for bicyclists. The 2013 Bicycle and Pedestrian Connectivity Action Plan further emphasizes the need for a signage system in order to encourage biking within the city. The wayfinding system will also be used to meet City Council's goal of establishing bicycle mile markers.

Directive bicycle signage is a cost-effective method for improving the City of Wilsonville's bicycling environment. The *Bicycle Wayfinding System: Sign Display and Placement Plan* was developed to be used by the City as a comprehensive guide to implement a directive signage system for bicyclists. The wayfinding system will work in coordination with the City's existing bicycle infrastructure, low traffic streets and trails, to provide a positive experience for those looking to reach key destinations within Wilsonville by bike. The *Sign Display and Placement Plan* will provide guidance regarding sign design, layout and display as well as propose locations for sign placement.

In order to maintain uniformity and continuity with City of Portland and surrounding municipalities' bicycle wayfinding signage, standards must be maintained throughout the system. For this reason, design guidance was taken from the Manual on Uniform Traffic Control Devices (MUTCD), National Association of City Transportation Officials (NACTO) Urban Bikeway Design Guide, the Oregon Department of Transportation, as well as City of Portland and City of Milwaukie bicycle wayfinding documents.

EXPECTED RESULTS:

Placing signs throughout the city which indicate to bicyclists the direction of key destinations, as well as the time and distance required to reach those destinations, will increase users' comfort with, and accessibility to, established bicycle infrastructure. An increase in comfort and accessibility will expectantly lead residents and visitors to view biking as a viable mode of transportation to reach destinations within the City.

TIMELINE:

The Sign Display and Placement Plan will be finalized in March 2016 in order to be used for the installation of signs which will begin in the spring of 2016. Signage installation should be completed by June 30, 2016 in order to make use of the established budget.

CURRENT YEAR BUDGET IMPACTS:

This project is funded by the 2015-2016 FY budget.

COMMUNITY INVOLVEMENT PROCESS:

Input from the public, through use of an online survey, was used to generate a comprehensive list of all possible destinations that could be signed to within Wilsonville. The survey generated 145 responses in total from residents, employees and visitors. Information from the survey was further used to develop a ranking system to help determine the importance of each destination and establish the physical distance from which locations are signed.

POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY:

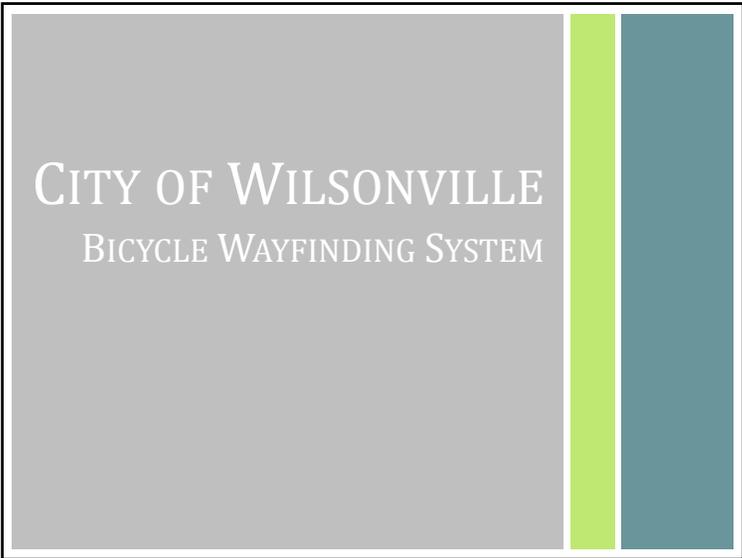
A concise directive signage system delivers benefits which will help to meet specific goals laid out by the Bike and Pedestrian Master Plan as well as the Bicycle and Pedestrian Connectivity Action Plan. Specific benefits include:

- a) Familiarizing bicyclists with Wilsonville's existing bicycle infrastructure
- b) Encouraging less experienced bicyclists by increasing awareness of safe routes to various destinations
- c) Enhancing bicyclists ability to navigate to key destinations by bike
- d) Promoting Wilsonville's identity
- e) Serving as a working visual to address misconceptions about the time it takes to reach destinations by bike

ATTACHMENTS:

- A. PowerPoint Slides
- B. 2006 Bike and Pedestrian Master Plan Excerpt
- C. Destination Categorization List
- D. Citywide Intersection Key Map
- E. Wilsonville Bike and Walk Map

ATTACHMENT A. PowerPoint Slides



INTRODUCTION

BICYCLE WAYFINDING SIGNAGE SYSTEM

“Comprehensive signing and/or pavement markings that guide bicyclists to their destinations along preferred bicycle routes”

NATIONAL ASSOCIATION OF CITY TRANSPORTATION OFFICIALS, NACTO

SUPPORT

- 2006 Bicycle and Pedestrian Master Plan
- 2013 Bike and Ped Connectivity Action Plan
- City Council’s Goal to Establish Bicycle Mile Markers

BENEFITS

- 1) Familiarizes users with the bicycle network.
- 2) Identifies best routes to destinations.
- 3) Overcomes “barrier to entry” for new bicyclists.
- 4) Addresses misconceptions regarding bike time.
- 5) Indicates that motorists should use caution.

REGULATORY FRAMEWORK

MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (MUTCD 2009)

Ensures that traffic control devices are visible, recognizable, understandable and necessary.

STANDARDS, GUIDANCE & OPTIONS

- Sign Design
- Sign Display
- Sign Placement

REGULATORY FRAMEWORK

MUTCD STANDARD

A required, mandatory, or specifically prohibitive practice.

MUTCD

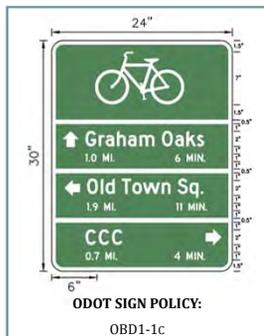
MUTCD GUIDANCE

A recommended, but not mandatory, practice with deviations allowed if engineering judgment indicates the deviation to be appropriate.

MUTCD

REGULATORY FRAMEWORK

ODOT SIGN POLICY



VS.



REGULATORY FRAMEWORK

MUTCD STANDARD

SECTION 2D.37 "If more than one destination is displayed in the same direction, the name of a nearer destination shall be displayed above the name of a destination that is further away."



REGULATORY FRAMEWORK

MUTCD GUIDANCE

SECTION 2D.37 “The closest destination lying straight ahead should be at the top of the sign or assembly, and below it the closest destinations to the left and to the right, in that order.”

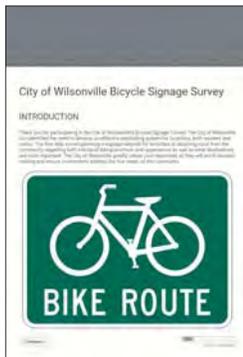


METHODOLOGY

6 MAIN STEPS

- 1) SELECT DESTINATIONS
- 2) RANK DESTINATIONS
- 3) DEVELOP BIKE NETWORK
- 4) HIGH LEVEL SIGN PLACEMENT
- 5) EXACT SIGN PLACEMENT
- 6) COMPILE REPORT

1 DESTINATION SELECTION



- | | |
|--------------------------------|------------------------------|
| Town Center | Sofia Park |
| Old Town Square | Courtside Park |
| Argyle Square | Piccadilly Park |
| Clackamas Community College | Palermo Park |
| Oregon Institute of Technology | Canyon Creek Park |
| Pioneer Pacific College | Green Acres Park |
| Korean War Memorial | Hathaway Park |
| Graham Oaks Nature Park | Mentor Graphics Path |
| Murase Plaza | Montebello Park |
| Memorial Park | Park at Merryfield |
| Town Center Park | River Fox Park |
| Boones Ferry Park | Tranquil City Park |
| Water Treatment Plant Park | Walt Morey Park |
| WES Transit Center | Willow Creek - Landover Park |
| Library | Lowrie's Marketplace |
| City Hall | Riverwood Center |
| Community Center | Marketplace |
| Post Office | The Village at Main Street |
| Community Gardens | Boones Ferry Pointe |
| Wilsonville High School | Arts & Tech High School |
| Inza Wood Middle School | Cinema: Regal Cinemas |
| Boeckman Creek Primary School | Family Fun Center |
| Boones Ferry Primary School | Museum: World of Speed |
| Lowrie Primary School | |

2 DESTINATION RANK

TABLE 1. DESTINATION RANK			
Category	I-IV Rank	Overall Rank	Distance of Signage
Commercial Centers: District	I Districts	1	3 Miles
Schools: College	I Districts	2	3 Miles
Regional Parks/Trails	II Regional	3	2 Miles
Public Transit	II Regional	4	2 Miles
Neighborhoods	II Regional	5	2 Miles
Civic/Community	III Local	6	1 Mile
Schools: High School	III Local	7	1 Mile
Schools: Elementary & Middle	IV Middle/Elementary	9	1/2 Mile
Local Parks/Trails	III Local	8	1 Mile
Commercial Centers: Local	III Local	10	1 Miles
Attraction	III Local	11	1 Mile

2 DESTINATION RANK

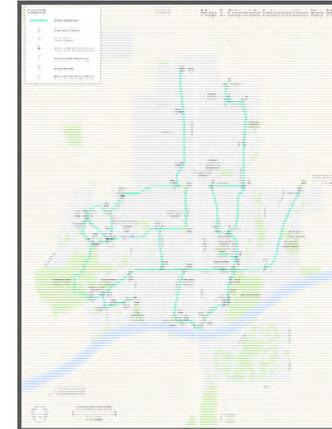
TABLE 2. DESTINATION CATEGORIZATION			
DESTINATION	OVERALL CATEGORY	CLASS	OVERALL RANK
Town Center	Commercial Center: District	I	1
Old Town Square	Commercial Center: District	I	2
Argyle Square	Commercial Center: District	I	3
Clackamas Community College	Schools: College	I	4
Oregon Institute of Technology	Schools: College	I	5
Pioneer Pacific College	Schools: College	I	6
Korean War Memorial	Regional Parks/Trails	II	7
Graham Oaks Nature Park	Regional Parks/Trails	II	8
Murase Plaza	Regional Parks/Trails	II	9
Memorial Park	Regional Parks/Trails	II	10
Town Center Park	Regional Parks/Trails	II	11
Boones Ferry Park	Regional Parks/Trails	II	12
Willamette River Water Treatment Plant Park	Regional Parks/Trails	II	13
WES Transit Center	Transit Station	II	14
Library	Civic/Community	III	15
City Hall	Civic/Community	III	16
Community Center	Civic/Community	III	17
Post Office	Civic/Community	III	18
Community Gardens	Civic/Community	III	19
Wilsonville High School	Schools: High School	III	20
Inza Wood Middle School	Schools: Elementary & Middle	IV	21
Boeckman Creek Primary School	Schools: Elementary & Middle	IV	22
Boones Ferry Primary School	Schools: Elementary & Middle	IV	23

*Destinations highlighted in blue are included in wayfinding signage.

*Destinations highlighted in grey are included on the Wilsonville Bike & Walk Map.

SEE ATTACHMENT C. DESTINATION CATEGORIZATION

3 BIKE NETWORK



SEE ATTACHMENT D. CITYWIDE INTERSECTION KEY MAP

SIGN TYPES

SIGN TYPES

1) BIKE ROUTE



MUTCD D11-1



MUTCD M4-11



MUTCD M4-12

2) TURN SIGN



MUTCD D11-1 & M6-1



MUTCD D1-3B

SIGN TYPES

SIGN TYPES (CONTINUED)

3) DECISION SIGN



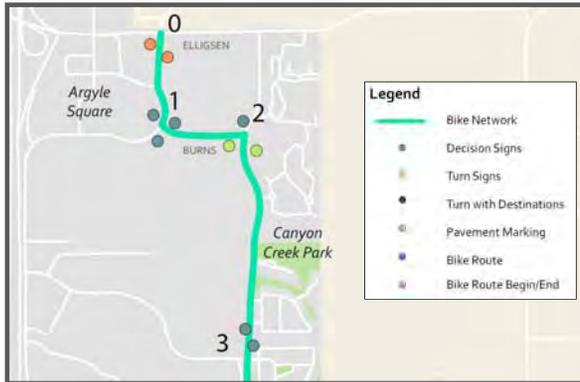
OBD1-1c

4) PAVEMENT MARKING



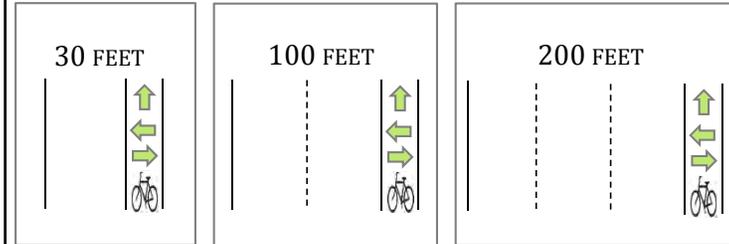
MUTCD 9c-9

4 → HIGH LEVEL SIGN PLACEMENT



5 → EXACT SIGN PLACEMENT

SIGN PLACEMENT IN ADVANCE OF TURN



6 → COMPILE REPORT

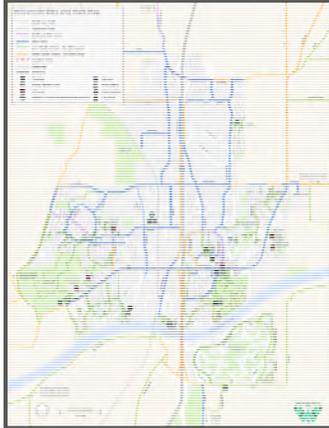
INTERSECTION 9A

INFORMATION	
SIGN TYPE	Go-Street Decision
INTERSECTION	Town Center Loop East & Winsorville Road
DIRECTION OF TRAVEL	Northbound
SIGN NUMBER	9a
INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Winsorville Road.
DISTANCE FROM INTERSECTION (feet)	105
LATERAL DISTANCE (feet)	
NOTES	
Place decision sign on the east side of Winsorville Road 105 feet south of Winsorville Road. Mounted sign indicates east streetwalk bar across Winsorville Road.	
Install 100 feet due to the need to accommodate current line of sight of traffic in order to turn left to reach destination. Additional 5 feet in order to use sight triangle. Street use sign.	

LIMITATIONS & CHALLENGES

- Restricted Creativity
- Limited Sign Space
- High Destination Density in Town Center
 - One Component of Signage
 - Bike Map

BIKE MAP



SEE ATTACHMENT E. WILSONVILLE BIKE & WALK MAP

TIMELINE & BUDGET

MARCH, 2016 → Planning and Documents Finalized

APRIL, 2016 → Begin Signage Installation

JUNE, 2016 → Complete Signage Installation

Project is funded by the FY 2015-2016 budget.

THANK YOU

ATTACHMENT B. 2006 Bike and Pedestrian Master Plan Excerpt

City Programs

Alternative Transportation Coordinator/Advisory Committee

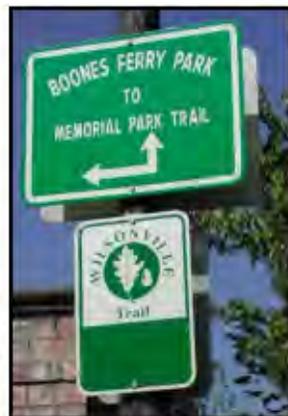
Wilsonville should enhance the existing Transportation Options coordinator position funded by SMART. This position is responsible for implementing the walkSMART program and other programs directed at reducing the number of vehicle miles traveled in the city. This position would oversee implementation of the various related plans, such as the Bicycle and Pedestrian Master Plan, as well as the Transit Master Plan. This position would also coordinate with other departments, such as Public Works and Community Development to ensure that the needs of bicyclists and pedestrians are being considered in all new projects.

Along with a coordinator, the city should encourage the formation of a full-time Bicycle and Pedestrian Advisory Committee that meets regularly to identify and discuss the needs of bicyclists and pedestrians within the City of Wilsonville.

Wayfinding / Signing Program

The ability to navigate through a town or city is informed by landmarks, natural features, and other visual cues. A signage system is a key component of a navigable environment and would inform pedestrians, bicyclists, and motorists, while also enhancing the identity of Wilsonville.

An effective wayfinding system communicates information clearly and concisely. Wilsonville currently has a Wilsonville Trail signage program, and is encouraged to continue those efforts. Similar signs could be posted along Wilsonville Road and other well-traveled bike routes. For example, a sign placed at Wilsonville Road and Boones Ferry Road could indicate to bicyclists that Boones Ferry Park is to the south, Graham Oaks Natural Area and Commuter Rail are to the west, and



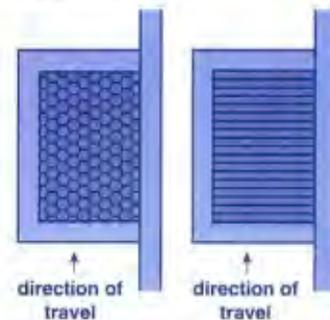
Wilsonville Trail signage

Town Center Loop and Memorial Park are to the east. A sign at Kinsman Road would then reinforce that Graham Oaks Natural Area and schools are still west, while Commuter Rail is to the north. The directions could be posted either separately, as shown in the photo to the right, or beneath the Wilsonville Trail or other city logo directly on the sign. Note that too many road signs tend to clutter the right-of-way, and it is recommended that these suggested signs be posted at a level most visible to bicyclists and pedestrians, rather than per vehicle signage standards. Care also needs to be taken that any signs are posted at the proper location and orientation to be visible to bicyclists.

Placing signs throughout the city indicating to bicyclists and pedestrians direction of travel, location of destinations, and the time/distance to those destinations will increase users' comfort and accessibility to the bicycle and pedestrian system. Additional information about signage and other network amenities can be found in the Design Standards and Guidelines chapter.

Drainage Grate Retrofit

Wilsonville has taken a number of steps to make storm drainage grates bicycle safe, as required by ORS 810.150. In Council minutes from early 2005, it was noted that, "The City has



completed a program to retrofit catch basin inlets that had parallel metal strips where bicycle tires could slip into the grating. This retrofit program was a joint effort between our Fleet Crew (who welded crossbars into each of the old-style grates) and our Roads Crew (who removed/reinstalled the grates). Now bicyclists can safely navigate all City streets without risk of having their tires drop into catch basin inlets."

Where the City has retrofitted the existing drainage grates, the City has begun a program to replace those grates as the road is resurfaced, since the new grates still have spacing that is narrow enough to catch narrow bicycle tires, or cause flat tires. In new road and curb construction, Wilsonville is installing inlets in the

ATTACHMENT C. Destination Categorization List

TABLE 2. DESTINATION CATEGORIZATION			
DESTINATION	OVERALL CATEGORY	CLASS	OVERALL RANK
Town Center	Commercial Center: District	I	1
Old Town Square	Commercial Center: District	I	2
Argyle Square	Commercial Center: District	I	3
Clackamas Community College	Schools: College	I	4
Oregon Institute of Technology	Schools: College	I	5
Pioneer Pacific College	Schools: College	I	6
Korean War Memorial	Regional Parks/Trails	II	7
Graham Oaks Nature Park	Regional Parks/Trails	II	8
Murase Plaza	Regional Parks/Trails	II	9
Memorial Park	Regional Parks/Trails	II	10
Town Center Park	Regional Parks/Trails	II	11
Boones Ferry Park	Regional Parks/Trails	II	12
Willamette River Water Treatment Plant Park	Regional Parks/Trails	II	13
WES Transit Center	Transit Station	II	14
Library	Civic/Community	III	15
City Hall	Civic/Community	III	16
Community Center	Civic/Community	III	17
Post Office	Civic/Community	III	18
Community Gardens	Civic/Community	III	19
Wilsonville High School	Schools: High School	III	20
Inza Wood Middle School	Schools: Elementary & Middle	IV	21
Boeckman Creek Primary School	Schools: Elementary & Middle	IV	22
Boones Ferry Primary School	Schools: Elementary & Middle	IV	23
Lowrie Primary School	Schools: Elementary & Middle	IV	24
Sofia Park	Local Parks/Trails	III	25
Courtside Park	Local Parks/Trails	III	26
Piccadilly Park	Local Parks/Trails	III	27
Palermo Park	Local Parks/Trails	III	28
Canyon Creek Park	Local Parks/Trails	III	29
Green Acres Park	Local Parks/Trails	III	30
Hathaway Park	Local Parks/Trails	III	31
Mentor Graphics Path	Local Parks/Trails	III	32
Montebello Park	Local Parks/Trails	III	33
Park at Merryfield	Local Parks/Trails	III	34
River Fox Park	Local Parks/Trails	III	35
Tranquil City Park	Local Parks/Trails	III	36
Walt Morey Park	Local Parks/Trails	III	37
Willow Creek - Landover Park	Local Parks/Trails	III	38
Lowrie's Marketplace	Commercial Center: Local	III	39
Riverwood Center	Commercial Center: Local	III	40
Marketplace	Commercial Center: Local	III	41
The Village at Main Street	Commercial Center: Local	III	42
Boones Ferry Pointe	Commercial Center: Local	III	43
Arts & Tech High School	Schools: High School	III	44
Cinema: Regal Cinemas	Attraction	III	45
Family Fun Center	Attraction	III	46
Museum: World of Speed	Attraction	III	47

*Destinations highlighted in grey-blue are included in wayfinding signage.

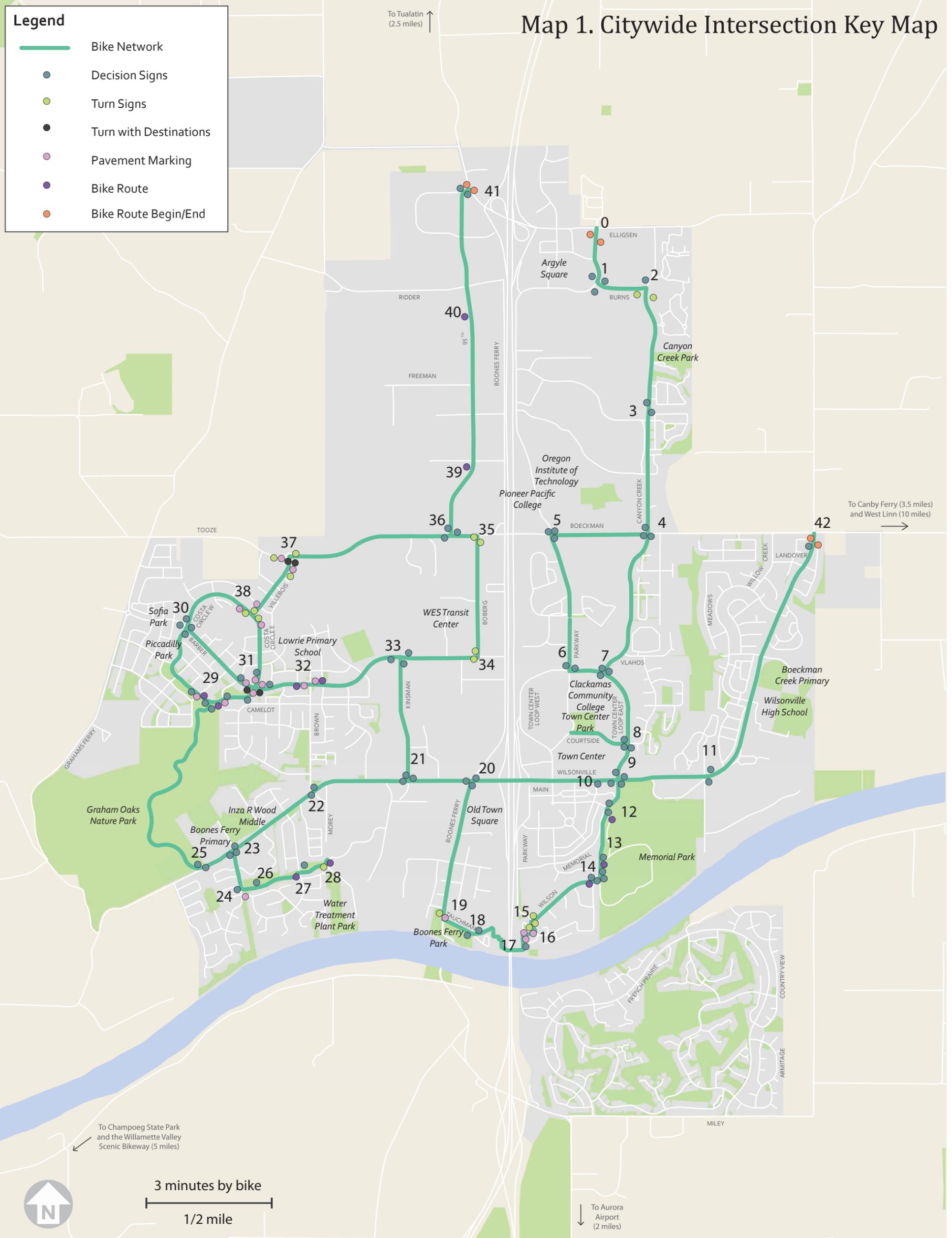
*Destinations highlighted in green are included on the Wilsonville Bike & Walk Map.

ATTACHMENT D. Citywide Intersection Key Map

Map 1. Citywide Intersection Key Map

Legend

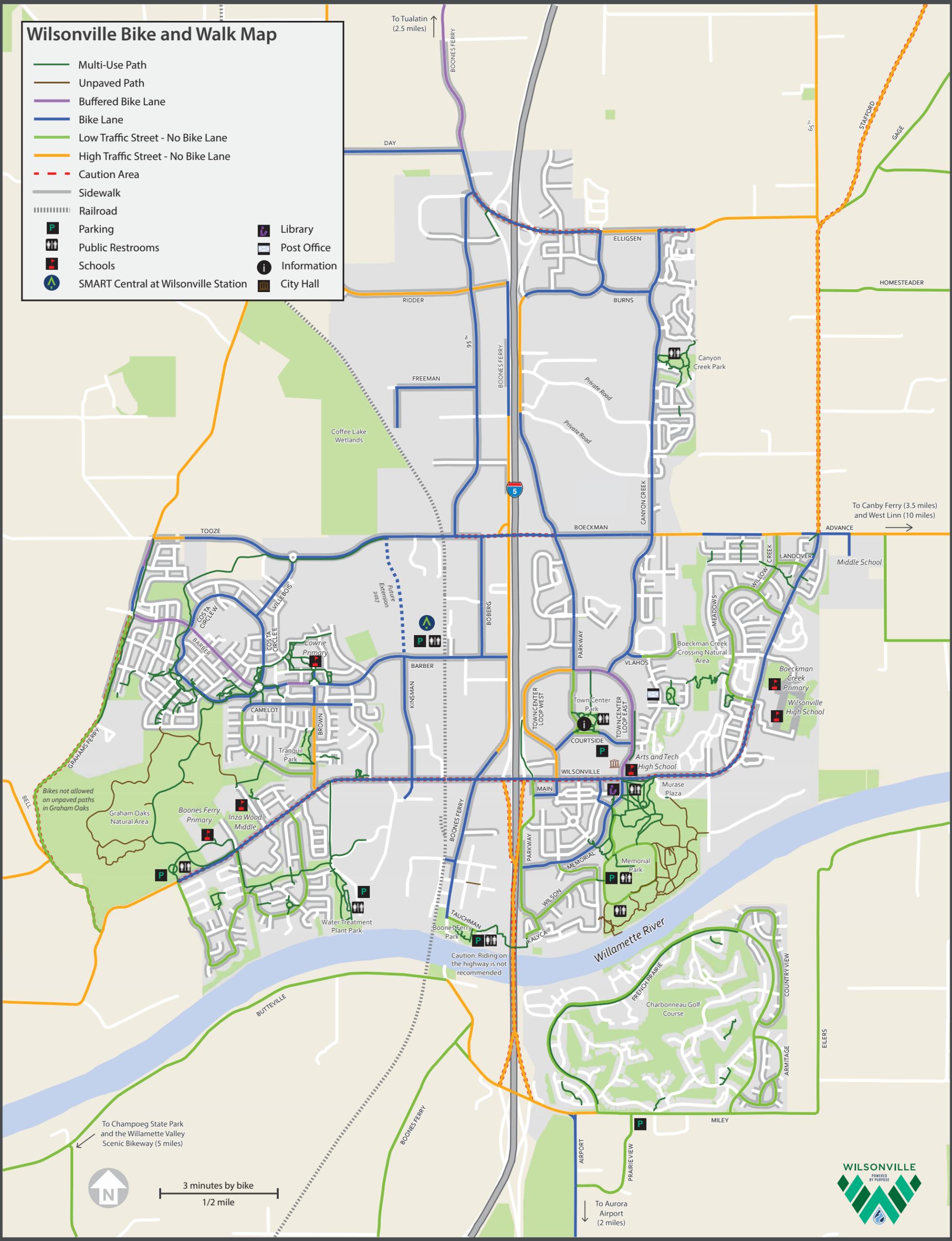
-  Bike Network
-  Decision Signs
-  Turn Signs
-  Turn with Destinations
-  Pavement Marking
-  Bike Route
-  Bike Route Begin/End



ATTACHMENT E. Wilsonville Bike and Walk Map

Wilsonville Bike and Walk Map

- Multi-Use Path
- Unpaved Path
- Buffered Bike Lane
- Bike Lane
- Low Traffic Street - No Bike Lane
- High Traffic Street - No Bike Lane
- - - Caution Area
- Sidewalk
- Railroad
- P Parking
- ♿ Public Restrooms
- 🏫 Schools
- 📍 SMART Central at Wilsonville Station
- 📖 Library
- 📮 Post Office
- 📰 Information
- 🏛️ City Hall



To Tualatin (2.5 miles) ↑

To Canby Ferry (3.5 miles) and West Linn (10 miles) →

Bikes not allowed on unpaved paths in Graham Oaks

Caution: Riding on the highway is not recommended



3 minutes by bike
1/2 mile

To Aurora Airport (2 miles) ↓



CITY OF WILSONVILLE
BICYCLE WAYFINDING SYSTEM
SIGN DISPLAY AND PLACEMENT PLAN

DRAFT

CITY OF WILSONVILLE BICYCLE WAYFINDING SYSTEM

SIGN DISPLAY AND PLACEMENT REPORT



PREPARED BY:

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CITY OF WILSONVILLE BICYCLE WAYFINDING SYSTEM: SIGN DISPLAY AND PLACEMENT REPORT

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INTRODUCTION

Directive bicycle signage is a cost-effective method for improving the City of Wilsonville's bicycling environment. The *Bicycle Wayfinding System Sign Display and Placement Plan* is intended to be used by the City as a comprehensive guide to implement a directive signage (wayfinding) system for bicyclists. The wayfinding system will work in coordination with the City's existing bicycle infrastructure, low traffic streets and trails, to provide a positive experience for those looking to reach key destinations within Wilsonville by bike. The *Sign Display and Placement Plan* will provide guidance regarding sign design, layout and display as well as propose locations for sign placement.

BASIS FOR WAYFINDING NEED

Wilsonville's wayfinding system addresses the need documented in the Wilsonville Bicycle and Pedestrian Master Plan to establish directive signage for bicyclists. The 2013 Bicycle and Pedestrian Connectivity Action Plan further emphasizes the need for a signage system in order to encourage biking within the city.

BICYCLE WAYFINDING BENEFITS

The ability to navigate through a city is informed by landmarks, natural features and other visual cues. A signage system is a key component of a navigable environment and would inform bicyclists while also enhancing the identity of Wilsonville. Placing signs throughout the city which indicate to bicyclists the direction of key destinations, as well as the time and distance required to reach those destinations, will increase users' comfort with, and accessibility to, established bicycle infrastructure.

A concise directive signage system delivers benefits which will help to meet specific goals laid out by the Bike and Pedestrian Master Plan as well as the Bicycle and Pedestrian Connectivity Action Plan. Specific benefits include:

- Familiarizing bicyclists with Wilsonville's existing bicycle infrastructure
- Encouraging less experienced bicyclists by increasing awareness of safe routes to various destinations
- Enhancing bicyclists ability to navigate to key destinations by bike
- Promoting Wilsonville's identity
- Serving as a working visual to address misconceptions about the time it takes to reach destinations by bike

DESTINATIONS

SELECTION

Input from city staff and the public was used to generate a comprehensive list of all possible destinations that could be signed to within Wilsonville. In order to consolidate the extensive list a decision was made to exclude individual businesses and task the general citywide signage project with neighborhood districts. By making the necessary omissions, the destination list was condensed to 47 individual destinations which can be found listed in Table 2.

City staff and public input, as well as advice from Alta Planning + Design, was further used to develop a ranking system to help determine the importance of each destination and establish the physical distance from which locations are signed. The category ranks, as well as their placement distances, are summarized in Table 1. Table 2 further illustrates how the category rank was applied to each individual destination. The overall rank of each destination, laid out in Table 2, was used to help determine which location should be added to signage when a new slot became available.

TABLE 1. CATEGORY RANKS			
Category	I-IV Rank	Overall Rank	Distance of Signage
Commercial Centers: District	I Districts	1	3 Miles
Schools: College	I Districts	2	3 Miles
Regional Parks/Trails	II Regional	3	2 Miles
Public Transit	II Regional	4	2 Miles
Neighborhoods	II Regional	5	2 Miles
Civic/Community	III Local	6	1 Mile
Schools: High School	III Local	7	1 Mile
Schools: Elementary & Middle	IV Middle/Elementary	9	1/2 Mile
Local Parks/Trails	III Local	8	1 Mile
Commercial Centers: Local	III Local	10	1 Miles
Attraction	III Local	11	1 Mile

TABLE 2. DESTINATION CATEGORIZATION

DESTINATION	OVERALL CATEGORY	CLASS	OVERALL RANK
Town Center	Commercial Center: District	I	1
Old Town Square	Commercial Center: District	I	2
Argyle Square	Commercial Center: District	I	3
Clackamas Community College	Schools: College	I	4
Oregon Institute of Technology	Schools: College	I	5
Pioneer Pacific College	Schools: College	I	6
Korean War Memorial	Regional Parks/Trails	II	7
Graham Oaks Nature Park	Regional Parks/Trails	II	8
Murase Plaza	Regional Parks/Trails	II	9
Memorial Park	Regional Parks/Trails	II	10
Town Center Park	Regional Parks/Trails	II	11
Boones Ferry Park	Regional Parks/Trails	II	12
Willamette River Water Treatment Plant Park	Regional Parks/Trails	II	13
WES Transit Center	Transit Station	II	14
Library	Civic/Community	III	15
City Hall	Civic/Community	III	16
Community Center	Civic/Community	III	17
Post Office	Civic/Community	III	18
Community Gardens	Civic/Community	III	19
Wilsonville High School	Schools: High School	III	20
Inza Wood Middle School	Schools: Elementary & Middle	IV	21
Boeckman Creek Primary School	Schools: Elementary & Middle	IV	22
Boones Ferry Primary School	Schools: Elementary & Middle	IV	23
Lowrie Primary School	Schools: Elementary & Middle	IV	24
Sofia Park	Local Parks/Trails	III	25
Courtside Park	Local Parks/Trails	III	26
Piccadilly Park	Local Parks/Trails	III	27
Palermo Park	Local Parks/Trails	III	28
Canyon Creek Park	Local Parks/Trails	III	29
Green Acres Park	Local Parks/Trails	III	30
Hathaway Park	Local Parks/Trails	III	31
Mentor Graphics Path	Local Parks/Trails	III	32
Montebello Park	Local Parks/Trails	III	33
Park at Merryfield	Local Parks/Trails	III	34
River Fox Park	Local Parks/Trails	III	35
Tranquil City Park	Local Parks/Trails	III	36
Walt Morey Park	Local Parks/Trails	III	37
Willow Creek - Landover Park	Local Parks/Trails	III	38
Lowrie's Marketplace	Commercial Center: Local	III	39
Riverwood Center	Commercial Center: Local	III	40
Marketplace	Commercial Center: Local	III	41
The Village at Main Street	Commercial Center: Local	III	42
Boones Ferry Pointe	Commercial Center: Local	III	43
Arts & Tech High School	Schools: High School	III	44
Cinema: Regal Cinemas	Attraction	III	45
Family Fun Center	Attraction	III	46
Museum: World of Speed	Attraction	III	47

***Destinations highlighted in grey-blue are included in wayfinding signage.**

***Destinations highlighted in green are included on the Wilsonville Bike & Walk Map.**

TABLE 3. SIGNS DISPLAYING DESTINATIONS

Destination	Sign Number
Argyle Square	1c, 3a, 4a, 4c, 5a, 5b, 7b, 7c, 8a, 8c
Boeckman Creek Primary School	11b, 42b
Boones Ferry Park	9c, 11a, 12a, 13a, 14a, 14b, 17b, 18a, 20c, 21a, 30a, 31a, 31c, 31d, 31h, 33b, 33c, 36c, 37f
Boones Ferry Primary School	22a
Canyon Creek Park	1a, 1b, 2b, 3a,
Clackamas Community College	4c, 6a, 7a, 7c, 8a, 9a, 9d, 10a, 12b, 13c, 20c
Colleges	1a, 1b, 2b
Graham Oaks Nature Park	9a, 9b, 10a, 12b, 13c, 14c, 18a, 20a, 20b, 21a, 21b, 22a, 23a, 23b, 26a, 27a, 29b, 29c, 29f, 30b, 30c, 30d, 31d, 31f, 31j, 33a, 33b, 36a, 36b, 37d, 41a, 41b
Inza Wood Middle School	22a
Lowrie Primary School	33a
Memorial Park	8b, 8c, 9b, 12a, 14c, 18b
Old Town Square	5b, 6a, 7a, 8b, 8c, 9b, 9c, 11a, 12a, 13a, 14a, 14b, 17b, 18a, 21a, 21c, 22b, 23a, 23c, 25a, 26a, 27a, 29a, 29b, 29f, 30a, 30b, 30d, 31a, 31c, 31d, 31h, 33b, 33c, 36a, 36c, 37f, 41a, 41b
Oregon Institute of Technology	3b, 4a, 4b, 5a, 5c, 6b, 7b
Piccadilly Park	29c, 30c, 31f, 31j, 36b, 37d
Pioneer Pacific College	3b, 4a, 4b, 5a, 5c, 6b, 7b, 20a
Sofia Park	29a, 29c, 30c, 31f, 31j, 36b, 37d
Town Center	1a, 1b, 2b, 3b, 4b, 4c, 5b, 5c, 6a, 7a, 7c, 11a, 20c, 21c, 22b, 42b
Town Center Park	8a, 8b, 9a, 9c, 9d, 10a, 12b, 13c, 14c
WES Transit Center	20a, 20b, 21b, 21c, 22b, 23a, 23c, 25a, 26a, 27a, 29a, 29f, 30a, 30b, 30d, 31a, 31c, 31h, 33a, 33c, 36a, 36c, 37f, 41a, 41b
Willamette River Water Treatment Plant Park	20b, 21b, 22a, 23b, 23c, 24a, 25a, 29b
Wilsonville High School	9d, 11b, 42b

TABLE 4. STANDARD ABBREVIATION LIST	
Destination Name	Standard Abbreviation
Boeckman Creek Primary School	Boeckman Crk. Pri. Sch.
Boones Ferry Park	Boones Ferry Pk.
Boones Ferry Primary School	Boones Ferry. Pri. Sch.
Canyon Creek Park	Canyon Crk. Pk.
Clackamas Community College	CCC
Graham Oaks Nature Park	Graham Oaks
Inza R. Wood Middle School	Wood Middle Sch.
Lowrie Primary School	Lowrie Pri. Sch.
Old Town Square	Old Town Sq.
Oregon Institute of Technology	Oregon Tech
Pioneer Pacific College	Pioneer Pac. Coll.
Town Center Park	Town Center Pk.
WES Transit Center	Transit Center
Willamette River Water Treatment Plant Park	Water Treatment Plant Park

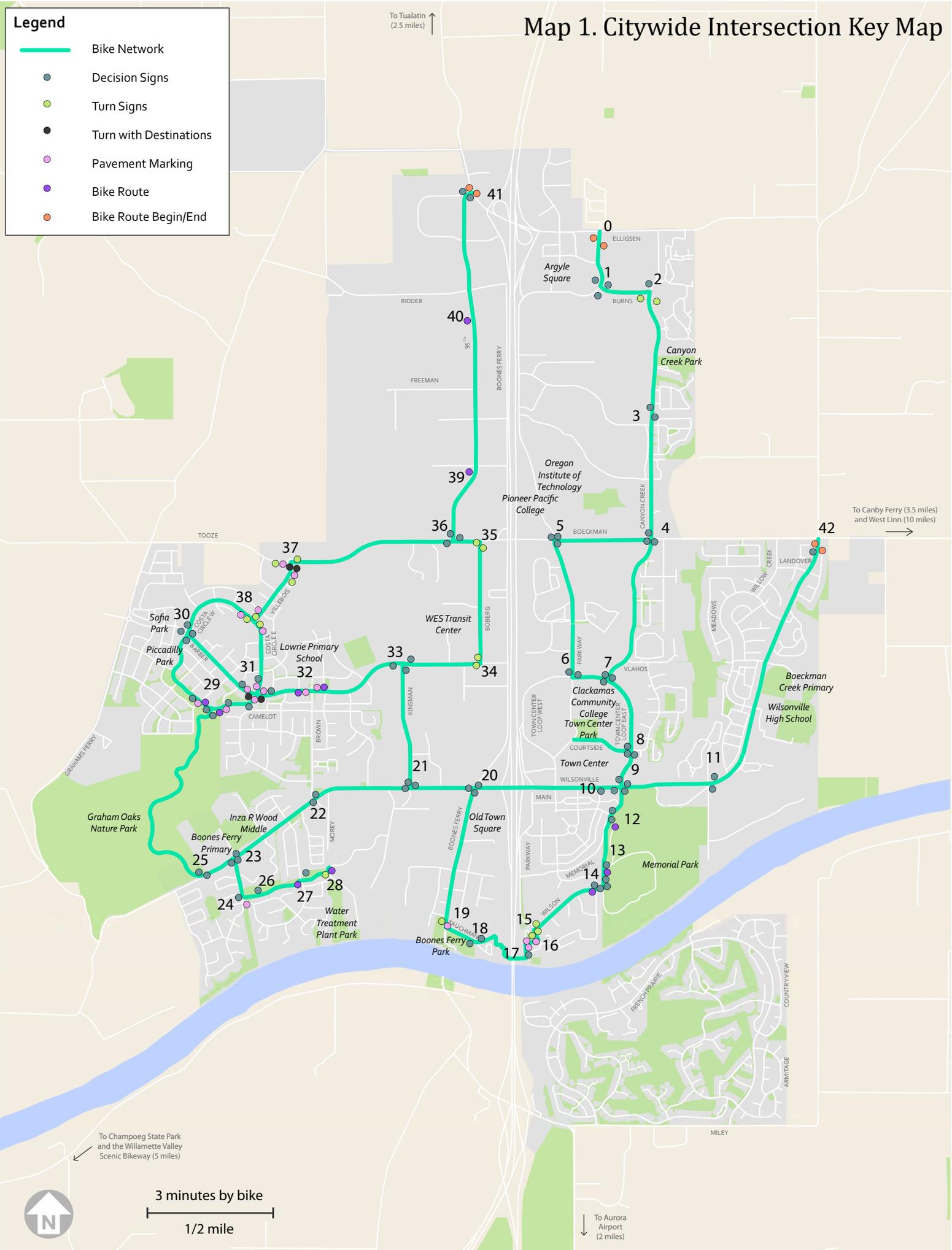
Map 1. Citywide Intersection Key Map

Legend

- Bike Network
- Decision Signs
- Turn Signs
- Turn with Destinations
- Pavement Marking
- Bike Route
- Bike Route Begin/End

To Tualatin
(2.5 miles) ↑

To Canby Ferry (3.5 miles)
and West Linn (10 miles) →



To Champoeg State Park
and the Willamette Valley
Scenic Bikeway (5 miles)

