



PLANNING COMMISSION

WEDNESDAY, AUGUST 11, 2021

II. WORK SESSION

B. I-5 Pedestrian Bridge (Weigel) (45 Minutes)



PLANNING COMMISSION STAFF REPORT

Meeting Date: August 11, 2021		Subject: I-5 Pedestrian Bridge and Gateway Plaza	
		Staff Member: Zach Weigel, PE, Capital Projects Engineering Manager; Kimberly Rybold, AICP, Senior Planner	
		Department: Community Development	
Action Required		Advisory Board/Commission Recommendation	
<input type="checkbox"/> Motion <input type="checkbox"/> Public Hearing Date: <input type="checkbox"/> Ordinance 1 st Reading Date: <input type="checkbox"/> Ordinance 2 nd Reading Date: <input type="checkbox"/> Resolution <input checked="" type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input type="checkbox"/> Council Direction <input type="checkbox"/> Consent Agenda		<input type="checkbox"/> Approval <input type="checkbox"/> Denial <input type="checkbox"/> None Forwarded <input checked="" type="checkbox"/> Not Applicable Comments: N/A	
Staff Recommendation: Review and provide feedback on the 60% design package for the I-5 Pedestrian Bridge and Gateway Plaza project in Wilsonville Town Center.			
Recommended Language for Motion: N/A			
Project / Issue Relates To:			
<input checked="" type="checkbox"/> Council Goals/Priorities Goal 1: Increase mobility for all in Wilsonville	<input checked="" type="checkbox"/> Adopted Master Plan(s) Bicycle & Pedestrian Connectivity Plan, Transportation System Plan, Town Center Plan	<input type="checkbox"/> Not Applicable	

ISSUE BEFORE COMMISSION:

The project team will provide an update on the progress of the bridge and plaza design and share selected design elements, including lighting, seating, walls, and landscape materials.

EXECUTIVE SUMMARY:

In 2017, the City was awarded a Metropolitan Transportation Improvement Program (MTIP) Regional Flexible Funds (RFFA) grant from Metro for the design of the I-5 Pedestrian Bridge. The project, first identified as a need in the 2006 Bicycle and Pedestrian Master Plan and subsequently added to the Transportation System Plan (TSP) in 2013, will provide a safe pedestrian and bike crossing of Interstate 5, connecting the Villebois neighborhood and the Wilsonville Transit Center to the Town Center and adjacent residential areas. The project also includes design of the Gateway Plaza, a community gathering space identified as a community priority as part of the Town Center Plan process, on a City-owned parcel at the east bridge landing. Per the IGA for this grant, 90% design for the project must be completed by the end of 2021.

In October 2020, City Council selected the Tied Arch as the preferred bridge alternative based on input provided through public outreach and Planning Commission recommendations. With this direction, City Council also supported a plaza design combining elements of the Drops and Ripples and River Oxbow concepts. Based on this, the project team began more detailed design work to advance these concepts. Planning Commission reviewed the 30% design package in March 2021 and provided feedback on the bridge color, lighting options, and protective screening, along with the refined plaza design and proposed elements.

To gather ideas for the retaining wall design on the western bridge approach ramp, the project team posted a brief design survey on *Let's Talk, Wilsonville!* during May and June. Over 170 people took this survey, expressing preferences for stamped concrete illustrating wildlife, grass, or basalt columns, along with art highlighting natural elements. While no specific retaining wall designs will be selected as part of the current bridge design process, staff will use this input to inform design decisions for this wall at the time of bridge construction.

At this work session, the project team will provide an update on the bridge and plaza 60% design. This package provides specific recommendations for the bridge and plaza elements to respond to feedback and preferences received during 30% design review. To continue refining bridge and plaza design, the project team seeks feedback on the following questions:

- For the bridge design, do the included design elements represent the project themes, feedback, and direction provided to the design team to date?
- For the plaza design, do the included design elements reflect the feedback and direction thus far? Are there any refinements to these elements that are needed?

The project team will present this design package, along with Planning Commission input, to the City Council at a work session on August 16. Following these work sessions, the project team will use the Planning Commission and City Council input to make additional refinements to the bridge and plaza designs.

EXPECTED RESULTS:

Planning Commission feedback will be shared with City Council at its August 16 work session to confirm design direction.

TIMELINE:

Project work will continue in fall 2021 to refine bridge design, plaza materials, and amenities with completion of 90% design anticipated in December 2021.

CURRENT YEAR BUDGET IMPACTS:

The budget for FY2021-22 includes \$5,000,000 in Transportation SDCs for CIP project #4202. The remaining project design work over the next year is estimated at \$1.0 million. The remaining budget is intended to begin to accrue funds to pay for project construction in future years.

FINANCIAL REVIEW / COMMENTS:

Reviewed by: Date:

LEGAL REVIEW / COMMENT:

Reviewed by: Date:

COMMUNITY INVOLVEMENT PROCESS:

The pedestrian and bikeway bridge was identified as a high priority project through the last update to the Wilsonville Transportation System Plan, which included an extensive community involvement process. Likewise, the RFFA grant process included a public review and comment period in which the project garnered positive feedback from the community. In addition, the Town Center Plan included a robust and inclusive public outreach process where the Bridge Project was identified as a key framework project through extensive community support.

There have been several opportunities to participate in the design of the Bridge Project and Gateway Plaza as scoped within the Public Engagement Plan for the project. In addition to the public kickoff event, outreach opportunities included an online open house, online surveys, stakeholder interviews, and pop-up information displays. The engagement plan is designed to reach as broad an audience as possible and to gather the variety of perspectives in the community. It also includes targeted outreach to specific stakeholders more impacted by activity in the Town Center.

POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY:

The Bridge Project will provide a safe bike and pedestrian crossing of I-5 that is separated from vehicular traffic with direct access to essential services in the Wilsonville Town Center, the Wilsonville Transit Center, employment areas, and educational resources. The bridge will serve all populations within and around the project area and will help the Town Center become a more vibrant, pedestrian and transit-supportive mixed-use district. Public art and environmental features integrated into the Bridge Project and plaza will help to create an attractive and accessible place for visitors and residents of all ages to shop, eat, live, work, learn, and play. The bridge and plaza investment will exemplify the City’s commitment to realizing the community’s vision for Town Center and serve as an incentive for private investment.

ALTERNATIVES:

The Commission can provide recommendations to refine bridge and plaza design elements as the project moves into the 90% design phase.

CITY MANAGER COMMENT:

N/A

ATTACHMENT:

- 1. Presentation Slides

WILSONVILLE TOWN CENTER I-5 PEDESTRIAN AND BICYCLE BRIDGE: BARBER STREET TO WILSONVILLE TOWN CENTER



Planning Commission/City Council
August 2021



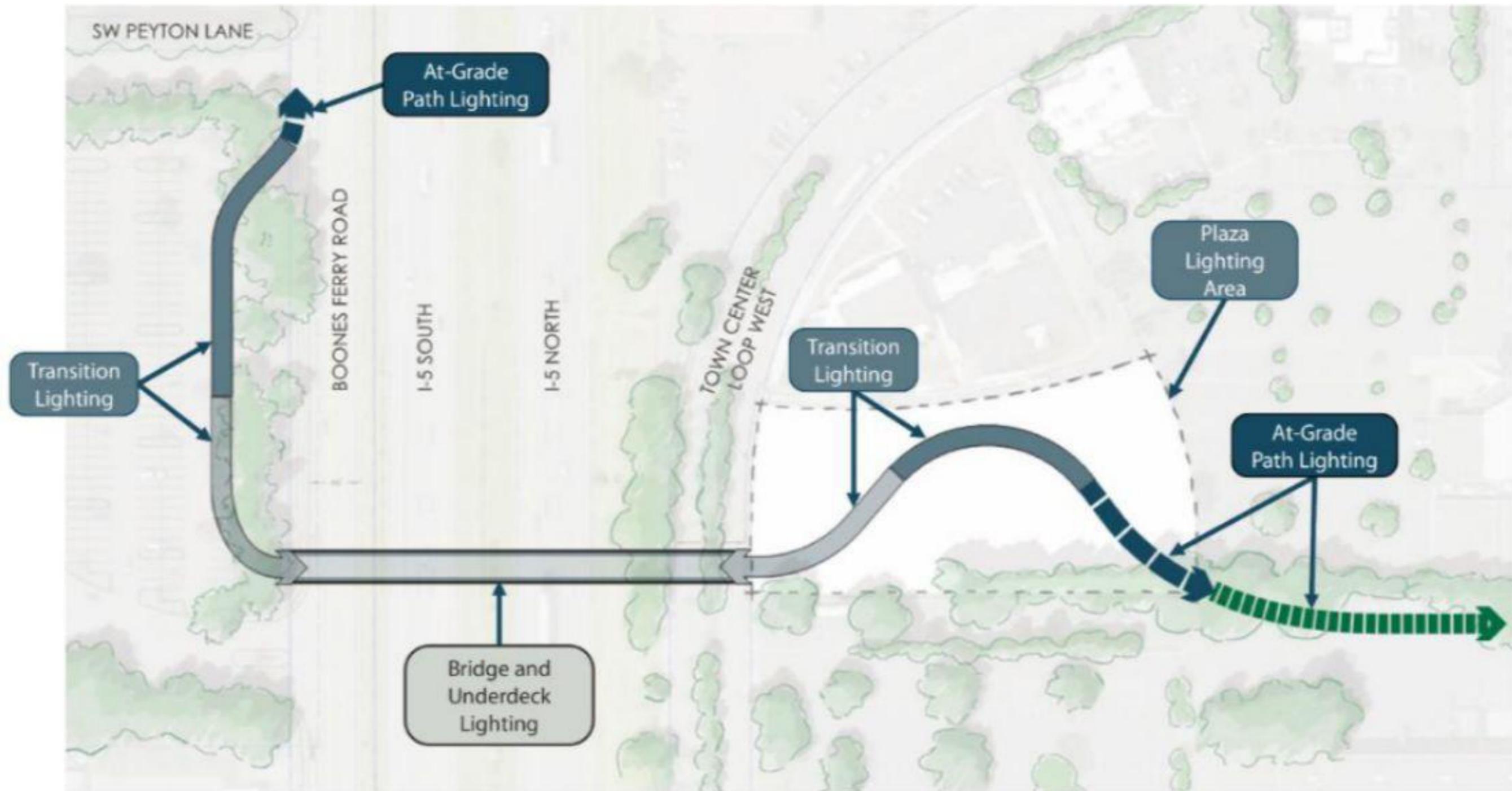
WILSONVILLE TOWN CENTER
I-5 PEDESTRIAN BRIDGE



TIED ARCH



Bridge Aesthetic Enhancement Options





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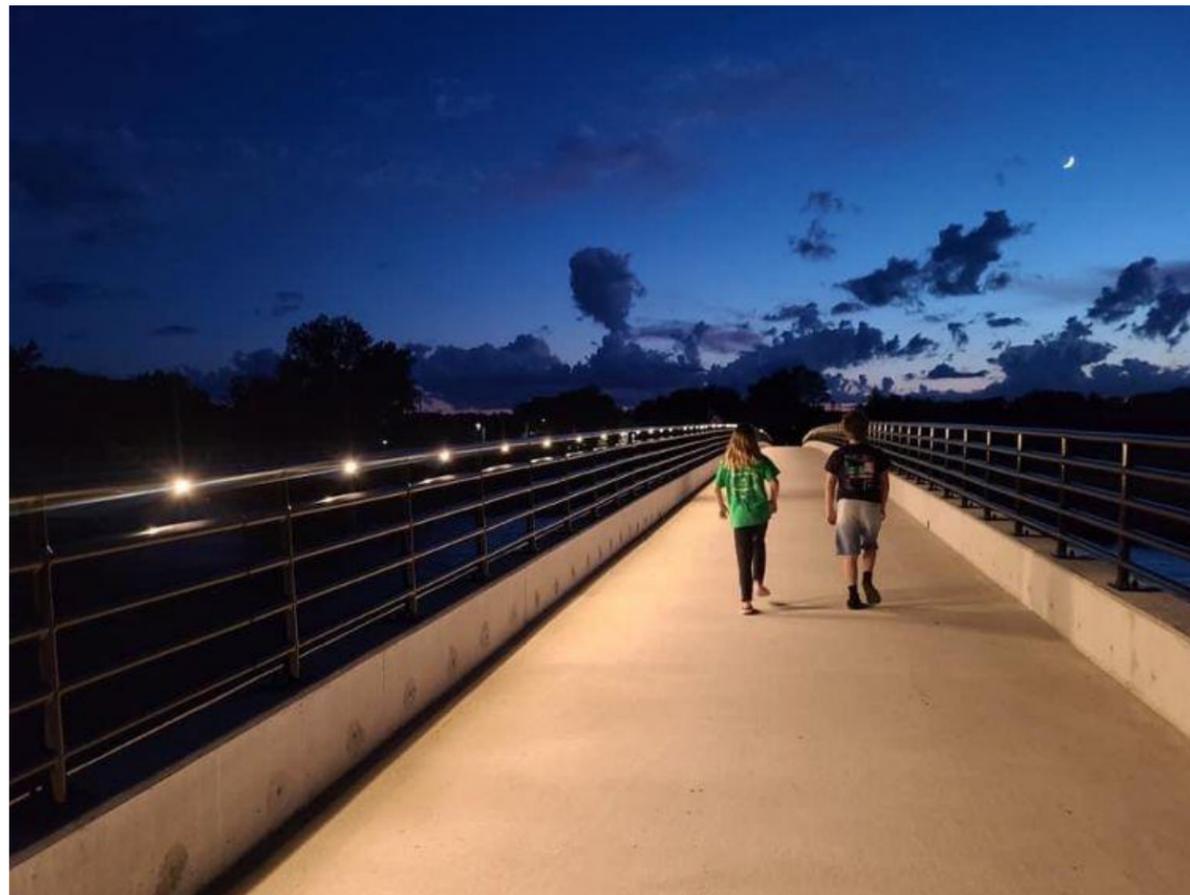
PATHWAY LIGHTING