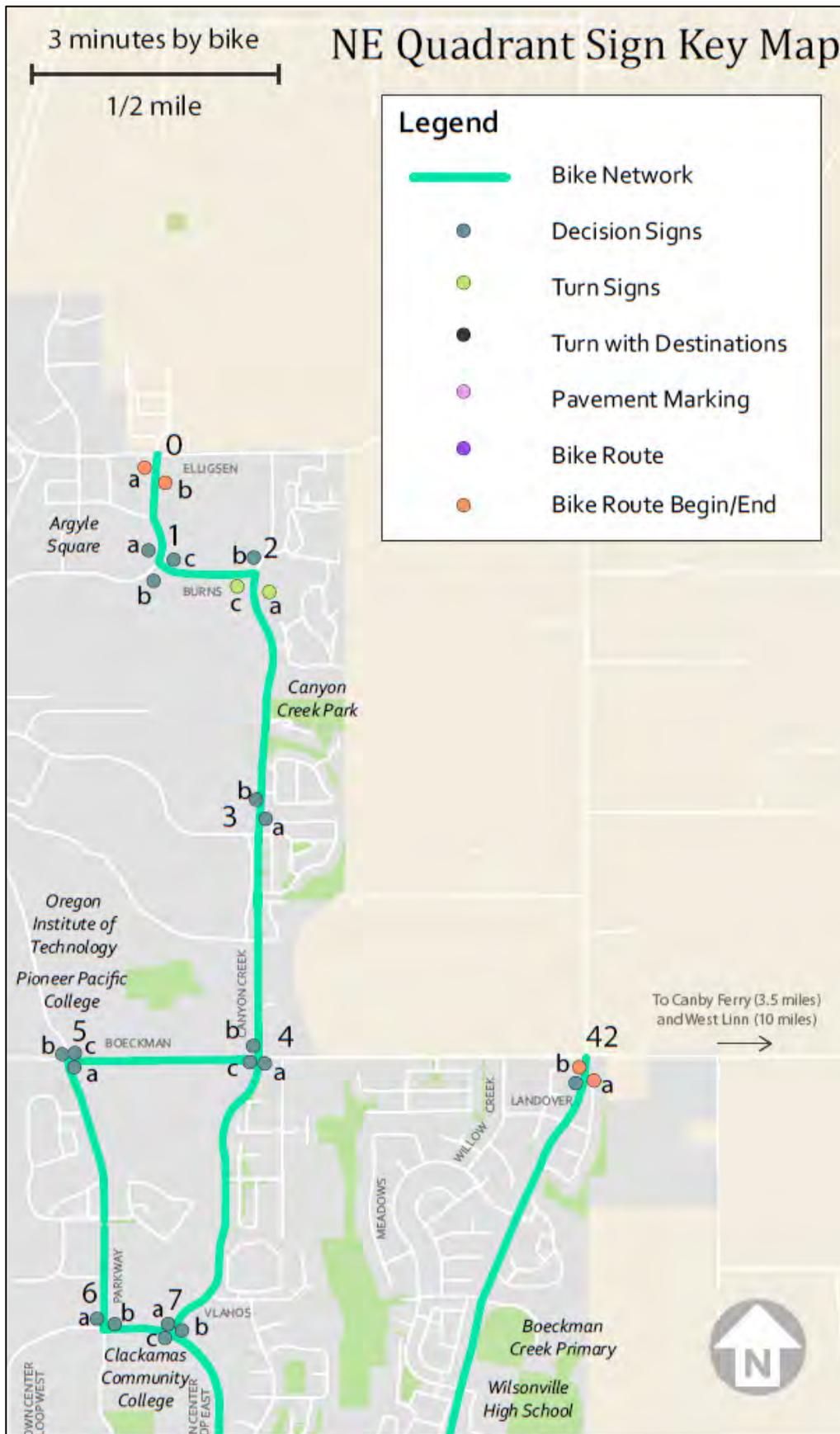


3 minutes by bike

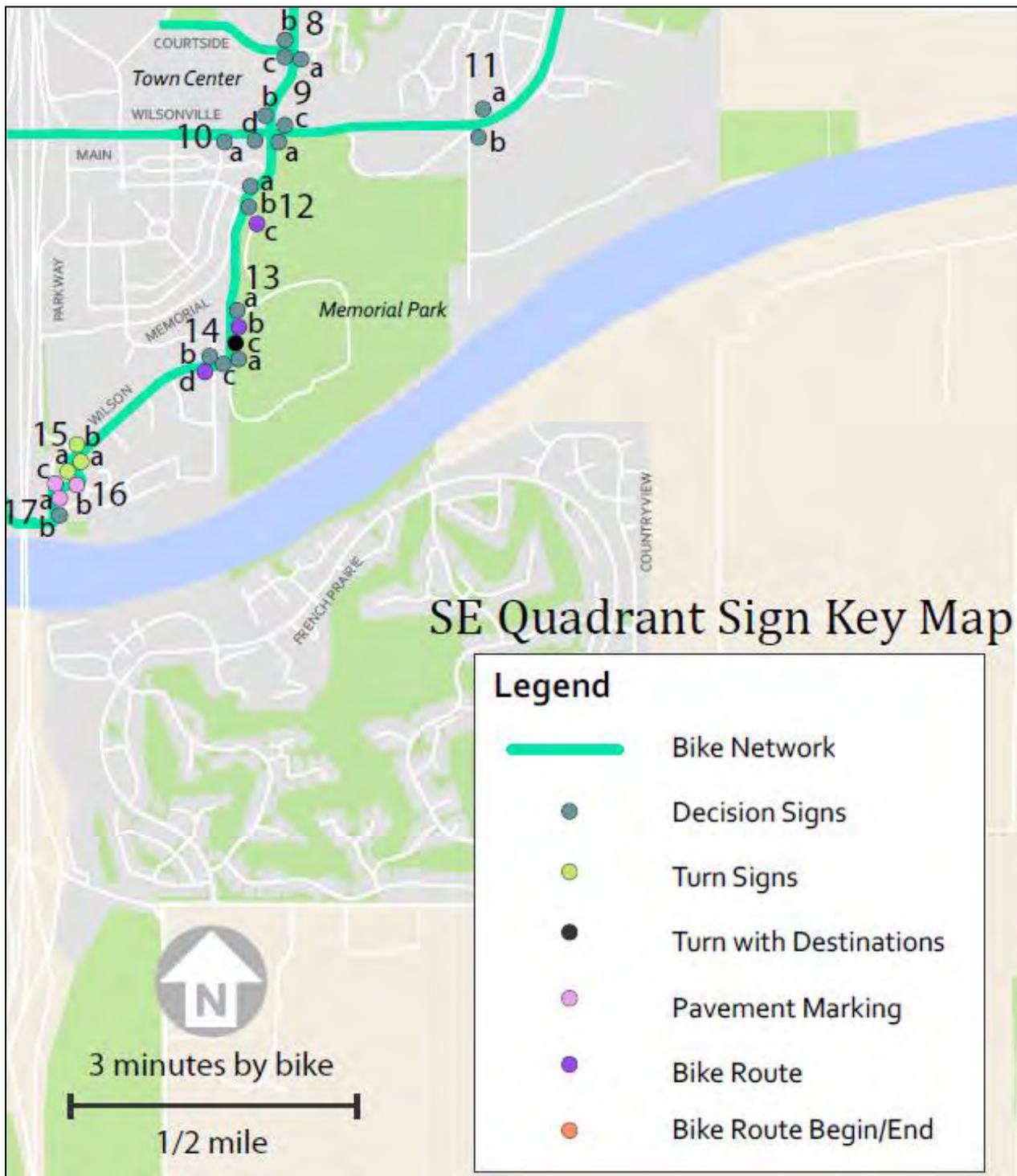


1/2 mile

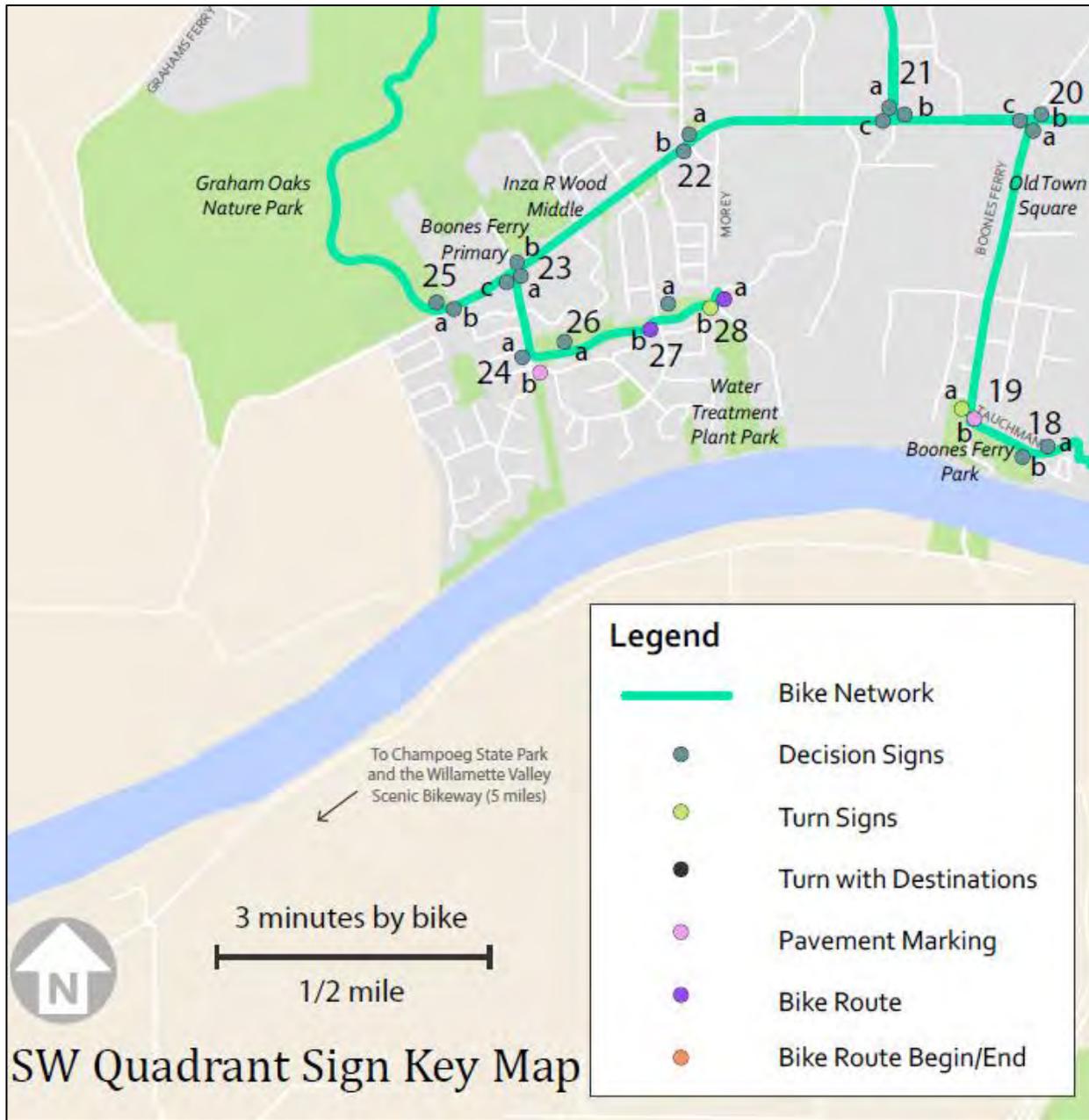
To Aurora
Airport
(2 miles) ↓



Map 2. NE Quadrant: Individual Sign Key Map



Map 3. SE Quadrant: Individual Sign Key Map



Map 4. SW Quadrant: Individual Sign Key Map



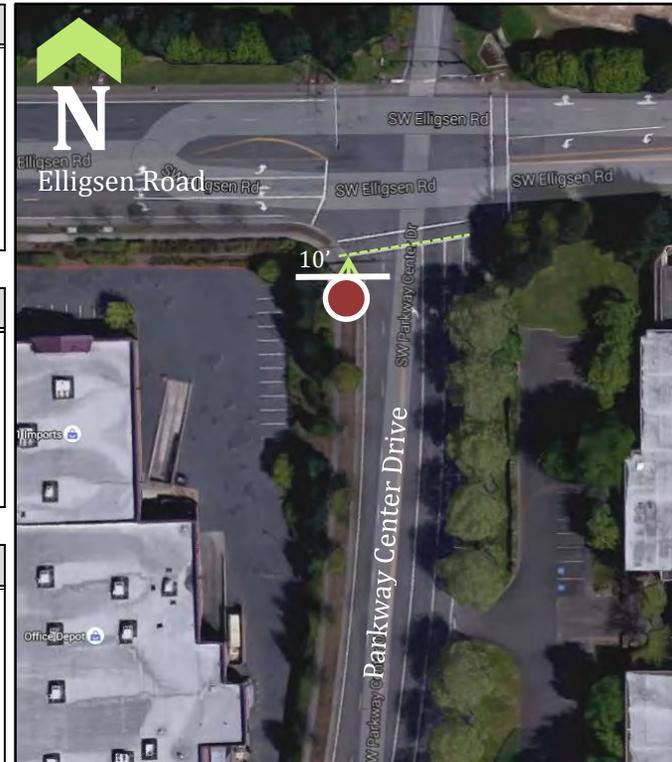
Map 5. NW Quadrant: Individual Sign Key Map

INTERSECTION 0a

INFORMATION	
SIGN TYPE	On-Street Bike Route Begin
INTERSECTION	Parkway Center Drive & Elligsen Road
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	0a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Parkway Center Drive
DISTANCE FROM INTERSECTION (Feet)	10
LATERAL DISTANCE (Feet)	-

NOTES	
Place bike route begin sign on the west side of Parkway Center Drive 10 feet south of Elligsen Road. Measured from southern-most crosswalk bar across Parkway Center Drive.	
<p><i>10 feet to accommodate use of traffic signal pole.</i></p> <p><i>Mount sign on traffic signal pole.</i></p>	



Satellite Location View (Location to be verified by contractor)

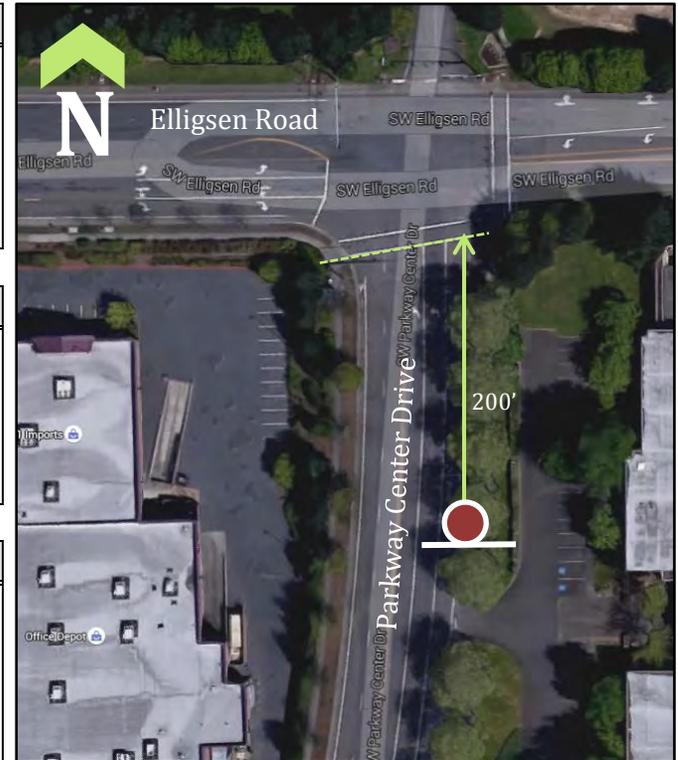


INTERSECTION 0b

INFORMATION	
SIGN TYPE	On-Street Bike Route End
INTERSECTION	Parkway Center Drive & Elligsen Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	0b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Parkway Center Drive
DISTANCE FROM INTERSECTION (Feet)	200
LATERAL DISTANCE (Feet)	-

NOTES	
Place bike route end sign on the east side of Parkway Center Drive 200 feet south of Elligsen Road. Measured from southern-most crosswalk bar across Parkway Center Drive.	
<p><i>200 feet to accommodate use of light pole.</i></p> <p><i>Mount sign on light pole.</i></p>	



Satellite Location View (location to be verified by contractor)

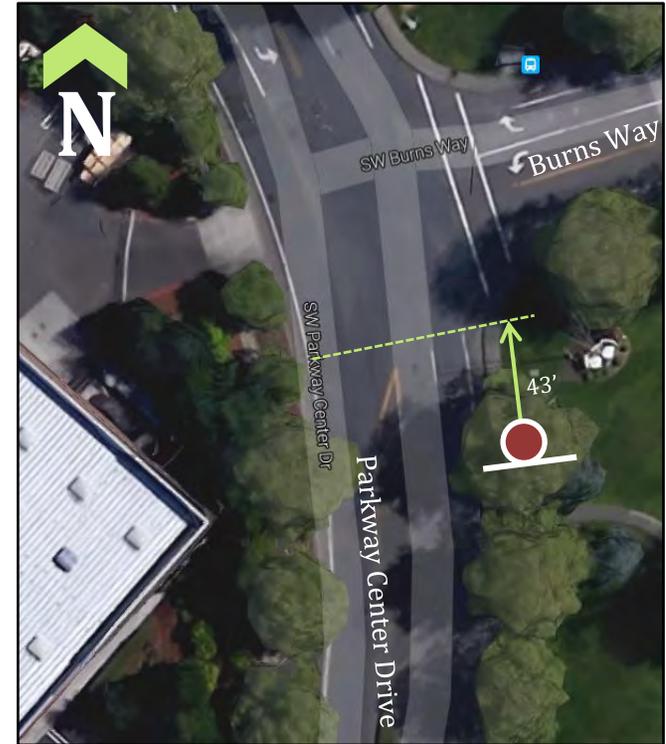


INTERSECTION 1A

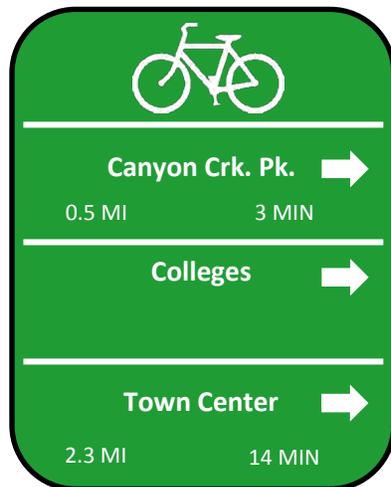
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Parkway Center Drive & Burns Way
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	1a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Parkway Center Drive
DISTANCE FROM INTERSECTION (Feet)	43
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES
Place decision sign on the east side of Parkway Center Drive 43 feet south of Burns Way. Measured from where Parkway Center Drive lane markings end.
<i>Initial 30 feet due to only right turn needed to reach destinations. Additional 13 feet to avoid tree.</i>



Satellite Location View (location to be verified by contractor)

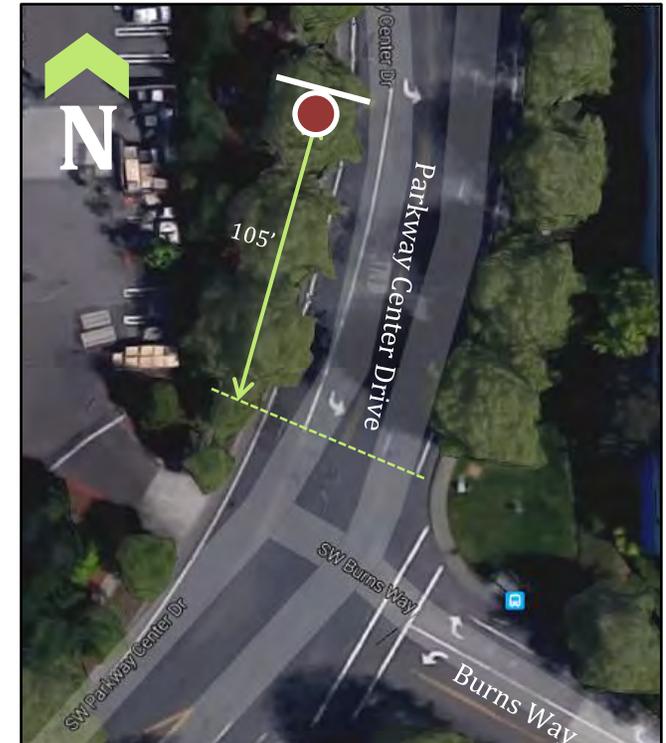


INTERSECTION 1B

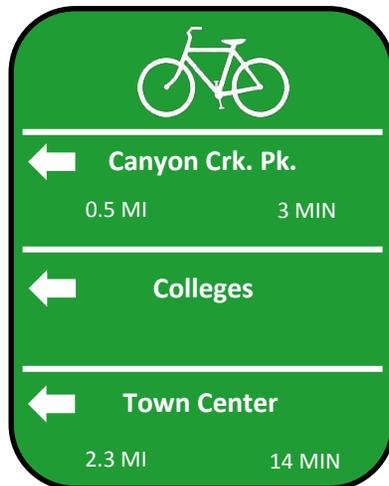
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Parkway Center Drive & Burns Way
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	1b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Parkway Center Drive
DISTANCE FROM INTERSECTION (Feet)	105
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES	
Place decision sign on the west side of Parkway Center Drive 105 feet north of Burns Way. Measured from where Parkway Center Drive lane markings end.	
<i>Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations. Additional 5 feet to avoid tree.</i>	



Satellite Location View (location to be verified by contractor)

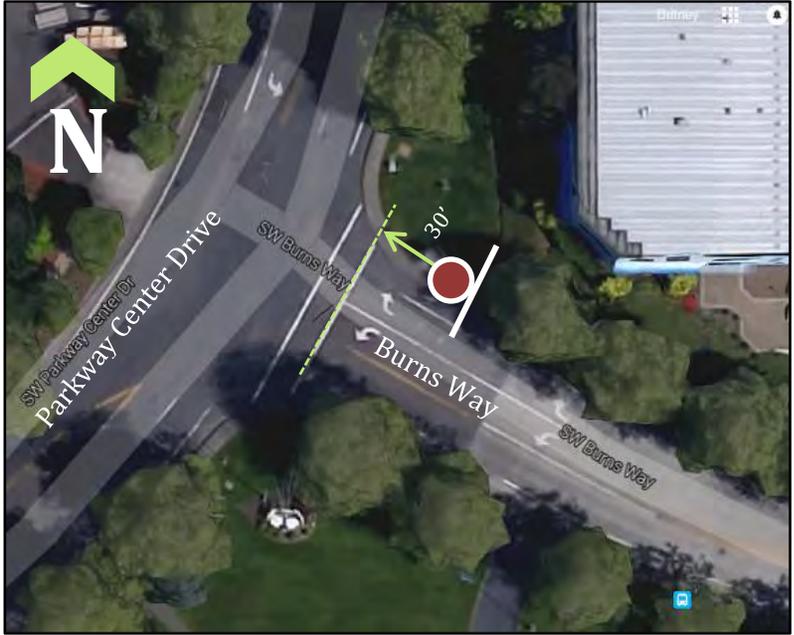


INTERSECTION 1c

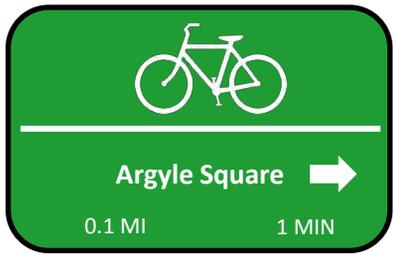
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Parkway Center Drive & Burns Way
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	1c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Burns Way
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES	
Place decision sign on the north side of Burns Way 30 feet east of Parkway Center Drive. Measured from the eastern-most crosswalk bar across Burns Way.	
<i>30 feet due to only right turn needed to reach destinations.</i>	



Satellite Location View (Location to be verified by contractor)

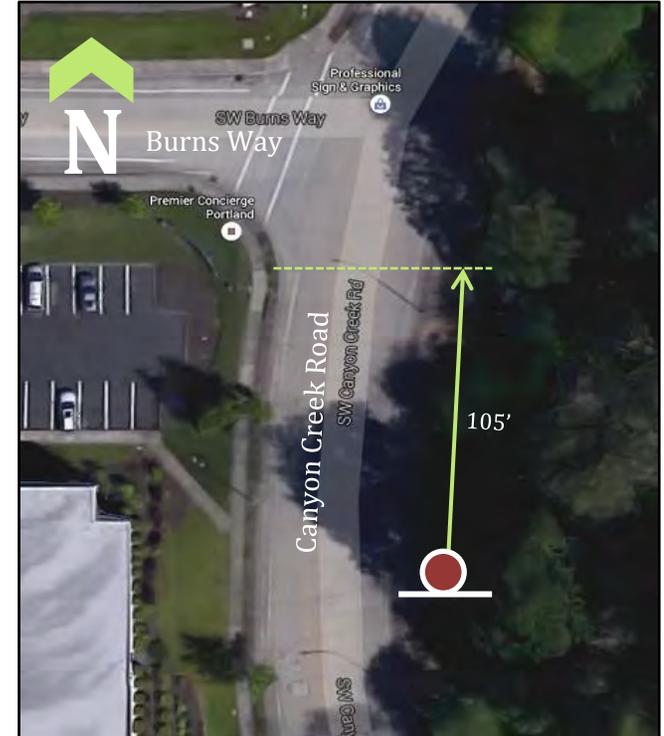


INTERSECTION 2A

INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Canyon Creek Road & Burns Way
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	2a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Canyon Creek Road
DISTANCE FROM INTERSECTION (Feet)	105
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES
Place turn sign on the east side of Canyon Creek Road 105 feet south of Burns Way. Measured from where Canyon Creek road markings end.
<i>Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations. Additional 5 feet to avoid tree</i>



Satellite Location View (Location to be verified by contractor)

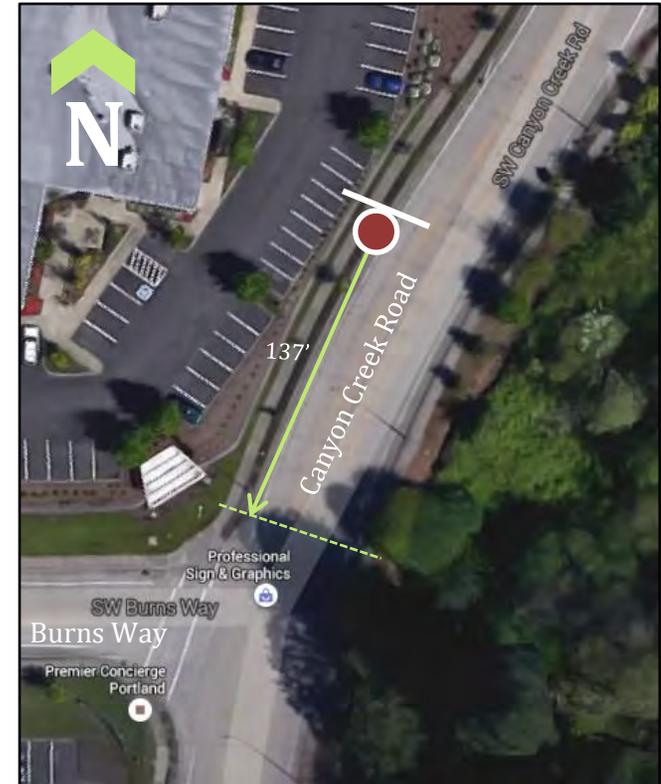


INTERSECTION 2B

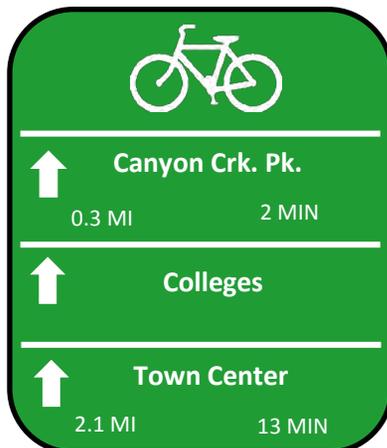
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Canyon Creek Road & Burns Way
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	2b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Canyon Creek Road
DISTANCE FROM INTERSECTION (Feet)	137
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the west side of Canyon Creek Road 137 feet north of Burns Way. Measured from where Canyon Creek road markings end.	
<i>Initial 30 feet due to cyclists only continuing straight to reach destinations. Additional 107 feet to accommodate using light pole.</i>	
<i>Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)

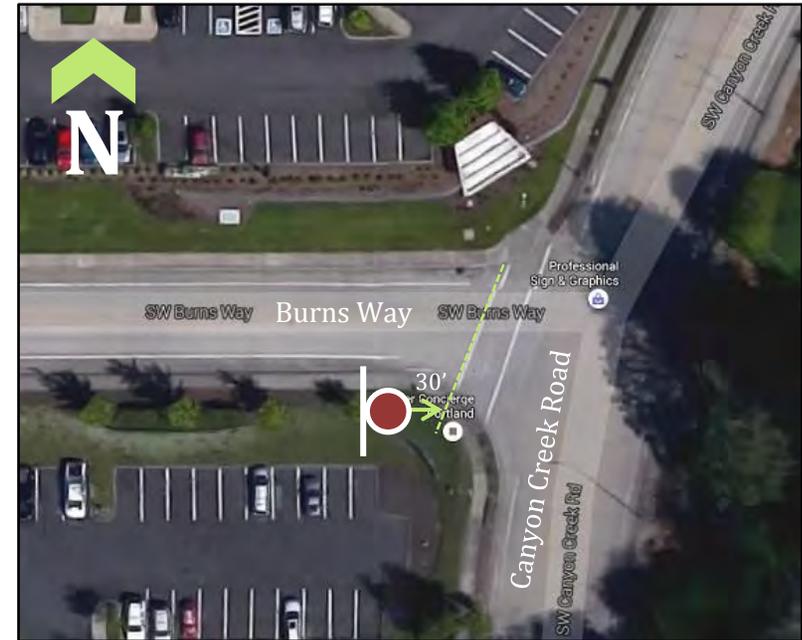


INTERSECTION 2c

INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Canyon Creek Road & Burns Way
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	2c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Burns Way
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	2 (Edge of Sidewalk)

NOTES
Place turn sign on the south side of Burns Way 30 feet West of Canyon Creek Road. Measured from western-most cross walk bar across Burns Way.
<i>30 feet due to only right turn needed to reach destinations.</i>



Satellite Location View (location to be verified by contractor)

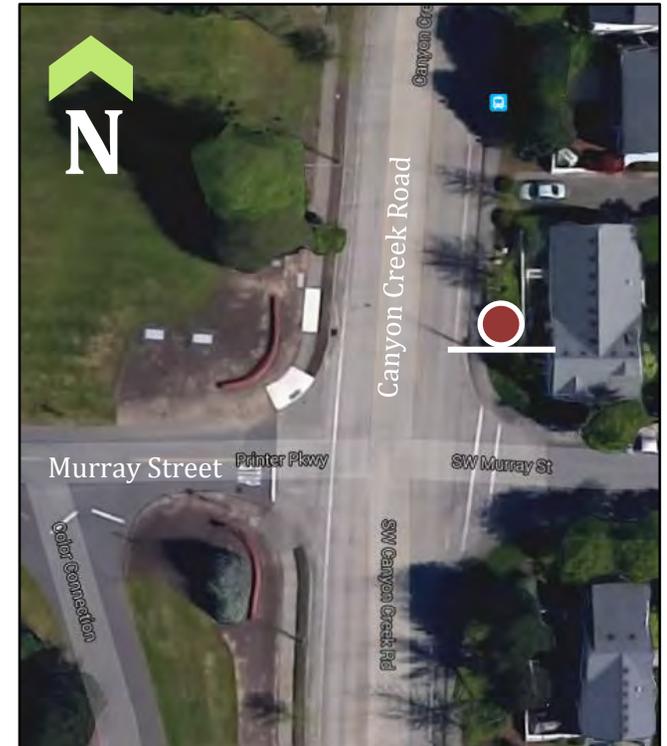


INTERSECTION 3A

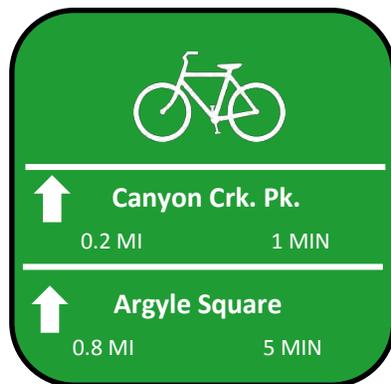
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Canyon Creek Road & Murray Street
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	3a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Canyon Creek Road
DISTANCE FROM INTERSECTION (Feet)	-
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the east side of Canyon Creek Road roughly 10 feet north of Murray Street.	
<i>Mount on light pole.</i>	



Satellite Location View (Location to be verified by contractor)

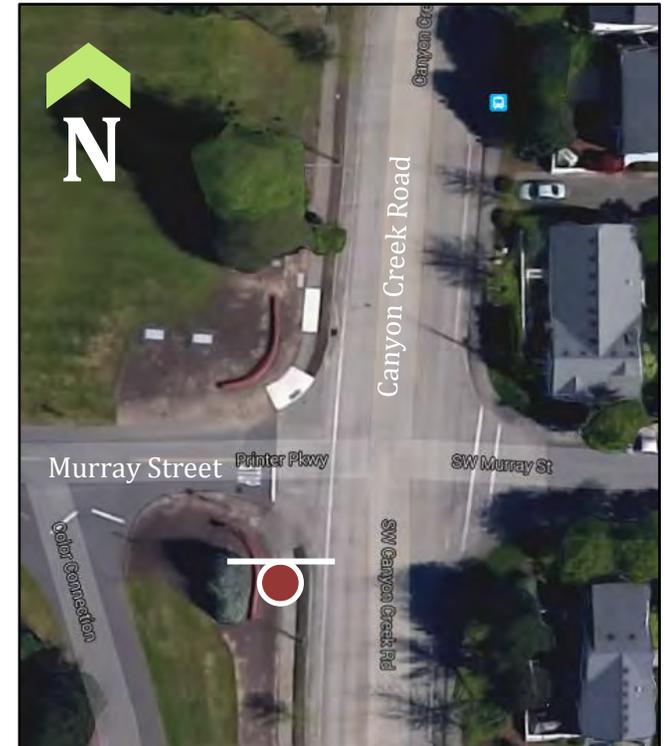


INTERSECTION 3B

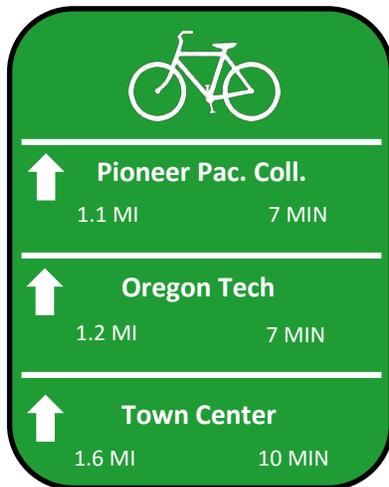
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Canyon Creek Road & Murray Street
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	3b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Canyon Creek Road
DISTANCE FROM INTERSECTION (Feet)	-
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the west side of Canyon Creek Road roughly 10 feet south of Murray Street.	
<i>Mount on Light Pole.</i>	



Satellite Location View (location to be verified by contractor)

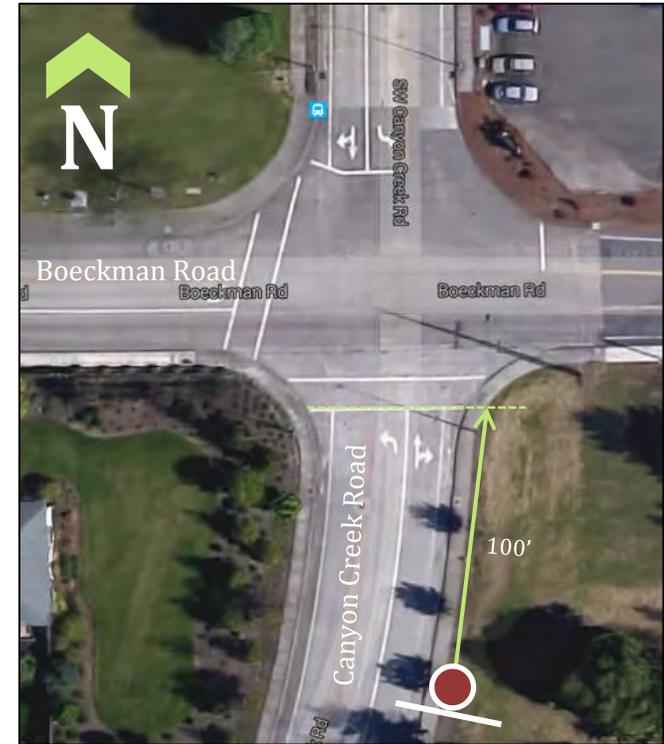


INTERSECTION 4A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Canyon Creek Road & Boeckman Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	4a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Canyon Creek Road
DISTANCE FROM INTERSECTION (Feet)	100
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES	
Place decision sign on the east side of Canyon Creek Road 100 feet south of Boeckman Road. Measured from southern-most crosswalk bar across Canyon Creek Road.	
<i>100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations.</i>	



Satellite Location View (Location to be verified by contractor)

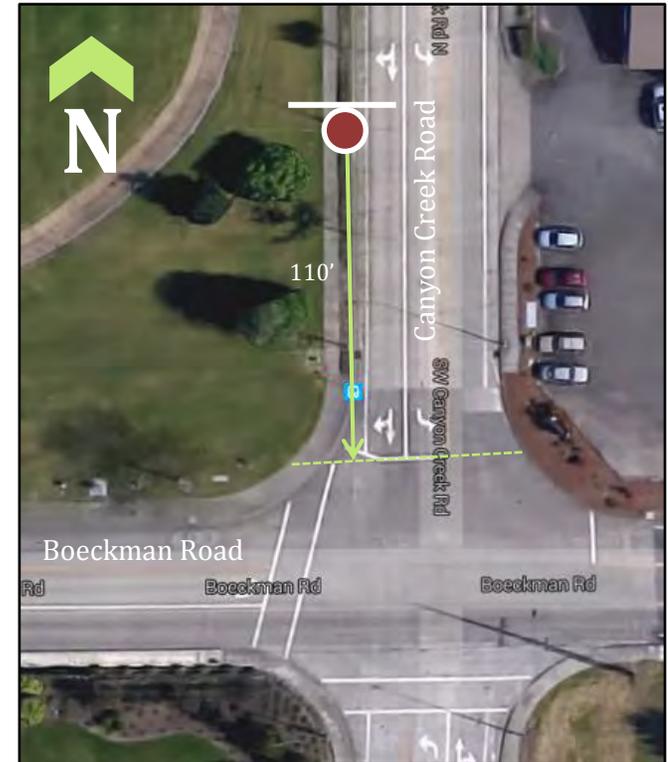


INTERSECTION 4B

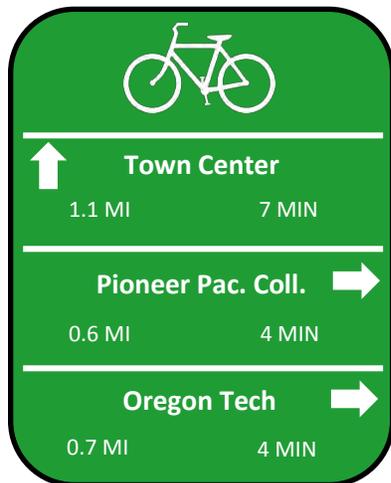
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Canyon Creek Road & Boeckman Road
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	4b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Canyon Creek Road
DISTANCE FROM INTERSECTION (Feet)	110
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the west side Canyon Creek Road 110 feet north of Boeckman Road. Measured from the stop bar across Canyon Creek Road.	
<i>Initial 30 feet due to only straight or right turn needed to reach destinations. Move back an additional 80 feet to mount on light pole.</i>	
<i>Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)

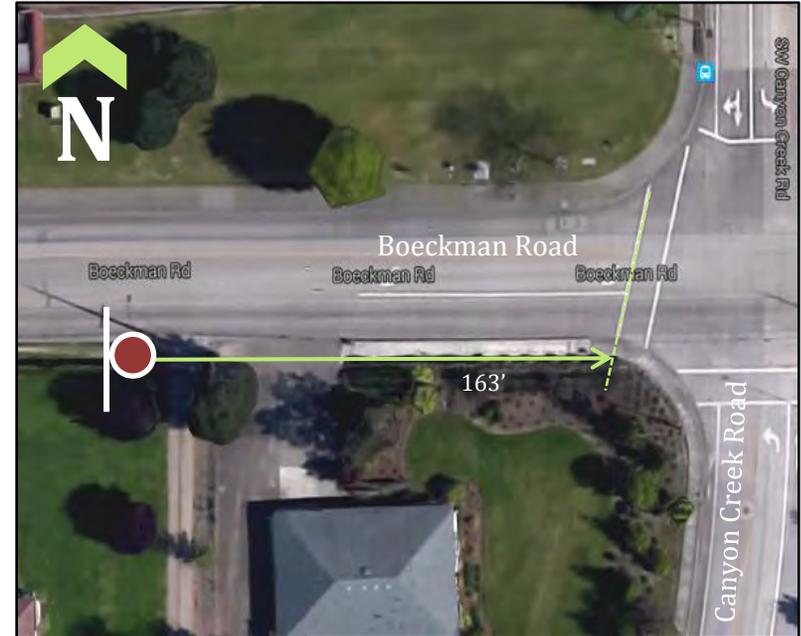


INTERSECTION 4c

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Canyon Creek Road & Boeckman Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	4c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boeckman Road
DISTANCE FROM INTERSECTION (Feet)	163
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the south side of Boeckman Road 163 feet west of Canyon Creek Road. Measured from western-most crosswalk bar across Boeckman Road.	
<i>Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations. Additional 63 feet to avoid driveway.</i>	
<i>Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 5A

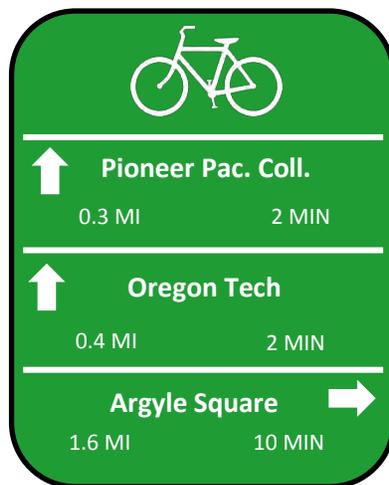
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Parkway Avenue & Boeckman Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	5a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Parkway Avenue
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES	
Place decision sign on the east side of Parkway Avenue 30 feet south of Boeckman Road. Measured from southern-most crosswalk bar across Parkway Avenue.	
<i>30 feet due to only straight or right turn needed to reach destinations.</i>	



Satellite Location View (Location to be verified by contractor)

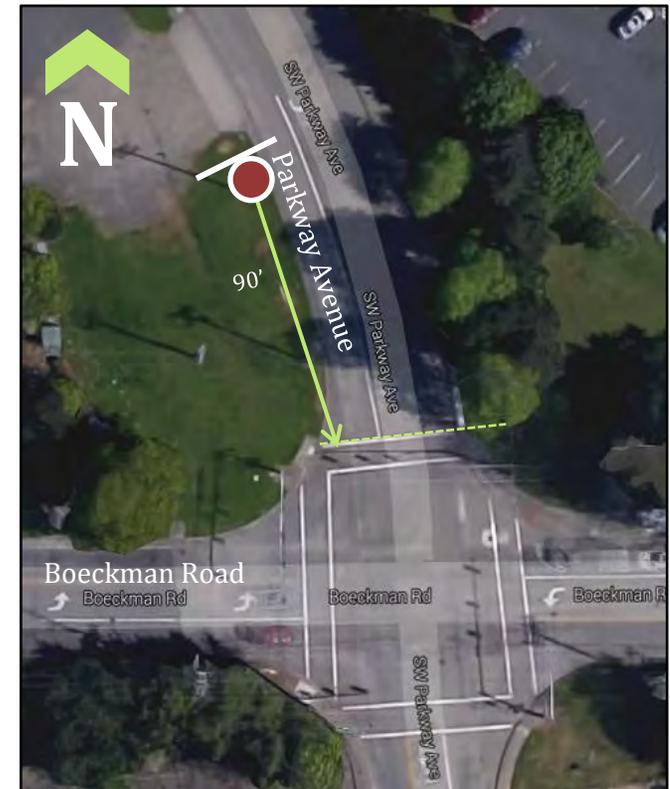


INTERSECTION 5B

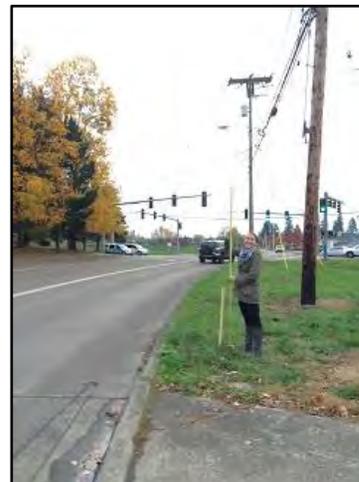
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Parkway Avenue & Boeckman Road
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	5b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Parkway Avenue
DISTANCE FROM INTERSECTION (Feet)	90
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the west side of Parkway Avenue 90 feet north of Boeckman Road. Measured from northern-most crosswalk bar across Parkway Avenue.	
<i>Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations. Move up 10 feet to accommodate using utility pole.</i>	
<i>Mount on utility pole.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 5c

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Parkway Avenue & Boeckman Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	5c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boeckman Road
DISTANCE FROM INTERSECTION (Feet)	100
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES

Place decision sign on the north side of Boeckman Road 100 feet east of Parkway Avenue. Measured from eastern-most crosswalk bar across Boeckman Road.

100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations.



Satellite Location View (location to be verified by contractor)

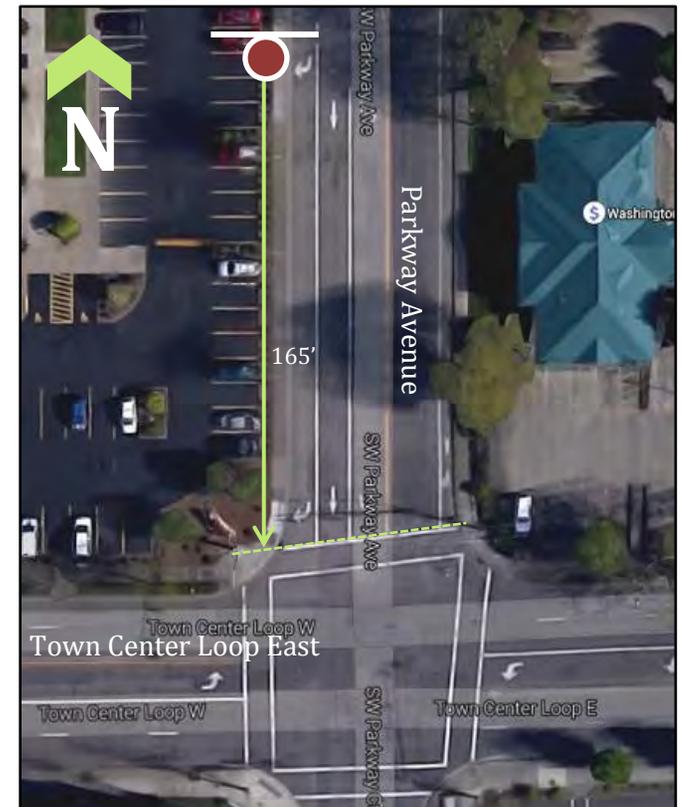
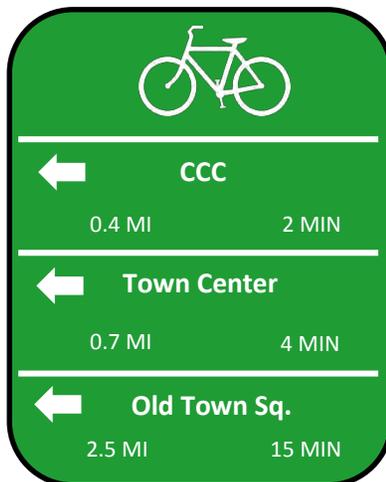


INTERSECTION 6A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Parkway Avenue & Town Center Loop East
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	6a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Parkway Avenue
DISTANCE FROM INTERSECTION (Feet)	165
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the west side of Parkway Avenue 165 feet north of Town Center Loop East. Measured from northern-most crosswalk bar across Parkway Avenue.	
<i>Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations. Additional 65 feet to mount on light pole. Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)

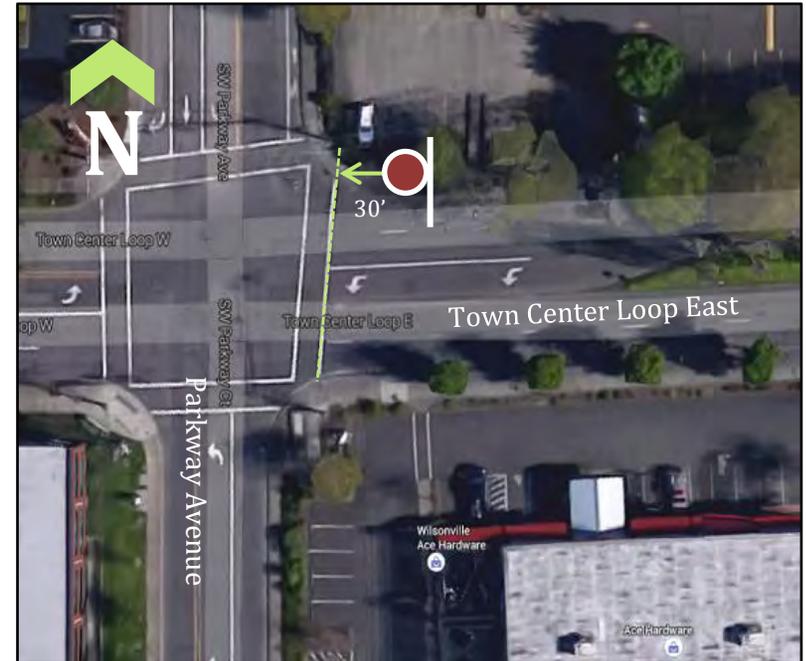


INTERSECTION 6B

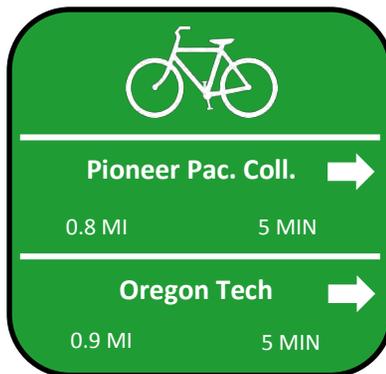
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Parkway Avenue & Town Center Loop East
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	6b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Town Center Loop East
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES	
Place decision sign on the north side of Town Center Loop East 30 feet east of Parkway Avenue. Measured from eastern-most crosswalk bar across Town Center Loop East.	
<i>30 feet due to only right turn needed to reach destinations.</i>	



Satellite Location View (Location to be verified by contractor)

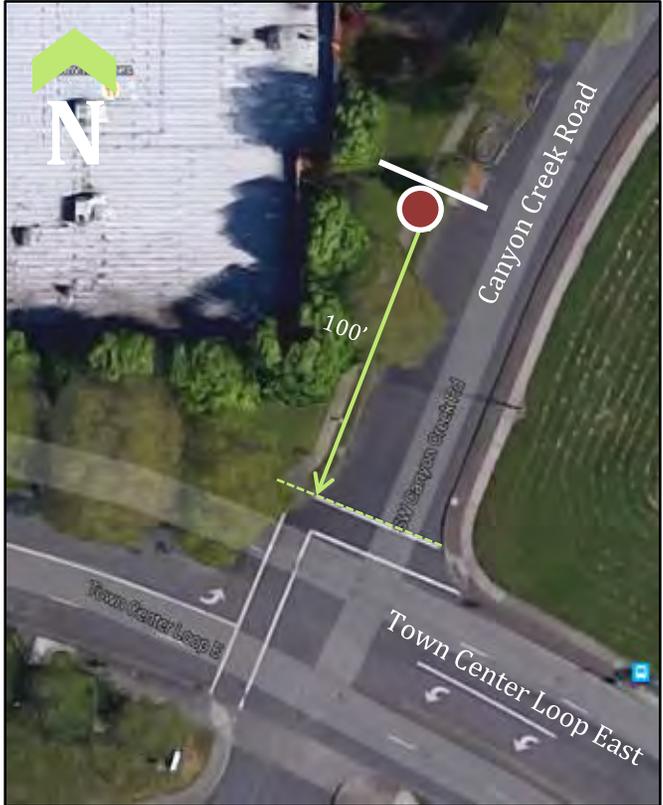


INTERSECTION 7A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Canyon Creek Road & Town Center Loop East
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	7a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Canyon Creek Road
DISTANCE FROM INTERSECTION (Feet)	100
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES	
Place decision sign on the west side of Canyon Creek Road 100 feet north of Town Center Loop East. Measured from northern-most crosswalk bar across Canyon Creek Road	
<i>100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)

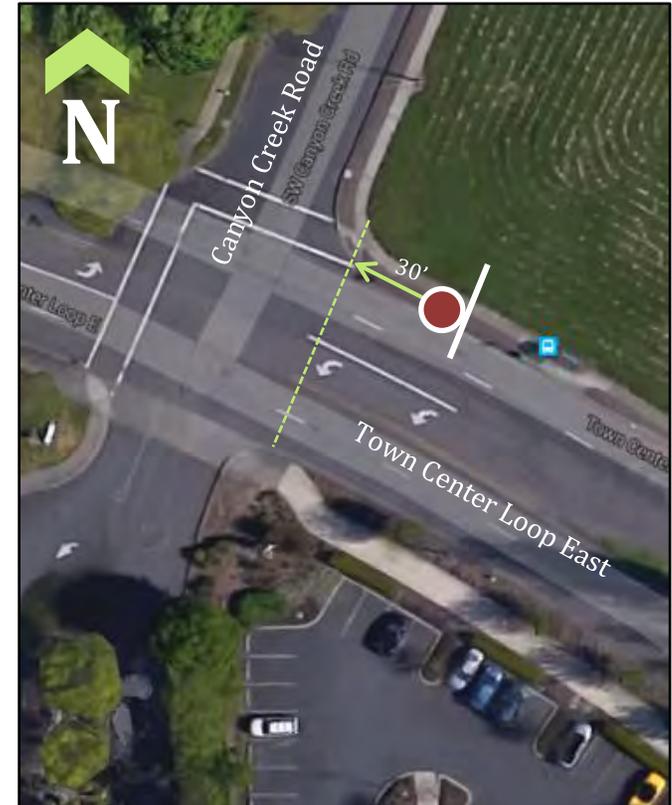


INTERSECTION 7B

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Canyon Creek Road & Town Center Loop East
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	7b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Town Center Loop East
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES	
Place decision sign on the north side of Town Center Loop East 30 feet east of Canyon Creek Road. Measured from where pavement markings end on Town Center Loop East.	
<i>30 feet due to only straight or right turn needed to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)

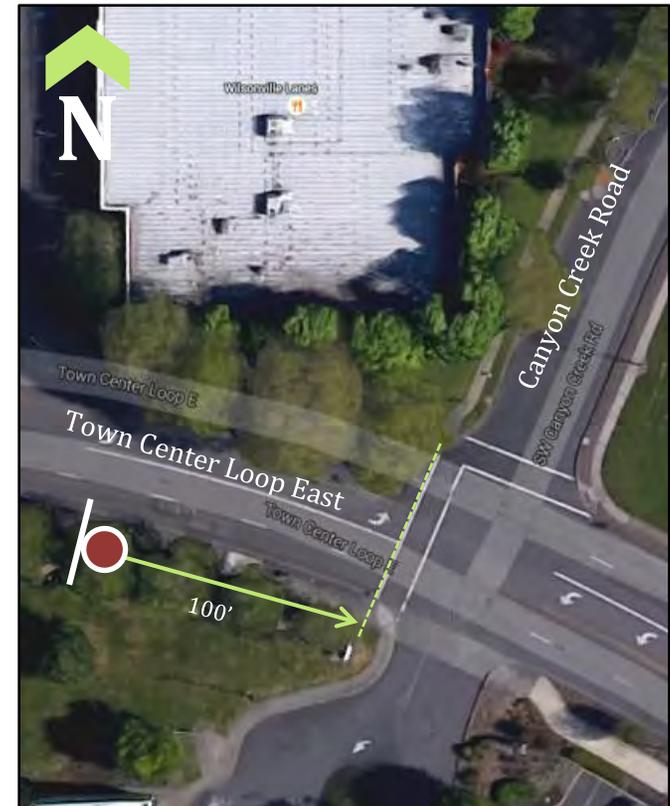


INTERSECTION 7C

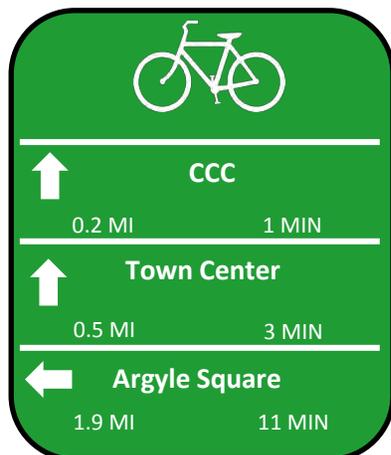
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Canyon Creek Road & Town Center Loop East
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	7c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Town Center Loop East
DISTANCE FROM INTERSECTION (Feet)	100
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES	
Place decision sign on the south side of Town Center Loop East 100 feet west of Canyon Creek Road. Measured from the western-most crosswalk bar across Town Center Loop East.	
<i>100 feet as cyclists will have to maneuver across a lane of traffic in order to turn left to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 8A

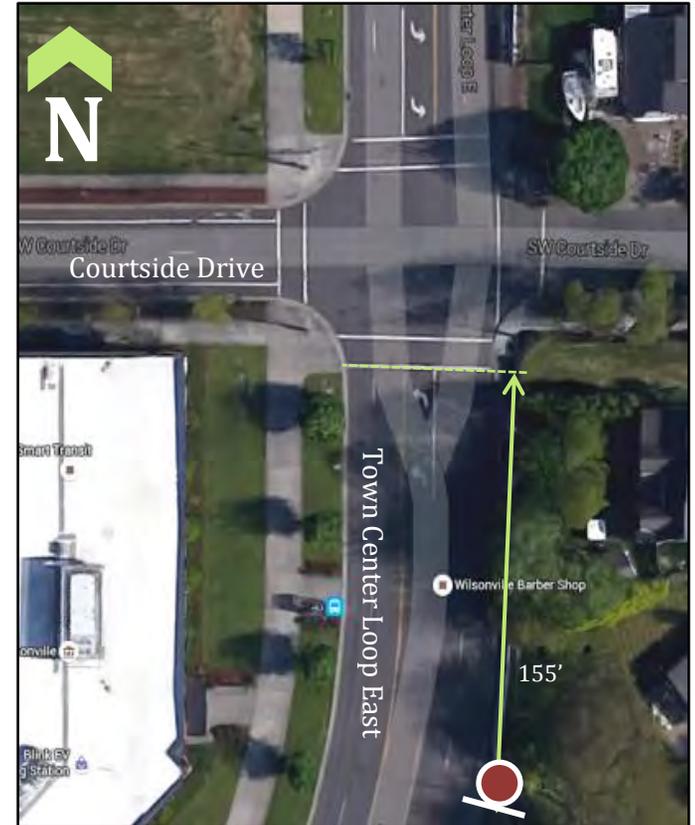
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Town Center Loop East & Courthouse Drive
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	8a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Town Center Loop East
DISTANCE FROM INTERSECTION (Feet)	155
LATERAL DISTANCE (Feet)	-

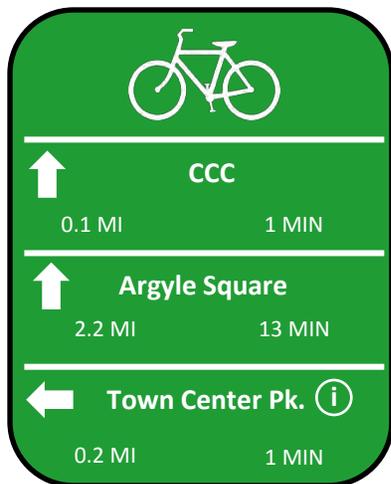
NOTES

Place decision sign on the east side of Town Center Loop East 155 feet south of Courthouse Drive. Measured from southern-most crosswalk bar across Town Center Loop East.

Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations. Additional 55 feet to avoid conflict with fencing. Mount on light pole.



Satellite Location View (Location to be verified by contractor)



INTERSECTION 8B

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Town Center Loop East & Courthouse Drive
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	8b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Town Center Loop East
DISTANCE FROM INTERSECTION (Feet)	90
LATERAL DISTANCE (Feet)	-

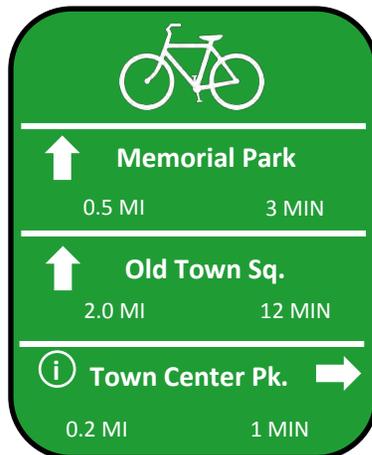
NOTES

Place decision sign on the west side of Town Center Loop East 90 feet north of Courthouse Drive. Measured from northern-most crosswalk bar across Town Center Loop East.

Initial 30 feet due to only straight or right turn needed to reach destinations. Additional 60 feet to accommodate using light pole. Mount on light pole.



Satellite Location View (location to be verified by contractor)



INTERSECTION 8C

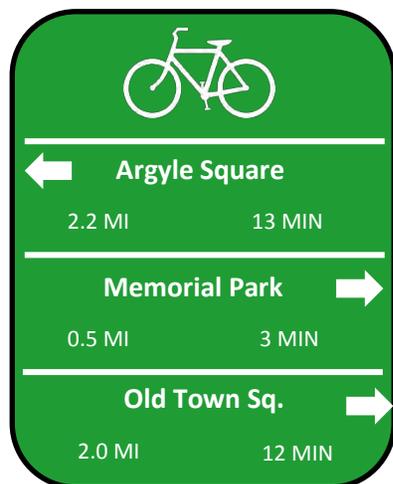
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Town Center Loop East & Courtside Drive
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	8c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Courtside Drive
DISTANCE FROM INTERSECTION (Feet)	40
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES	
Place decision sign on the south side of Courtside Drive 40 feet west of Town Center Loop East. Measured from western-most crosswalk bar across Courtside Drive.	
<i>Initial 30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations. Additional 10 feet to avoid damage to tree roots.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 9A

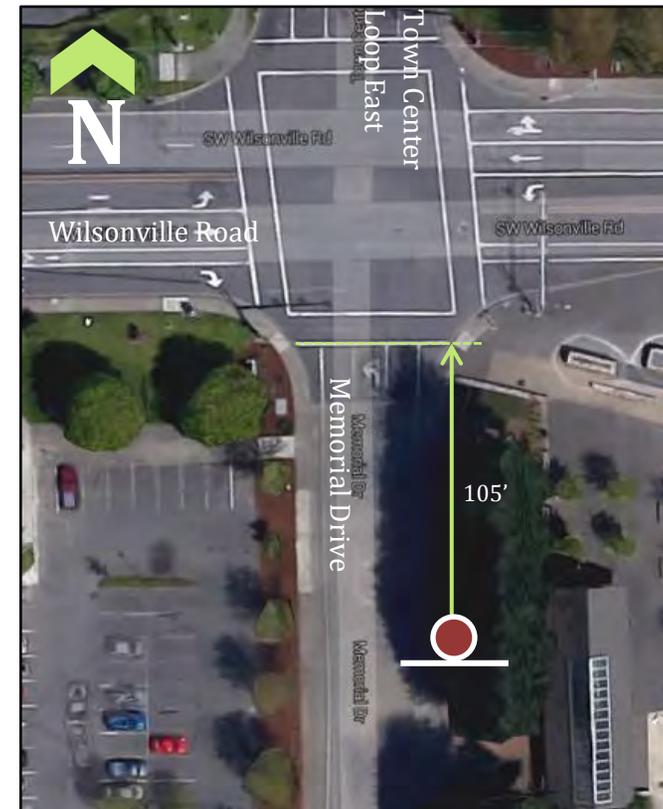
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Town Center Loop East & Wilsonville Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	9a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Memorial Drive
DISTANCE FROM INTERSECTION (Feet)	105
LATERAL DISTANCE (Feet)	-

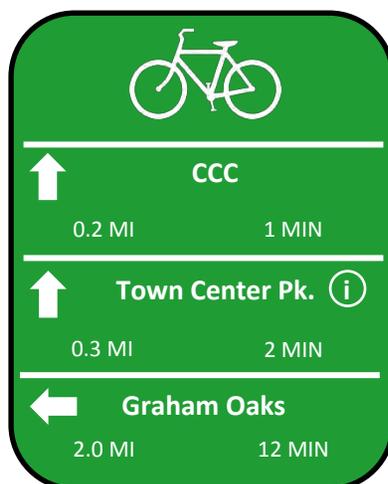
NOTES

Place decision sign on the east side of Memorial Drive 105 feet south of Wilsonville Road. Measured from southern-most crosswalk bar across Memorial Drive.

Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations. Additional 5 feet in order to use light pole. Mount on light pole.



Satellite Location View (location to be verified by contractor)

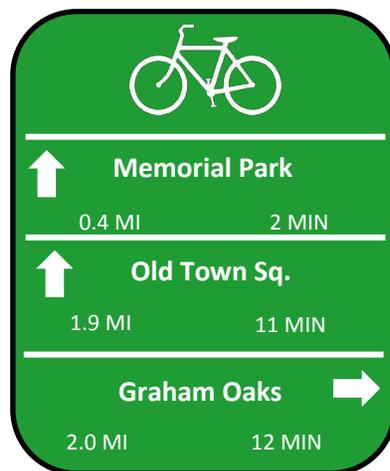


INTERSECTION 9B

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Town Center Loop East & Wilsonville Road
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	9b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Town Center Loop East
DISTANCE FROM INTERSECTION (Feet)	193
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES	
Place decision sign on the west side of Town Center Loop East 193 feet north of Wilsonville Road. Measured from northern-most crosswalk bar across Town Center Loop East	
<i>Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations. Additional 93 feet to accommodate transition in green bike transition box.</i>	



Satellite Location View (location to be verified by contractor)

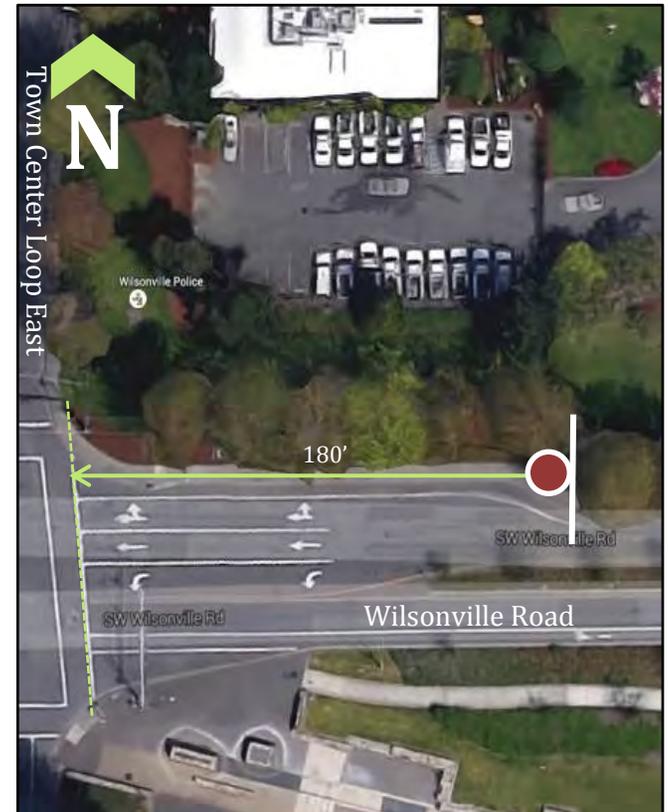


INTERSECTION 9c

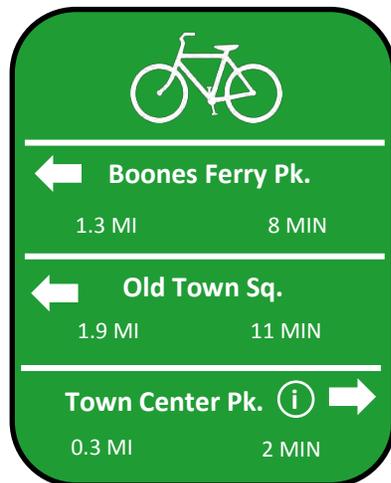
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Town Center Loop East & Wilsonville Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	9c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	180
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the north side of Wilsonville Road 180 feet east of Town Center Loop East. Measured from eastern-most crosswalk bar across Wilsonville Road.	
<i>200 feet due to the need to maneuver across two lanes of traffic in order to turn left to reach destinations. Moved up 20 feet in order to mount to light pole. Mount on light pole.</i>	



Satellite Location View (Location to be verified by contractor)

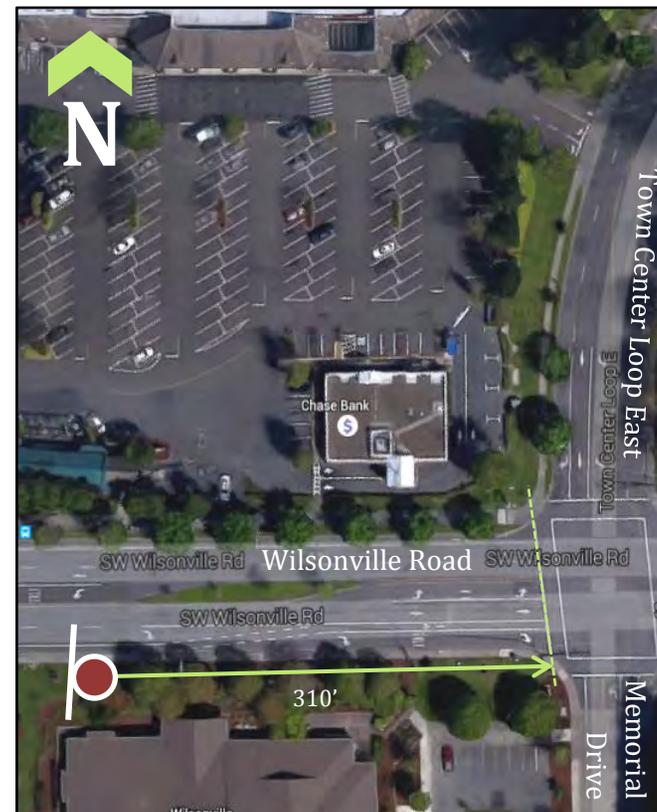


INTERSECTION 9D

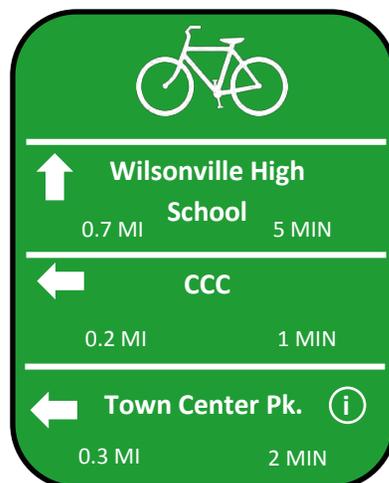
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Town Center Loop East & Wilsonville Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	9d

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	310
LATERAL DISTANCE (Feet)	3 (<i>Edge of Sidewalk</i>)

NOTES	
Place decision sign on the south side of Wilsonville Road 310 feet west of Memorial Drive. Measured from western-most crosswalk bar across Wilsonville Road.	
<i>Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations. Additional 210 feet to place sign before bike transition.</i>	



Satellite Location View (Location to be verified by contractor)



INTERSECTION 10A

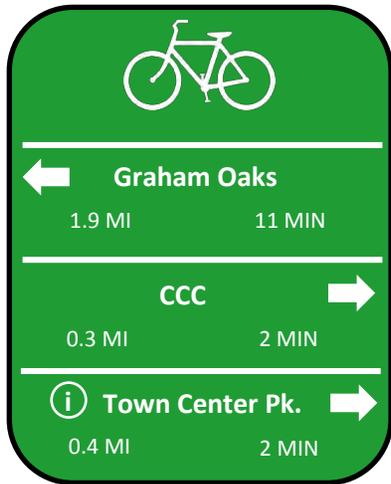
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Rebekah Street
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	10a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Rebekah Street
DISTANCE FROM INTERSECTION (Feet)	75
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES	
Place decision sign on the east side of Rebekah Street 75 feet south of Wilsonville Road. Measured from southern-most crosswalk bar across Rebekah Street.	
<i>100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations. Moved up 15 feet in order to place sign in the planting strip.</i>	



Satellite Location View (Location to be verified by contractor)



INTERSECTION 11A

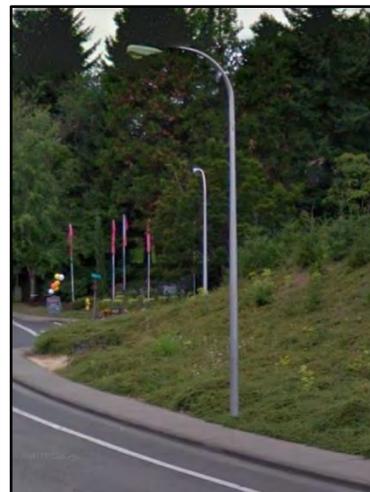
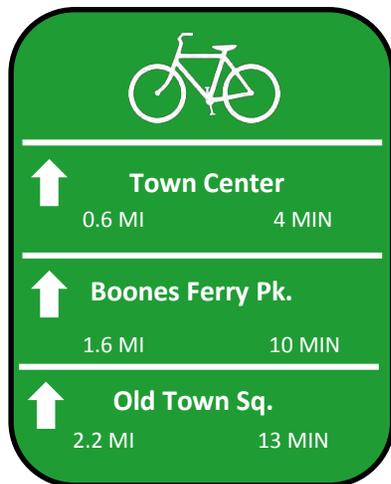
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Rose Lane
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	11a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	102
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the north side of Wilsonville Road 102 feet east of Rose Lane. Measured from where pavement markings end on Wilsonville Road.	
<p><i>No set distance due to decision sign acting as a confirmation sign.</i></p> <p><i>Mount on light pole.</i></p>	



Satellite Location View (Location to be verified by contractor)

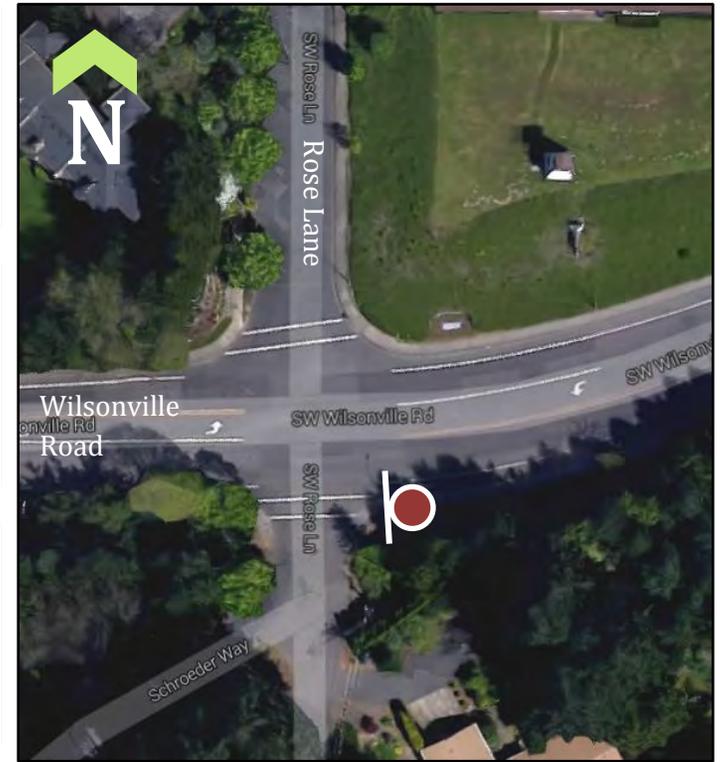


INTERSECTION 11B

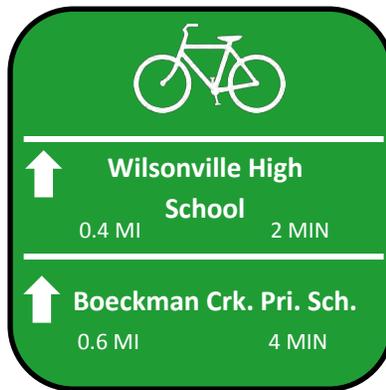
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Rose Lane
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	11b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	-
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the south side of Wilsonville Road at the intersection of Wilsonville Road and Rose Lane.	
<p><i>No set distance due to decision sign acting as a confirmation sign.</i></p> <p><i>Mount on light pole.</i></p>	



Satellite Location View (Location to be verified by contractor)

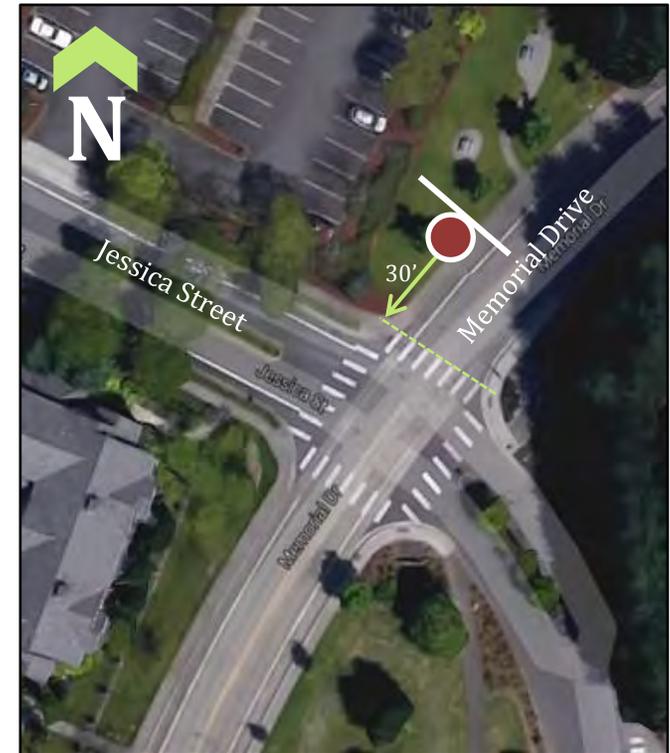


INTERSECTION 12A

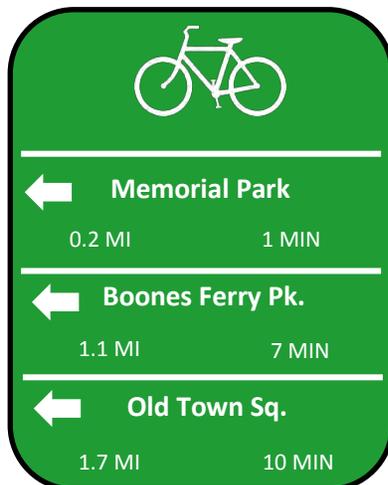
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Memorial Drive & Jessica Street
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	12a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Memorial Drive
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Edge of Sidewalk</i>)

NOTES	
Place decision sign on the west side of Memorial Drive 30 feet north of Jessica Street. Measured from northern-most edge of crosswalk width across Memorial Drive.	
<i>30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations.</i>	



Satellite Location View (Location to be verified by contractor)



INTERSECTION 12B

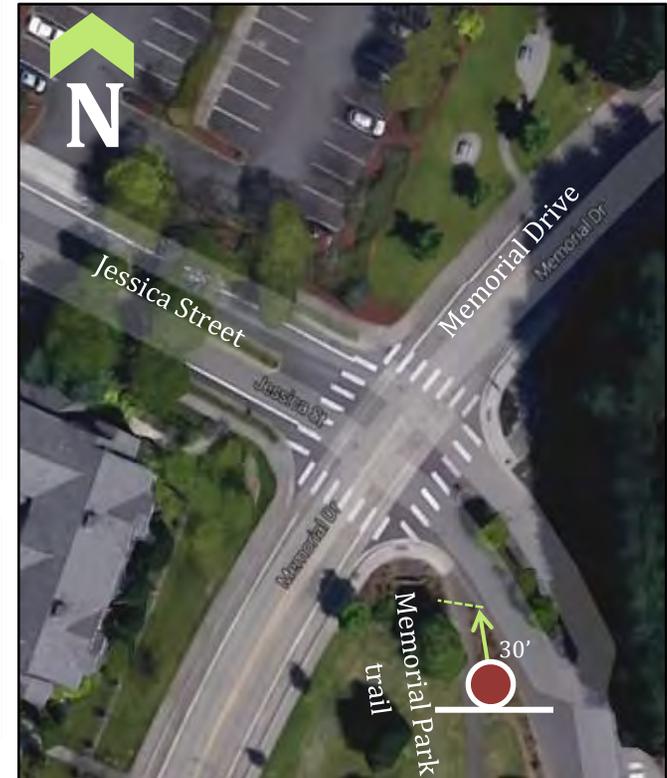
INFORMATION	
SIGN TYPE	On-Trail Decision
INTERSECTION	Memorial Drive & Jessica Street
DIRECTION OF TRAVEL	Northwest Bound
SIGN NUMBER	12b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Memorial Park trail
DISTANCE FROM TRAIL START (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Edge of Pavement</i>)

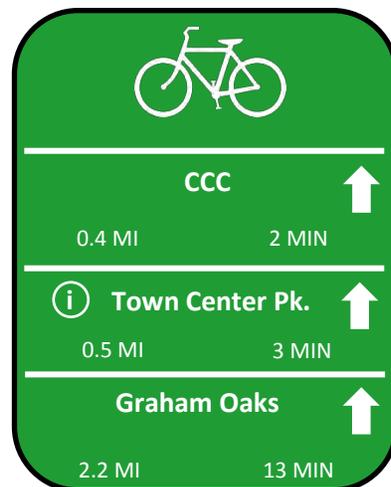
NOTES

Place decision sign on the northeast side of Memorial Park trail 30 feet southeast of Jessica Street. Measured from point of transition from trail to sidewalk.

30 feet as cyclists will be continuing straight only.



Satellite Location View (location to be verified by contractor)

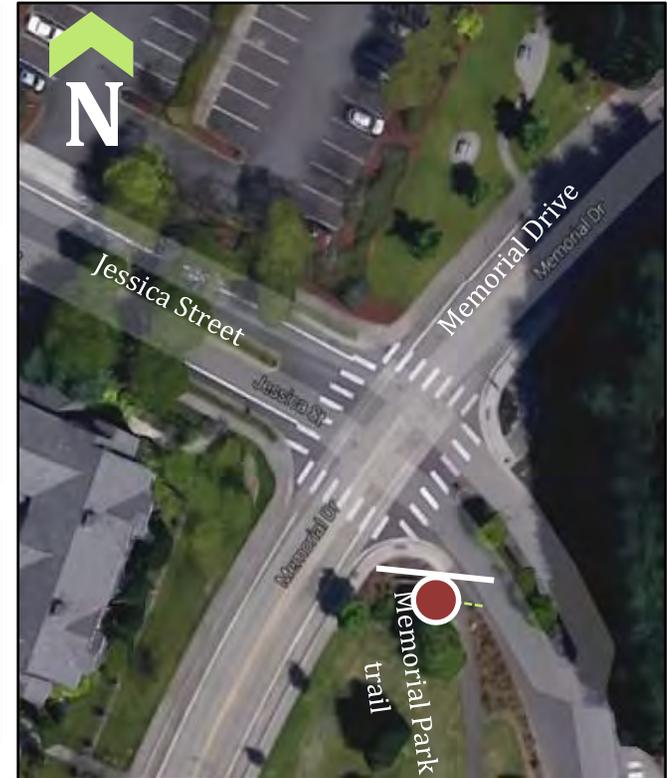


INTERSECTION 12c

INFORMATION	
SIGN TYPE	On-Trail Bike Route
INTERSECTION	Memorial Drive & Jessica Street
DIRECTION OF TRAVEL	Southeast Bound
SIGN NUMBER	12c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Memorial Park trail
DISTANCE FROM TRAIL START (Feet)	0
LATERAL DISTANCE (Feet)	2 (<i>Edge of Pavement</i>)

NOTES
Place bike route sign on the west side of Memorial Park trail at the start of the trail. Bike route sign should be in view for bicyclists traveling southbound on Wilsonville Road.

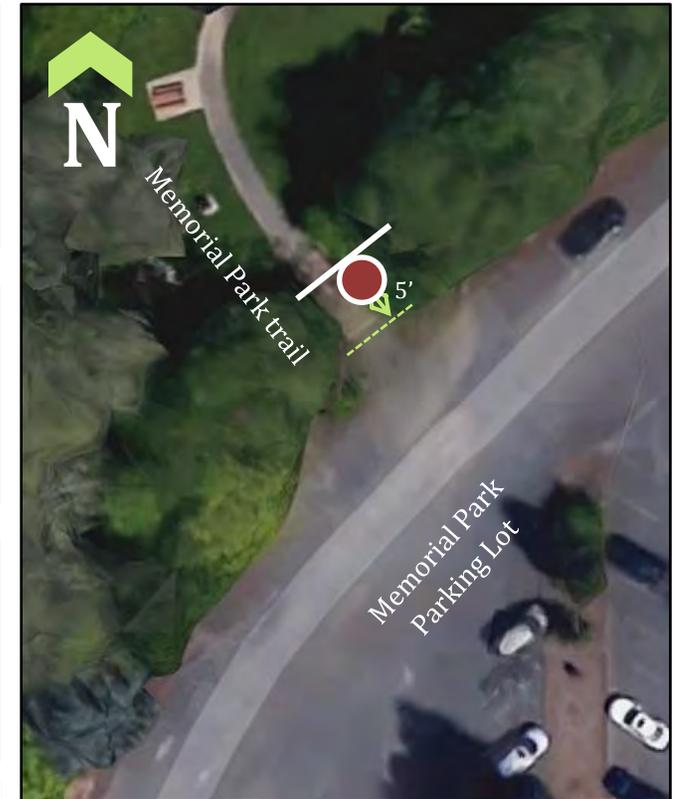


INTERSECTION 13A

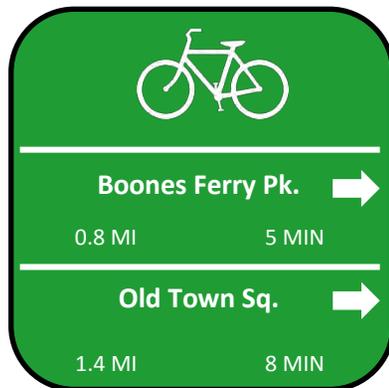
INFORMATION	
SIGN TYPE	On-Trail Decision
INTERSECTION	Memorial Park trail & Memorial Park Parking Lot
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	13a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of the Memorial Park trail
DISTANCE FROM INTERSECTION (Feet)	5
LATERAL DISTANCE (Feet)	2 (<i>Edge of Pavement</i>)

NOTES
Place decision sign on the east side of the Memorial Park trail 5 feet north of the Memorial Park Parking Lot. Measured from edge of parking lot pavement.



Satellite Location View (location to be verified by contractor)

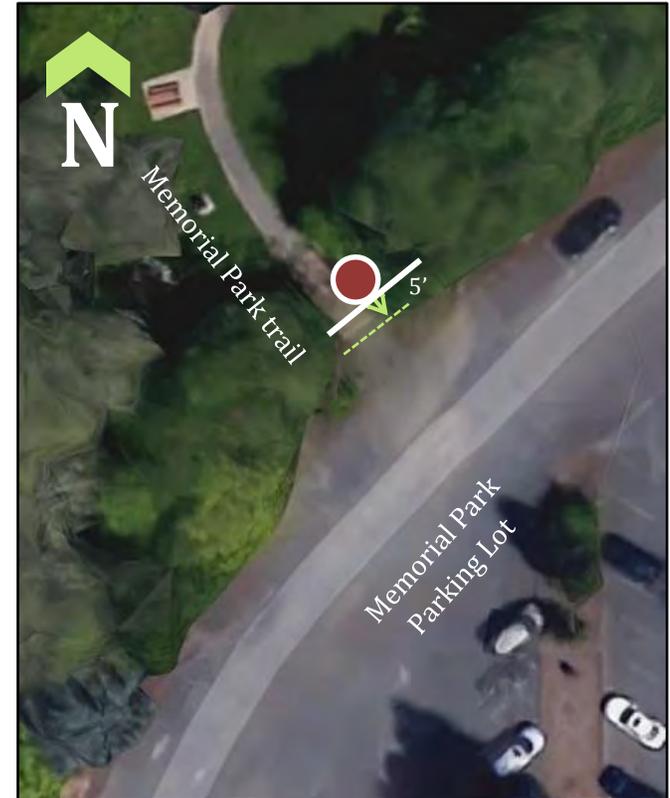


INTERSECTION 13B

INFORMATION	
SIGN TYPE	On-Trail Bike Route
INTERSECTION	Memorial Park trail & Memorial Park Parking Lot
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	13b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of the Memorial Park trail
DISTANCE FROM INTERSECTION (Feet)	5
LATERAL DISTANCE (Feet)	2 (<i>Edge of Pavement</i>)

NOTES
Place bike route sign on the east side of the Memorial Park trail 5 feet north of the Memorial Park Parking Lot. Measure from edge of parking lot pavement.
<i>Mount on opposite side of the pole used for sign 13a.</i>



Satellite Location View (location to be verified by contractor)

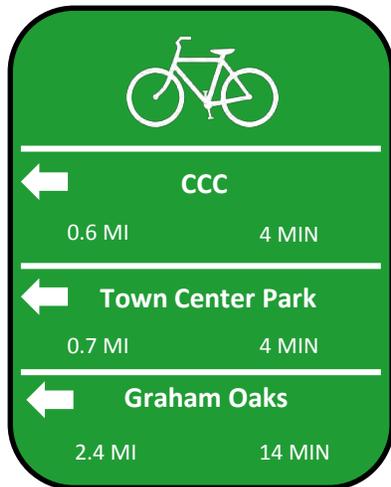
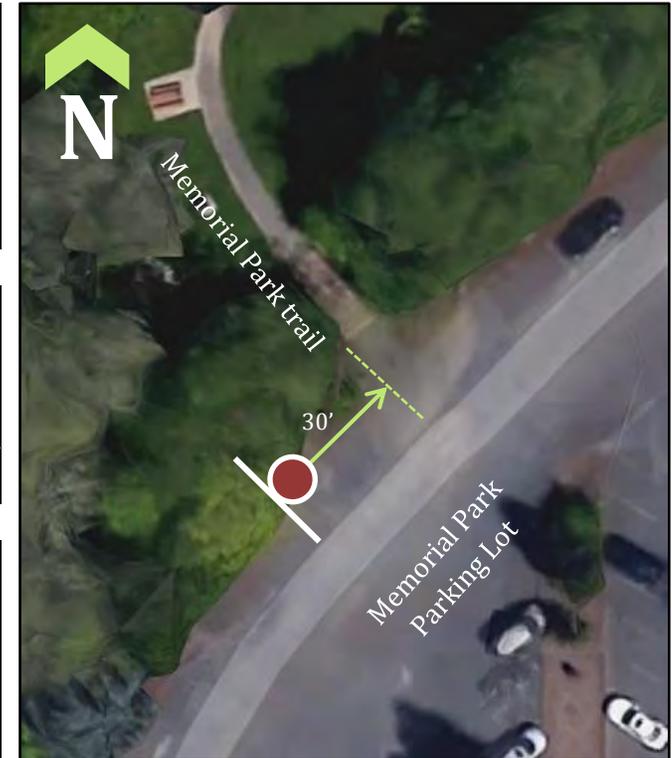


INTERSECTION 13C

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Memorial Park trail & Memorial Park Parking Lot
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	13c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of the Memorial Park Parking Lot
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the north side of the Memorial Park Parking Lot 30 feet west of the Memorial Park trail. Measured from edge of Memorial Park Trail.	
<i>Mount on light pole.</i>	

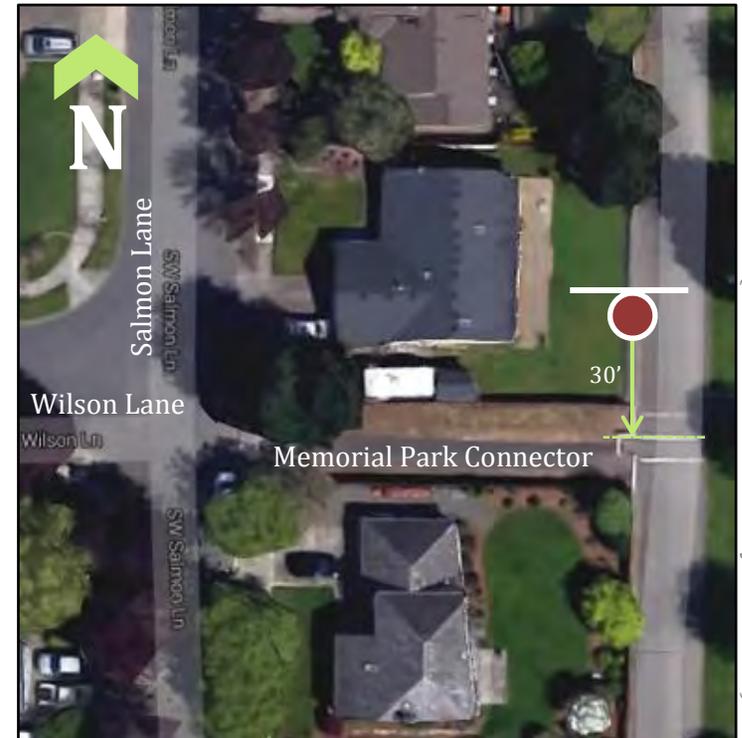


INTERSECTION 14A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Salmon Lane & Wilson Lane
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	14a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of the Memorial Park access road
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	2 (Edge of Sidewalk)

NOTES	
Place decision sign on the west side of the Memorial Park Parking Lot 30 feet north of the Memorial Park Connector. Measure from northern edged of the crosswalk width across the Memorial Park access road.	