-Transition

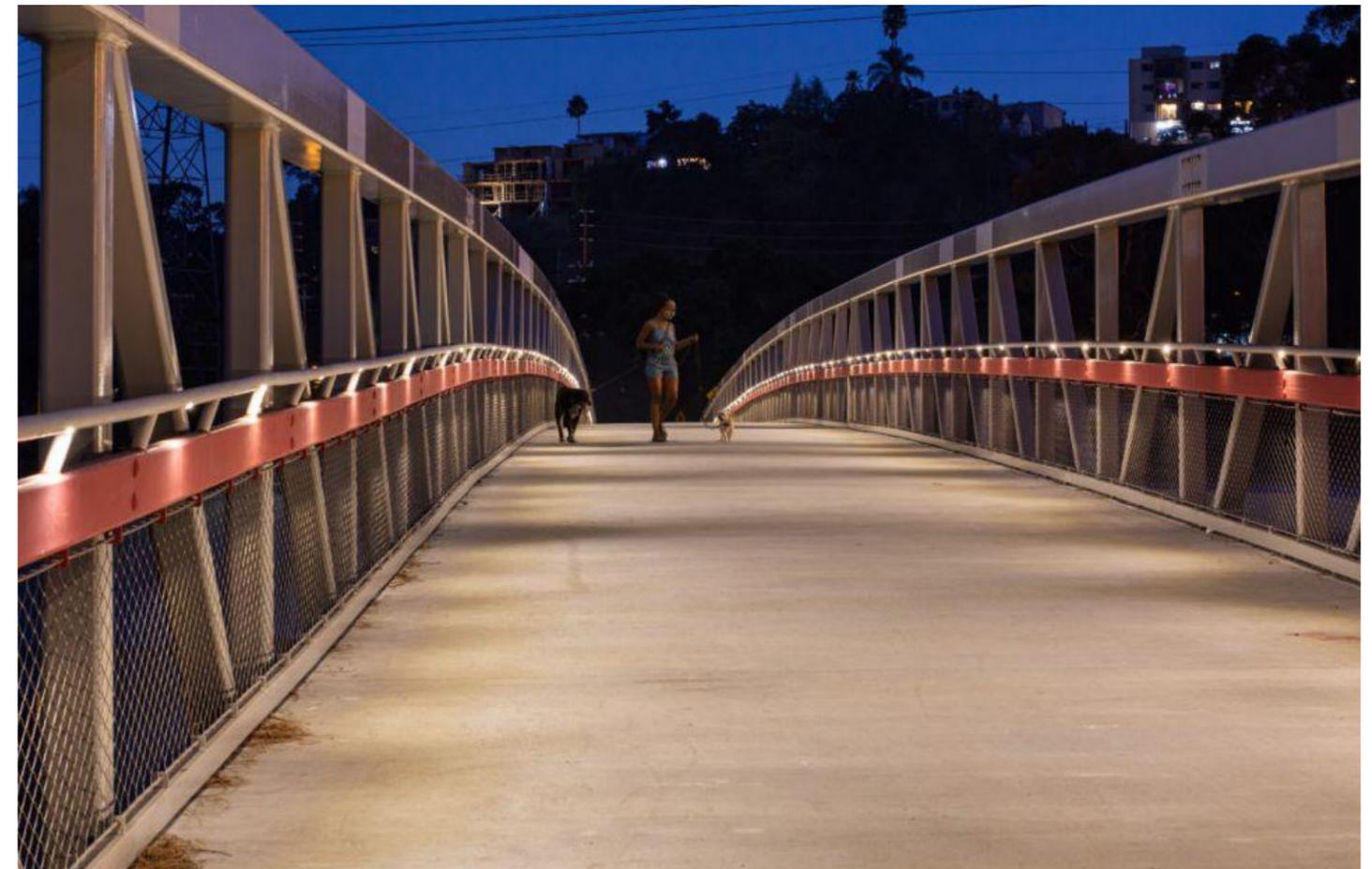
-Bridge



KLIK XL28 PODS



ELGIN TRAIL BRIDGE, ELGIN, IOWA
SOURCE: KLIK USA

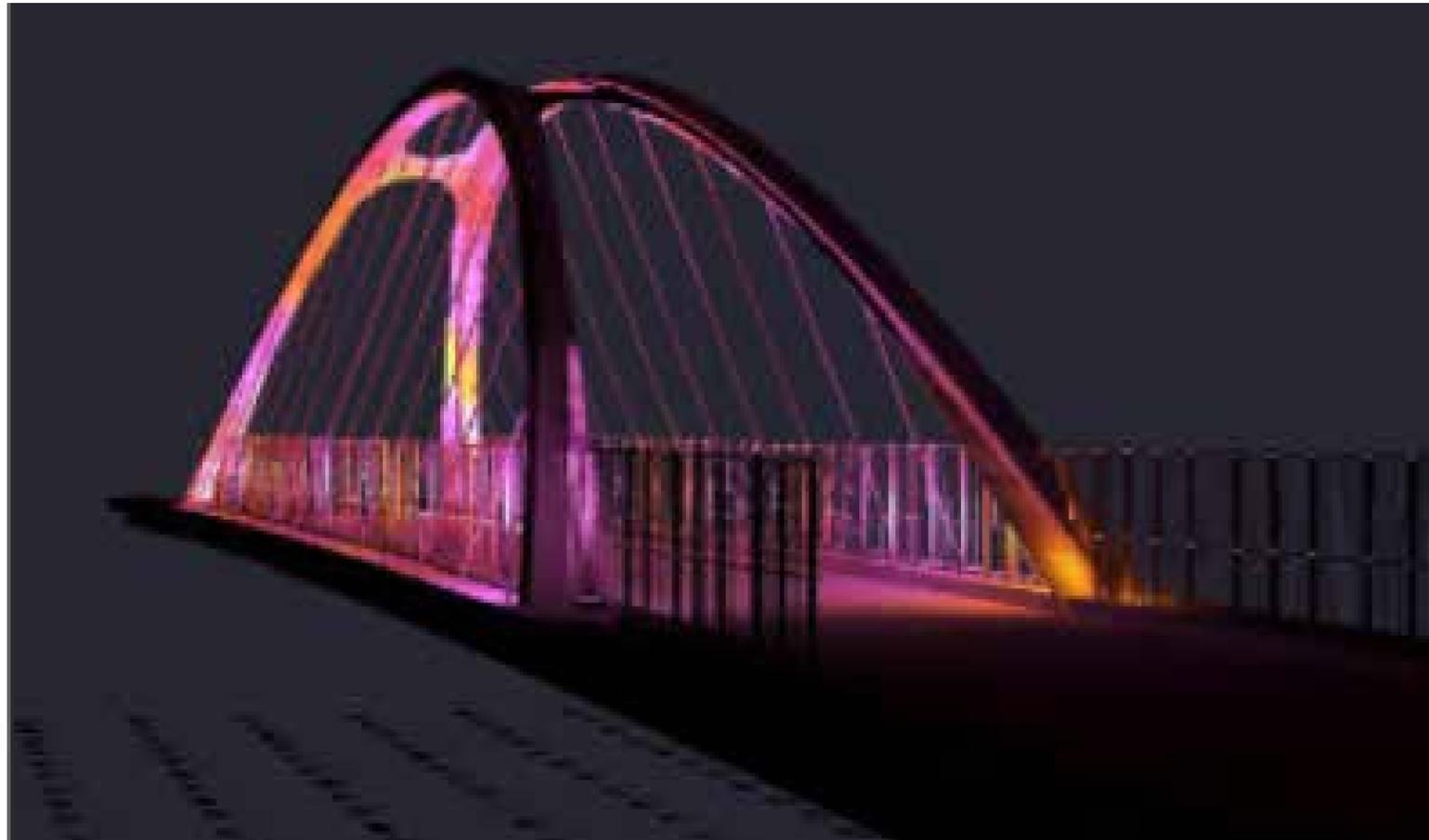
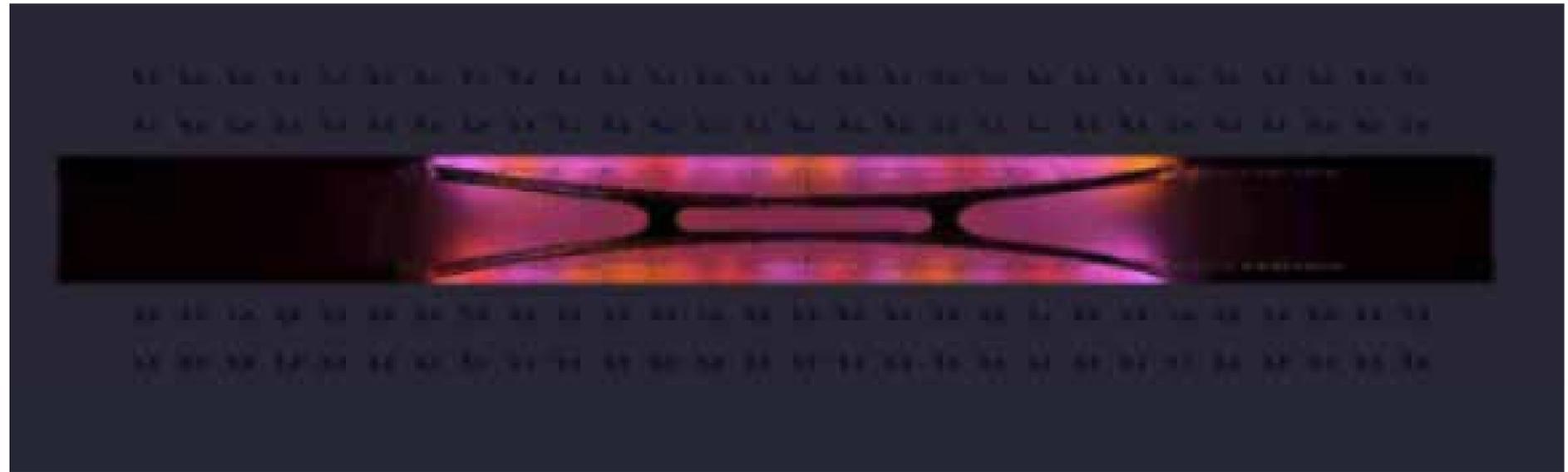