Satellite Location View (Location to be verified by contractor)

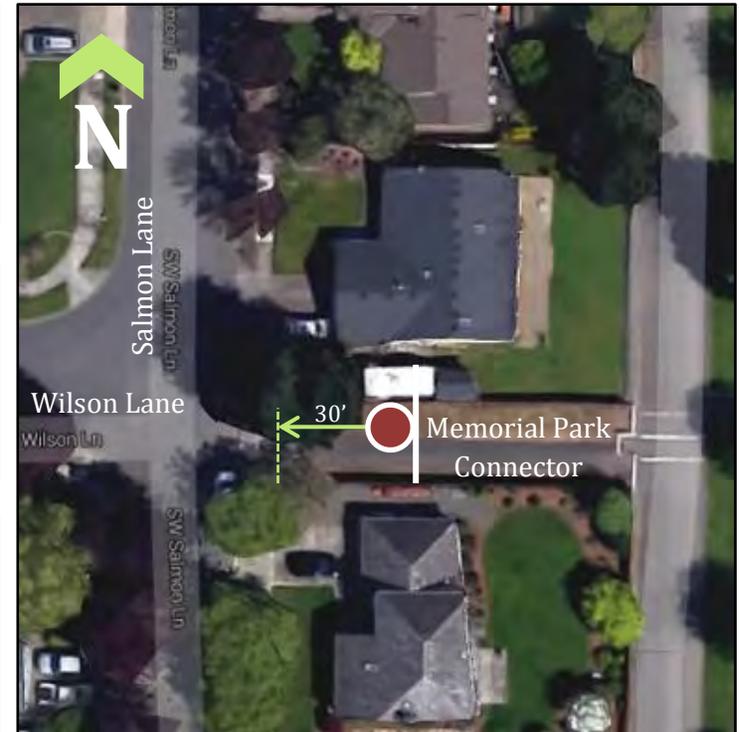


INTERSECTION 14B

INFORMATION	
SIGN TYPE	On-Trail Decision
INTERSECTION	Salmon Lane & Wilson Lane
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	14b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of the Memorial Park Connector
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (Edge of Pavement)

NOTES	
Place decision sign on the north side of the Memorial Park Connector 30 feet east of Salmon Lane. Measured from eastern-most edge of Salmon Lane sidewalk.	



Satellite Location View (location to be verified by contractor)

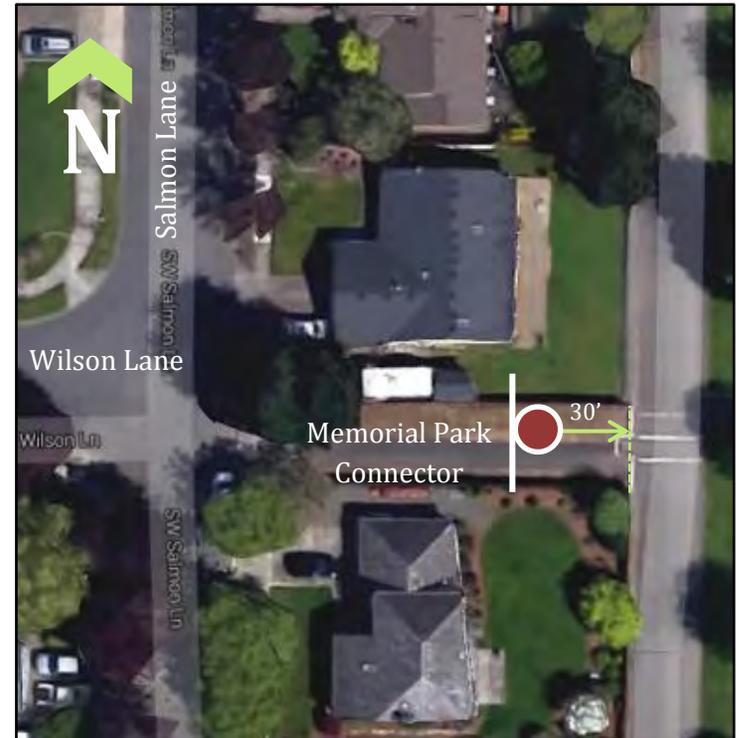


INTERSECTION 14C

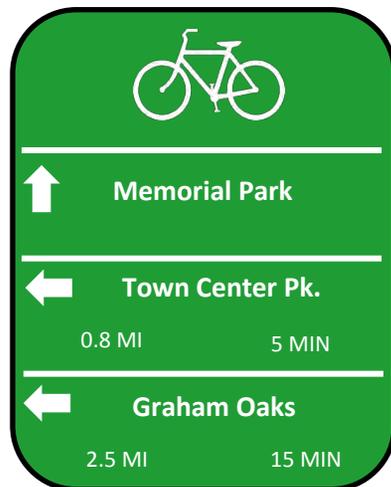
INFORMATION	
SIGN TYPE	On-Trail Decision
INTERSECTION	Salmon Lane & Wilson Lane
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	14c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of the Memorial Park Connector
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Edge of Pavement</i>)

NOTES	
Place decision sign on the north side of the Memorial Park Connector 30 feet west of the Memorial Park Parking Lot. Measured from western edge of Memorial Park access road.	



Satellite Location View (location to be verified by contractor)



INTERSECTION 14D

INFORMATION	
SIGN TYPE	On-Street Bike Route
INTERSECTION	Salmon Lane & Wilson Lane
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	14d

INSTALLATION	
ORIENTATION	Sign face is parallel to the edge of Salmon Lane
DISTANCE FROM INTERSECTION (Feet)	5
LATERAL DISTANCE (Feet)	-

NOTES	
Place bike route sign on the east side of Salmon Lane 5 feet south of the Memorial Park Connector. Measure from southern edge of the Memorial Park Connector.	
<i>Mount on light pole.</i>	



Satellite Location View (Location to be verified by contractor)

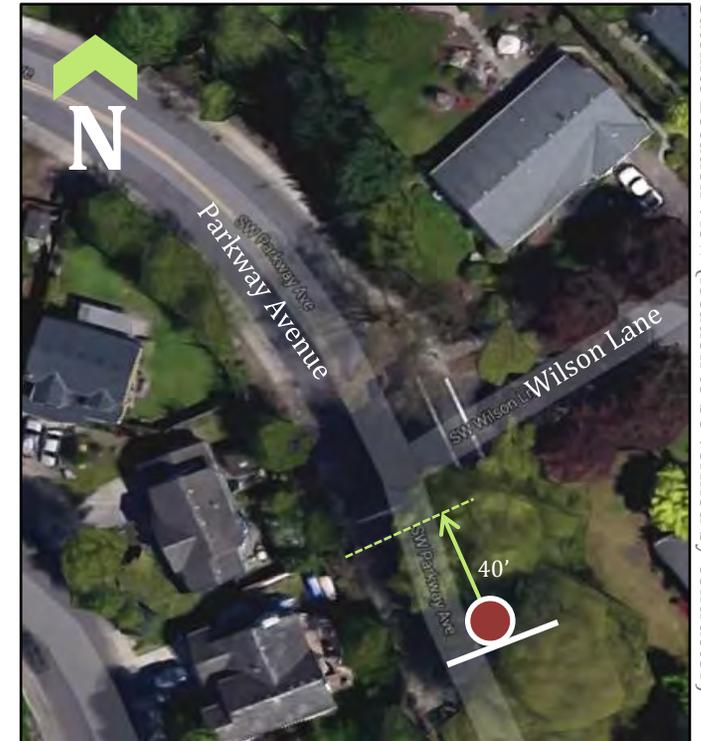


INTERSECTION 15A

INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Parkway Avenue & Wilson Lane
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	15a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Parkway Avenue
DISTANCE FROM INTERSECTION (Feet)	40
LATERAL DISTANCE (Feet)	2 (Edge of Sidewalk)

NOTES	
Place turn sign on the east side of Parkway Avenue 40 feet south of Wilson Lane. Measured from southern-most crosswalk bar across Parkway Avenue.	
<i>Initial 30 feet due to only right turn needed to reach destinations. Additional 10 feet to avoid tree.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 15B

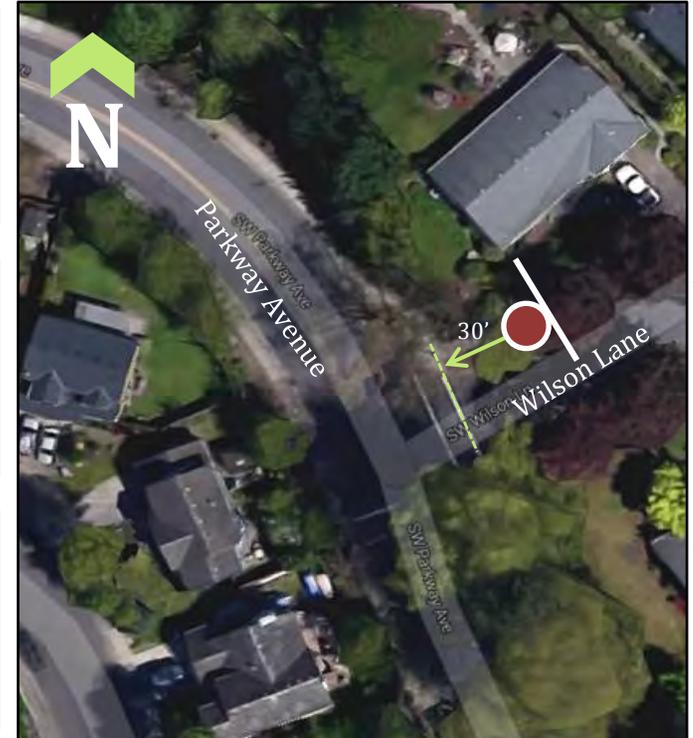
INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Parkway Avenue & Wilson Lane
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	15b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilson Lane
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES

Place turn sign on the north side of Wilson Lane 30 feet east of Parkway Avenue. Measured from eastern-most crosswalk bar across Wilson Lane.

30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations.



Satellite Location View (location to be verified by contractor)

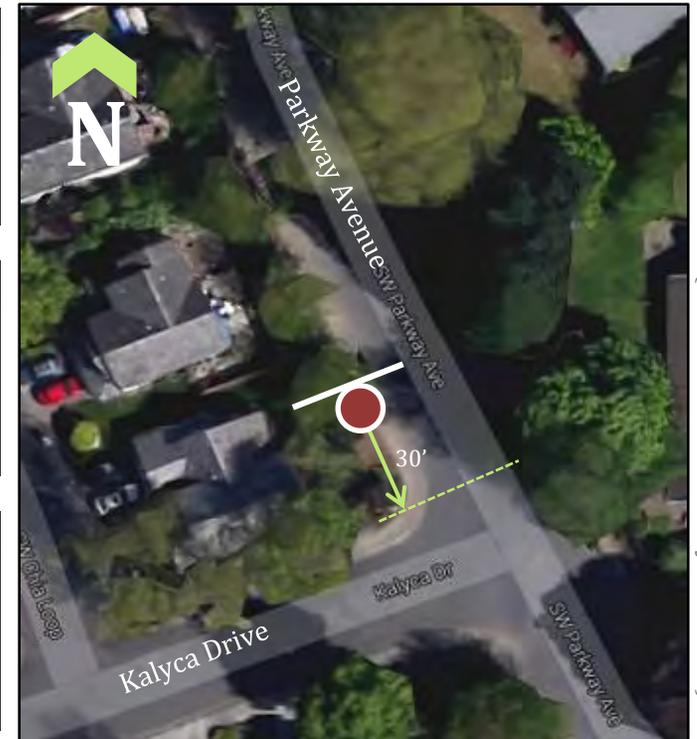


INTERSECTION 16A

INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Parkway Avenue & Kalyca Drive
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	16a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Parkway Avenue
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Edge of Sidewalk</i>)

NOTES	
Place turn sign on the west side of Parkway Avenue 30 feet north of Kalyca Drive. Measured from northern edge of sidewalk along Kalyca Drive.	
<i>30 feet due to only right turn needed to reach destinations</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 16B

INFORMATION

SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Parkway Avenue & Kalyca Drive
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	16b

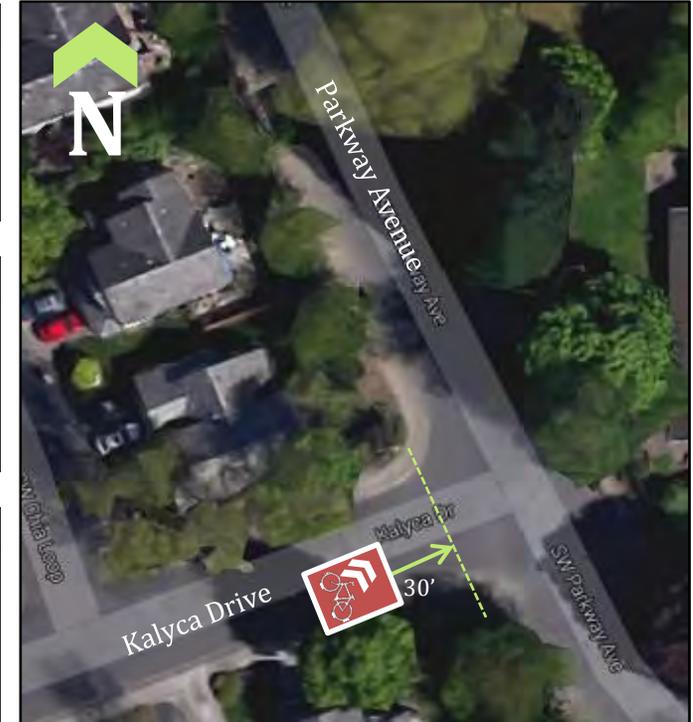
INSTALLATION

ORIENTATION	Arrows point northeast
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES

Place pavement marking on the southern half of Kalyca Drive 30 feet west of Parkway Avenue. Measured from western edge of sidewalk along Parkway Avenue.

30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations.



Satellite Location View (location to be verified by contractor)

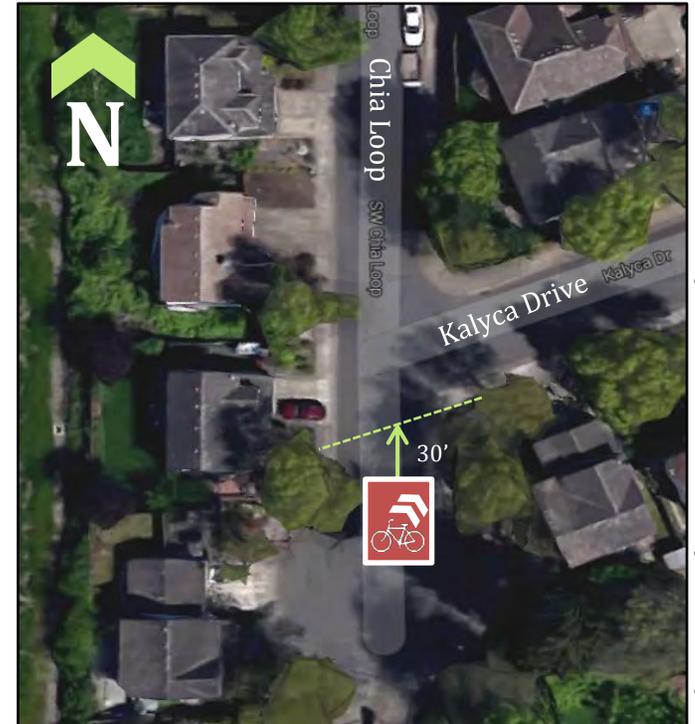


INTERSECTION 17A

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Chia Loop & Kalyca Drive
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	17a

INSTALLATION	
ORIENTATION	Arrows point northeast
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES	
Place pavement marking on the eastern half of Chia Loop 30 feet south of Kalyca Drive. Measured from back edge of sidewalk along Kalyca Drive.	
<i>30 feet due to only right turn needed to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)

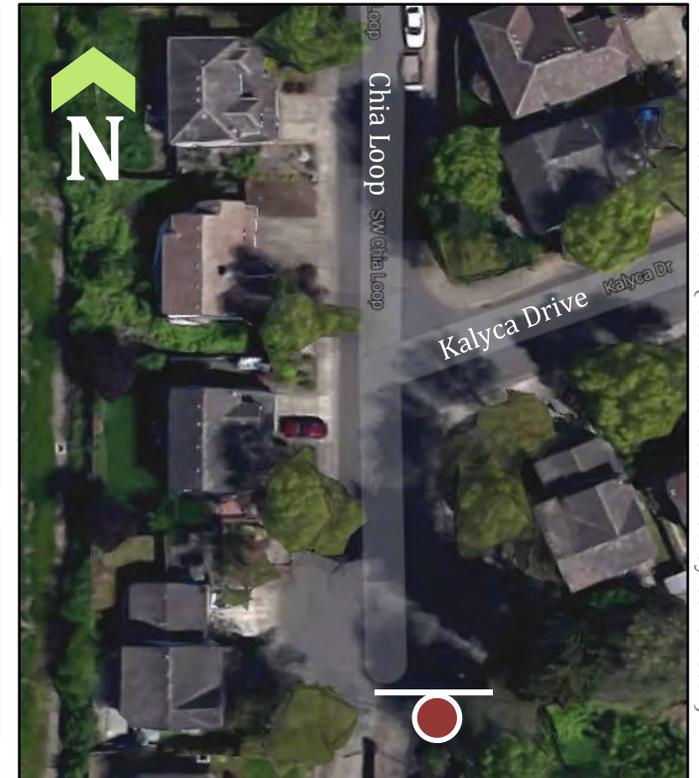


INTERSECTION 17B

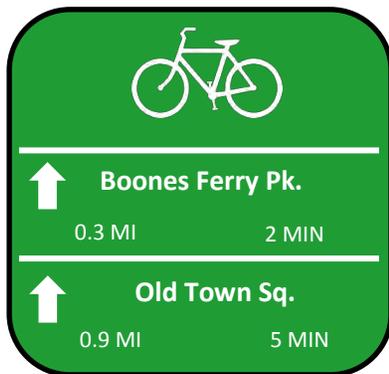
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Chia Loop & Kalyca Drive
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	17b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Chia Loop
DISTANCE FROM INTERSECTION (Feet)	At Trail Head
LATERAL DISTANCE (Feet)	3 (Edge of Memorial Park - Boones Ferry Park Trail)

NOTES	
Place confirmation sign that the end of Chia Loop on the east side of the trail head where existing "Memorial Park - Boones Ferry Park" sign is located.	



Satellite Location View (location to be verified by contractor)



INTERSECTION 17c

INFORMATION

SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Chia Loop & Kalyca Drive
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	17c

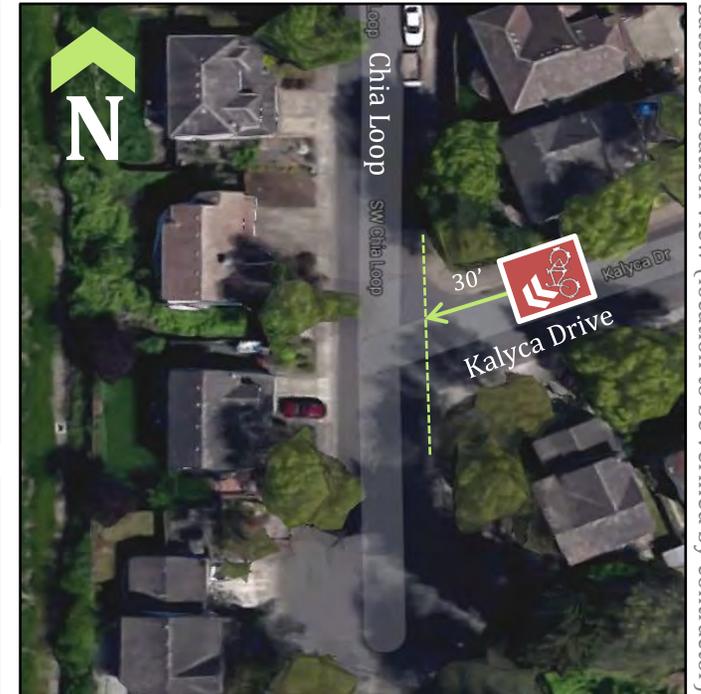
INSTALLATION

ORIENTATION	Arrows point southwest
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	4(On Street, from Edge of Curb)

NOTES

Place pavement marking on the northern half of Kalyca Drive 30 feet east of Chia Loop. Measured from front edge of curb along Chia Loop.

30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations.

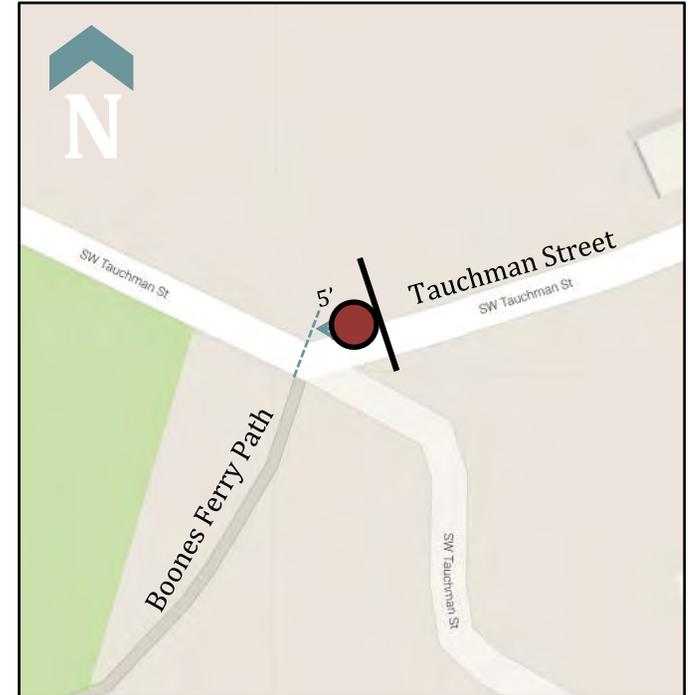


Satellite Location View (Location to be verified by contractor)

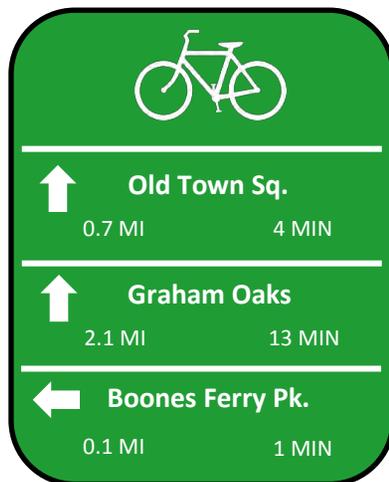


INTERSECTION 18A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Tauchman Street & Boones Ferry Path
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	18a
INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Tauchman Street
DISTANCE FROM INTERSECTION (Feet)	5
LATERAL DISTANCE (Feet)	3 (Edge of Pavement)
NOTES	
Place decision sign on the north side of Tauchman Street 5 feet east of the Boones Ferry Path. Measured from western edge of Boones Ferry Path.	



Satellite Location View (location to be verified by contractor)

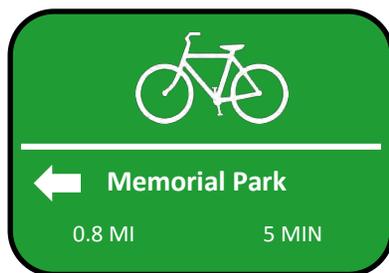


INTERSECTION 18B

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Tauchman Street & Boones Ferry Path
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	18b
INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Tauchman Street
DISTANCE FROM INTERSECTION (Feet)	5
LATERAL DISTANCE (Feet)	3 (Edge of Pavement)
NOTES	
Place decision sign on the south side of Tauchman Street 5 feet west of the Boones Ferry Path. Measured from western edge of Boones Ferry Path.	



Satellite Location View (location to be verified by contractor)

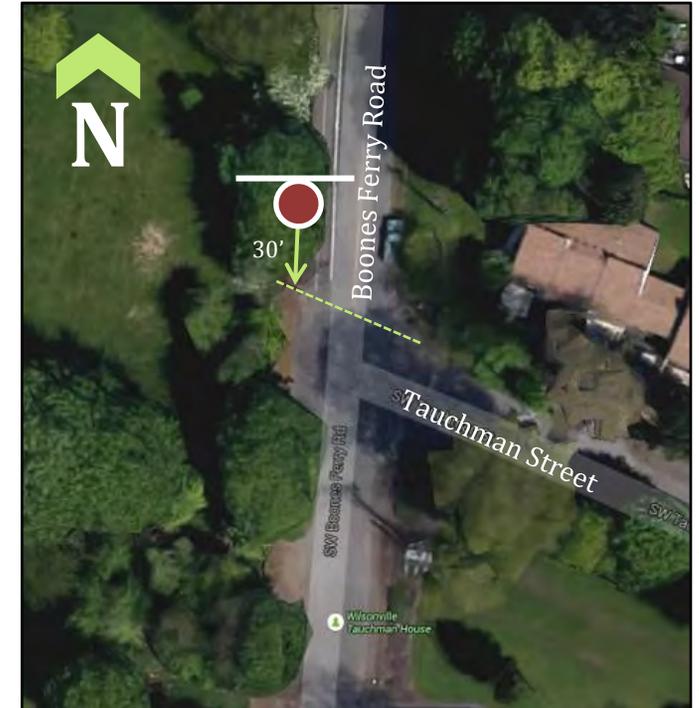


INTERSECTION 19A

INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Tauchman Street & Boones Ferry Road
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	19a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boones Ferry Road
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (Edge of Pavement)

NOTES
Place turn sign on the west side of Boones Ferry Road 30 feet north of Tauchman Street. Measured from northern edge of Tauchman Street.
<i>30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations.</i>



Satellite Location View (location to be verified by contractor)

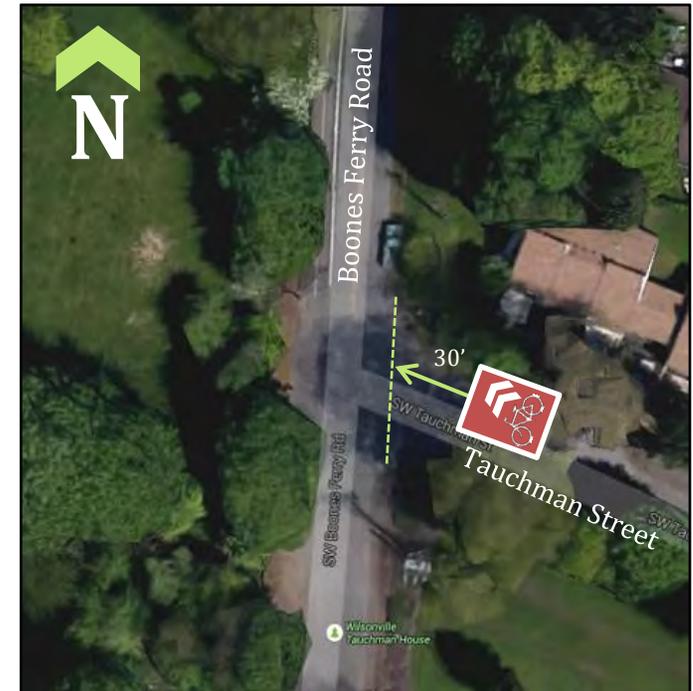


INTERSECTION 19B

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Tauchman Street & Boones Ferry Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	19b

INSTALLATION	
ORIENTATION	Arrows point northwest
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES	
Place pavement marking on the northern half of Tauchman Street 30 feet east of Boones Ferry Road. Measured from eastern edge of Boones ferry Road.	
<i>30 feet due to only right turn needed to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 20A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Boones Ferry Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	20a
INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boones Ferry Road
DISTANCE FROM INTERSECTION (Feet)	100
LATERAL DISTANCE (Feet)	3 (Back of Curb)
NOTES	
Place decision sign on the east side of Boones Ferry Road 100 feet south of Wilsonville Road. Measured from southern most edge of crosswalk width across Boones Ferry Road.	
<i>100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations.</i>	



Satellite Location View (Location to be verified by contractor)

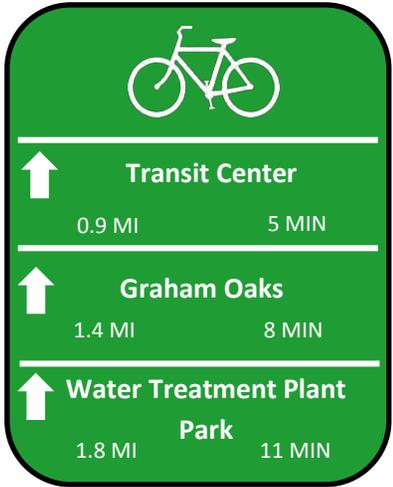


INTERSECTION 20B

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Boones Ferry Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	20b
INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	250
LATERAL DISTANCE (Feet)	-
NOTES	
Place decision sign on the north side of Wilsonville Road 250 feet east of Boones Ferry Road. Measured from eastern-most crosswalk bar across Wilsonville Road.	
<i>Initial 30 feet due to cyclists continuing straight in order to reach destinations. Additional 220 due to busyness of Wilsonville Road and to accommodate using light pole.</i>	
<i>Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 20c

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Boones Ferry Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	20c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	120
LATERAL DISTANCE (Feet)	-

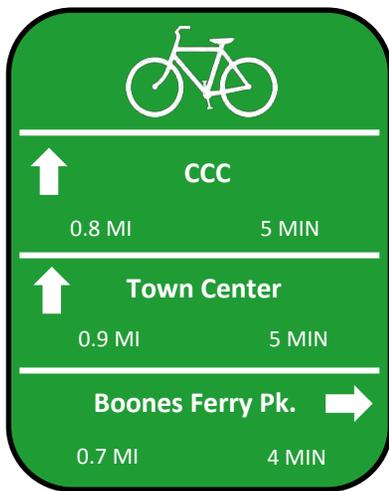
NOTES

Place decision sign on the south side of Wilsonville Road 120 feet west of Boones Ferry Road. Measured from western-most edge of crosswalk width across Wilsonville Road.

Initial 30 feet due to cyclists continuing straight or turning right in order to reach destinations. Moved back an additional 90 feet to accommodate using light pole.
Mount on light pole.



Satellite Location View (Location to be verified by contractor)



INTERSECTION 21A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Kinsman Road
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	21a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Kinsman Road
DISTANCE FROM INTERSECTION (Feet)	230
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES

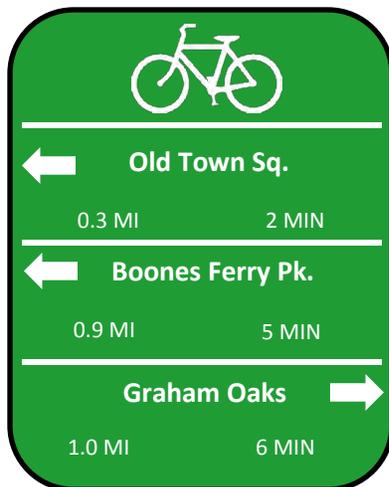
Place decision sign on the west side of Kinsman Road 230 feet north of Wilsonville Road. Measured from northern-most crosswalk bar across Kinsman Road.

Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left. Additional 130 feet to direct when the bike lane ends.

Mount on utility pole.



Satellite Location View (location to be verified by contractor)

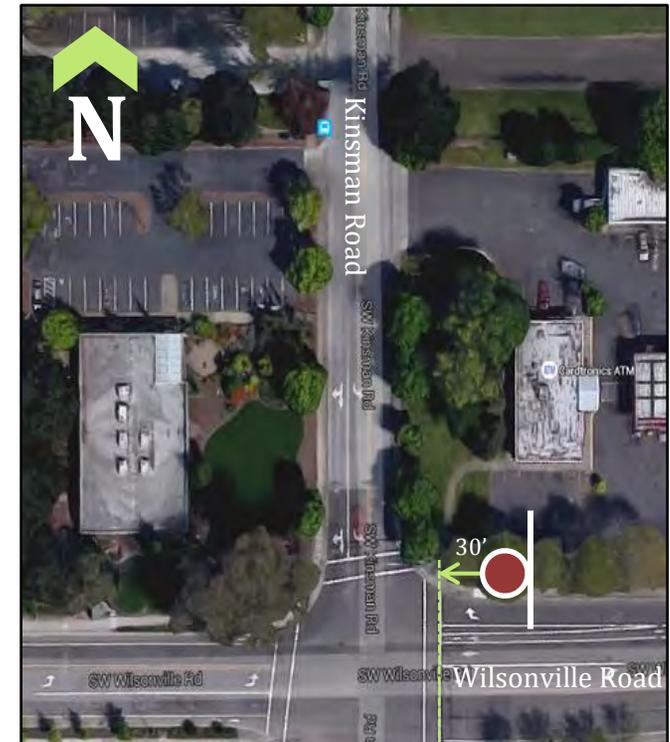


INTERSECTION 21B

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Kinsman Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	21b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Edge of Sidewalk</i>)

NOTES	
Place decision sign on the north side of Wilsonville Road 30 feet east of Kinsman Road. Measured from the eastern-most crosswalk bar across Wilsonville Road.	
<i>30 feet due to only straight or right turn needed to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 21c

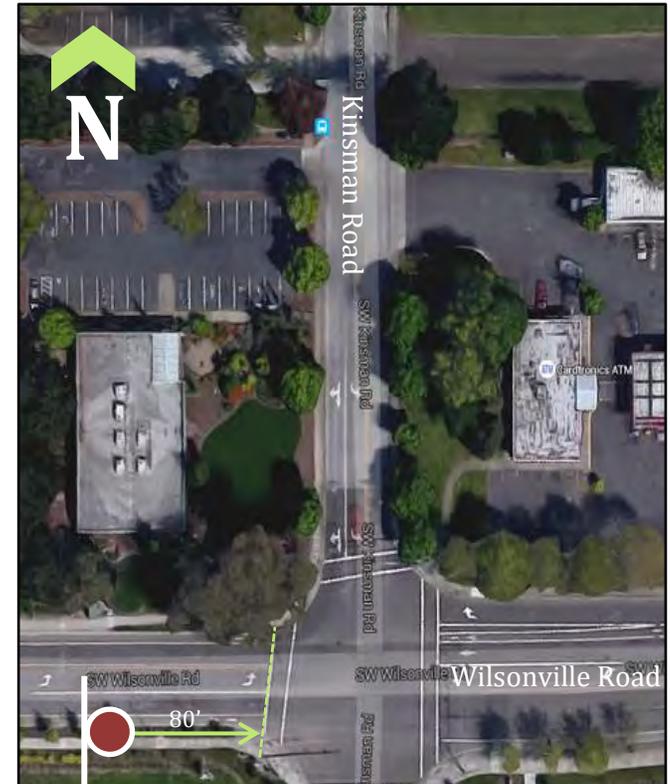
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Kinsman Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	21c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	80
LATERAL DISTANCE (Feet)	-

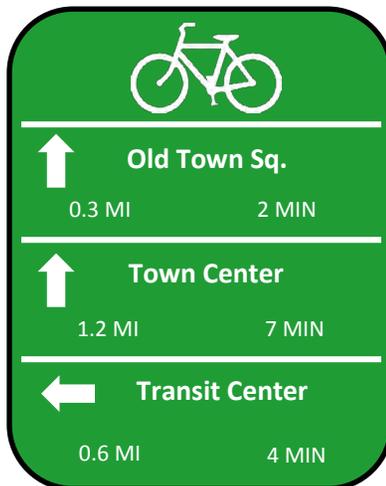
NOTES

Place decision sign on the south side of Wilsonville Road 80 feet west of Kinsman Road. Measured from the western-most crosswalk bar across Wilsonville Road.

100 feet due to the need to maneuver across one lane of traffic in order to turn left. Moved forward 20 feet due to accommodate using light pole. Mount on light pole.



Satellite Location View (location to be verified by contractor)

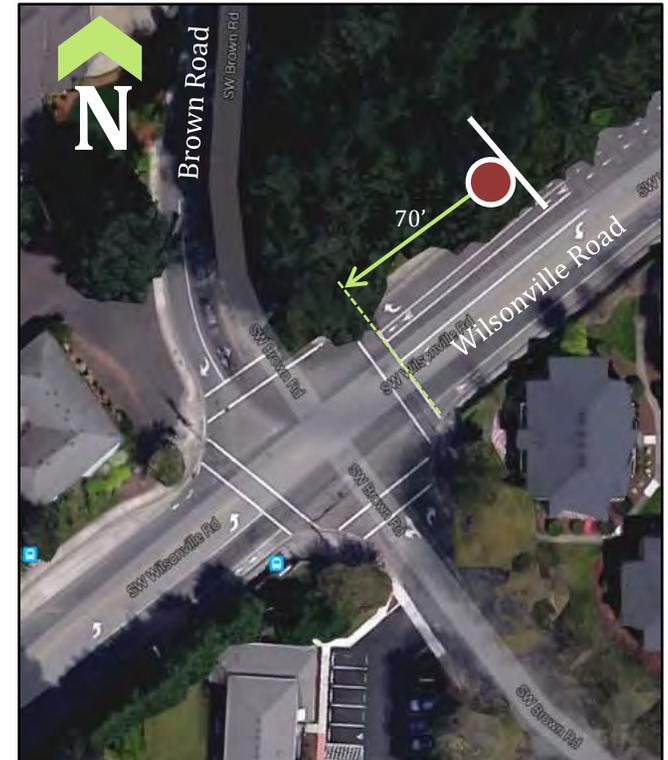


INTERSECTION 22A

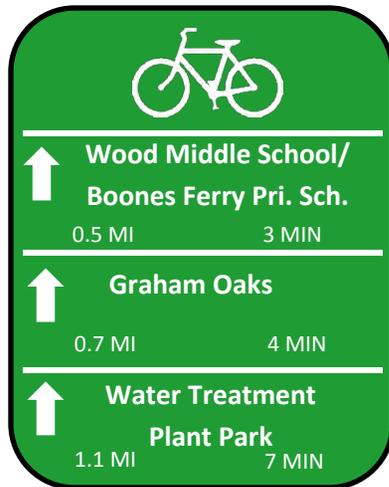
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Brown Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	22a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	70
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the north side of Wilsonville Road 70 feet east of Brown Road. Measured from the eastern-most crosswalk bar across Wilsonville Road. <i>Initial 30 feet due to cyclists continuing straight. Additional 40 feet due to accommodate using light pole. Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)

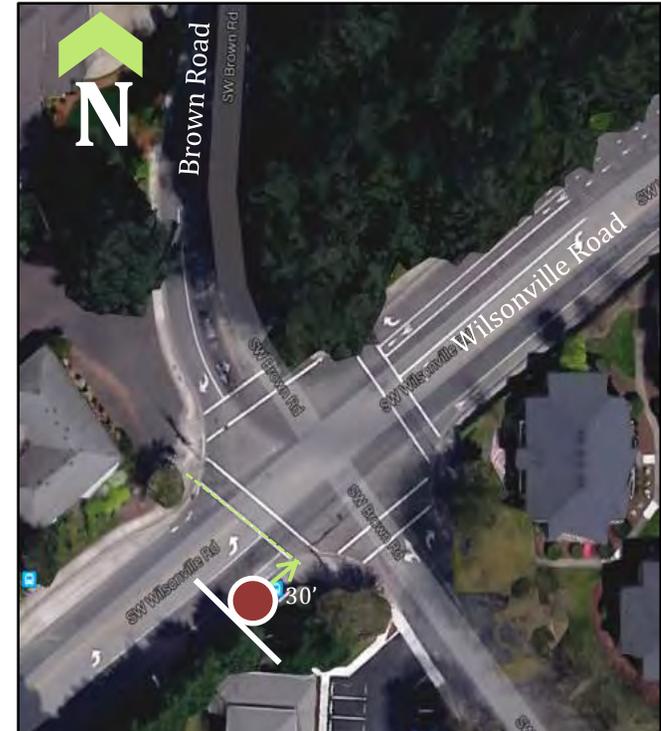


INTERSECTION 22B

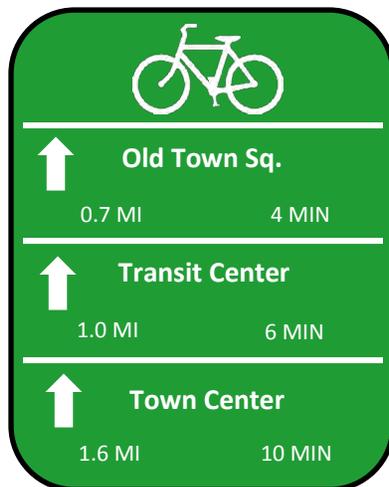
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Brown Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	22b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	1 (<i>Edge Sidewalk</i>)

NOTES	
Place decision sign on the south side of Wilsonville Road 30 feet west of Brown Road. Measured from the western-most crosswalk bar across Wilsonville Road. <i>30 feet due to cyclists continuing straight.</i>	



Satellite Location View (location to be verified by contractor)

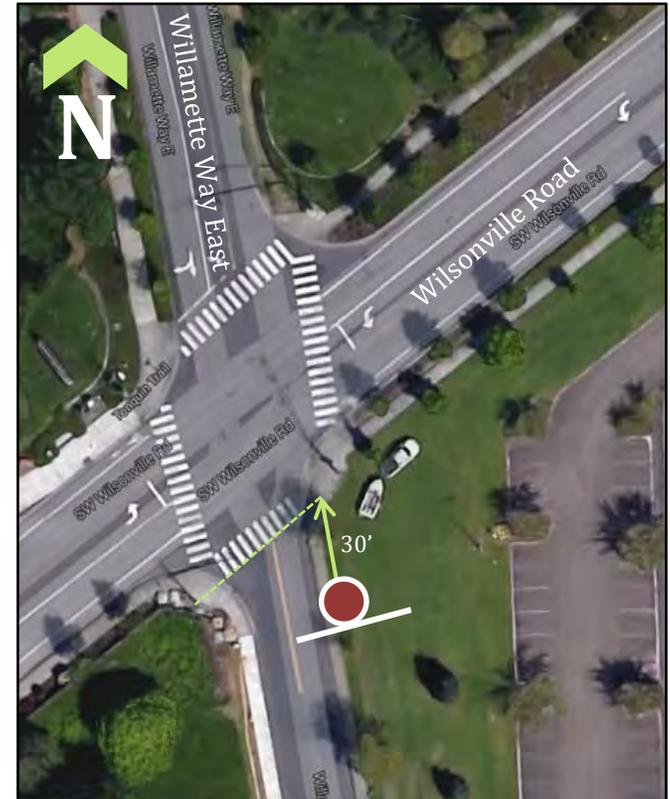


INTERSECTION 23A

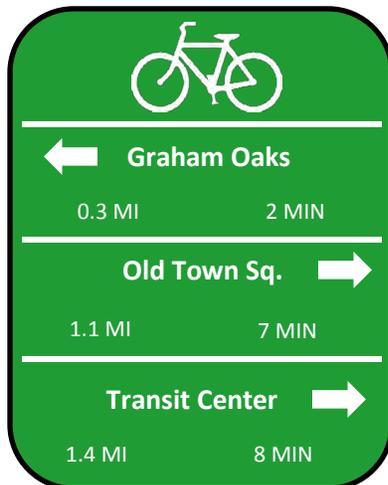
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Willamette Way East
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	23a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Willamette Way East
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES	
Place decision sign on the east side of Willamette Way East 30 feet south of Wilsonville Road. Measured from the southern-most edge of the crosswalk width across Willamette Way East.	
<i>30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 23B

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Willamette Way East
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	23b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	95
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the north side of Wilsonville Road 95 feet east of Willamette Way East. Measured from the eastern-most edge of the crosswalk width across Wilsonville Road.	
<i>Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left.</i>	
<i>Moved forward 5 feet accommodate using light pole. Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 23C

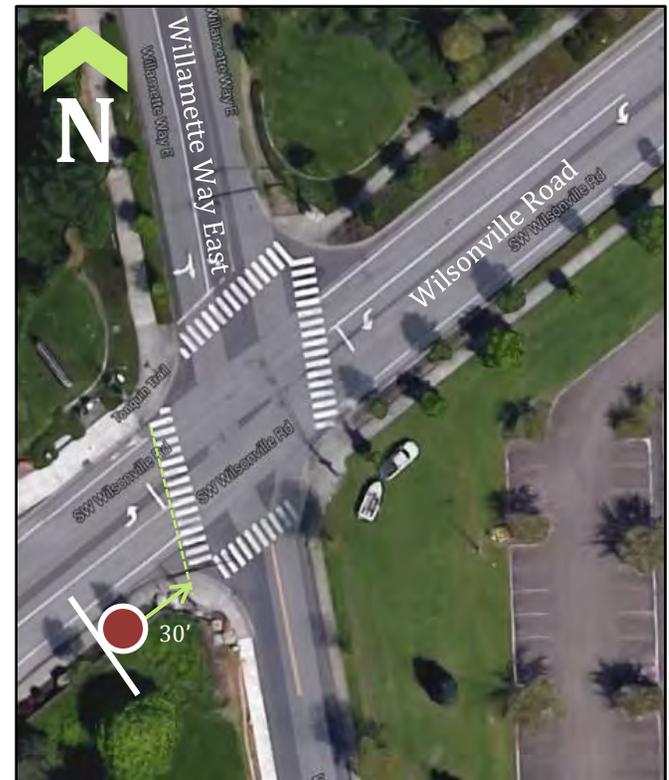
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Wilsonville Road & Willamette Way East
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	23c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Edge of Sidewalk</i>)

NOTES

Place decision sign on the south side of Wilsonville Road 30 feet west of Willamette Way East. Measured from western-most edge of crosswalk width across Wilsonville Road.

30 feet due to only straight or right turn needed to reach destinations.



Satellite Location View (Location to be verified by contractor)



INTERSECTION 24A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Willamette Way East & Parkview Drive
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	24a

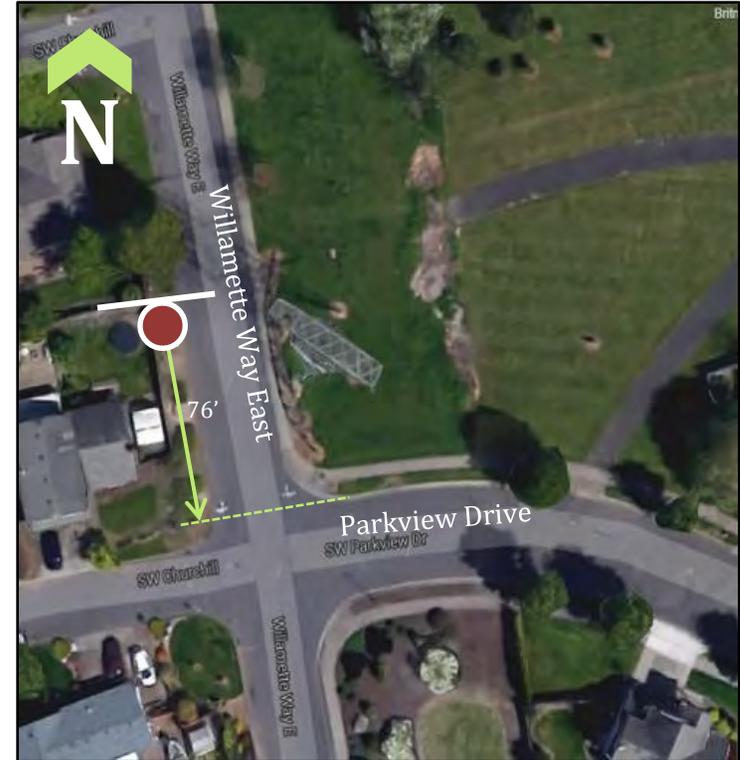
INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Willamette Way East
DISTANCE FROM INTERSECTION (Feet)	76
LATERAL DISTANCE (Feet)	-

NOTES

Place decision sign on the west side of Willamette Way East 76 feet north of Parkview Drive. Measured from the edge of curb along Parkway Drive.

Initial 30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations. Additional 46 feet in to accommodate using utility pole.

Mount on utility pole.



Satellite Location View (location to be verified by contractor)

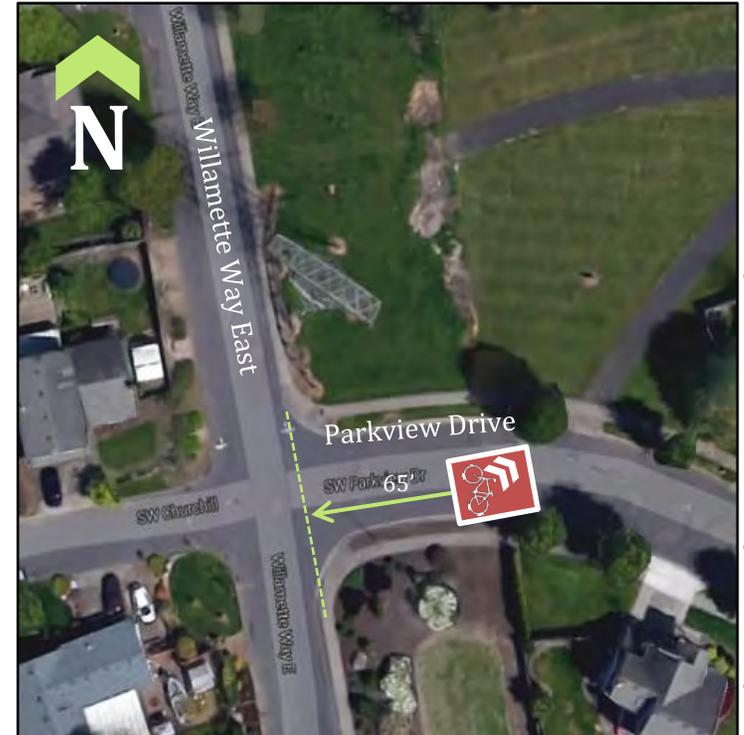


INTERSECTION 24B

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Willamette Way East & Parkview Drive
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	24b

INSTALLATION	
ORIENTATION	Arrows point northeast
DISTANCE FROM INTERSECTION (Feet)	65
LATERAL DISTANCE (Feet)	4 (On Street, From Edge of Curb)

NOTES
Place pavement marking on the southern half of Parkway Drive 65 feet east of Willamette Way East. Measured from the edge of curb along Willamette Way East.



Satellite Location View (location to be verified by contractor)

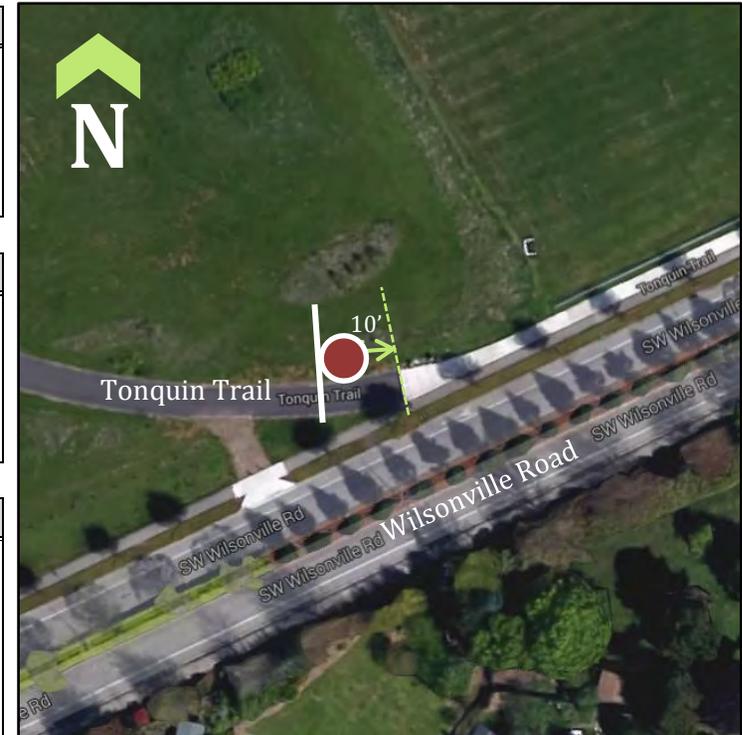


INTERSECTION 25A

INFORMATION	
SIGN TYPE	On-Trail Decision Sign
INTERSECTION	Tonquin Trail
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	25a

INSTALLATION	
ORIENTATION	Sign Face is Perpendicular to the edge of the Tonquin Trail
DISTANCE FROM INTERSECTION (Feet)	10
LATERAL DISTANCE (Feet)	3 (Edge of Pavement)

NOTES
Place decision sign on the north side of the Tonquin Trail 10 feet west of where the pavement transitions into sidewalk. Measured from where the pavement section of the Tonquin Trail transitions into concrete sidewalk.



INTERSECTION 25B

INFORMATION	
SIGN TYPE	On-Street Decision Sign
INTERSECTION	Tonquin Trail & Wilsonville Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	25b

INSTALLATION	
ORIENTATION	Sign Face is Perpendicular to the edge of the Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES	
Place decision sign on the north side Wilsonville Road 30 feet east the curb cutout. Measured from the east side of the curb cutout on Wilsonville Road.	



Satellite Location View (Location to be verified by contractor)



INTERSECTION 26A

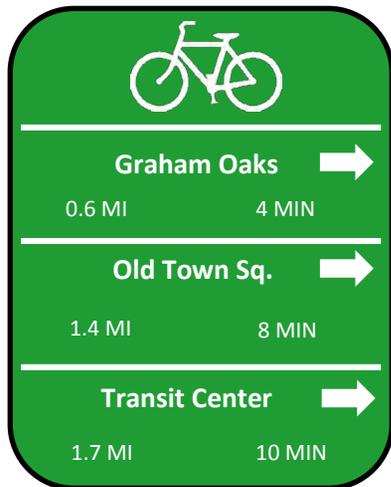
INFORMATION	
SIGN TYPE	On-Trail Decision
INTERSECTION	Morey's Landing Trail Fork
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	26a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Morey's Landing Trail
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (Edge of Pavement)

NOTES
Place decision sign on the north side of the Morey's Landing Trail 30 feet east of the fork. Measured from where Morey's landing trail splits in two directions.



Satellite Location View (location to be verified by contractor)

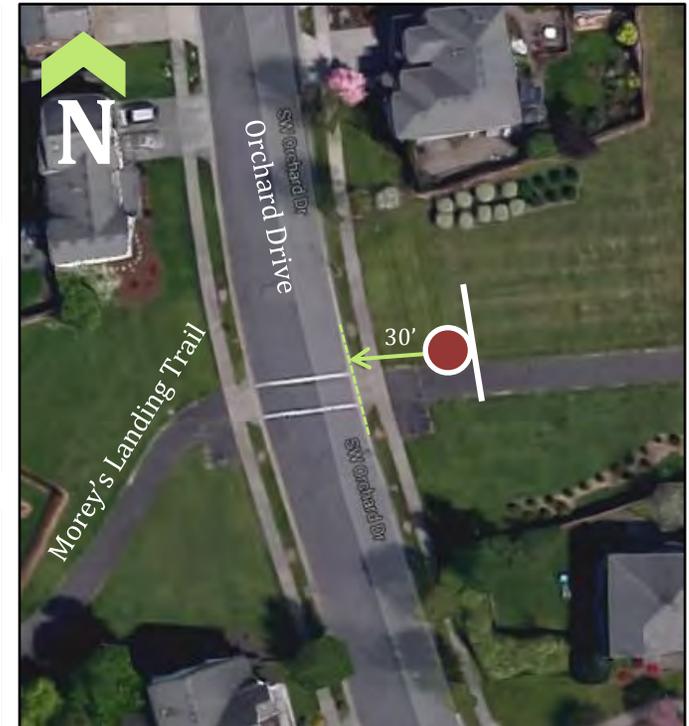


INTERSECTION 27A

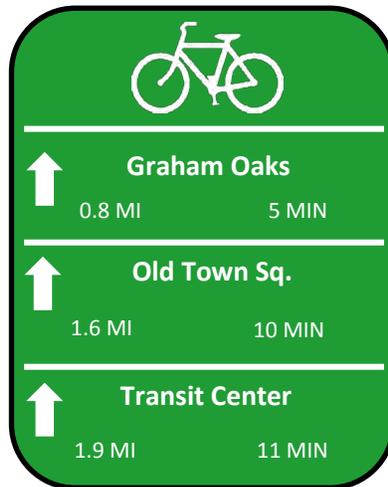
INFORMATION	
SIGN TYPE	On-Trail Decision
INTERSECTION	Morey's Landing Trail & Orchard Drive
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	27a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Morey's Landing Trail
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (Edge of Pavement)

NOTES	
Place decision sign on the north side of Morey's Landing Trail 30 feet east of Orchard Drive. Measured from edge of curb along the east side of Orchard Drive.	
<i>30 feet due to cyclists continuing straight.</i>	



Satellite Location View (location to be verified by contractor)

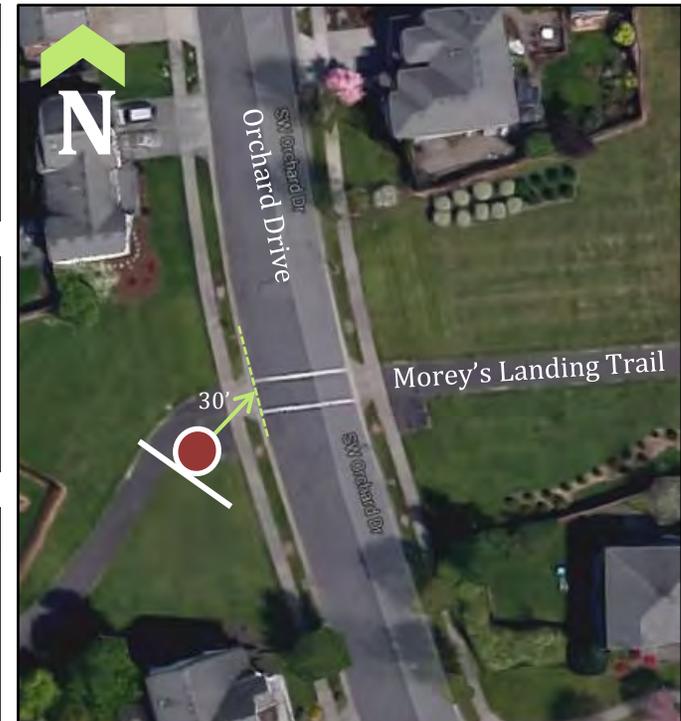


INTERSECTION 27B

INFORMATION	
SIGN TYPE	On-Street Bike Route
INTERSECTION	Morey's Landing Trail & Orchard Drive
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	27b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Morey's Landing Trail
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Edge of Pavement</i>)

NOTES	
Place bike route sign on the south side of Morey's Landing Trail 30 feet west of the Orchard Drive. Measured from edge of curb along the west side of Orchard Drive.	
<i>30 feet due to cyclists continuing straight.</i>	



Satellite Location View (location to be verified by contractor)

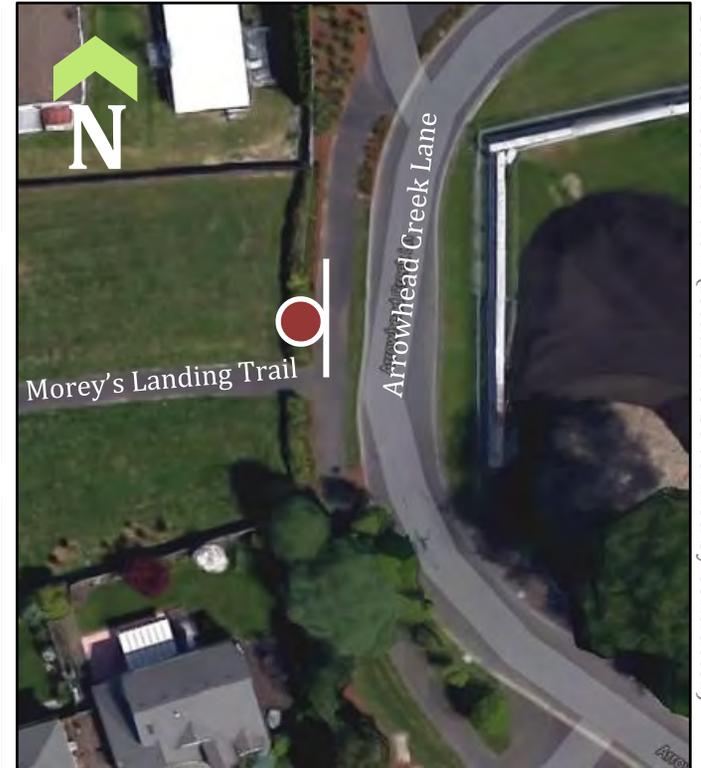


INTERSECTION 28A

INFORMATION	
SIGN TYPE	On-Path Bike Route
INTERSECTION	Morey's Landing Trail & Arrowhead Creek Lane
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	28a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Morey's Landing Trail
DISTANCE FROM INTERSECTION (Feet)	-
LATERAL DISTANCE (Feet)	-

NOTES
Place bike route sign on the west side of Arrowhead Creek Lane 3 feet north of Morey's Landing Trail. The sign will be placed up against bushes located on the north side of Morey's Landing trail so that it can be seen by cyclists approaching from either the north or south on Arrowhead Creek Lane.



Satellite Location View (location to be verified by contractor)

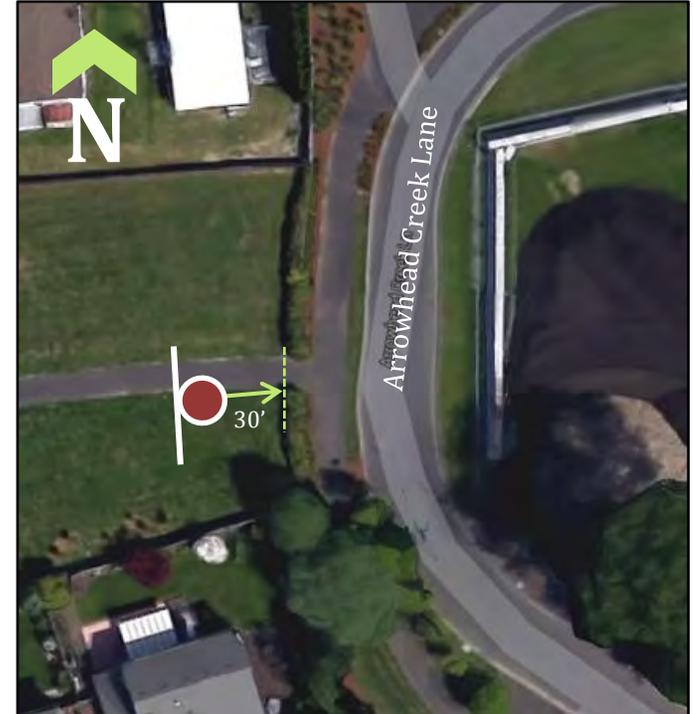


INTERSECTION 28B

INFORMATION	
SIGN TYPE	On-Path Turn
INTERSECTION	Morey's Landing Trail & Arrowhead Creek Lane
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	28b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Morey's Landing Path
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (Edge of Pavement)

NOTES
Place turn sign on the south side of Morey's Landing Path 30 feet west of the bushes at end of trail. Measured from the western edge of the bushes at the end of Morey's Landing Trail. <i>30 feet due to only right turn needed to reach destinations</i>



Satellite Location View (location to be verified by contractor)



INTERSECTION 29A

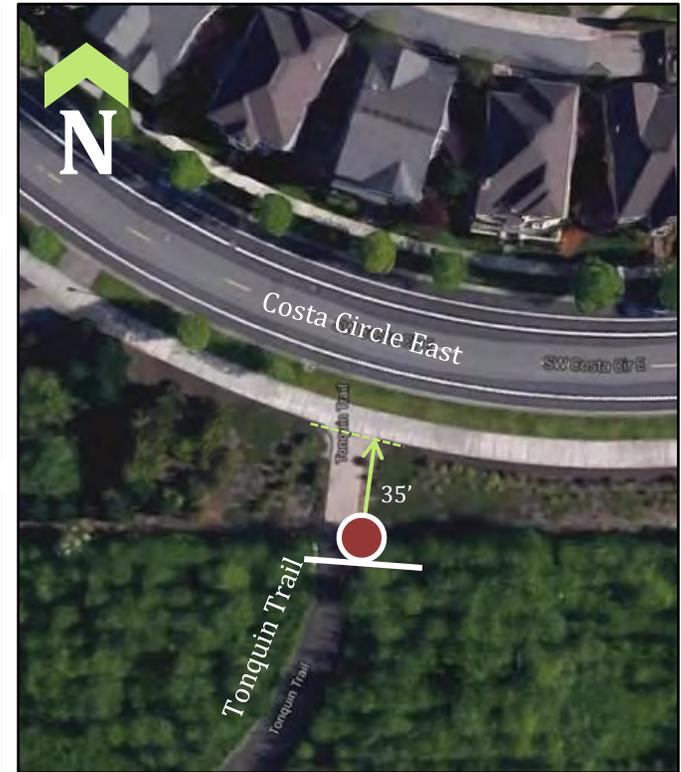
INFORMATION	
SIGN TYPE	On-Trail Decision
INTERSECTION	Tonquin Trail & Costa Circle East
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	29a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Tonquin Trail
DISTANCE FROM INTERSECTION (Feet)	35
LATERAL DISTANCE (Feet)	3 (Edge of Pavement)

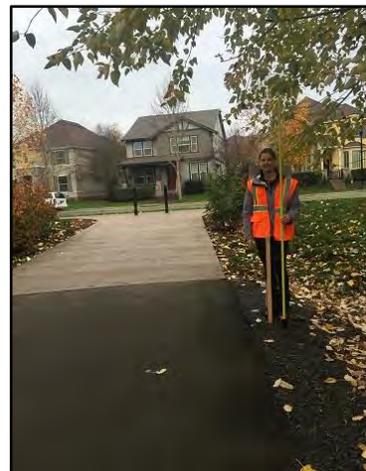
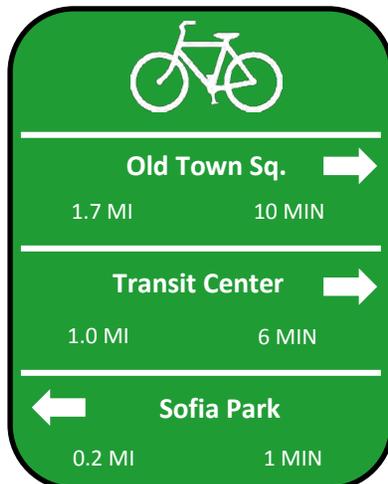
NOTES

Place decision sign on the east side of the Tonquin Trail 35 feet south of the sidewalk along Costa Circle East. Measured from the southern edge of sidewalk along Costa Circle East.

*Initial 30 feet due to left turn not requiring the cyclists to maneuver across a lane of traffic.
Additional 5 feet to avoid branches.*



Satellite Location View (location to be verified by contractor)



INTERSECTION 29B

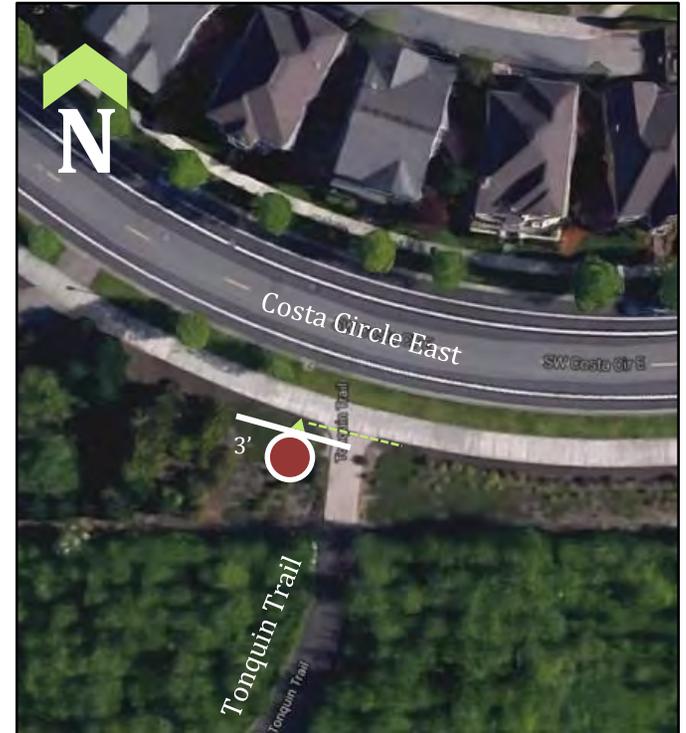
INFORMATION	
SIGN TYPE	On-Trail Decision
INTERSECTION	Tonquin Trail & Costa Circle East
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	29b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Tonquin Trail
DISTANCE FROM INTERSECTION (Feet)	3
LATERAL DISTANCE (Feet)	3 (<i>Edge of Pavement</i>)

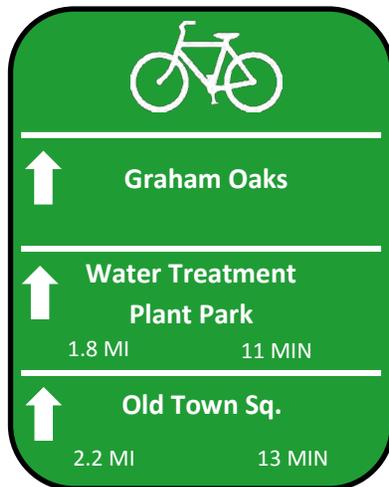
NOTES

Place decision sign on the west of the Tonquin Trail 3 feet south of the sidewalk along Costa Circle East. Measured from the southern edge of sidewalk along Costa Circle East.

3 feet due to sign being used to collect bicyclists onto the Tonquin Trail.



Satellite Location View (location to be verified by contractor)

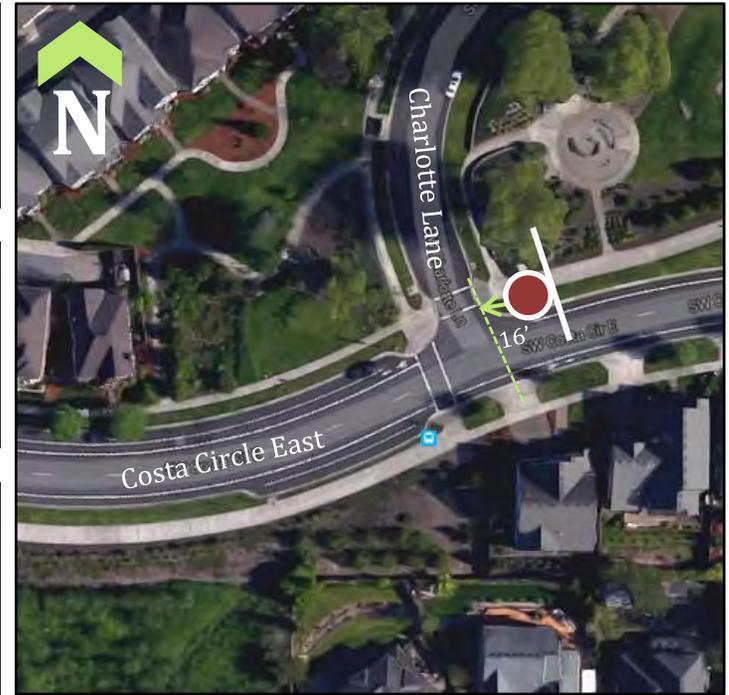


INTERSECTION 29c

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Costa Circle East & Charlotte Lane
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	29c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Costa Circle East
DISTANCE FROM INTERSECTION (Feet)	16
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES	
Place decision sign on the north side of Costa Circle East 16 feet east of Charlotte Lane. Measured from the eastern edge of curb along Charlotte Lane. <i>16 feet due to intersection being 300 feet from left turn onto Tonquin Trail and to accommodate using light pole.</i> <i>Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)

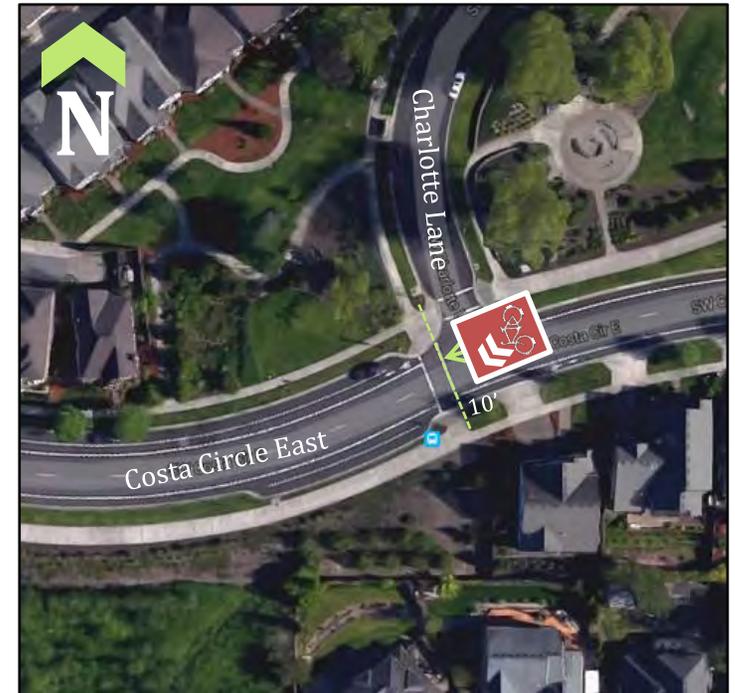


INTERSECTION 29D

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Costa Circle East & Charlotte Lane
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	29d

INSTALLATION	
ORIENTATION	Arrows point southwest
DISTANCE FROM INTERSECTION (Feet)	10
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES
Place pavement marking on the northern half of Costa Circle East 10 feet east of Charlotte Lane. Measured from the eastern-most crosswalk bar across Costa Circle East.
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on sidewalk to reach Graham Oaks Nature Park section of the Tonquin Trail.</i>



Satellite Location View (Location to be verified by contractor)

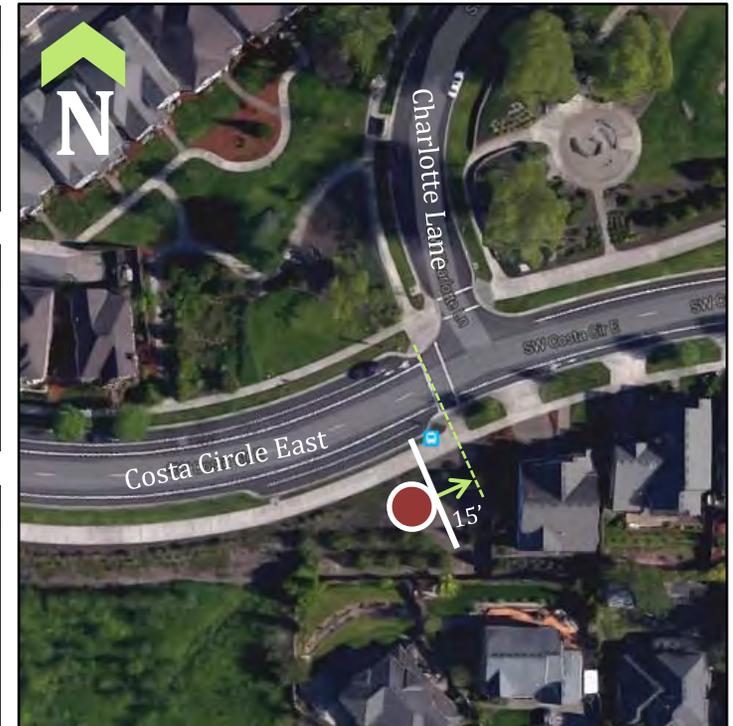


INTERSECTION 29E

INFORMATION	
SIGN TYPE	Bike Route on Sidewalk
INTERSECTION	Costa Circle East & Charlotte Lane
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	29e

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Costa Circle East
DISTANCE FROM INTERSECTION (Feet)	15
LATERAL DISTANCE (Feet)	3 (<i>Edge of Sidewalk</i>)

NOTES	
Place bike route sign on the south side of the sidewalk along Costa Circle East 15 feet west of Charlotte Lane. Measured from the western-most crosswalk bar across Costa Circle East.	
<i>Bike Route on Sidewalk sign placed to direct bicyclists to use sidewalk to reach Graham Oaks Nature Park section of the Tonquin Trail.</i>	



Satellite Location View (Location to be verified by contractor)



INTERSECTION 29F

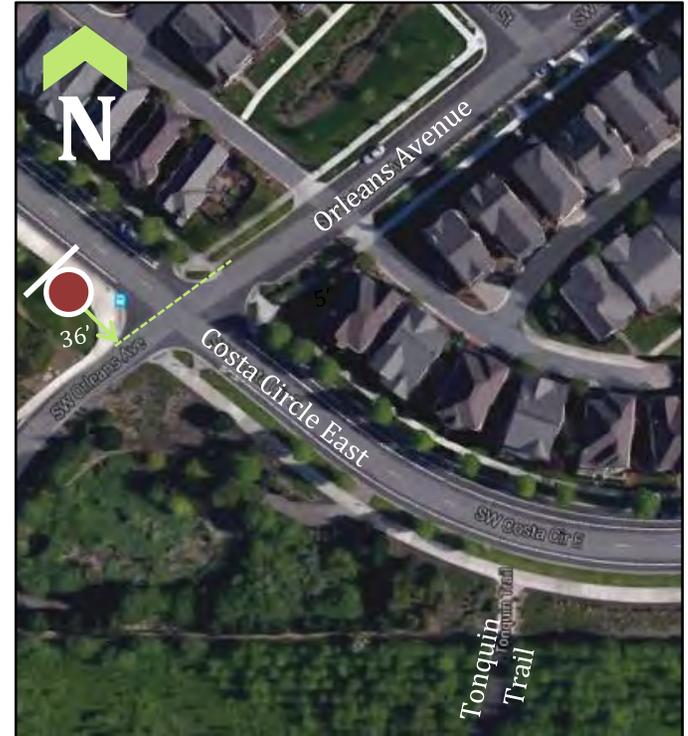
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Costa Circle East & Orleans Avenue
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	29f

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Costa Circle East
DISTANCE FROM INTERSECTION (Feet)	36
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES

Place decision sign on the south side of Costa Circle East 36 feet west of the Orleans Avenue. Measured from edge of curb along Orleans Avenue.

Initial 30 feet due to cyclists continuing straight in order to reach destinations. Additional 6 feet to avoid tree.



Satellite Location View (location to be verified by contractor)

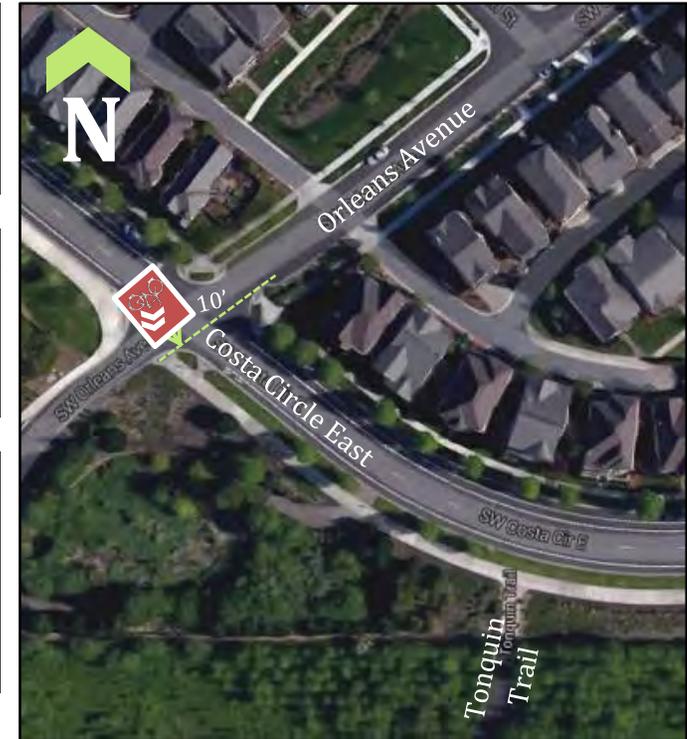


INTERSECTION 29G

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Costa Circle East & Orleans Avenue
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	29g

INSTALLATION	
ORIENTATION	Arrows point southeast
DISTANCE FROM INTERSECTION (Feet)	10
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES
Place pavement marking on the southern half of Costa Circle East 10 feet west of the Orleans Avenue. Measured from the eastern edge of curb along Orleans Avenue.
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on sidewalk to reach Graham Oaks Nature Park section of the Tonquin Trail.</i>



Satellite Location View (location to be verified by contractor)

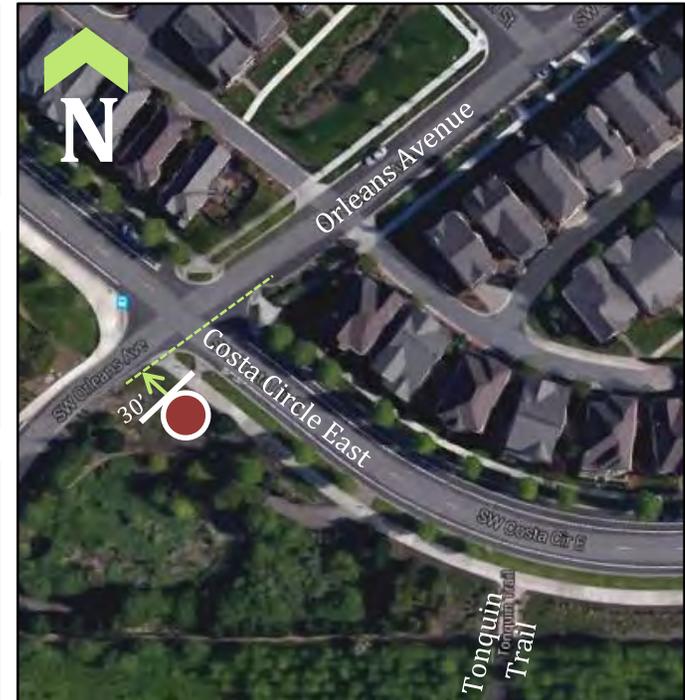


INTERSECTION 29H

INFORMATION	
SIGN TYPE	Bike Route on Sidewalk
INTERSECTION	Costa Circle East & Orleans Avenue
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	29h

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Costa Circle East
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES	
Place bike route sign on the south side of the sidewalk along Costa Circle East 30 feet east of the Orleans Avenue. Measured from the edge of pavement along Orleans Avenue.	
<i>Bike Route on Sidewalk sign placed to direct bicyclists to use sidewalk to reach Graham Oaks Nature Park section of the Tonquin Trail.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 30A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Costa Circle West & Barber Street
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	30a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Costa Circle West
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

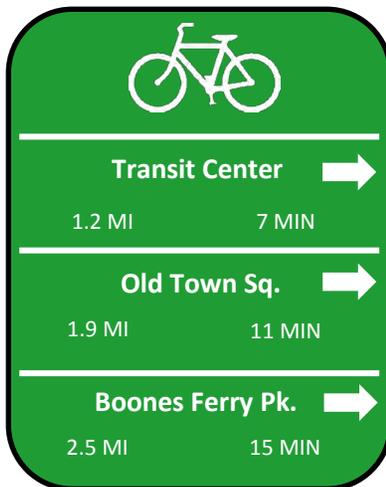
NOTES

Place decision sign on the east side of Costa Circle West 30 feet south of Barber Street. Measured from the southern-most crosswalk bar across Costa Circle West.

30 feet due to only right turn needed to reach destinations.



Satellite Location View (location to be verified by contractor)



INTERSECTION 30B

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Costa Circle West & Barber Street
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	30b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Costa Circle West
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	-

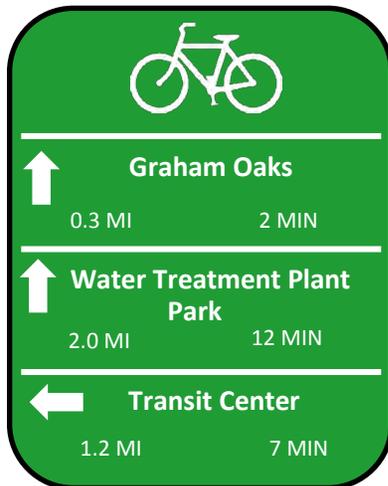
NOTES

Place decision sign on the west side of Costa Circle West 30 feet north of Barber Street. Measured from the northern-most crosswalk bar across Costa Circle West.

30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations.
Mount on light pole.



Satellite Location View (location to be verified by contractor)



INTERSECTION 30c

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Costa Circle West & Barber Street
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	30c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Barber Street
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

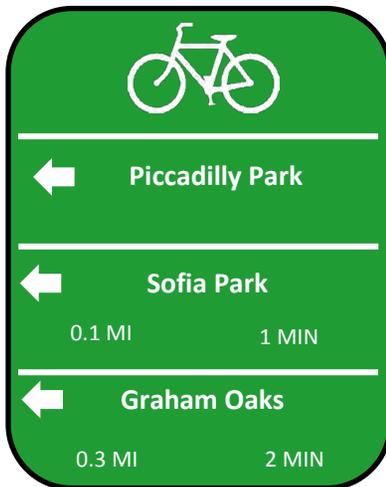
NOTES

Place decision sign on the north side of Barber Street 30 feet east of Costa Circle West. Measured from the eastern-most crosswalk bar across Barber Street.

30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations.



Satellite Location View (Location to be verified by contractor)



INTERSECTION 30D

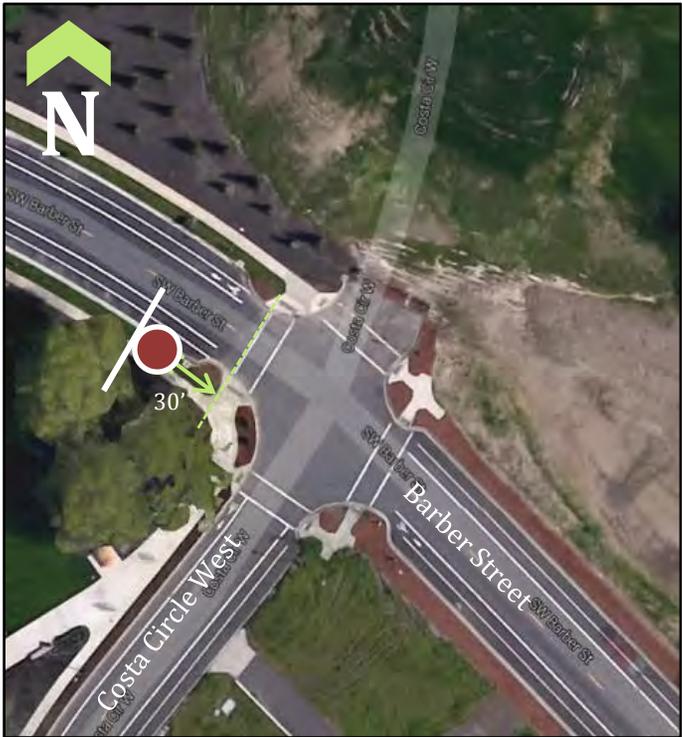
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Costa Circle West & Barber Street
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	30d

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Barber Street
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

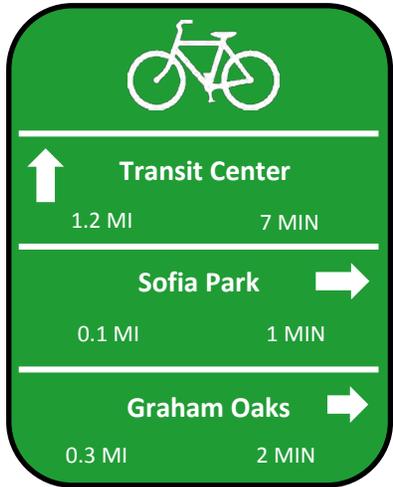
NOTES

Place decision sign on the south side of Barber Street 30 feet west of Costa Circle West. Measured from western-most crosswalk bar across Barber Street.

30 feet due to cyclists continuing straight.