RED CAR PEDESTRIAN BRIDGE, GLENDALE, CA
SOURCE: KLIK USA



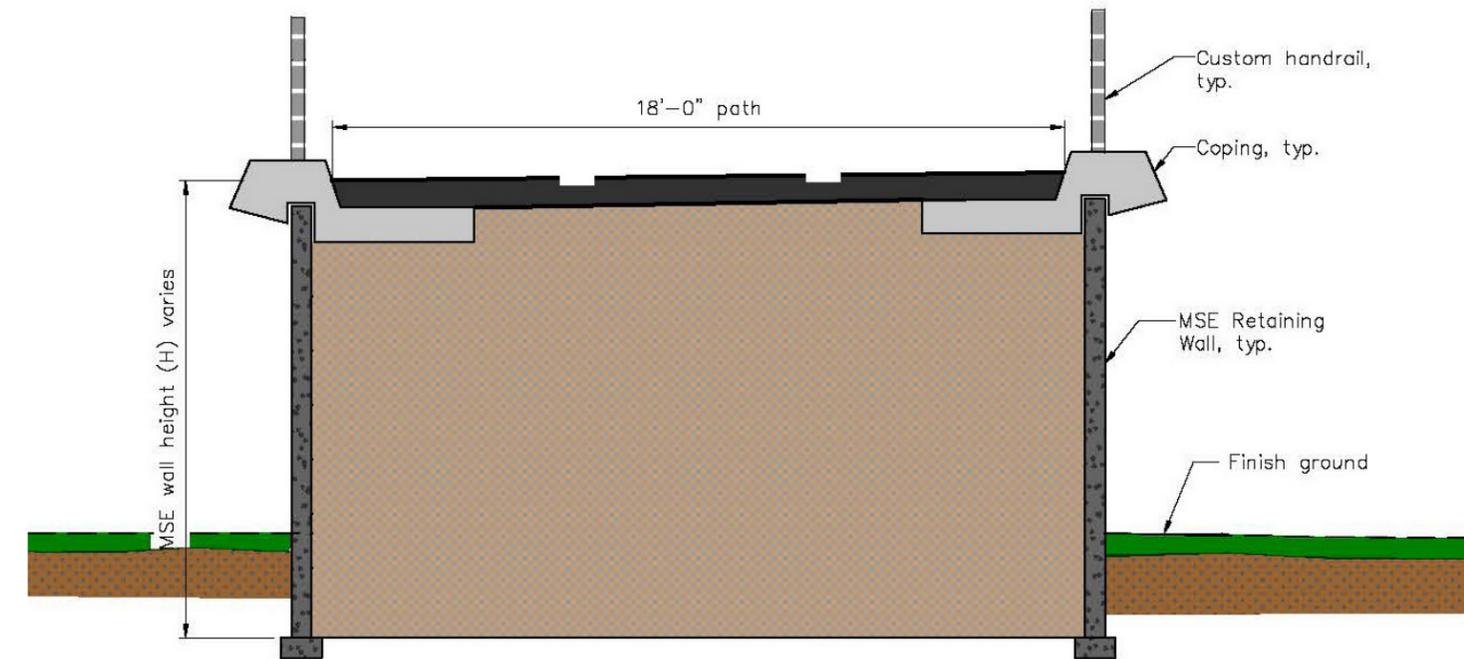
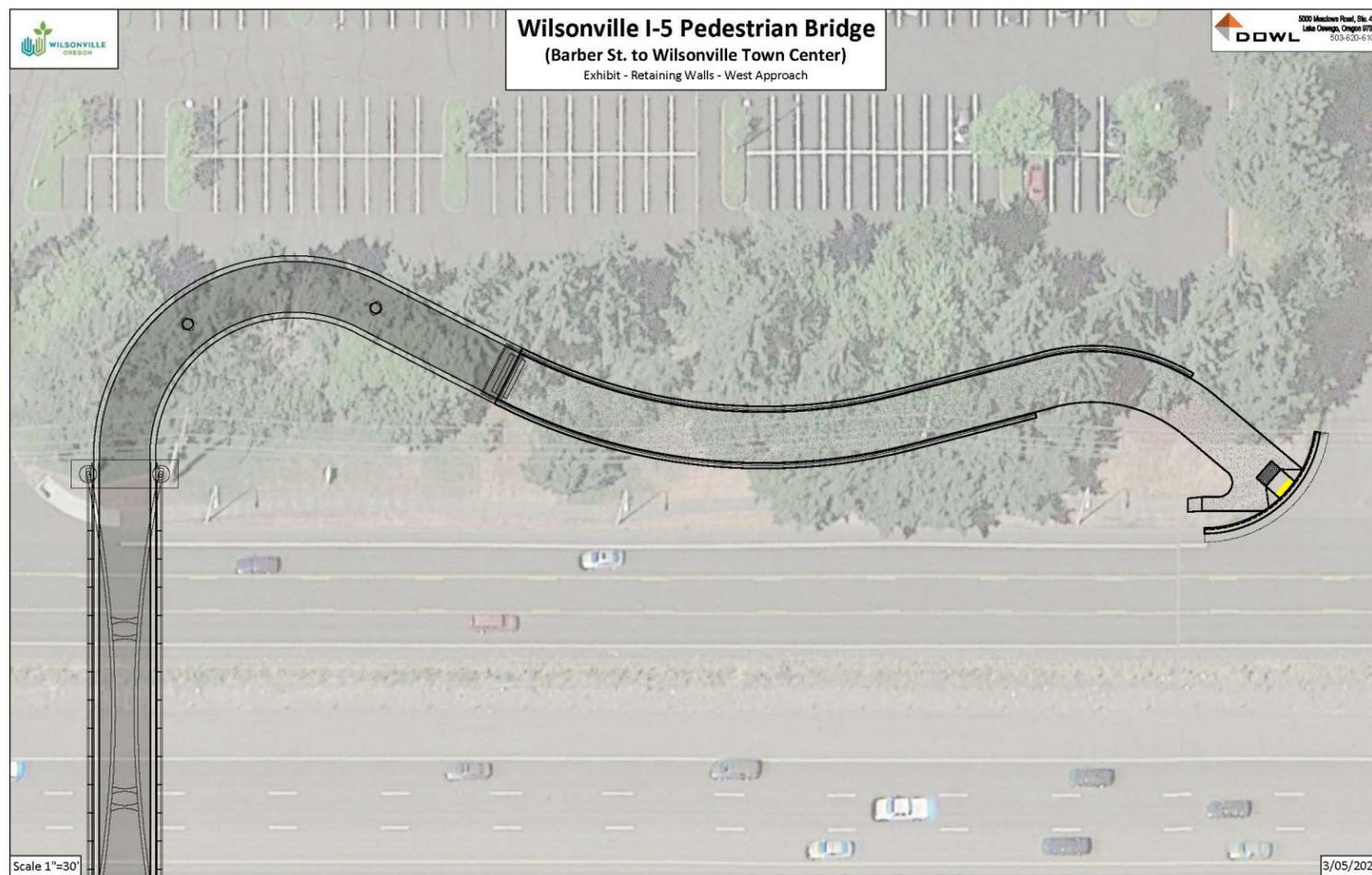
PROJECTION LIGHTING