Satellite Location View (location to be verified by contractor)



INTERSECTION 31A

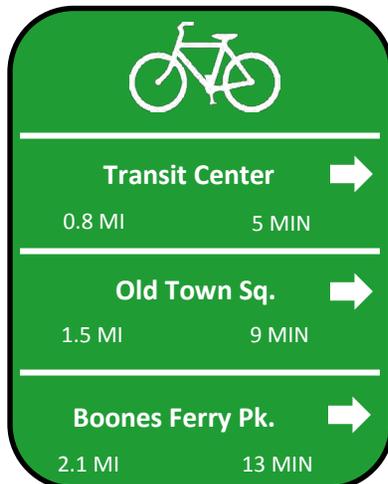
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Costa Circle East & Barber Street
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	31a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Costa Circle East
DISTANCE FROM INTERSECTION (Feet)	90
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES
Place decision sign on the east side of Costa Circle East 90 feet south of Barber Street. Measured from the southern-most edge of crosswalk width across Costa Circle East.
<i>Initial 30 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations. Additional 60 feet to allow cyclists to maneuver onto sidewalk.</i>



Satellite Location View (location to be verified by contractor)

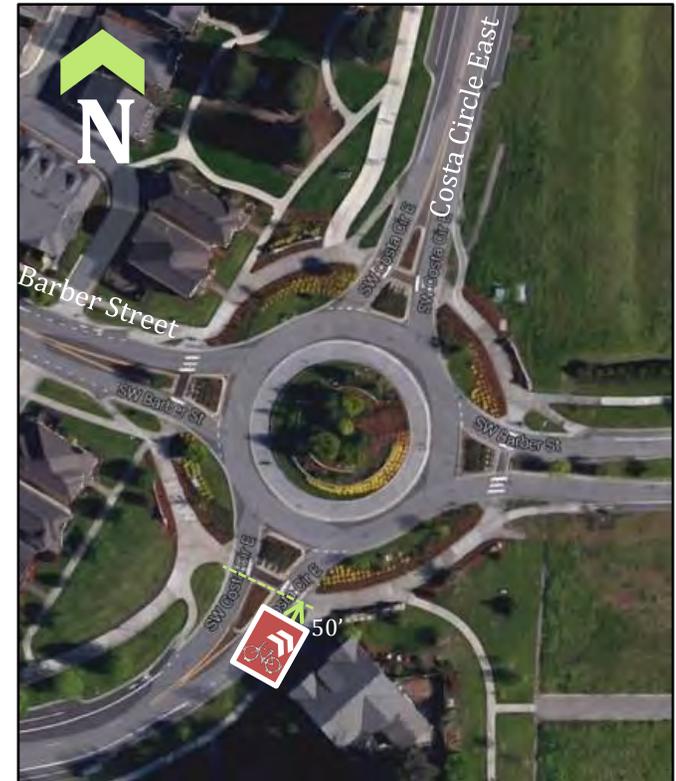


INTERSECTION 31B

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Costa Circle East & Barber Street
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	31b

INSTALLATION	
ORIENTATION	Arrows point northeast
DISTANCE FROM INTERSECTION (Feet)	50
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES	
Place pavement marking on the eastern half of Costa Circle East 50 feet south of Barber Street. Measured from the southern-most edge of crosswalk width across Costa Circle East.	
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on roundabout sidewalk to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)

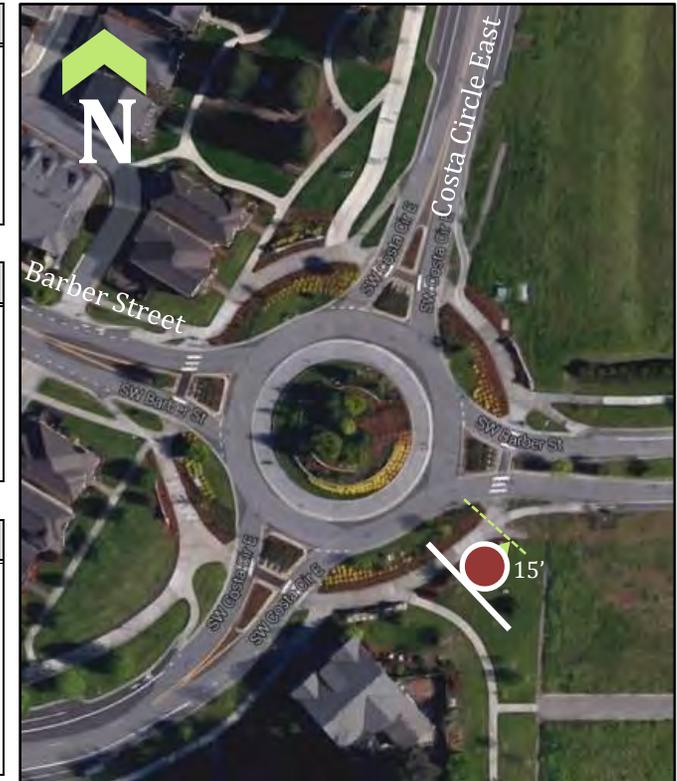


INTERSECTION 31c

INFORMATION	
SIGN TYPE	On-Street Turn with Destinations
INTERSECTION	Costa Circle East & Barber Street
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	31c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of the roundabout sidewalk
DISTANCE FROM INTERSECTION (Feet)	15
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES	
Place turn sign with destinations on the east side of the roundabout sidewalk 15 feet south of where the sidewalk splits. Measured from where roundabout sidewalk splits into two directions.	
<i>Sign placed to direct bicyclists within the roundabout to specific destinations.</i>	



Satellite Location View (location to be verified by contractor)

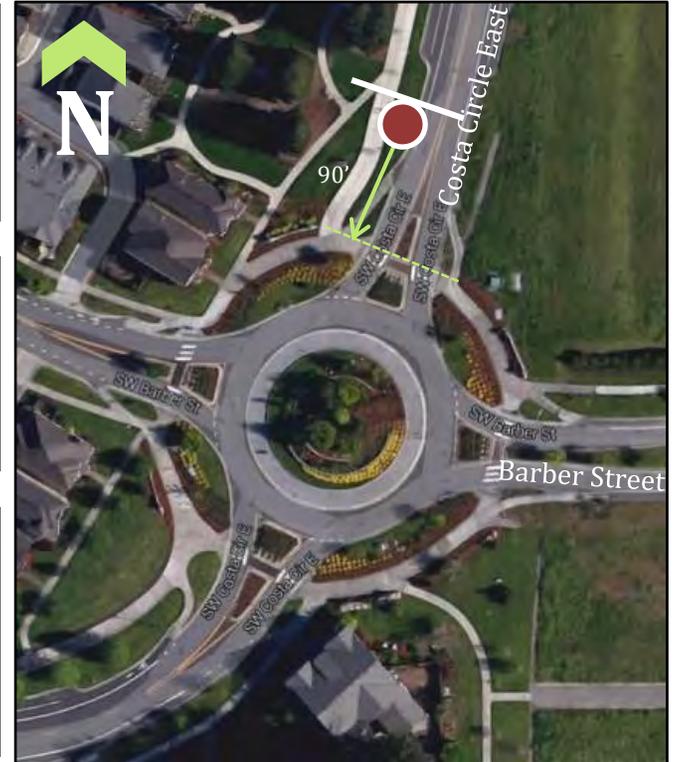


INTERSECTION 31D

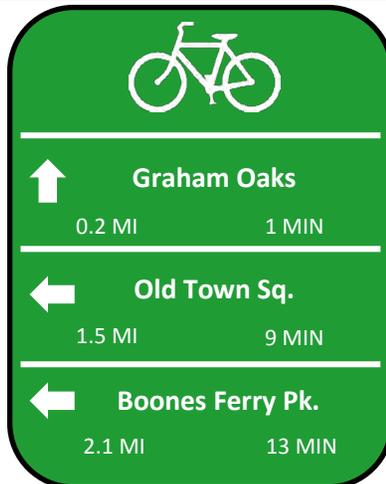
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Costa Circle East & Barber Street
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	31d

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Costa Circle East
DISTANCE FROM INTERSECTION (Feet)	90
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES
Place decision sign on the west side of Costa Circle East 90 feet north of Barber Street. Measured from northern-most edge of crosswalk width across Costa Circle East.
<i>Initial 30 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations. Additional 60 feet to allow cyclists to maneuver onto sidewalk.</i>



Satellite Location View (Location to be verified by contractor)



INTERSECTION 31E

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Costa Circle East & Barber Street
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	31e

INSTALLATION	
ORIENTATION	Arrows point southwest
DISTANCE FROM INTERSECTION (Feet)	50
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES	
Place pavement marking on the western half of Costa Circle East 50 feet north of Barber Street. Measured from northern-most edge of crosswalk width across Costa Circle East.	
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on roundabout sidewalk to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 31F

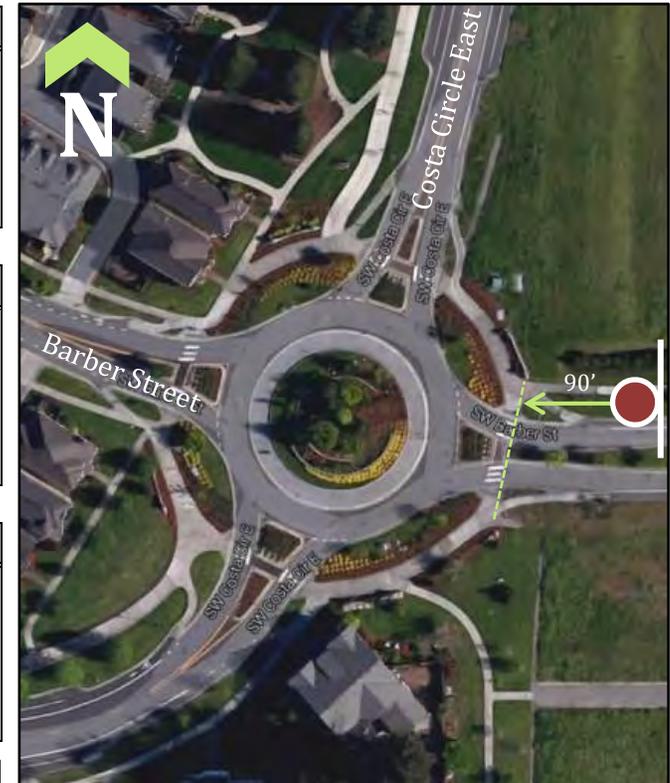
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Costa Circle East & Barber Street
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	31f

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Barber Street
DISTANCE FROM INTERSECTION (Feet)	90
LATERAL DISTANCE (Feet)	3 (Back of Curb)

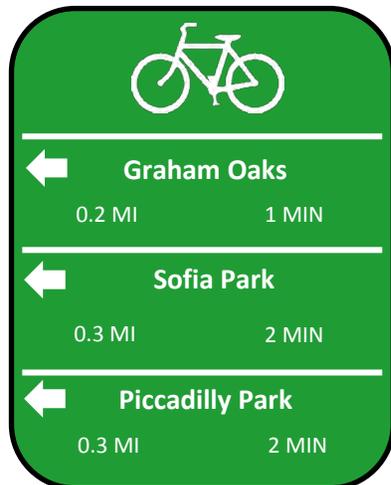
NOTES

Place decision sign on the north side of Barber Street 90 feet east of Costa Circle East. Measured from eastern-most edge of crosswalk width across Barber Street.

Initial 30 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations. Additional 60 feet to allow cyclists to maneuver onto sidewalk.



Satellite Location View (location to be verified by contractor)



INTERSECTION 31G

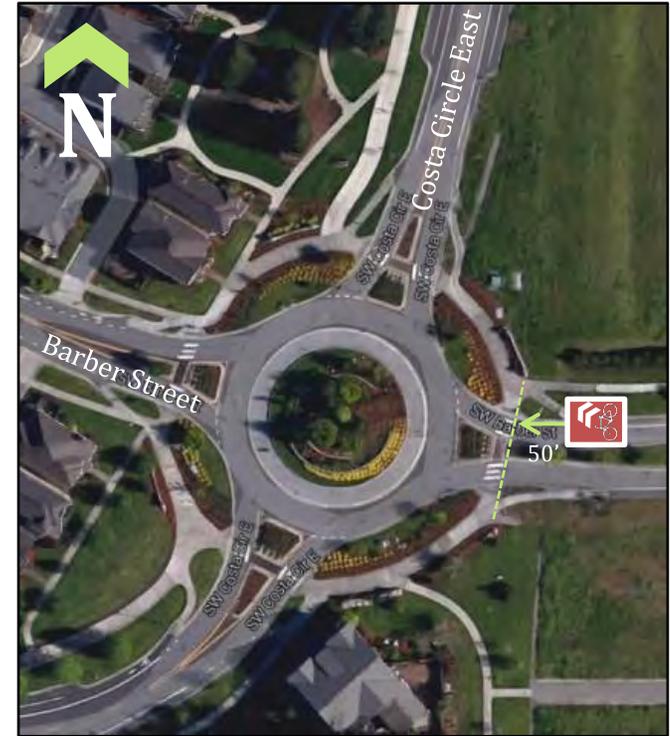
INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Costa Circle East & Barber Street
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	31g

INSTALLATION	
ORIENTATION	Arrows point northwest
DISTANCE FROM INTERSECTION (Feet)	50
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES

Place pavement marking on the northern half of Barber Street 50 feet east of Costa Circle East. Measured from eastern-most edge of crosswalk width across Barber Street.

Pavement marking placed to direct bicyclists to curb cut in order to ride on roundabout sidewalk to reach destinations.



INTERSECTION 31H

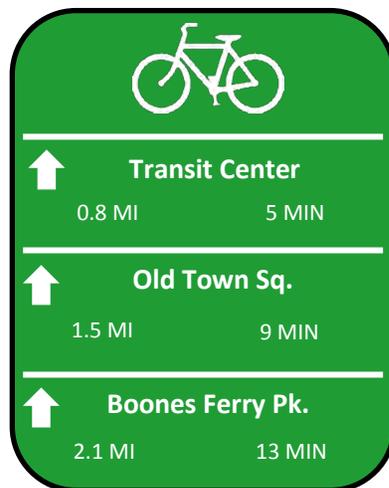
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Costa Circle East & Barber Street
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	31h

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Barber Street
DISTANCE FROM INTERSECTION (Feet)	70
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES	
Place decision sign on the south side of Barber Street 70 feet west of Costa Circle East. Measured from western-most edge of crosswalk width across Barber Street.	
<i>Initial 30 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations. Additional 40 feet to allow cyclists to maneuver onto sidewalk.</i>	



Satellite Location View (location to be verified by contractor)

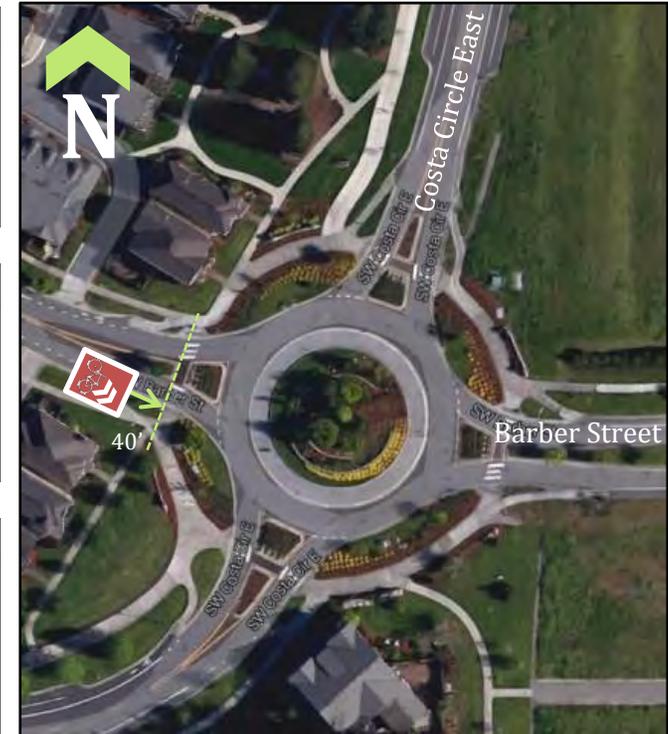


INTERSECTION 31i

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Costa Circle East & Barber Street
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	31i

INSTALLATION	
ORIENTATION	Arrows point southeast
DISTANCE FROM INTERSECTION (Feet)	40
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES	
Place pavement marking on the south side of Barber Street 40 feet west of Costa Circle East. Measured from western-most edge of crosswalk width across Barber Street.	
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on roundabout sidewalk to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 31J

INFORMATION	
SIGN TYPE	On-Street Turn with Destinations
INTERSECTION	Costa Circle East & Barber Street
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	31j

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of the roundabout sidewalk
DISTANCE FROM INTERSECTION (Feet)	10
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES	
Place turn sign with destinations on the south side of roundabout sidewalk 10 feet west of where the sidewalk splits. Measured from where roundabout sidewalk splits into three directions.	
<i>Sign placed to direct bicyclists within the roundabout to specific destinations.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 32A

INFORMATION	
SIGN TYPE	On-Street Bike Route
INTERSECTION	Barber Street & Brown Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	32a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Barber Street
DISTANCE FROM INTERSECTION (Feet)	90
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES
Place bike route sign on the north side of Barber Street 90 feet east of Brown Road. Measured from eastern-most edge of crosswalk width across Barber Street.
<i>Initial 30 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations. Additional 60 feet to allow cyclists to maneuver onto sidewalk.</i>



Satellite Location View (location to be verified by contractor)

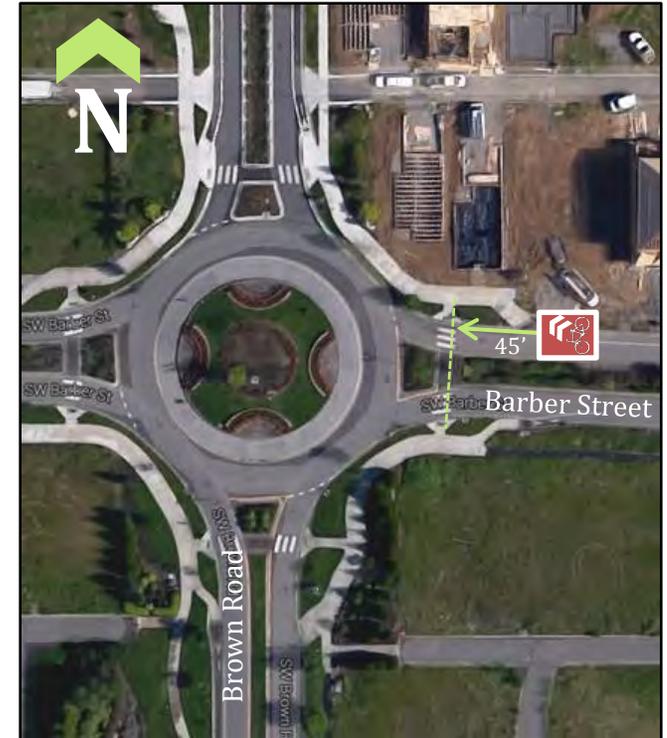


INTERSECTION 32B

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Barber Street & Brown Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	32b

INSTALLATION	
ORIENTATION	Arrows point northeast
DISTANCE FROM INTERSECTION (Feet)	45
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES	
Place pavement marking on the northern half of Barber Street 45 feet east of Brown Road. Measured from eastern-most edge of crosswalk across Barber Street.	
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on sidewalk to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 32c

INFORMATION	
SIGN TYPE	On-Street Bike Route
INTERSECTION	Barber Street & Brown Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	32c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Barber Street
DISTANCE FROM INTERSECTION (Feet)	90
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES

Place bike route sign on the south side of Barber Street 90 feet west of Brown Road. Measured from western-most edge of crosswalk width across Barber Street.

Initial 30 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations. Additional 60 feet to allow cyclists to maneuver onto sidewalk.



Satellite Location View (location to be verified by contractor)

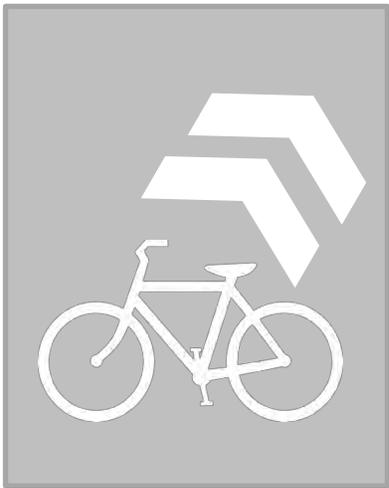


INTERSECTION 32D

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Barber Street & Brown Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	32d

INSTALLATION	
ORIENTATION	Arrows point southeast
DISTANCE FROM INTERSECTION (Feet)	45
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES	
Place pavement marking on the south side of Barber Street 45 feet west of Brown Road. Measured from western-most edge of crosswalk width across Barber Street.	
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on sidewalk to reach destinations.</i>	



Satellite Location View (Location to be verified by contractor)

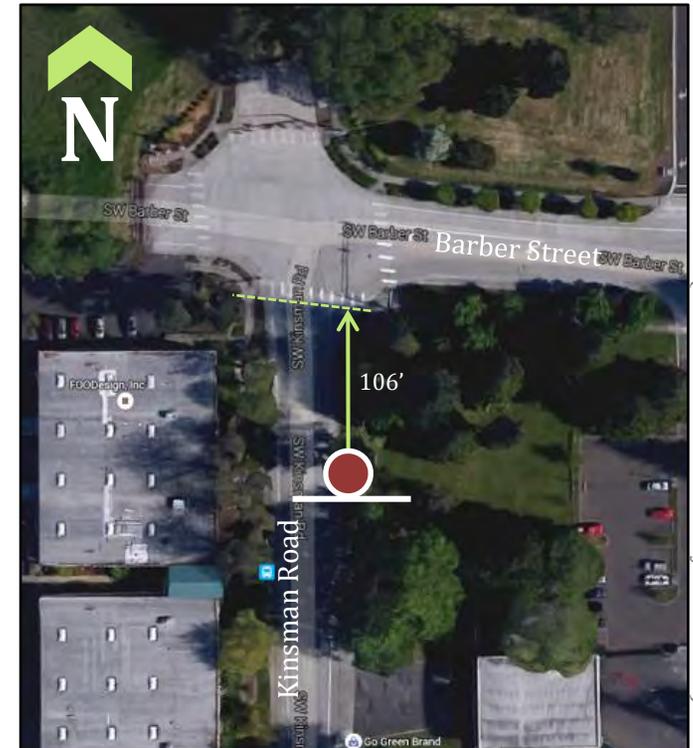


INTERSECTION 33A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Barber Street & Kinsman Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	33a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Kinsman Road
DISTANCE FROM INTERSECTION (Feet)	106
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES	
Place decision sign on the east side of Kinsman Road 106 feet south of Barber Street. Measured from southern-most edge of crosswalk width across Kinsman Road.	
<i>Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left. Additional 6 feet to accommodate using use sign pole. Mount to Sign Pole.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 33B

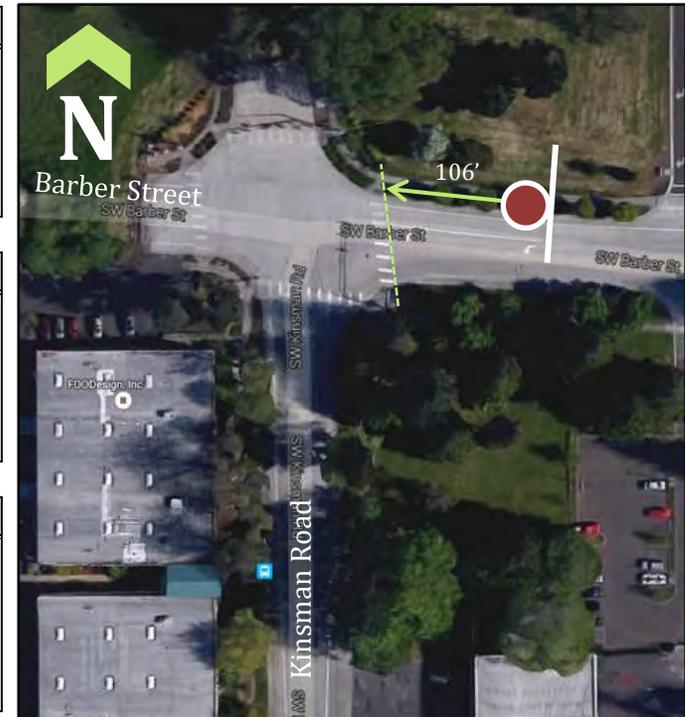
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Barber Street & Kinsman Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	33b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Barber Street
DISTANCE FROM INTERSECTION (Feet)	106
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

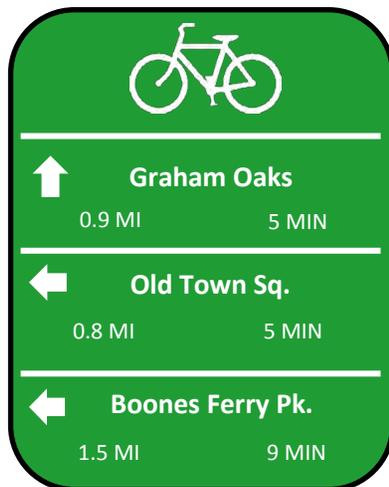
NOTES

Place decision sign on the north side of Barber Street 106 feet east of Kinsman Road. Measured from eastern-most edge of crosswalk width across Barber Street.

*Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left.
Additional 6 feet to avoid utility box.*



Satellite Location View (Location to be verified by contractor)



INTERSECTION 33C

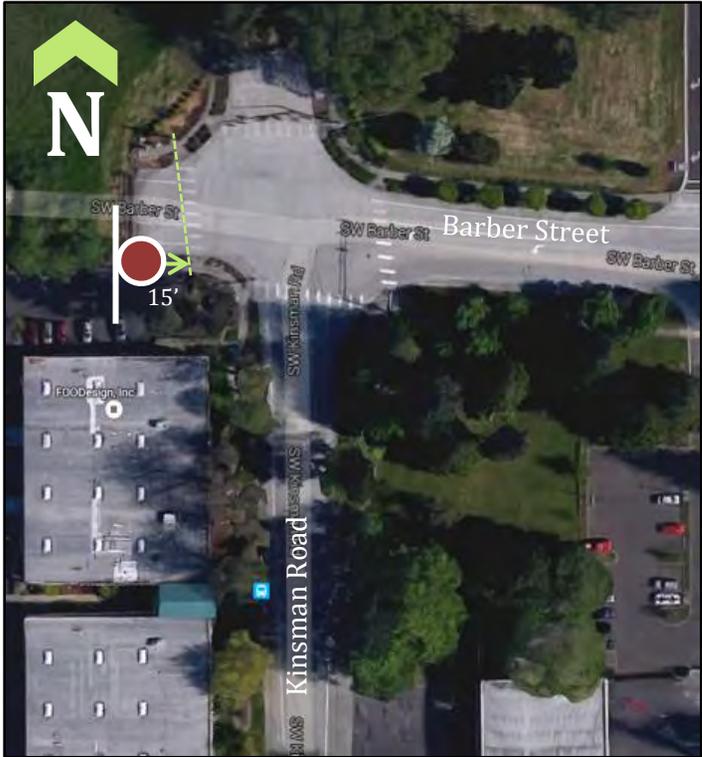
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Barber Street & Kinsman Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	33c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Barber Street
DISTANCE FROM INTERSECTION (Feet)	15
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

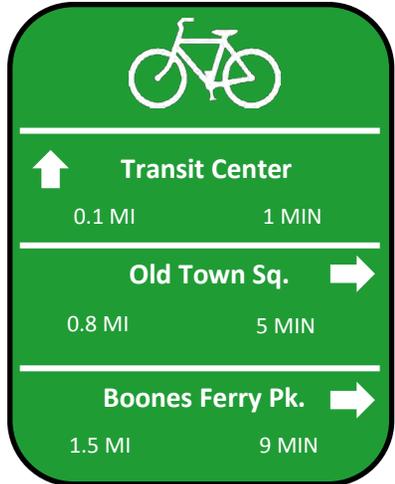
NOTES

Place decision sign on the south side of Barber Street 15 feet west of Kinsman Road. Measured from the western-most edge of crosswalk width across Barber Street.

Initial 30 feet due to only straight or right turn needed to reach destinations. Move up 15 feet to avoid placing sign on sidewalk.



Satellite Location View (location to be verified by contractor)



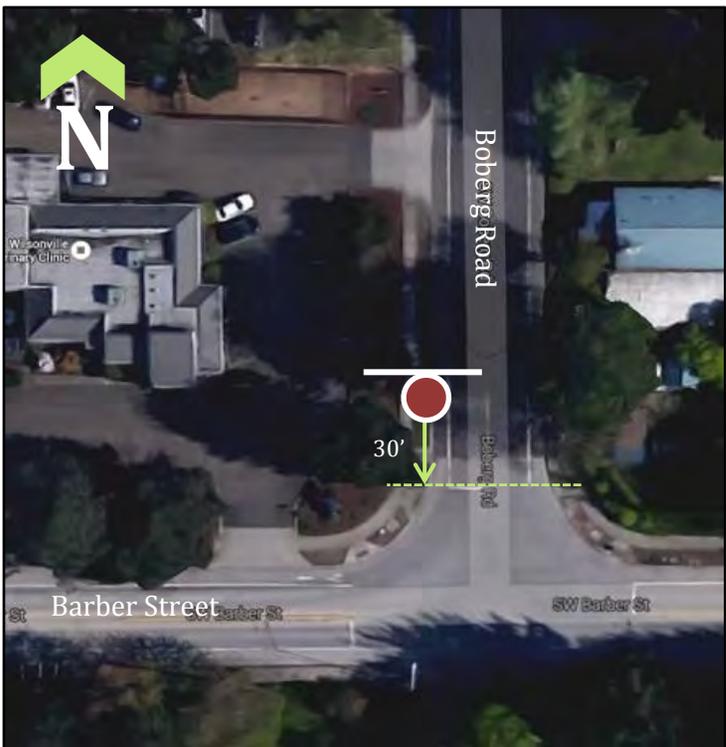
INTERSECTION 34A

INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Barber Street & Boberg Road
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	34a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to Boberg Road
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES
 Place turn sign on the west side of Boberg Road 30 feet north of Barber Street. Measured from where pavement markings end along Boberg Road.

30 feet due to only right turn needed to reach destinations.



Satellite Location View (location to be verified by contractor)



INTERSECTION 34B

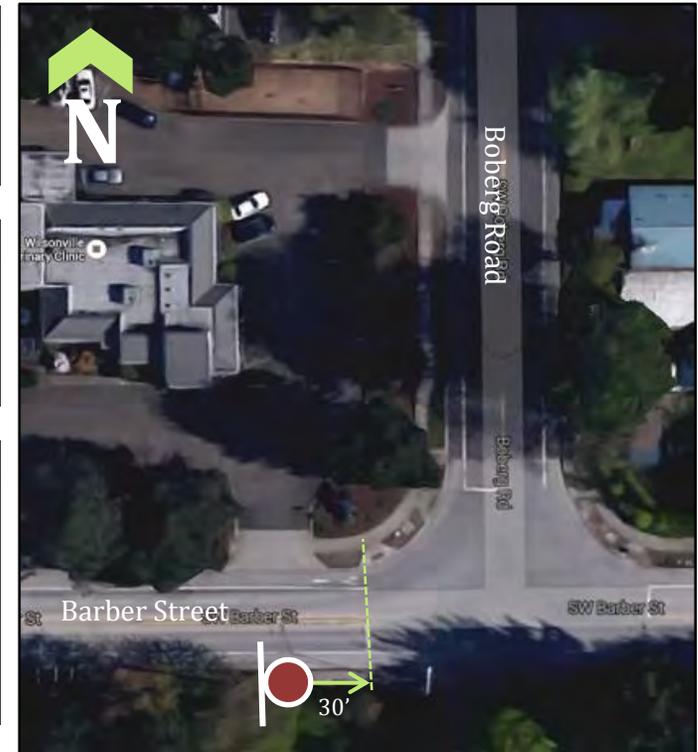
INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Barber Street & Boberg Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	34b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Barber Street
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	2 (<i>Edge of Sidewalk</i>)

NOTES

Place turn sign on the south side of Barber Street 30 feet west of Boberg Road. Measured from the western-most edge of the crosswalk width across Barber Street.

30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations.



Satellite Location View (location to be verified by contractor)

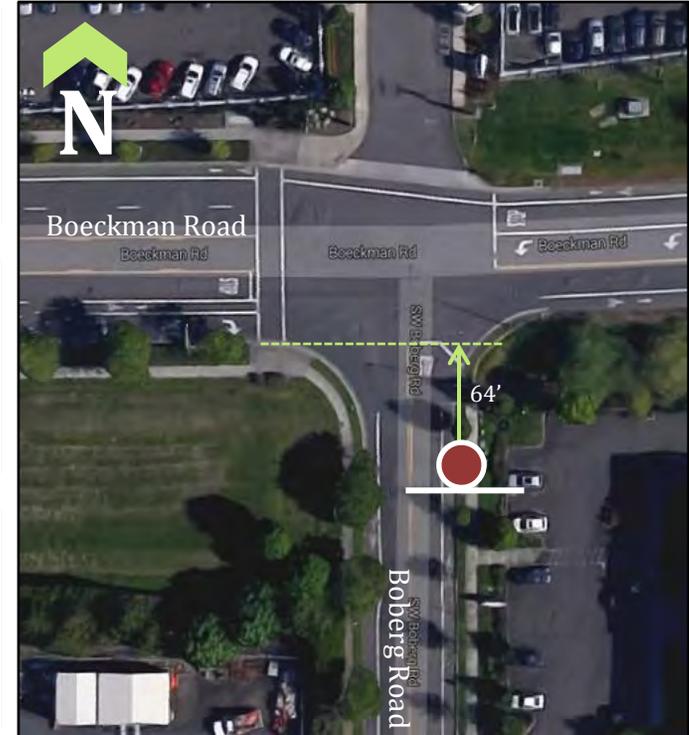


INTERSECTION 35A

INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Boberg Road & Boeckman Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	35a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boberg Road
DISTANCE FROM INTERSECTION (Feet)	64
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES
Place turn sign on the east side of Boberg Road 64 feet south of Boeckman Road. Measured from stop bar on Boberg Road.
<i>Initial 30 feet as a left turn does not require the cyclist to maneuver across a lane of traffic in order to reach destinations. Additional 34 feet to avoid conflict with stop sign.</i>



Satellite Location View (location to be verified by contractor)



INTERSECTION 35B

INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Boberg Road & Boeckman Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	35b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boeckman Road.
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES	
Place turn sign on the south side of Boeckman Road 30 feet west of Boberg Road. Measured from the western-most crosswalk bar across Boeckman Road.	
<i>30 feet due to only right turn needed to reach destinations. Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 36A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	95 th Avenue & Boeckman Road
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	36a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of 95 th Avenue
DISTANCE FROM INTERSECTION (Feet)	189
LATERAL DISTANCE (Feet)	-

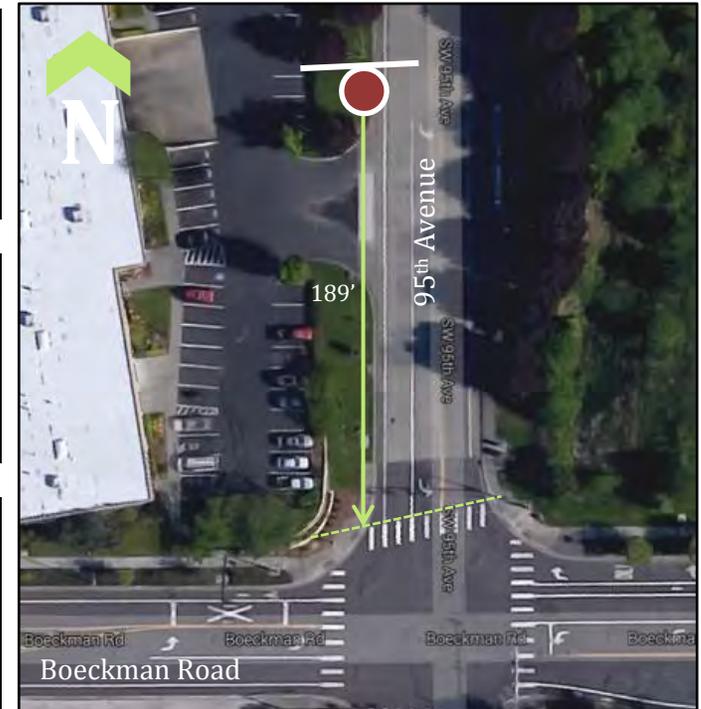
NOTES

Place decision sign on the west side of 95th Avenue 189 feet north of Boeckman Road. Measured from northern-most edge of crosswalk width across 95th Avenue.

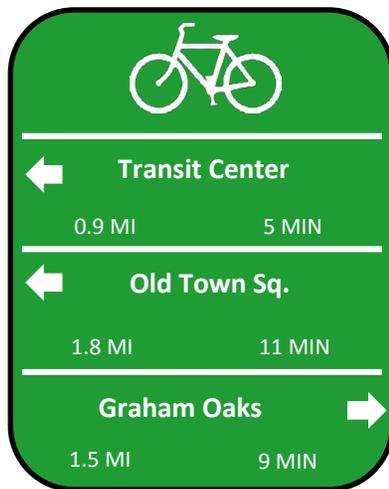
Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left.

Additional 89 feet to avoid conflict with driveway and accommodate using light pole.

Mount on light pole.



Satellite Location View (location to be verified by contractor)

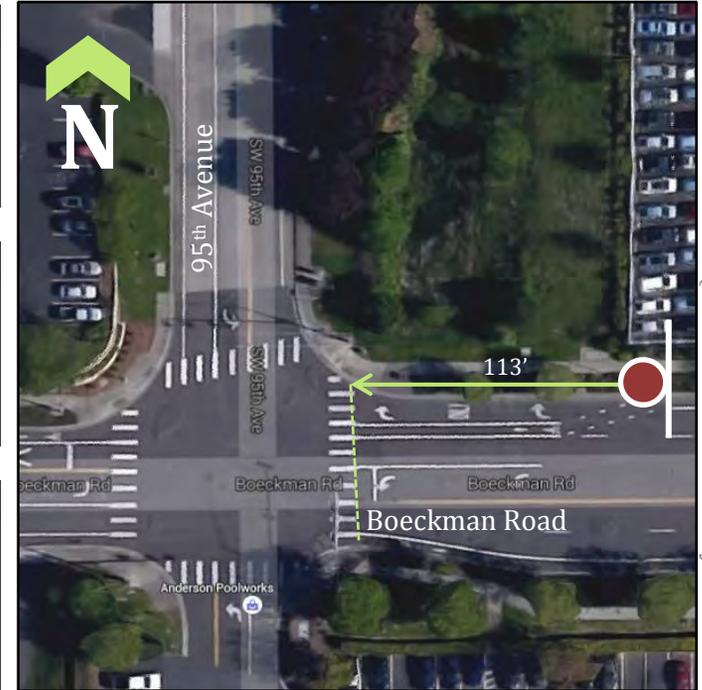


INTERSECTION 36B

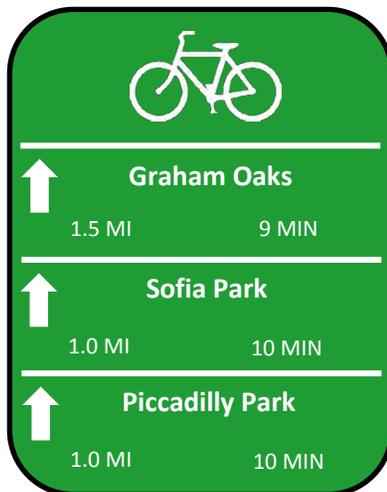
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	95 th Avenue & Boeckman Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	36b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boeckman Road
DISTANCE FROM INTERSECTION (Feet)	113
LATERAL DISTANCE (Feet)	-

NOTES	
Place decision sign on the north side of Boeckman Road 113 feet east of 95 th Avenue. Measured from eastern-most edge of crosswalk width across Boeckman Road. <i>Initial 30 feet due to cyclists continuing straight to reach destinations. Additional 83 feet in to accommodate using light pole.</i> <i>Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)

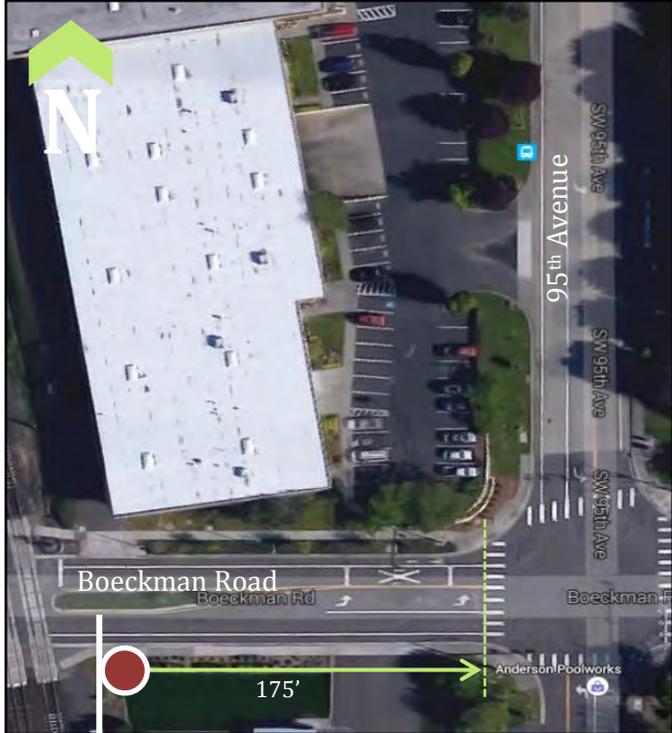


INTERSECTION 36C

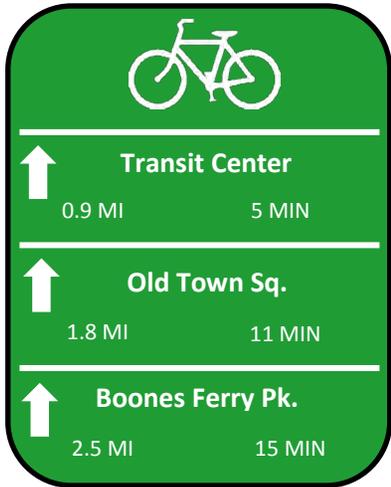
INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	95 th Avenue & Boeckman Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	36c

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boeckman Road
DISTANCE FROM INTERSECTION (Feet)	175
LATERAL DISTANCE (Feet)	3 (Edge Sidewalk)

NOTES	
Place decision sign on the south side of Boeckman Road 175 west of 95 th Avenue. Measured from western-most edge of crosswalk width across Boeckman Road. <i>Initial 30 feet due to cyclists continuing straight to reach destinations. Additional 145 feet to avoid driveway and private property.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 37A

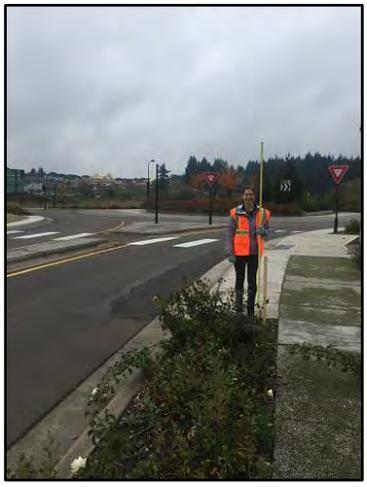
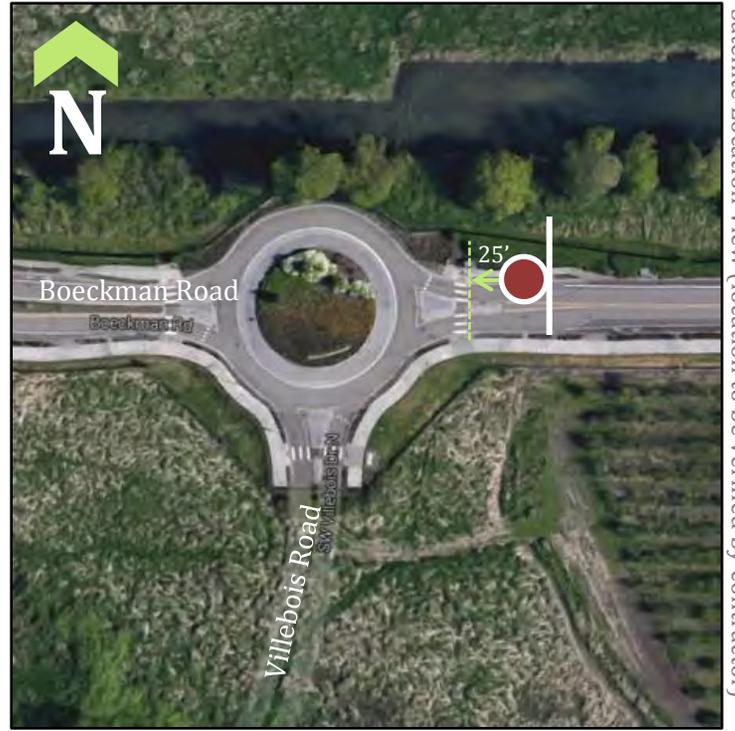
INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Boeckman Road & Villebois Road
DIRECTION OF TRAVEL	West Bound
SIGN NUMBER	37a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boeckman Road
DISTANCE FROM INTERSECTION (Feet)	25
LATERAL DISTANCE (Feet)	1 (Back of Curb)

NOTES

Place turn sign on the north side of Boeckman Road 25 feet east of Villebois Road. Measured from eastern-most edge of crosswalk width across Boeckman Road.

25 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations.



INTERSECTION 37B

INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Boeckman Road & Villebois Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	37b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boeckman Road
DISTANCE FROM INTERSECTION (Feet)	150
LATERAL DISTANCE (Feet)	1 (<i>Back of Curb</i>)

NOTES	
Place decision sign on the south side of Boeckman Road 150 feet west of Villebois Road. Measured from western-most edge of crosswalk width across Boeckman Road.	
<i>Initial 30 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations. Additional 120 feet to allow cyclists to maneuver onto the sidewalk.</i>	



Satellite Location View (Location to be verified by contractor)

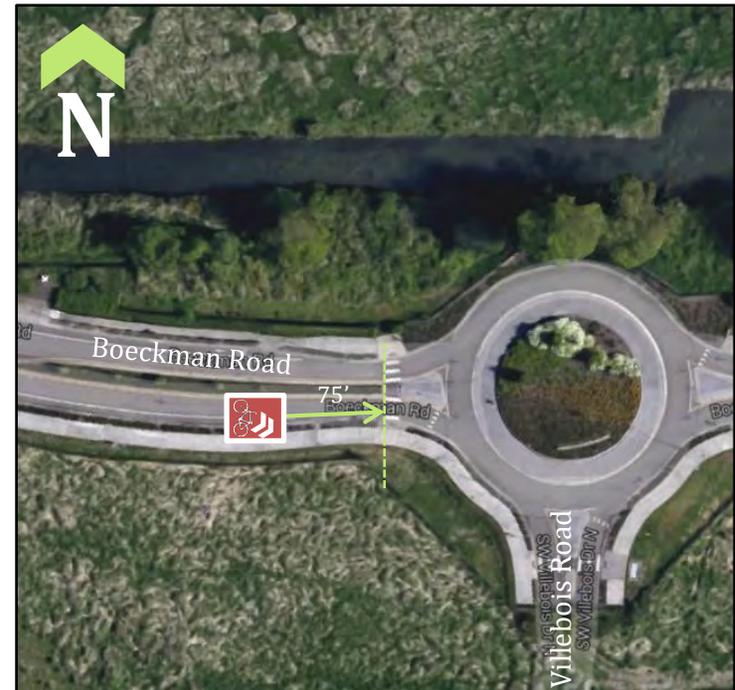


INTERSECTION 37c

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Boeckman Road & Villebois Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	37c

INSTALLATION	
ORIENTATION	Arrows point southeast
DISTANCE FROM INTERSECTION (Feet)	75
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES	
Place pavement marking on the southern half of Boeckman Road 75 feet west of Villebois Road. Measured from western-most edge of crosswalk width across Boeckman Road.	
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on sidewalk to reach destinations.</i>	



Satellite Location View (Location to be verified by contractor)

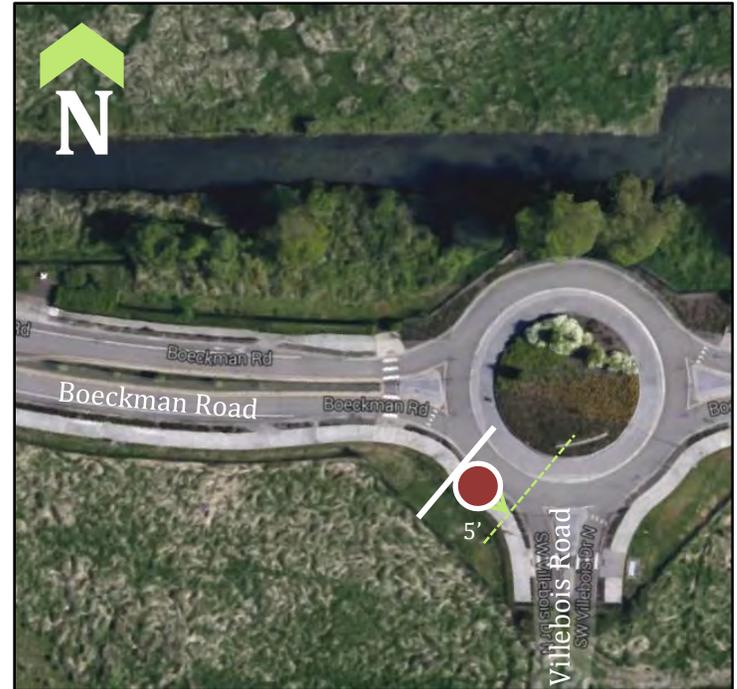


INTERSECTION 37D

INFORMATION	
SIGN TYPE	On-Street Turn with Destination
INTERSECTION	Boeckman Road & Villebois Road
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	37d

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of the roundabout sidewalk
DISTANCE FROM INTERSECTION (Feet)	5
LATERAL DISTANCE (Feet)	-

NOTES	
Place turn sign with destinations on the south side of Boeckman Road 5 feet west of roundabout turn off for Villebois Road.	
<i>Sign placed to direct bicyclists within the roundabout to specific destinations.</i>	
<i>Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)

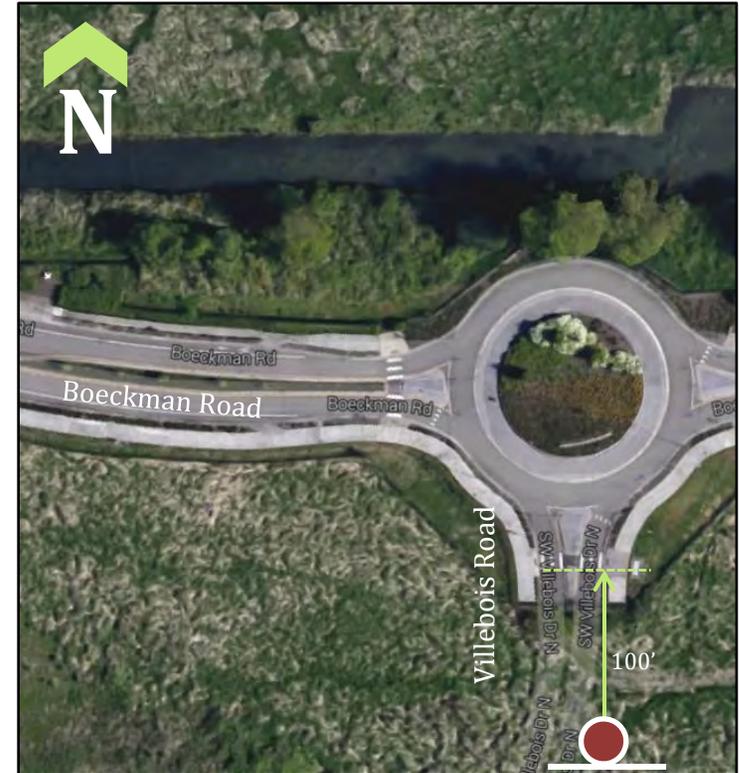


INTERSECTION 37E

INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Boeckman Road & Villebois Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	37e

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Villebois Road
DISTANCE FROM INTERSECTION (Feet)	100
LATERAL DISTANCE (Feet)	1 (<i>Edge of Curb</i>)

NOTES
Place turn sign on the east side of Villebois Road 100 feet south of Boeckman Road. Measure from southern-most edge of crosswalk width across Villebois Road.
<i>Initial 30 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations. Additional 70 feet to allow cyclists to maneuver onto the sidewalk.</i>



Satellite Location View (Location to be verified by contractor)

INTERSECTION 37F

INFORMATION

SIGN TYPE	On-Street Turn with Destinations
INTERSECTION	Boeckman Road & Villebois Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	37f

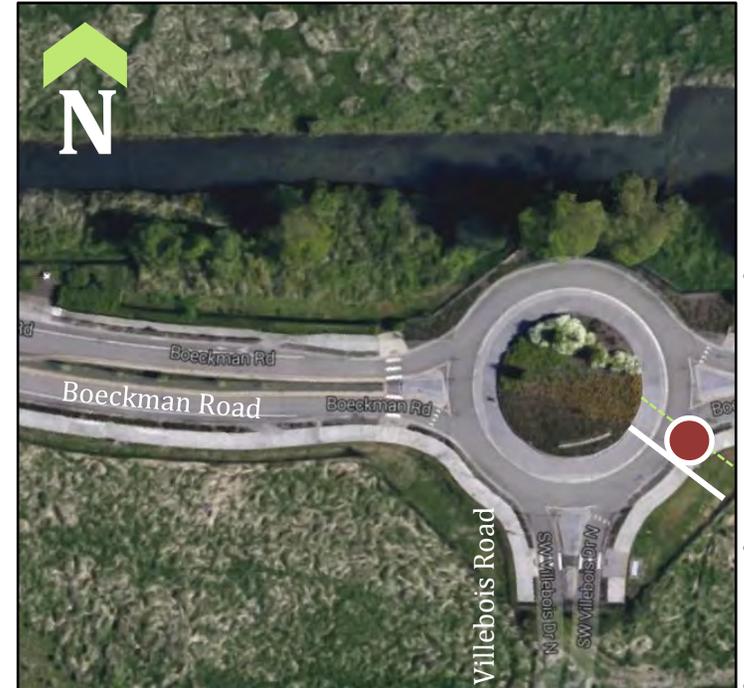
INSTALLATION

ORIENTATION	Sign face is perpendicular to the edge of the roundabout pavement
DISTANCE FROM INTERSECTION (Feet)	-
LATERAL DISTANCE (Feet)	-

NOTES

Place turn sign with destinations on the east side of Villebois Road at the roundabout turn off for Boeckman Road.

*Sign placed to direct bicyclists within the roundabout to specific destinations.
Mount on light pole.*



Satellite Location View (location to be verified by contractor)

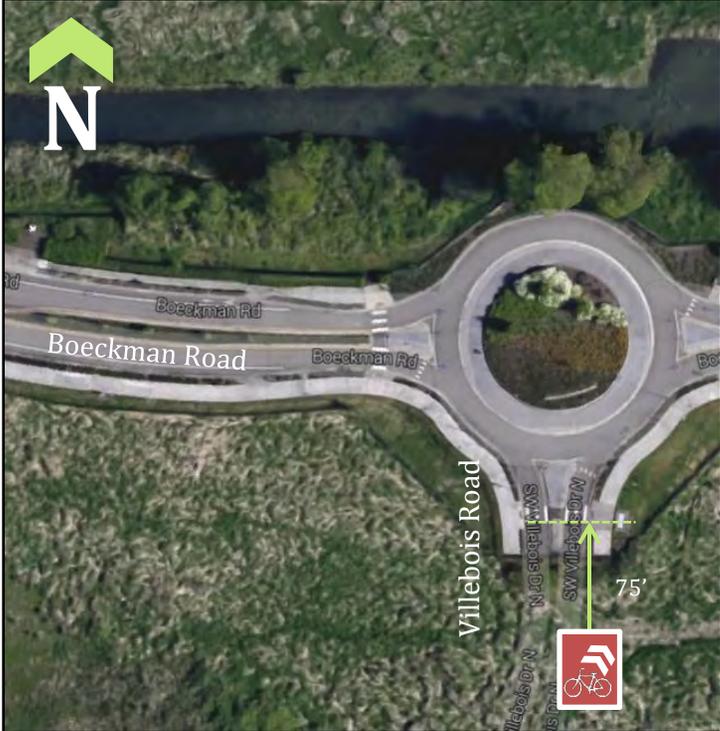


INTERSECTION 37G

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Boeckman Road & Villebois Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	37g

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Villebois Road
DISTANCE FROM INTERSECTION (Feet)	75
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES	
Place pavement marking on the east half of Villebois Road at the roundabout turn off for Boeckman Road. Measured from the southern-most edge of crosswalk width across Villebois Road.	
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on sidewalk to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)

INTERSECTION 38A

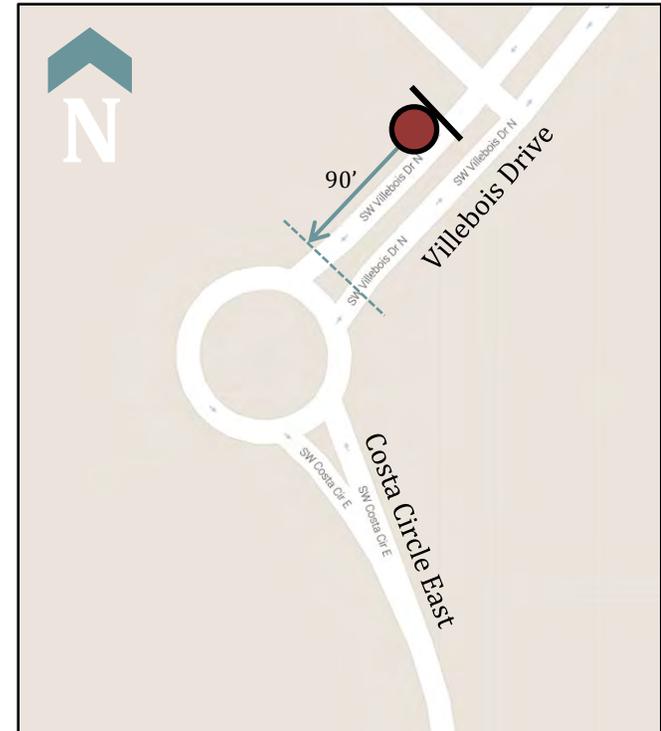
INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Costa Circle East & Villebois Drive
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	38a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Villebois Drive
DISTANCE FROM INTERSECTION (Feet)	90
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES

Place turn sign on the west side of Villebois Drive 90 feet north of Costa Circle East. Measured from the northern-most edge of crosswalk width across Villebois Drive.

Initial 30 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations. Additional 60 feet to allow cyclists to maneuver onto sidewalk.



Satellite Location View (location to be verified by contractor)



INTERSECTION 38B

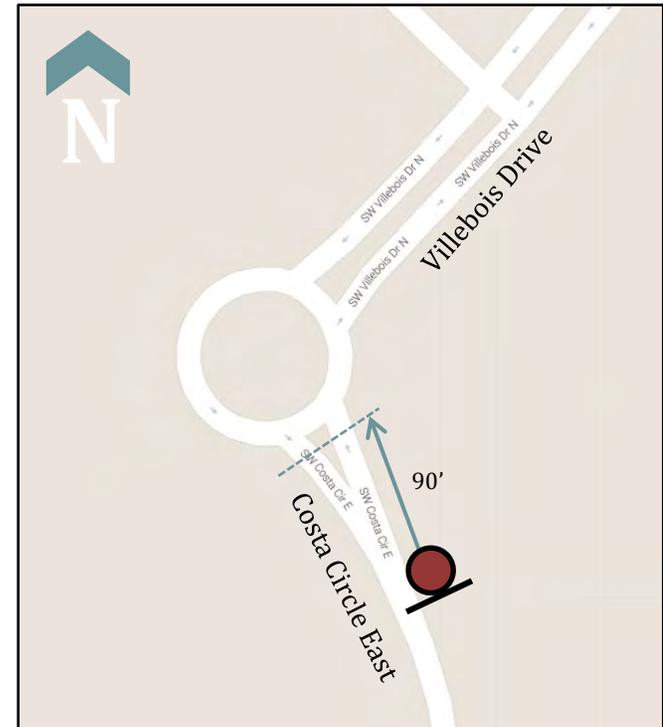
INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Costa Circle East & Villebois Drive
DIRECTION OF TRAVEL	Northwest Bound
SIGN NUMBER	38b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Costa Circle East
DISTANCE FROM INTERSECTION (Feet)	90
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES

Place turn sign on the east side of Costa Circle East 90 feet south of Villebois Drive. Measured from eastern-most edge of crosswalk width across Costa Circle East.

Initial 30 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations. Additional 60 feet to allow cyclists to maneuver onto sidewalk.



INTERSECTION 38C

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Costa Circle East & Villebois Drive
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	38c
INSTALLATION	
ORIENTATION	Arrows point southwest
DISTANCE FROM INTERSECTION (Feet)	50
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)
NOTES	
Place turn sign on the western half of Villebois Drive 50 feet north of Costa Circle East. Measured from the northern-most edge of crosswalk width across Villebois Drive.	
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on sidewalk to reach destinations.</i>	



Satellite Location View (Location to be verified by contractor)

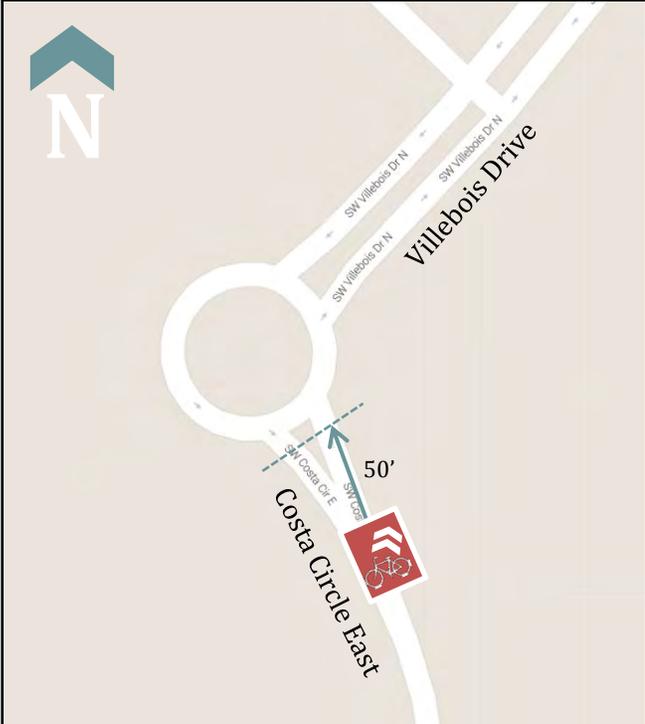


INTERSECTION 38D

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Costa Circle East & Villebois Drive
DIRECTION OF TRAVEL	Northwest Bound
SIGN NUMBER	38d

INSTALLATION	
ORIENTATION	Arrows point north
DISTANCE FROM INTERSECTION (Feet)	50
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES
Place turn sign on the eastern half of Costa Circle East 50 feet south of Villebois Drive. Measured from the southern-most edge of crosswalk width across Costa Circle East.
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on sidewalk to reach destinations.</i>



Satellite Location View (location to be verified by contractor)

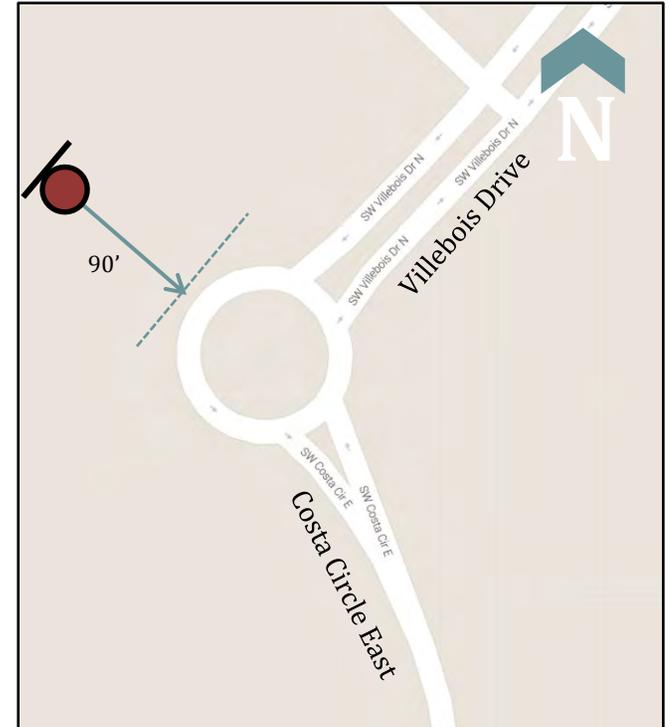


INTERSECTION 38E

INFORMATION	
SIGN TYPE	On-Street Turn
INTERSECTION	Costa Circle East & Villebois Drive
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	38e

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Costa Circle East
DISTANCE FROM INTERSECTION (Feet)	90
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES
Place turn sign on the south side of Costa Circle East 90 feet west of Villebois Drive. Measured from western-most edge of crosswalk width across Costa Circle East.
<i>Initial 30 feet due to the roundabout, cyclists will not have to turn left in order to reach destinations. Additional 60 feet to allow cyclists to maneuver onto sidewalk.</i>



Satellite Location View (location to be verified by contractor)

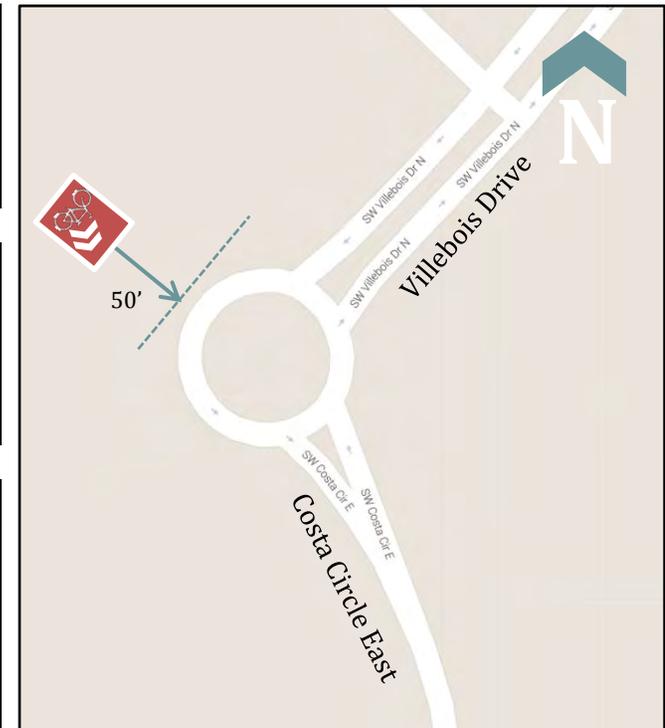


INTERSECTION 38F

INFORMATION	
SIGN TYPE	On-Street Pavement Marking
INTERSECTION	Costa Circle East & Villebois Drive
DIRECTION OF TRAVEL	East Bound
SIGN NUMBER	38f

INSTALLATION	
ORIENTATION	Arrows point south
DISTANCE FROM INTERSECTION (Feet)	50
LATERAL DISTANCE (Feet)	4 (On Street, from Edge of Curb)

NOTES	
Place turn sign on the southern half of Costa Circle East 50 feet west of Villebois Drive. Measured from the western-most edge of crosswalk width across Costa Circle East.	
<i>Pavement marking placed to direct bicyclists to curb cut in order to ride on sidewalk to reach destinations.</i>	



Satellite Location View (location to be verified by contractor)

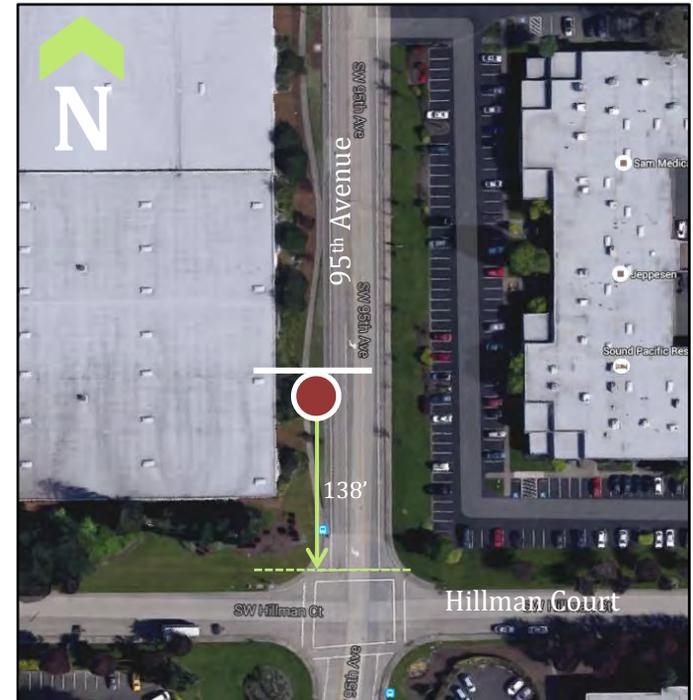


INTERSECTION 39A

INFORMATION	
SIGN TYPE	On-Street Bike Route
INTERSECTION	Hillman Court & 95 th Avenue
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	39a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of 95 th Avenue
DISTANCE FROM INTERSECTION (Feet)	138
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES	
Place bike route sign on the west side of 95 th Avenue 138 feet north of Hillman Court. Measured from northern-most crosswalk bar across 95 th Avenue.	
<i>There should be a confirmation (bike route) sign placed every span of ¼ - ½ mile in the absence of destination or turn signs. Mount on light pole.</i>	



Satellite Location View (location to be verified by contractor)



INTERSECTION 40A

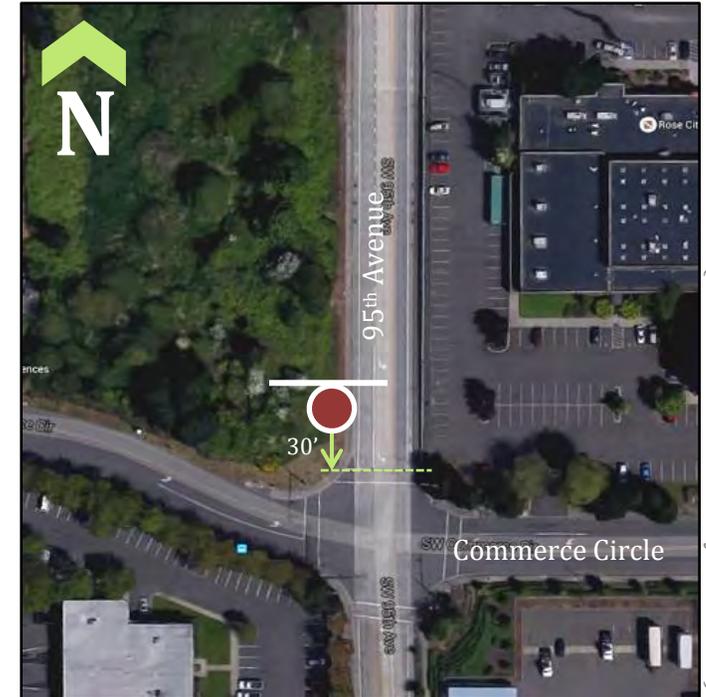
INFORMATION	
SIGN TYPE	On-Street Bike Route
INTERSECTION	Commerce Circle & 95 th Avenue
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	40a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of 95 th Avenue
DISTANCE FROM INTERSECTION (Feet)	30
LATERAL DISTANCE (Feet)	3 (Edge of Sidewalk)

NOTES

Place bike route sign on the west side of 95th Avenue 30 feet north of Commerce Circle. Measured from northern-most crosswalk bar across 95th Avenue.

There should be a confirmation (bike route) sign placed every span of ¼ - ½ mile in the absence of destination or turn signs.

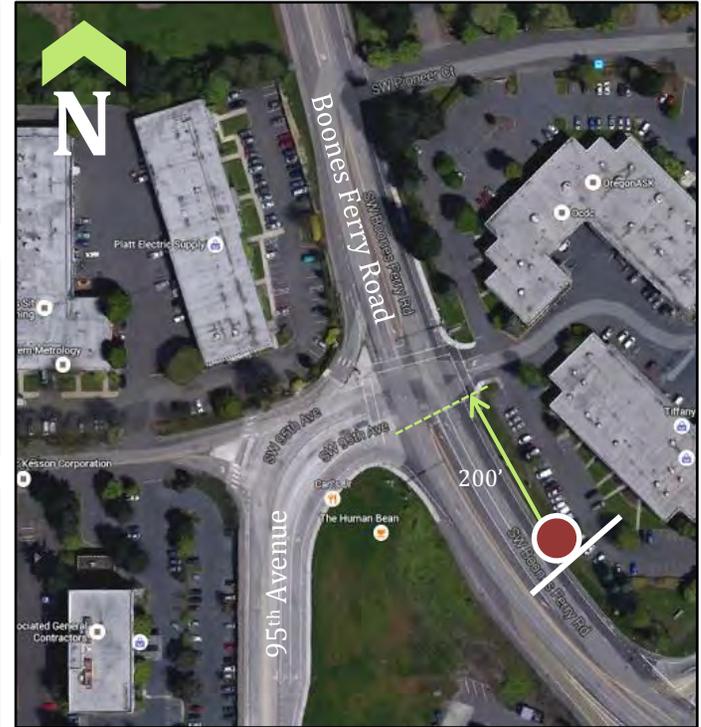


INTERSECTION 41A

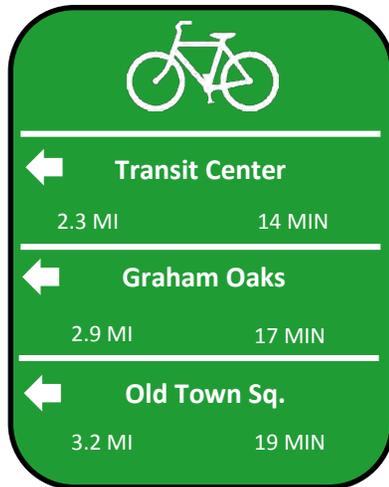
INFORMATION	
SIGN TYPE	On-Street Bike Route Begin and Decision
INTERSECTION	Boones Ferry Road & 95 th Avenue
DIRECTION OF TRAVEL	Northwest Bound
SIGN NUMBER	41a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boones Ferry Road
DISTANCE FROM INTERSECTION (Feet)	200
LATERAL DISTANCE (Feet)	3 (<i>Back of Curb</i>)

NOTES
 Place bike route begin and decision sign on the east side of Boones Ferry Road 200 feet south of 95th Avenue. Stack bike route begin sign above decision sign. Measured from stop bar across Boones Ferry Road.
200 feet due to the need to maneuver across two lanes of traffic in order to turn left.



Satellite Location View (Location to be verified by contractor)



INTERSECTION 41B

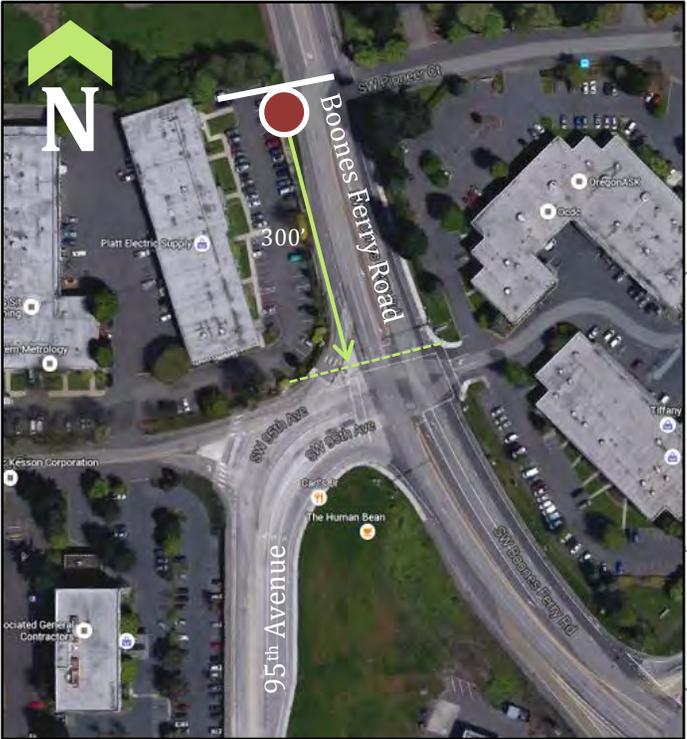
INFORMATION	
SIGN TYPE	On-Street Bike Route Begin and Decision
INTERSECTION	Boones Ferry Road & 95 th Avenue
DIRECTION OF TRAVEL	Southeast Bound
SIGN NUMBER	41b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Boones Ferry Road
DISTANCE FROM INTERSECTION (Feet)	300
LATERAL DISTANCE (Feet)	3 (Back of Curb)

NOTES

Place bike route begin and decision sign on the west side of Boones Ferry Road 300 feet north of 95th Avenue. Stack bike route begin sign above decision sign. Measured from the northern-most crosswalk bar across Boones Ferry Road.

Initial 30 feet due to only right turn needed to reach destinations. Additional 270 feet so the sign is placed where the cyclist must make a decision on direction of travel.



Satellite Location View (location to be verified by contractor)

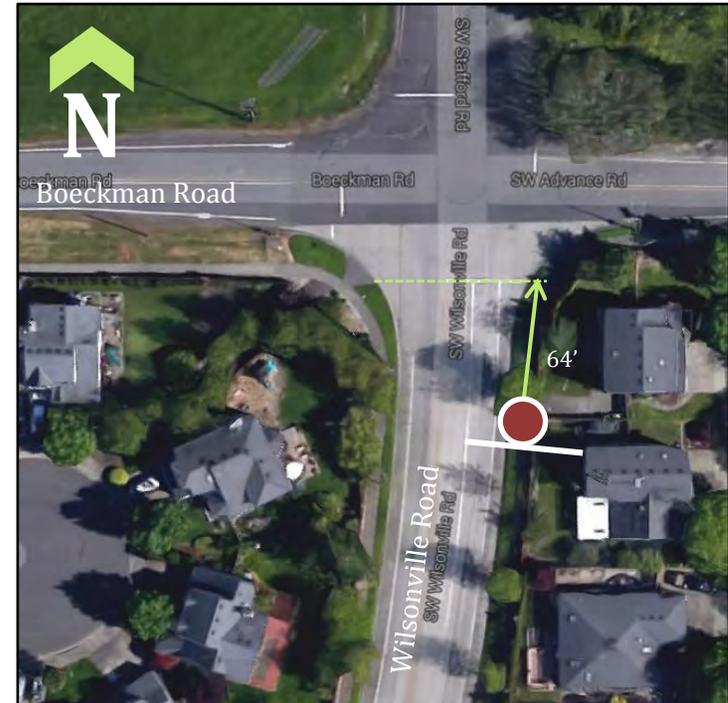


Transit Center	➔
2.3 MI	14 MIN
Graham Oaks	➔
2.9 MI	17 MIN
Old Town Sq.	➔
3.2 MI	19 MIN



INTERSECTION 42A

INFORMATION	
SIGN TYPE	On-Street Bike Route End
INTERSECTION	Boeckman Road & Wilsonville Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	42a
INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	64
LATERAL DISTANCE (Feet)	-
NOTES	
Place bike route end sign on the east side of Wilsonville Road 64 feet south of Boeckman Road. Measured from stop bar on Wilsonville Road.	
<i>Mount on light pole.</i>	



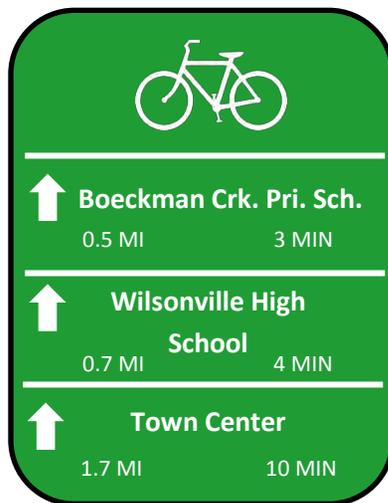
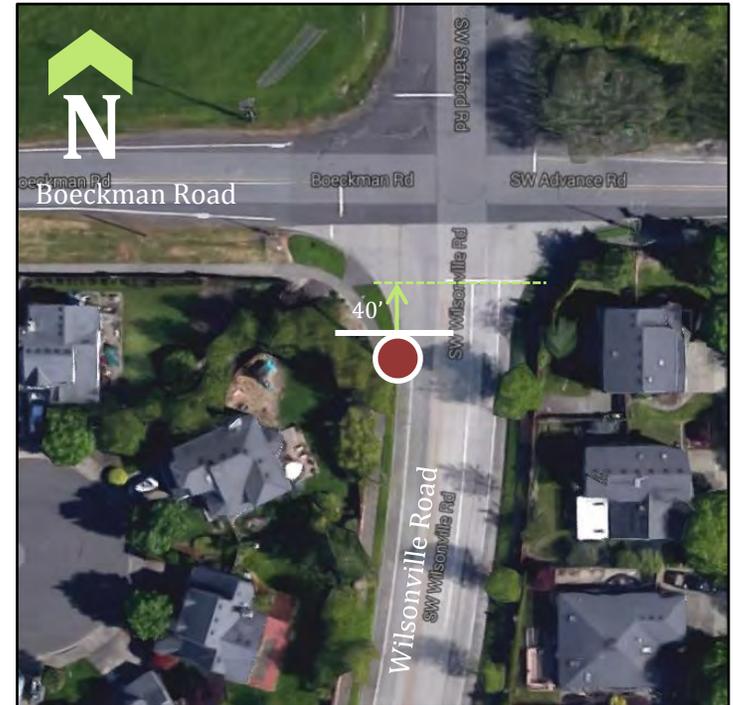
INTERSECTION 42B

INFORMATION	
SIGN TYPE	On-Street Bike Route Begin and Decision
INTERSECTION	Boeckman Road & Wilsonville Road
DIRECTION OF TRAVEL	South Bound
SIGN NUMBER	42b

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Wilsonville Road
DISTANCE FROM INTERSECTION (Feet)	40
LATERAL DISTANCE (Feet)	-

NOTES
Place bike route begin and decision sign on the west side of Wilsonville Road 40 feet south of Boeckman Road. Stack bike route begin sign above decision sign. Measured from stop bar on Wilsonville Road.

Mount on light pole.



BICYCLE WAYFINDING DESIGN GUIDANCE

In order to maintain uniformity and continuity with City of Portland and surrounding municipalities' bicycle wayfinding signage, standards must be put in place and maintained throughout the system. For this reason, design guidance was taken from the Manual on Uniform Traffic Control Devices (MUTCD), National Association of City Transportation Officials (NACTO) Urban Bikeway Design Guide, the Oregon Department of Transportation, as well as City of Portland and City of Milwaukie bicycle wayfinding documents.

MUTCD

The 2009 edition of the MUTCD was used when preparing this document. The MUTCD provides standards, options, and guidance for the design of bicycle wayfinding signage. Standards are usually required or mandatory practice for traffic control devices. Options are permissive conditions that are not necessarily required, but are good practice. Finally, guidance is a recommended practice where deviation is allowed if engineering judgment deems deviation appropriate.

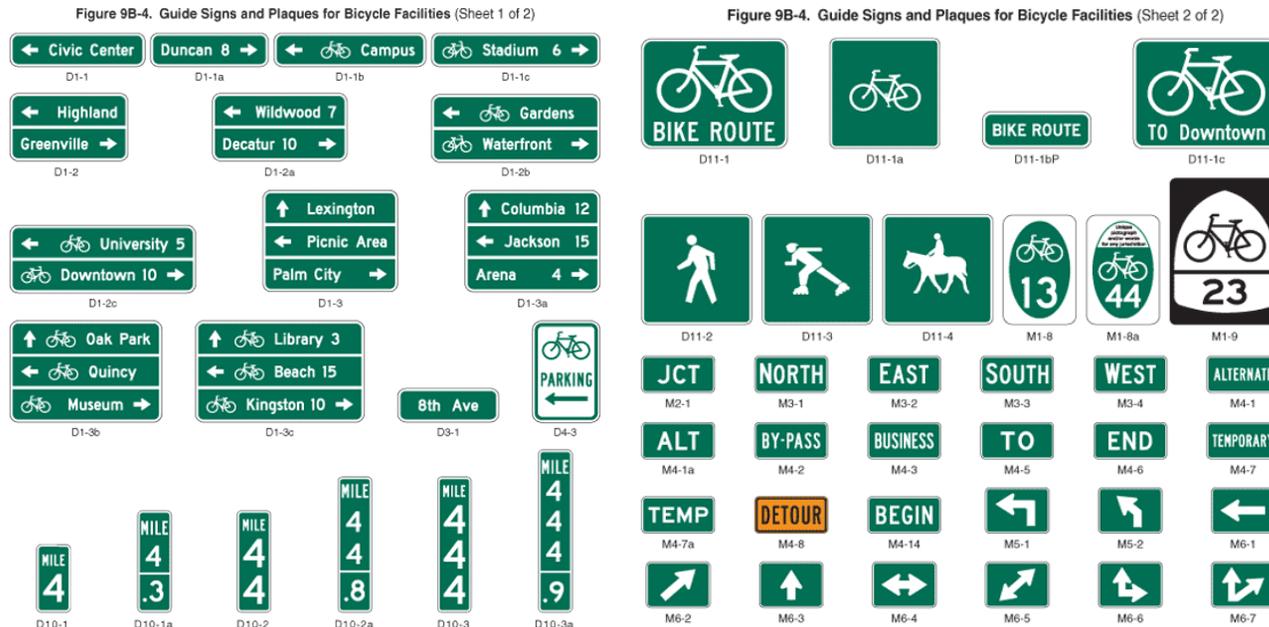


Figure 1. MUTCD Figure 9B-4. Guide Signs and Plaques for Bicycle Facilities

STANDARD

An arrow pointing to the right, if used, shall be at the extreme right-hand side of the sign. An arrow pointing left or up, if used, shall be at the extreme left-hand side of the sign. The distance numerals, if used, shall be placed to the right of the destination names.

On Bicycle Turn Signs with Destinations, a bicycle symbol shall be placed next to each destination or group of destinations. If an arrow is at the extreme left, the bicycle symbol shall be placed to the right of the respective arrow.

If more than one destination is displayed in the same direction, the name of a nearer destination shall be displayed above the name of a destination that is further away.

GUIDANCE

Adequate separation should be made between any destination or group of destinations in one direction and those in other directions by suitable design of the arrow, spacing of lines of legend, heavy lines entirely across the sign, or separate signs.

Unless a sloping arrow will convey a clearer indication of the direction to be followed, the directional arrows should be horizontal or vertical.

The bicycle symbol should be to the left of the destination legend.

If several individual name signs are assembled into a group, all signs in the assembly should have the same horizontal width.

Because of their smaller size, Bicycle Decision signs should not be used as a substitute for vehicular destination signs when the message is also intended to be seen by motorists.

The closest destination lying straight ahead should be at the top of the sign or assembly, and below it the closest destinations to the left and to the right, in that order.

OPTION

Bike Route Guide (D11-1) shown in Figure 1 may be provided along designated bicycle routes to inform bicyclists of bicycle route direction changes and to confirm route direction, distance, and destination.

If used, Bike Route Guide signs may be repeated at regular intervals so that bicyclists entering from side streets will have an opportunity to know that they are on a bicycle route. Similar guide signing may be used for shared roadways with intermediate signs placed for bicyclist guidance.

Destination (D1-1, D1-1a) signs, Street Name (D3-1) signs, or Bicycle Destination (D1-1b, D1-1c, D1-2b, D1-2c, D1-3b, D1-3c) signs (see Figure 9B-4) may be installed to provide direction, destination, and distance information as needed for bicycle travel. If several destinations are to be shown at a single location, they may be placed on a single sign with an arrow (and the distance, if desired) for each name. If more than one destination lies in the same direction, a single arrow may be used for the destinations.

PLACEMENT STANDARDS

VERTICAL CLEARANCE

Mounting height for post-mounted signs shall be a minimum of 7 feet, measured vertically from the bottom of the sign to the elevation of the near edge ground surface.

HORIZONTAL CLEARANCE

For "Roadside Sign in Business, Commercial, or Residential Area" the distance from the edge of the pavement to the near edge of the sign is shown as a minimum dimension of 2 feet.

NACTO URBAN BIKEWAY DESIGN GUIDE

The NACTO Urban Bikeway Design Guide includes a chapter on Bike Route Wayfinding Signage and Markings System(s) which outlines various requirements, recommendations, and options for bicycle wayfinding networks. The chapter begins by defining three typical sign types commonly found in these networks. These include confirmation signs, turn signs, and decision signs.

CONFIRMATION SIGNS

Confirmation signs are used to tell cyclists that they are on a designated bikeway. They may include destinations as well as the distance and time to those destinations, but no directional arrows are placed on these signs. Placement of confirmation signs should be "Every ¼ to ½ mile on off-street facilities and every 2 to 3 blocks along bicycle facilities, unless another type of sign is used (e.g., within 150 feet of a turn or decision sign)."

TURN SIGNS

Turn signs indicate to cyclists where the bikeway turns from one street to another and include only destinations and arrows. They should be placed on the near side of intersections in which bike routes turn. Many cities exclude the destinations from these signs and simply indicate to cyclists that they are on a bike route and it turns either left or right.

DECISION SIGNS

Decision signs occur at the intersection of two or more bikeways. They inform cyclists of up to three destinations, distance to those destinations, directional arrows, and often the time to specific destinations. These signs should be placed on the near side of intersections where multiple bicycle routes meet. They are also used along routes to indicate that a destination is nearby.

REQUIRED

The NACTO Urban Bikeway Design Guide suggests that cities follow MUTCD standards, as outlined above.

RECOMMENDED

Decision signs should be placed in advance of all turns (near side of the intersection) or decision points along the bicycle route.

Decision signs should include destinations, directional arrows, and distance. Travel time required to reach the destination provides bicyclists with additional information may also be included. It is recommended that a 10 mph bicycle speed be used for travel time calculations.

Place the closest destination to each sign in the top slot. Destinations that are further away can be placed in slots two and three. This allows the nearest destination to “fall off” the sign and subsequent destinations to move up the sign as the bicyclist approaches. For longer routes, show intermediate destinations rather than include all destinations on a single sign.

Turn signs should be placed on the near-side of the intersection to indicate where the bike route turns.

Clearview Hwy font is recommended, as it is commonly used for guide signs in the United States.

OPTIONAL

Signs may be placed on “feeder” streets between the bicycle route and nearby destinations.

Bicycle route signs may be periodically placed along bike routes to provide additional wayfinding benefits to users.

Conventional street name signs along bicycle routes may be redesigned to incorporate the street’s identity as a bicycle route.

The placement of wayfinding signs may be limited specifically to the designated bicycle network, as other streets may be difficult or dangerous for bicyclists.

Pavement markings may be used to help reinforce routes and directional signage. Pavement markings may be useful where signs are difficult to see (due to vegetation or parked cars) and can help bicyclists navigate difficult turns and provide route reinforcement. Pavement markings may also be a standard component of bicycle routes.

There is no standard color for bicycle wayfinding signage. Section 1A.12 of the MUTCD establishes the general meaning for signage colors. Green is the color used for directional guidance and is the most common color of bicycle wayfinding signage in the US, including those included in the MUTCD.

Signed bicycle routes may be partnered with a printed or on-line bicycle route map. Many online services, such as Google, now offer bicycle route mapping that may differ from signed routes. Cities may wish to consider such advancements in technology when planning wayfinding programs.

ODOT, CITY OF PORTLAND, CITY OF MILWAUKIE

ODOT's Sign Policy document contains standard detail drawings of the OBD1-3c directional sign, shown in Figure 2, which is approved for use across the state of Oregon. These signs are also used by the City of Portland and the City of Milwaukie, and are intended to be used in place of the signs shown in the MUTCD. The signs are green and measure 24 inches by 30 inches. At the top is the bicycle symbol, with room for up to three destinations below. Standard detail is provided for these signs in the appendix of this document.

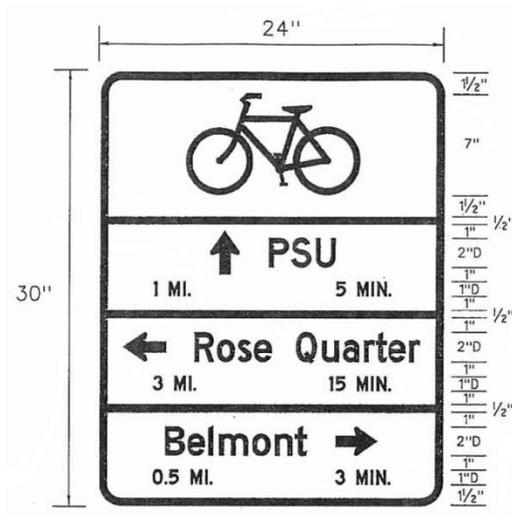


Figure 2. ODOT Approved Bicycle Wayfinding Sign OBD1 1-3c

REQUIRED

Ground mounted signs not protected by guardrail or barrier should be installed on breakaway posts. Any sign support that could be struck by a vehicle should be of the breakaway type. Nominal 4" x 4" wood posts are considered to be breakaway.

RECOMMENDATIONS

The City of Wilsonville should adopt ODOT's bikeway sign guidance in order to maintain uniformity with the greater Portland region. This will allow cyclists to travel with ease between cities, with recognizable signage for those cycling between Wilsonville and Portland.

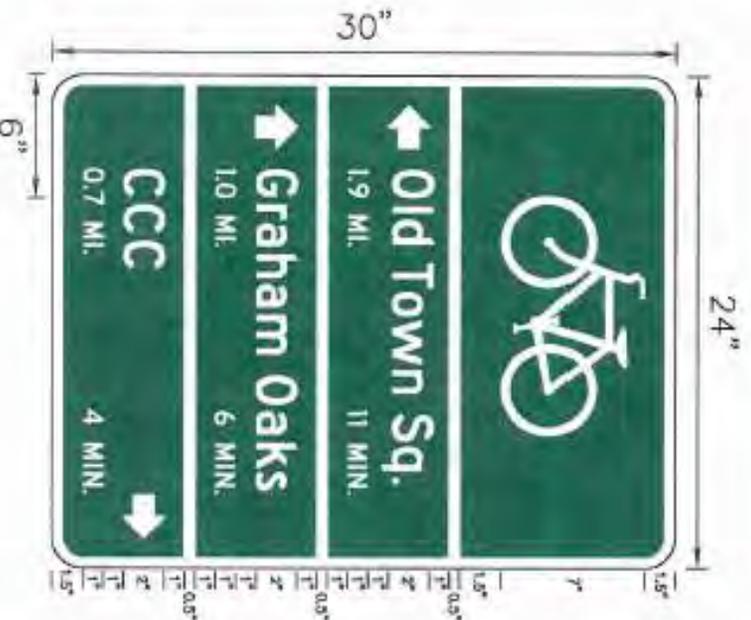
Destination names may be abbreviated to accommodate long destination names. This ensures that the text is large enough to be legible by fast-moving cyclists. It is preferred that destination names fit on a single line.

Placement guidelines suggest that signs be placed 30 feet before the intersection if cyclists do not need to change lanes to stay on their route, 100 feet if cyclists have to cross over one lane of traffic, and 200 feet if cyclists have to cross two or more lanes of traffic.

As the City of Wilsonville grows, the bicycle network and wayfinding signage must be continuously updated. This report includes guidance for an initial system which should be amended as the city expands.

APPENDIX A: WAYFINDING SIGNAGE DETAILS

This Detail Drawing may not be altered or changed in any manner except by the City Engineer. It is the responsibility of the user to acquire the most current version.



NOTES:

- Border = 1/2"
- Radius = 1 1/2"
- 2" x 2" Arrows
- SIGN BACKGROUND: Green, Retro- Reflective
- SIGN LEGEND: White, Retro- Reflective
- LETTERING:
 - All lettering should conform to standard Clearview Highway font.
 - Destination names shall be 2" in height and Justified left.
 - Mileage and ride time shall 1" in height.