LUMASCAPE LINEALUX



SOURCE: THE LIGHTING GROUP AGI RENDERING

Bridge Wing and Abutment Wall Treatment (west approach and main plaza)

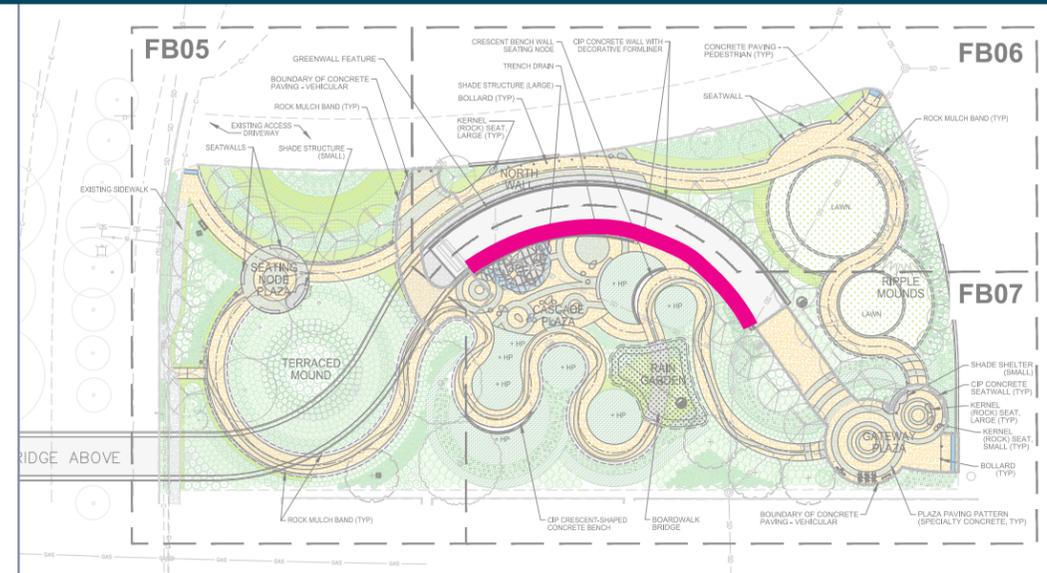


Bridge Wing and Abutment Wall Treatment (west approach and main plaza)

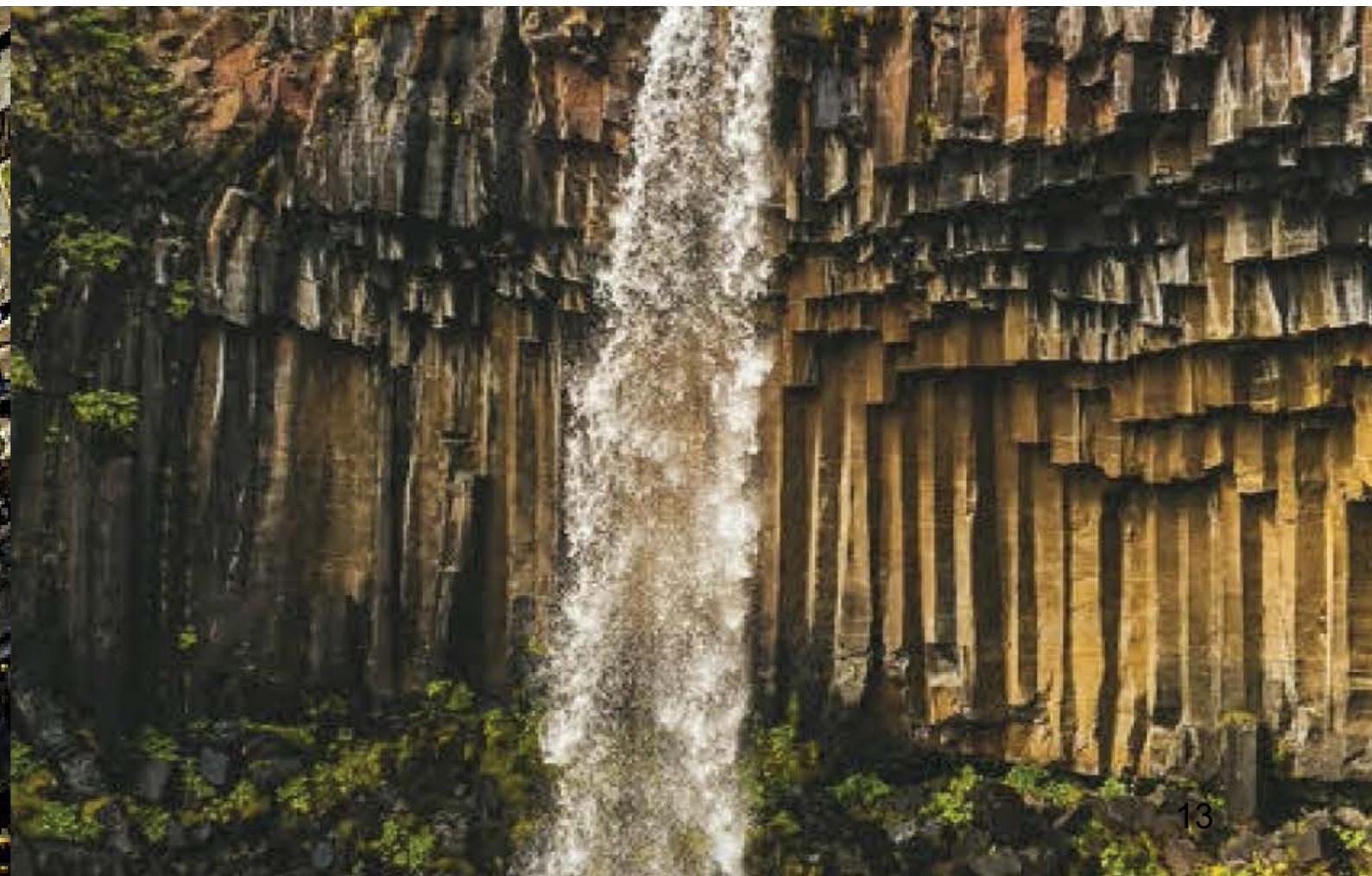
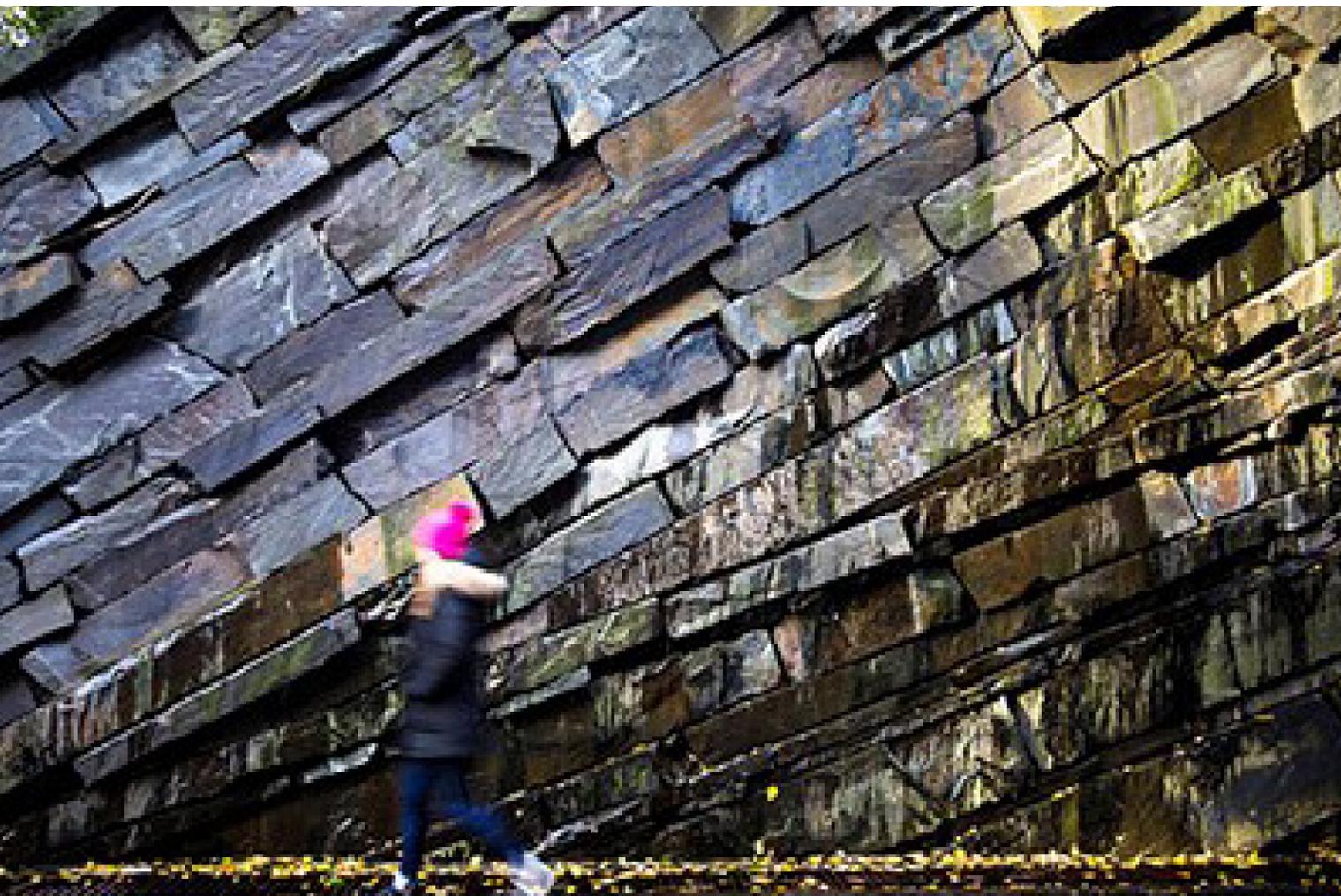