Bicycle Decision Sign

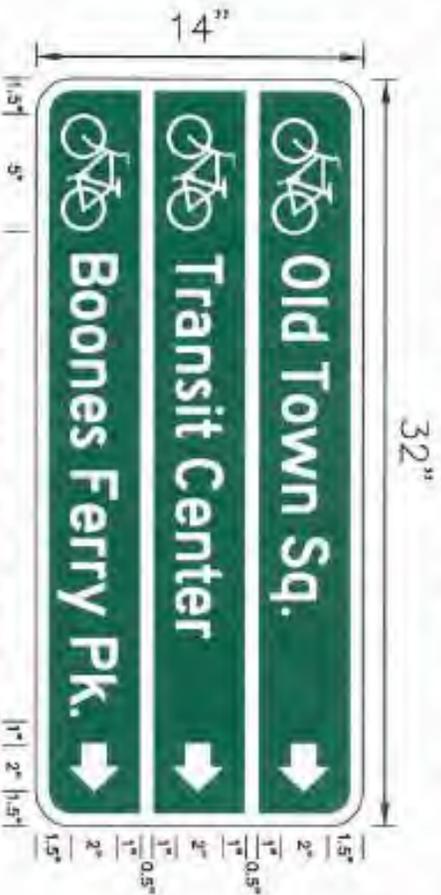
DRAWING NUMBER: X-XXXX	DRAWN BY: BB	SCALE: N.T.S.
FILE NAME: X-XXXX.dwg	APPROVED BY:	DATE: 12/30/15

CITY OF WILSONVILLE



PUBLIC WORKS STANDARDS

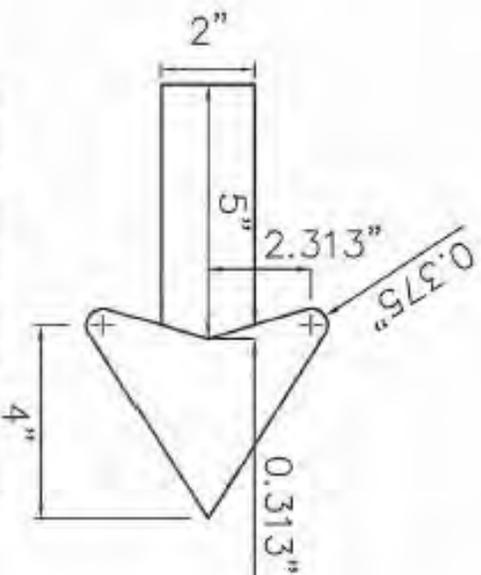
This Detail Drawing may not be altered or changed in any manner except by the City Engineer. It is the responsibility of the user to acquire the most current version.



BIKE TURN SIGN WITH DESTINATIONS



BIKE TURN SIGN



TURN ARROW DIMENSIONS

NOTES:

BIKE TURN SIGN

Border = 0.375"

All bike turn signs should be accompanied by and placed below a Bike Route sign.

BIKE TURN SIGN WITH DESTINATIONS

Border = 1/2"
2" x 2" Arrows

SIGN BACKGROUND: Green, Retro- Reflective

SIGN LEGEND: White, Retro- Reflective

LETTERING

All lettering should conform to standard Clearview Highway font.

Destination names shall be 2" in height and justified left.

Bicycle Turn Signs

DRAWING NUMBER: X-XXXX

DRAWN BY: BB

SCALE: N.T.S.

CITY OF
WILSONVILLE



FILE NAME: X-XXXX.dwg

APPROVED BY:

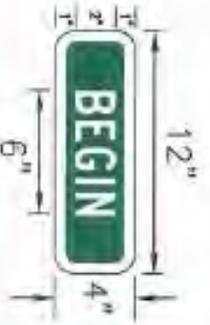
DATE: 1/20/15

PUBLIC WORKS STANDARDS

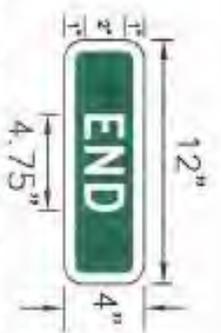
This Detail Drawing may not be altered or changed in any manner except by the City Engineer. It is the responsibility of the user to acquire the most current version.



BIKE ROUTE SIGN



SUPPLEMENTAL PLAQUE: BEGIN



SUPPLEMENTAL PLAQUE: END

NOTES:

BIKE ROUTE SIGN
 Radius = 1 1/2"
 Border = 1/2"

SIGN BACKGROUND: Green, Retro- Reflective
SIGN LEGEND: White, Retro- Reflective

SUPPLEMENTAL PLAQUES
 Radius = 1"
 Border = 1/2"

LETTERING
 All lettering should conform to standard
 Clearview Highway font.

All supplemental plaques should be accompanied by and placed above a Bike Route sign.

Bike Route Sign			<p>CITY OF WILSONVILLE PUBLIC WORKS STANDARDS</p>
DRAWING NUMBER: X-XXXX	DRAWN BY: BB	SCALE: N.T.S.	
FILE NAME: X-XXXX.dwg	APPROVED BY:	DATE: 1/20/16	

This Detail Drawing may not be altered or changed in any manner except by the City Engineer. It is the responsibility of the user to acquire the most current version.



NOTES:

Radius = 1.5"
Border = .375"

SIGN BACKGROUND: White, Retro- Reflective
SIGN LEGEND: Black

LETTERING

All lettering should conform to standard
Clearview Highway font.

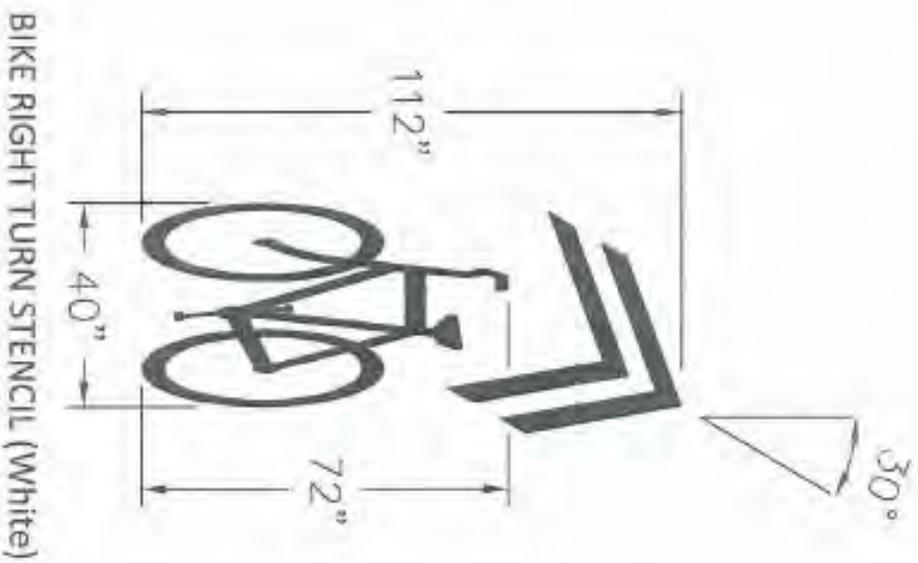
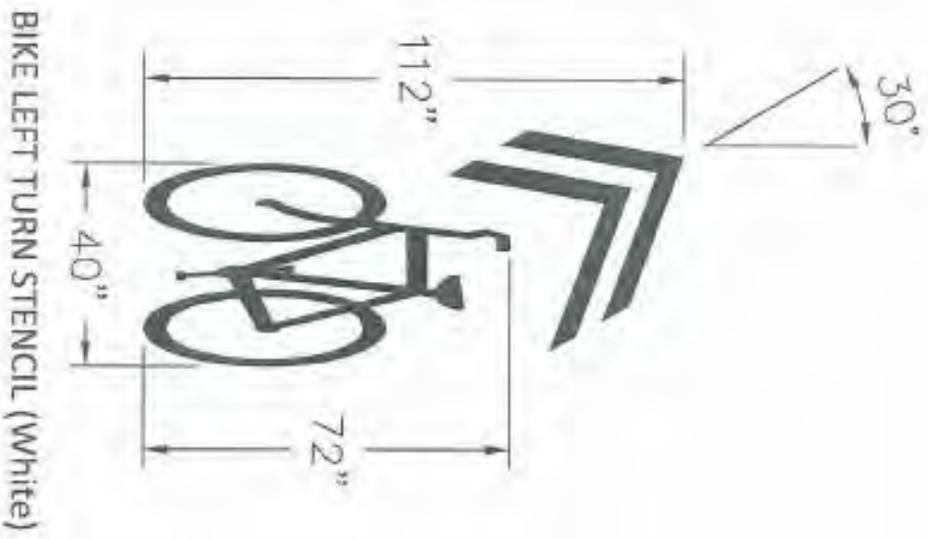
Bike Route on Sidewalk Sign

DRAWING NUMBER: X-XXXX	DRAWN BY: BB	SCALE: N.T.S.
FILE NAME: X-XXXX.dwg	APPROVED BY:	DATE: 1/20/16

CITY OF WILSONVILLE
PUBLIC WORKS STANDARDS



This detail drawing may not be altered or changed in any manner except by the City Engineer. It is the responsibility of the user to acquire the most current version.



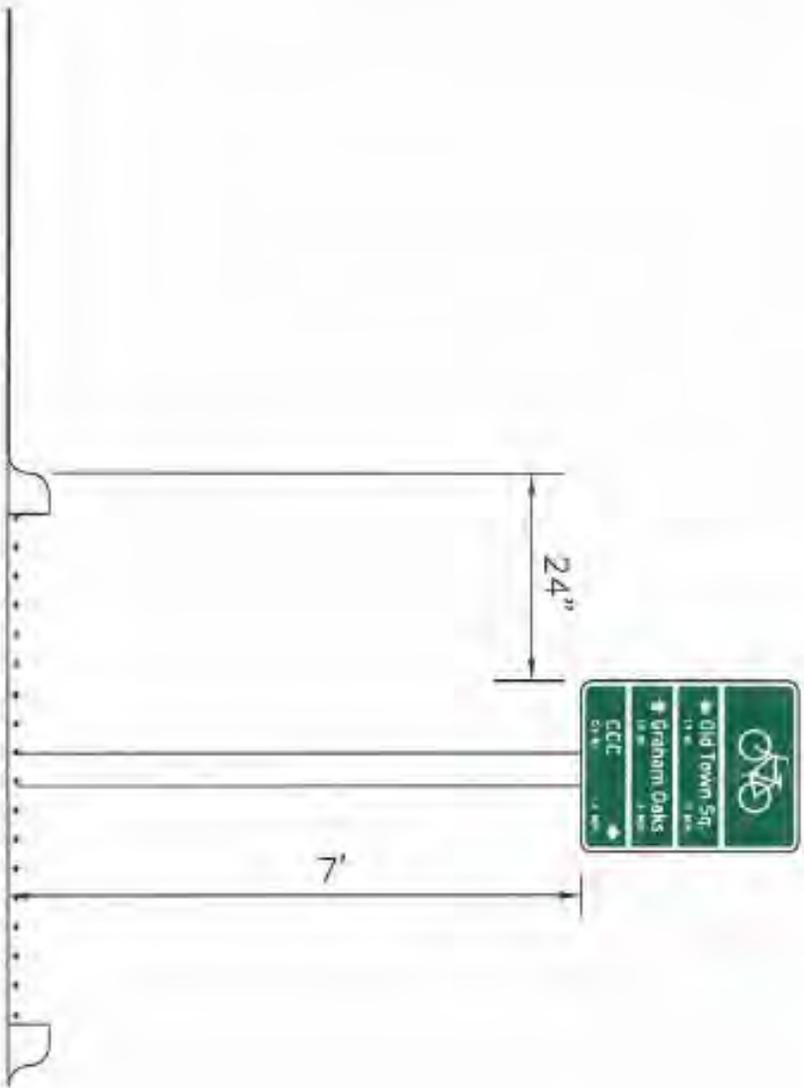
Bike Turn Pavement Markings

DRAWING NUMBER: X-XXXX	DRAWN BY: BB	SCALE: N.T.S.
FILE NAME: X-XXXX.dwg	APPROVED BY:	DATE: 1/19/16

CITY OF
WILSONVILLE
PUBLIC WORKS STANDARDS



This Detail Drawing may not be altered or changed in any manner except by the City Engineer. It is the responsibility of the user to acquire the most current version.



NOTES:

Mounting height for post-mounted signs shall be a minimum of 7 feet measured vertically from the bottom of the sign to the elevation of the near edge ground surface.

The distance measured from the front of curb to the near edge of the sign shall be a minimum of 2 feet.

Bike Sign Mounting

DRAWING NUMBER: X-XXXX	DRAWN BY: BB	SCALE: N.T.S.
FILE NAME: X-XXXX.dwg	APPROVED BY:	DATE: 1/20/16

CITY OF
WILSONVILLE



PUBLIC WORKS STANDARDS

APPENDIX B: MATERIALS LIST

TABLE 5. POLE TYPES USED		
Pole Type	Sign Number	Total Quantity
Existing Sign Pole	17b, 29c, 33a,	3
(No Pole) Thermoplastic	16b, 17a, 17c, 19b, 24b, 29d, 29g, 31b, 31e, 31g, 31i, 32b, 32d, 37c, 37f, 37g,	16
4" x 4" Wood Posts (One Sign per Post)	1a, 1b, 1c, 2a, 2c, 4a, 5a, 5c, 6b, 7a, 7b, 7c, 8c, 9b, 9d, 10a, 12a, 12b, 12c, 14a, 14b, 14c, 15a, 15b, 16a, 18a, 18b, 19a, 20a, 20b, 21b, 22b, 23a, 23c, 25a, 25b, 26a, 27a, 27b, 28a, 28b, 29a, 29b, 29e, 29f, 29h, 30a, 30c, 30d, 31a, 31c, 31d, 31f, 31h, 31j, 32a, 32c, 33b, 33c, 34a, 34b, 35a, 36c, 37a, 37b, 37e, 38a, 38b, 40a, 41a, 41b,	71
4" x 4" Wood Posts (Two Signs per Post)	13a & 13b	1
Traffic Signal Pole	0a	1
Utility Pole	4c, 5b, 21a, 24a,	4
Light Pole		
*Diameters to be Specified	0b, 2b, 3a, 3b, 4b, 6a, 8a, 8b, 9a, 9c, 11a, 11b, 13c, 14d, 20c, 21c, 22a, 23b, 30b, 35b, 36a, 36b, 37d, 39a, 42a, 42b,	26

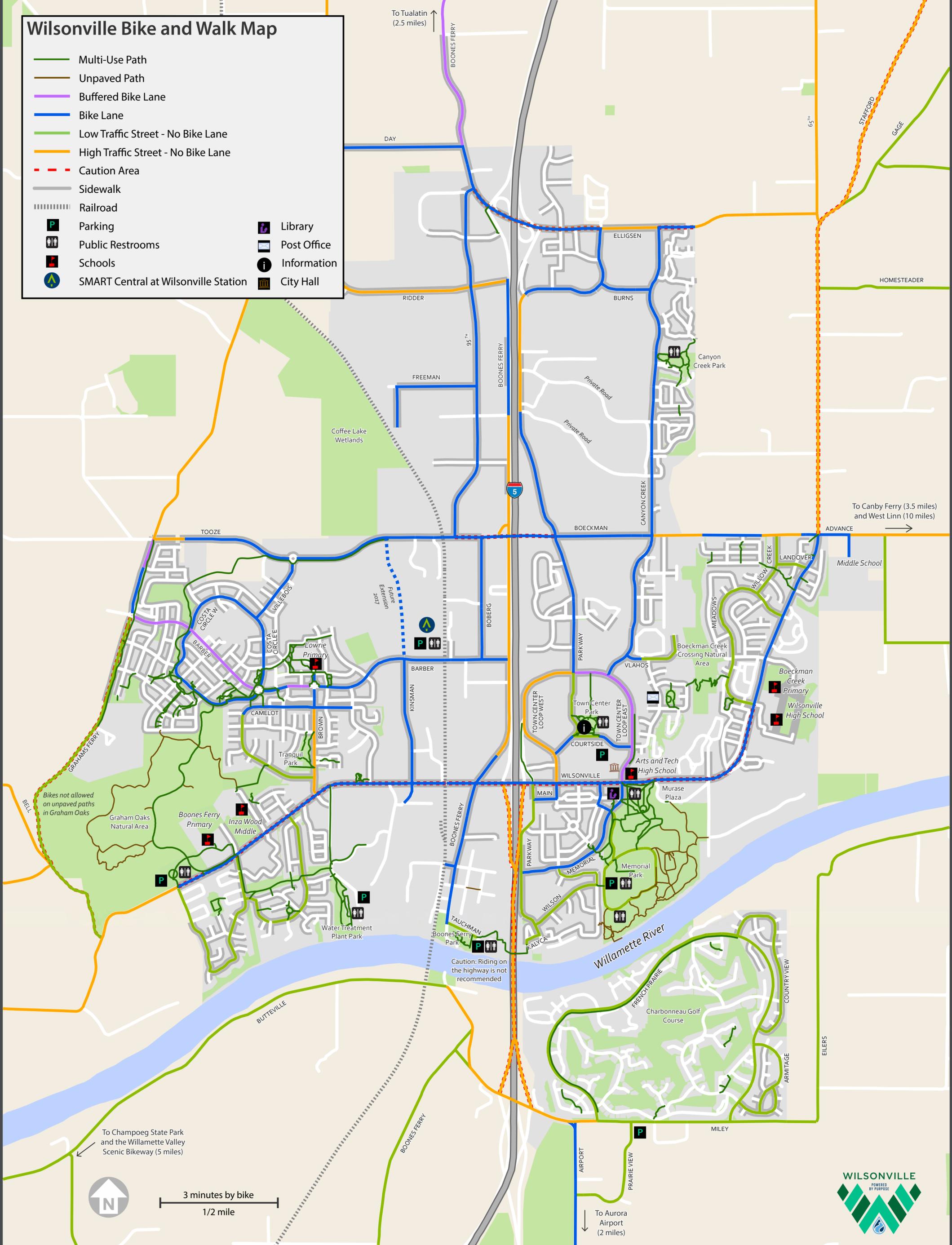
TABLE 6. SIGNS TYPES USED

Destination	Sign Number	Total Quantity
Bike Route End	0b, 42a	2
Bike Route Begin	0a, 41a, 41b, 42b	4
Bike Route	12c, 13b, 14d, 27b, 32a, 32c, 39a, 40a	8
Bike Route on Sidewalk	29e, 29h	2
Pavement Marking	16b, 17a, 17c, 19b, 24b, 29d, 29g, 31b, 31e, 31g, 31i, 32b, 32d, 37c	14
Turn: Right	2c, 15a, 16a, 28b, 34a, 35b, 37b, 37e, 38b	9
Turn: Left	2a, 15b, 19a, 28a, 34b, 35a, 27a, 37a, 38a	9
Turn with Destination	31c, 31j, 37d, 37f	4
Decision: 3 Panes	1a, 1b, 2b, 3b, 4a, 4b, 4c, 5a, 5b, 5c, 6a, 7a, 7b, 7c, 8a, 8b, 8c, 9a, 9b, 9c, 9d, 10a, 11a, 12a, 12b, 13c, 14c, 18a, 20a, 20b, 20c, 21a, 21b, 21c, 22a, 22b, 23a, 23c, 25a, 26a, 27a, 29a, 29b, 29c, 29f, 30a, 30b, 30c, 30d, 31a, 31d, 31f, 31h, 33a, 33b, 33c, 36a, 36b, 36c, 41a, 41b, 42b	62
Decision: 2 Panes	3a, 6b, 11b, 13a, 14a, 14b, 17b, 23b	8
Decision: 1 Pane	1c, 18b, 24a	3

SIGN MOCK-UPS

Wilsonville Bike and Walk Map

-  Multi-Use Path
-  Unpaved Path
-  Buffered Bike Lane
-  Bike Lane
-  Low Traffic Street - No Bike Lane
-  High Traffic Street - No Bike Lane
-  Caution Area
-  Sidewalk
-  Railroad
-  Parking
-  Public Restrooms
-  Schools
-  SMART Central at Wilsonville Station
-  Library
-  Post Office
-  Information
-  City Hall



To Tualatin (2.5 miles)

To Canby Ferry (3.5 miles) and West Linn (10 miles)

Bikes not allowed on unpaved paths in Graham Oaks

Caution: Riding on the highway is not recommended



3 minutes by bike
1/2 mile

To Champoeg State Park and the Willamette Valley Scenic Bikeway (5 miles)

To Aurora Airport (2 miles)



CITY OF WILSONVILLE
BICYCLE WAYFINDING
SIGNAGE SYSTEM

INTRODUCTION

BICYCLE WAYFINDING SIGNAGE SYSTEM

“Comprehensive signing and/or pavement markings that guide bicyclists to their destinations along preferred bicycle routes”

NATIONAL ASSOCIATION OF CITY TRANSPORTATION OFFICIALS, NACTO

SUPPORT

- 2006 Bicycle and Pedestrian Master Plan
- 2013 Bike and Ped Connectivity Action Plan
- City Council's Goal to Establish Bicycle Mile Markers

BENEFITS

- 1) Familiarizes users with the bicycle network.
- 2) Identifies best routes to destinations.
- 3) Overcomes “barrier to entry” for new bicyclists.
- 4) Addresses misconceptions regarding bike time.
- 5) Indicates that motorists should use caution.

REGULATORY FRAMEWORK

MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (MUTCD 2009)

Ensures that traffic control devices are visible, recognizable, understandable and necessary.

STANDARDS, GUIDANCE & OPTIONS

- Sign Design
- Sign Display
- Sign Placement

SIGN TYPES

SIGN TYPES

1) DECISION



OBD1-1c

2) TURN SIGN



MUTCD D11-1 & M6-1

SIGN TYPES

SIGN TYPES (CONTINUED)

3) BIKE ROUTE



MUTCD D11-1



MUTCD M4-11



MUTCD M4-12

4) PAVEMENT MARKING



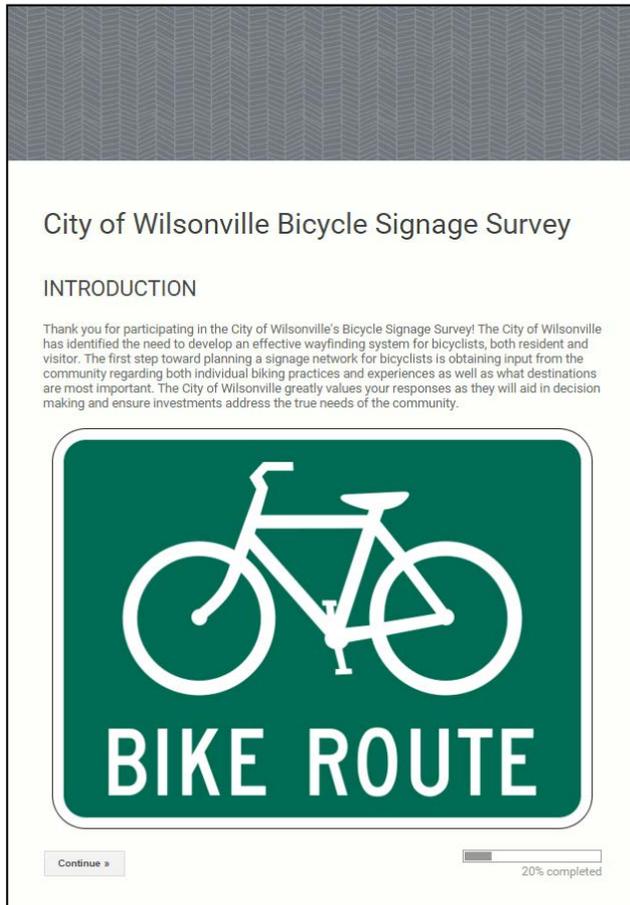
MUTCD 9c-9

METHODOLOGY

6 MAIN STEPS

- 1) SELECT DESTINATIONS
- 2) RANK DESTINATIONS
- 3) DEVELOP BIKE NETWORK
- 4) HIGH LEVEL SIGN PLACEMENT
- 5) EXACT SIGN PLACEMENT
- 6) COMPILE REPORT

1> DESTINATION SELECTION



Town Center
Old Town Square
Argyle Square
Clackamas Community College
Oregon Institute of Technology
Pioneer Pacific College
Korean War Memorial
Graham Oaks Nature Park
Murase Plaza
Memorial Park
Town Center Park
Boones Ferry Park
Water Treatment Plant Park
WES Transit Center
Library
City Hall
Community Center
Post Office
Community Gardens
Wilsonville High School
Inza Wood Middle School
Boeckman Creek Primary School
Boones Ferry Primary School
Lowrie Primary School

Sofia Park
Courtside Park
Piccadilly Park
Palermo Park
Canyon Creek Park
Green Acres Park
Hathaway Park
Mentor Graphics Path
Montebello Park
Park at Merryfield
River Fox Park
Tranquil City Park
Walt Morey Park
Willow Creek - Landover Park
Lowrie's Marketplace
Riverwood Center
Marketplace
The Village at Main Street
Boones Ferry Pointe
Arts & Tech High School
Cinema: Regal Cinemas
Family Fun Center
Museum: World of Speed

2 → DESTINATION RANK

TABLE 1. DESTINATION CLASSIFICATION

Destination Category	Classification	Destination Category Rank	Signage Distance
Commercial Centers: District	I District	1	3 Miles
Schools: College	I District	2	3 Miles
Regional Parks/Trails	II Regional	3	2 Miles
Public Transit	II Regional	4	2 Miles
Neighborhoods	II Regional	5	2 Miles
Civic/Community	III Local	6	1 Mile
Schools: High School	III Local	7	1 Mile
Schools: Elementary & Middle	IV Middle/Elementary	8	1/2 Mile
Local Parks/Trails	III Local	9	1 Mile
Commercial Centers: Local	III Local	10	1 Miles
Attraction	III Local	11	1 Mile

2 ...» DESTINATION RANK

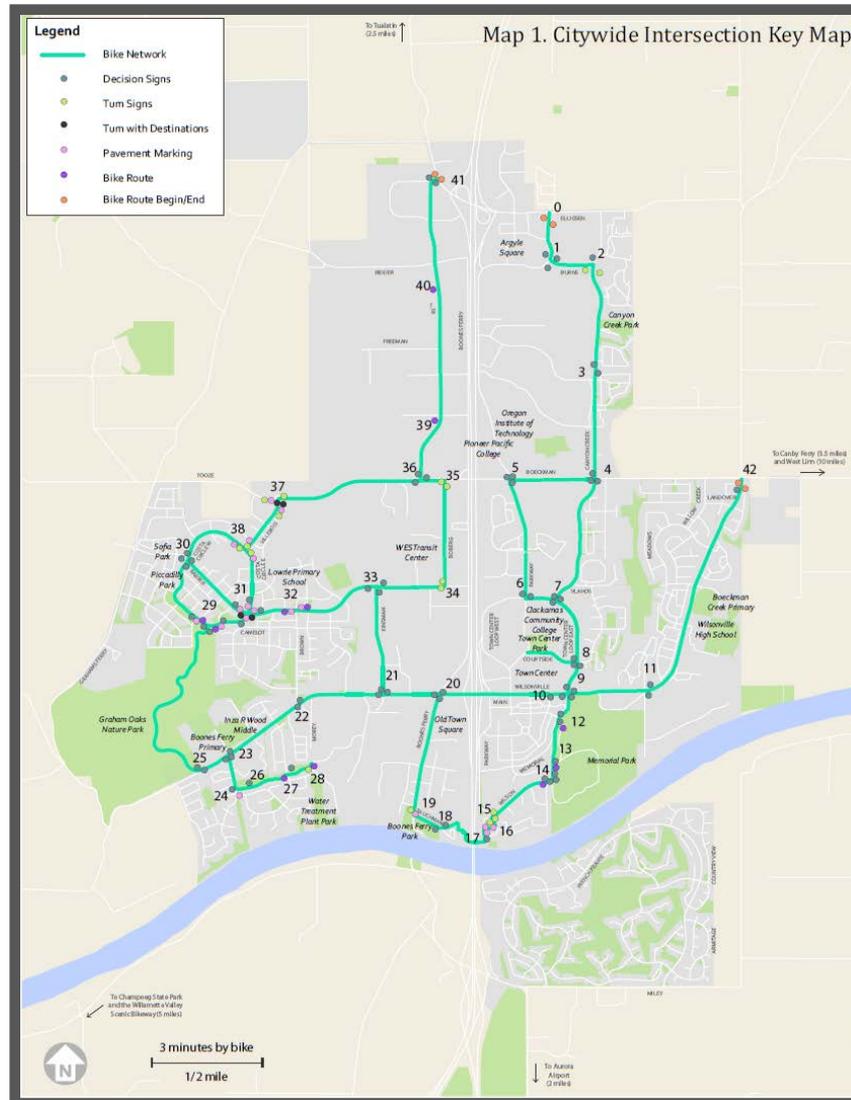
TABLE 2. DESTINATION RANK

Individual Destination	Destination Category	Classification	Overall Rank
Town Center	Commercial Center: District	I	1
Old Town Square	Commercial Center: District	I	2
Argyle Square	Commercial Center: District	I	3
Clackamas Community College	Schools: College	I	4
Oregon Institute of Technology	Schools: College	I	5
Pioneer Pacific College	Schools: College	I	6
Korean War Memorial	Regional Parks/Trails	II	7
Graham Oaks Nature Park	Regional Parks/Trails	II	8
Murase Plaza	Regional Parks/Trails	II	9
Memorial Park	Regional Parks/Trails	II	10
Town Center Park	Regional Parks/Trails	II	11
Boones Ferry Park	Regional Parks/Trails	II	12
Willamette River Water Treatment Plant Park	Regional Parks/Trails	II	13
WES Transit Center	Transit Station	II	14
Library	Civic/Community	III	15
City Hall	Civic/Community	III	16
Community Center	Civic/Community	III	17
Post Office	Civic/Community	III	18
Community Gardens	Civic/Community	III	19
Wilsonville High School	Schools: High School	III	20
Inza Wood Middle School	Schools: Elementary & Middle	IV	21
Boeckman Creek Primary School	Schools: Elementary & Middle	IV	22

***Destinations highlighted in grey-blue are included in wayfinding signage.**

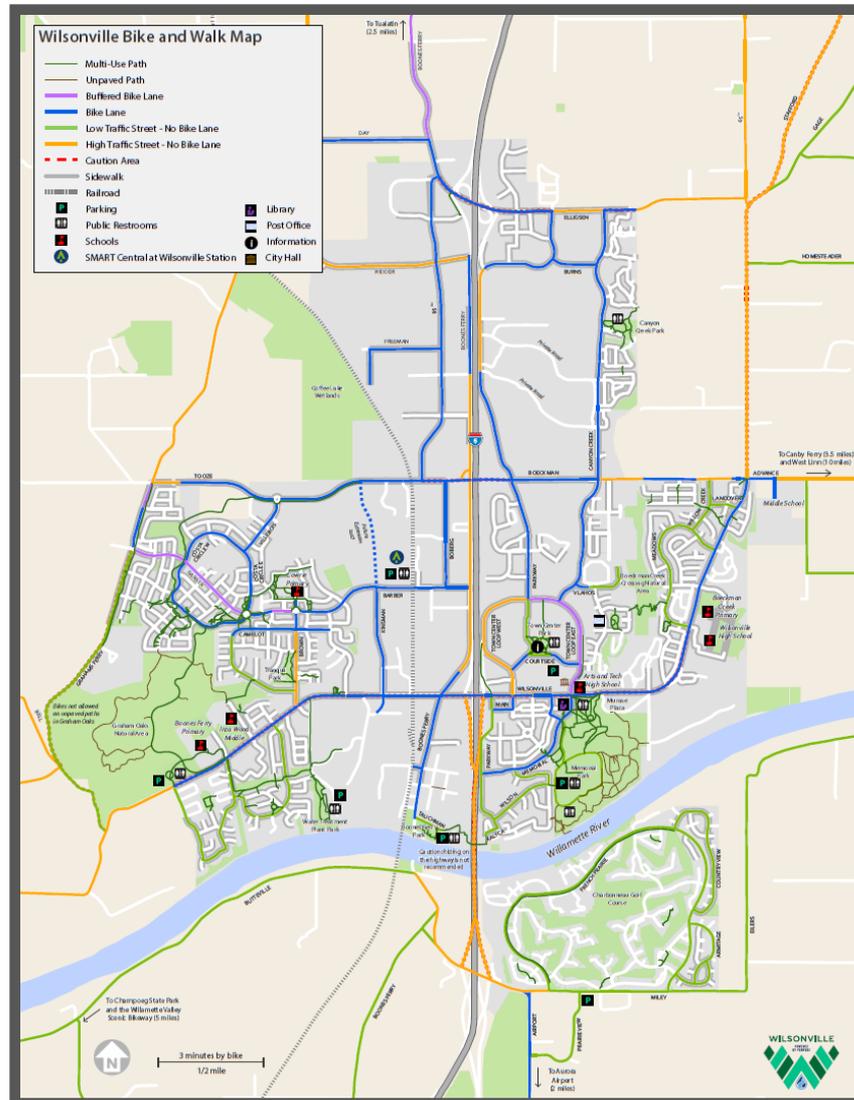
***Destinations highlighted in green are included on the Wilsonville Bike & Walk Map.**

3> BIKE NETWORK



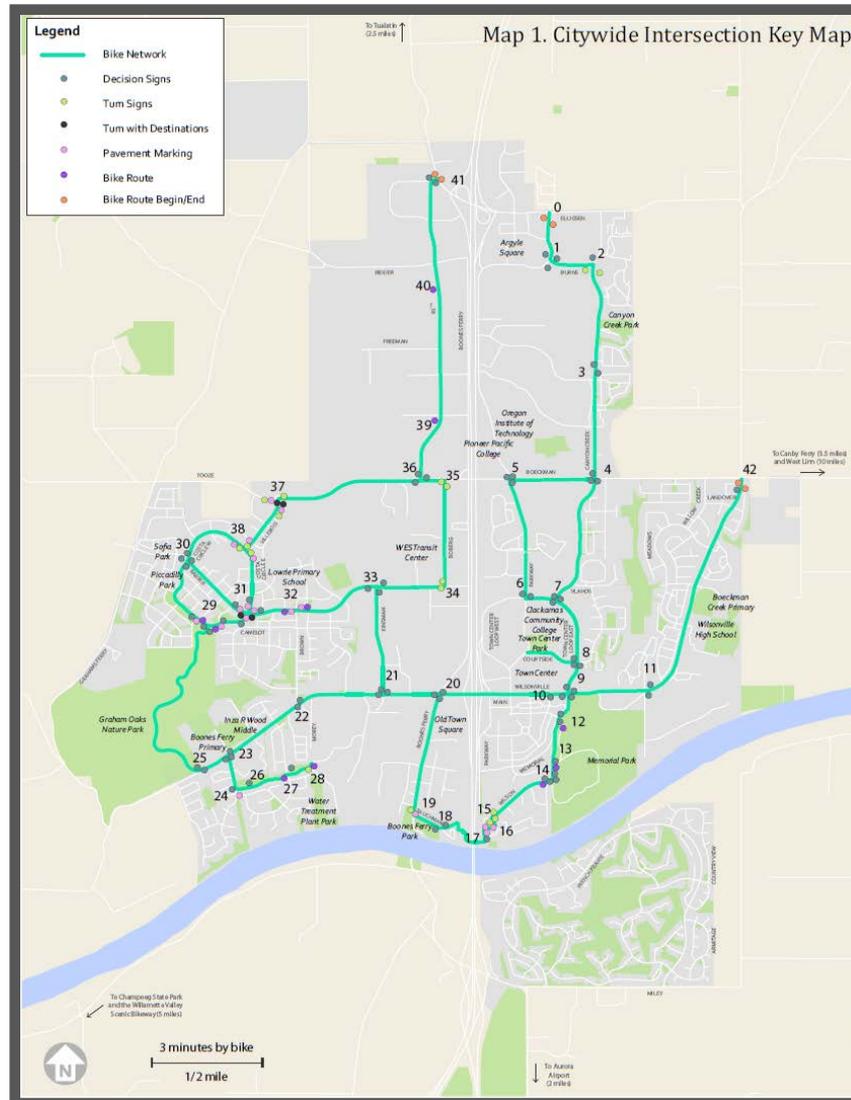
SEE ATTACHMENT D. CITYWIDE INTERSECTION KEY MAP

BIKE MAP



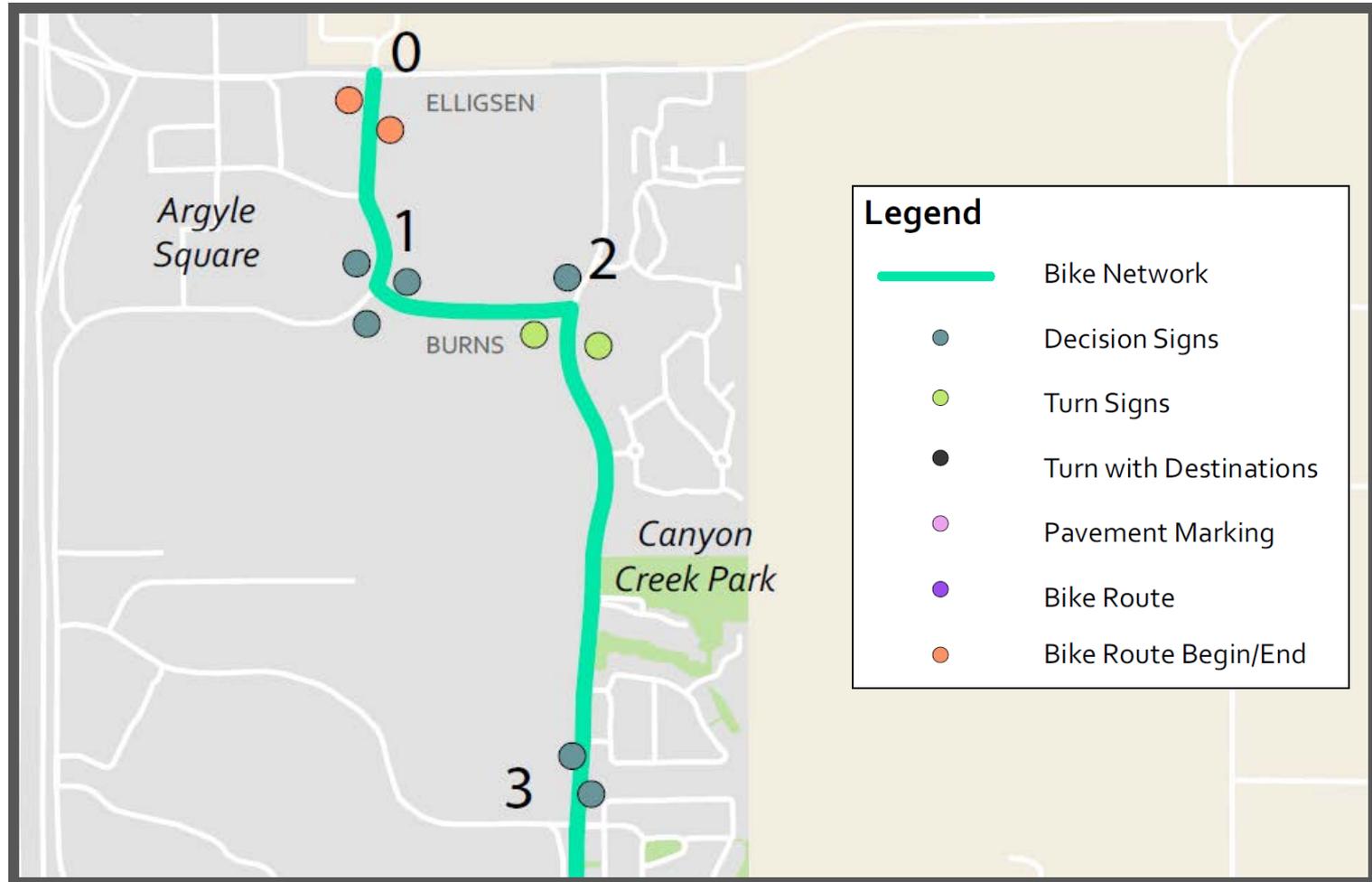
SEE ATTACHMENT E. WILSONVILLE BIKE & WALK MAP

3 → BIKE NETWORK



SEE ATTACHMENT D. CITYWIDE INTERSECTION KEY MAP

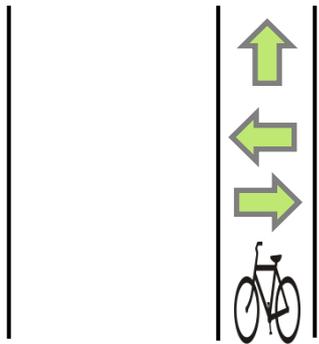
4 → HIGH LEVEL SIGN PLACEMENT



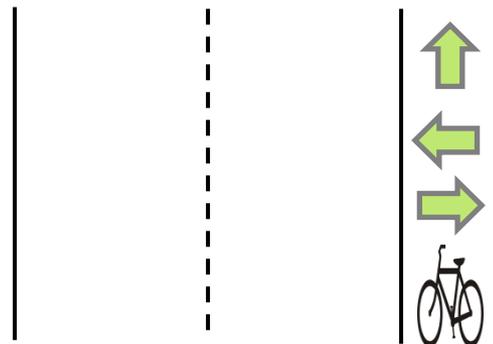
5 → EXACT SIGN PLACEMENT

SIGN PLACEMENT IN ADVANCE OF TURN

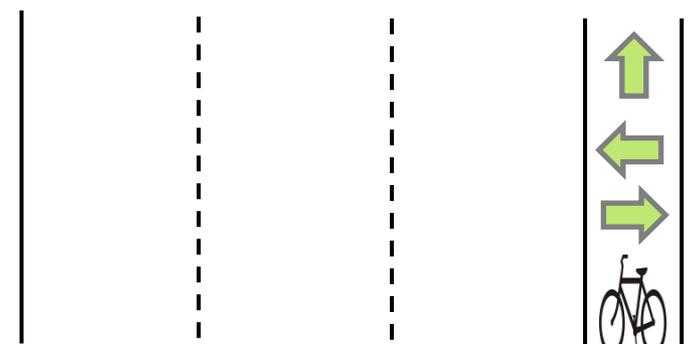
30 FEET



100 FEET



200 FEET



6 → COMPILE REPORT

INTERSECTION 9A

INFORMATION	
SIGN TYPE	On-Street Decision
INTERSECTION	Town Center Loop East & Wilsonville Road
DIRECTION OF TRAVEL	North Bound
SIGN NUMBER	9a

INSTALLATION	
ORIENTATION	Sign face is perpendicular to the edge of Memorial Drive
DISTANCE FROM INTERSECTION (Feet)	105
LATERAL DISTANCE (Feet)	-

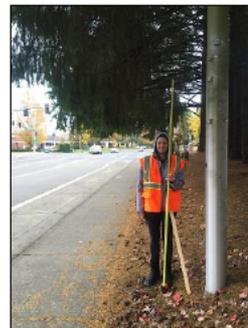
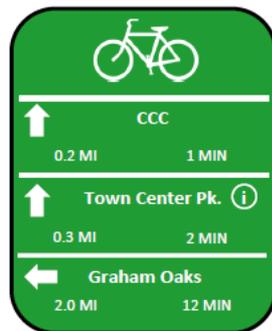
NOTES

Place decision sign on the east side of Memorial Drive 105 feet south of Wilsonville Road. Measured from southern-most crosswalk bar across Memorial Drive.

Initial 100 feet due to the need to maneuver across one lane of traffic in order to turn left to reach destinations. Additional 5 feet in order to use light pole. Mount on light pole.



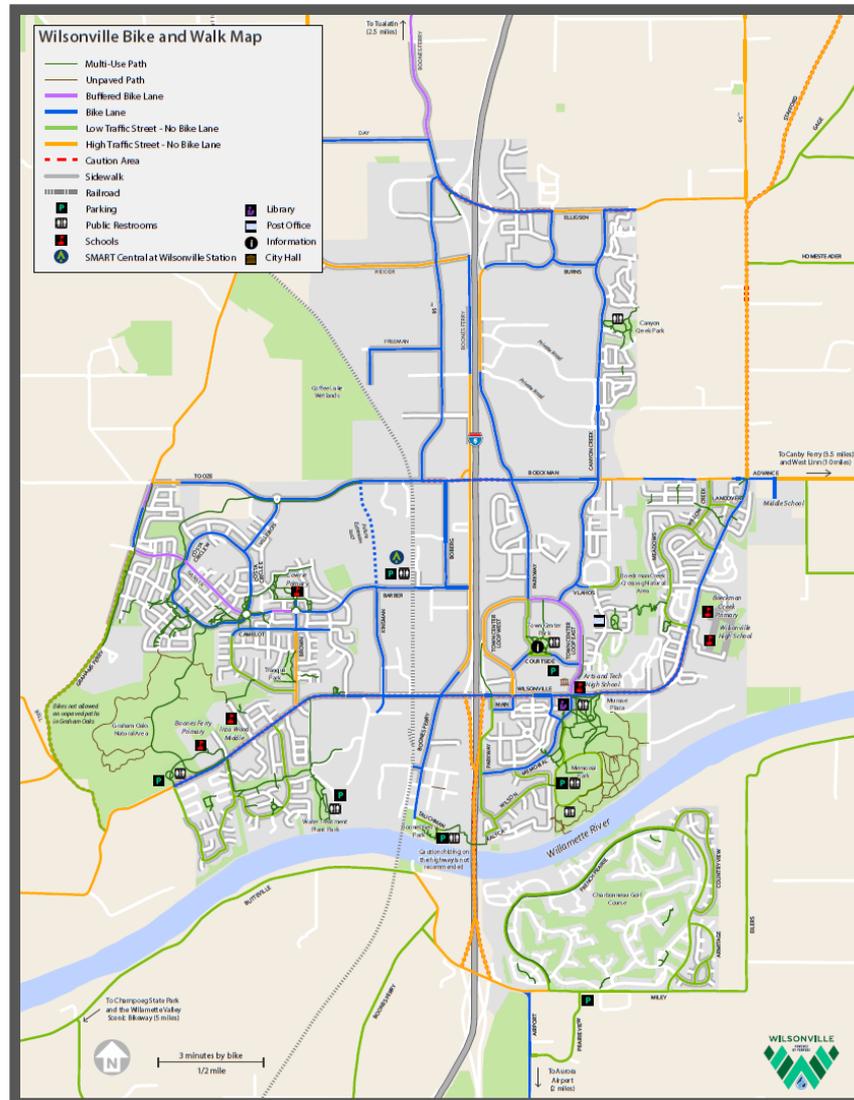
Satellite Location View (location to be verified by contractor)



LIMITATIONS & CHALLENGES

- Restricted Creativity
- Limited Sign Space
- High Destination Density in Town Center
 -> One Component of Signage
 -> Bike Map

BIKE MAP



SEE ATTACHMENT E. WILSONVILLE BIKE & WALK MAP

TIMELINE & BUDGET

MARCH, 2016 → Planning and Documents Finalized

APRIL, 2016 → Begin Signage Installation

JUNE, 2016 → Complete Signage Installation

Project is funded by the FY 2015-2016 budget.

THANK YOU