Ramp + Plaza Design

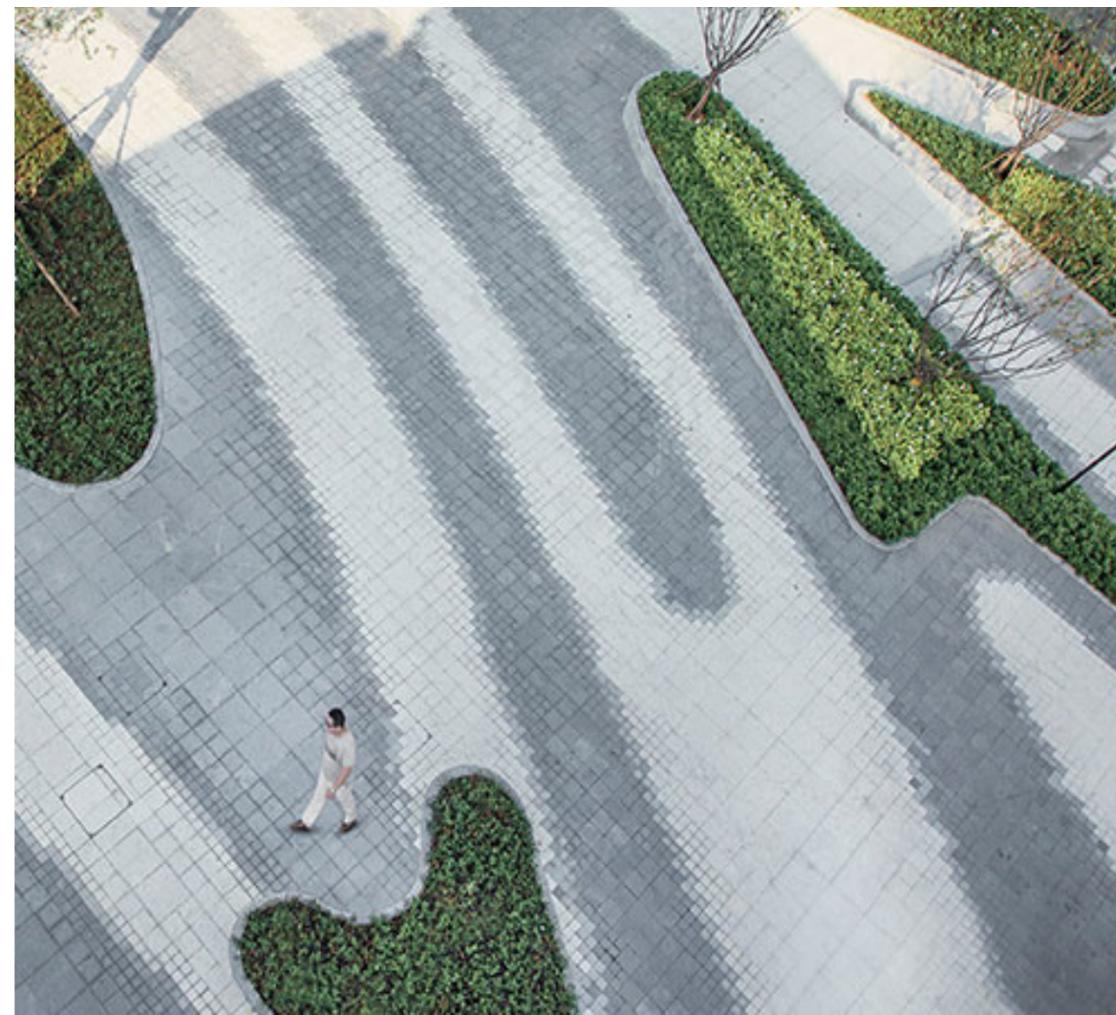
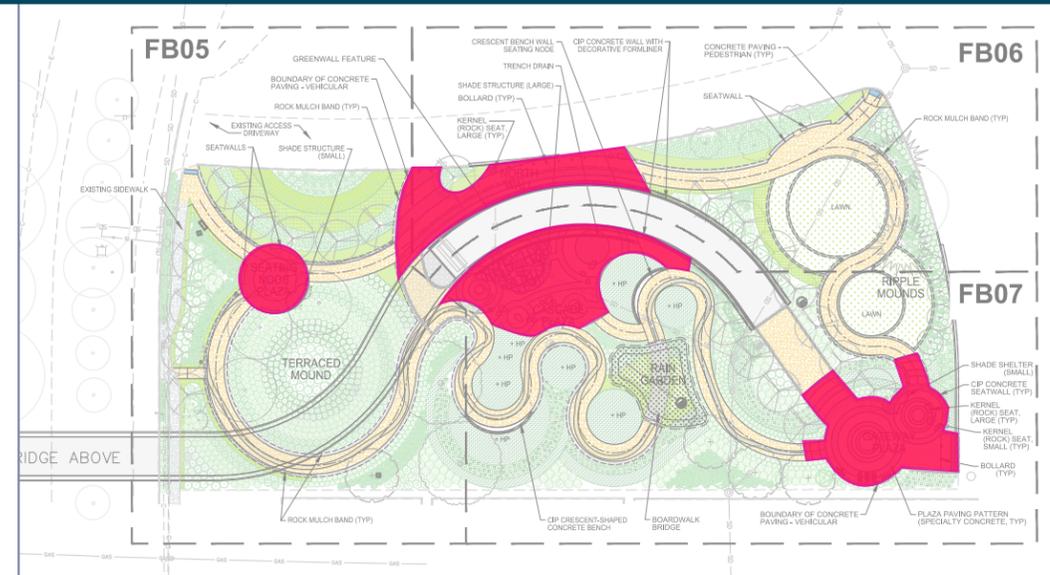
30% Design Features - CASCADE PLAZA ROCK WALL option 1: involves sourcing basalt



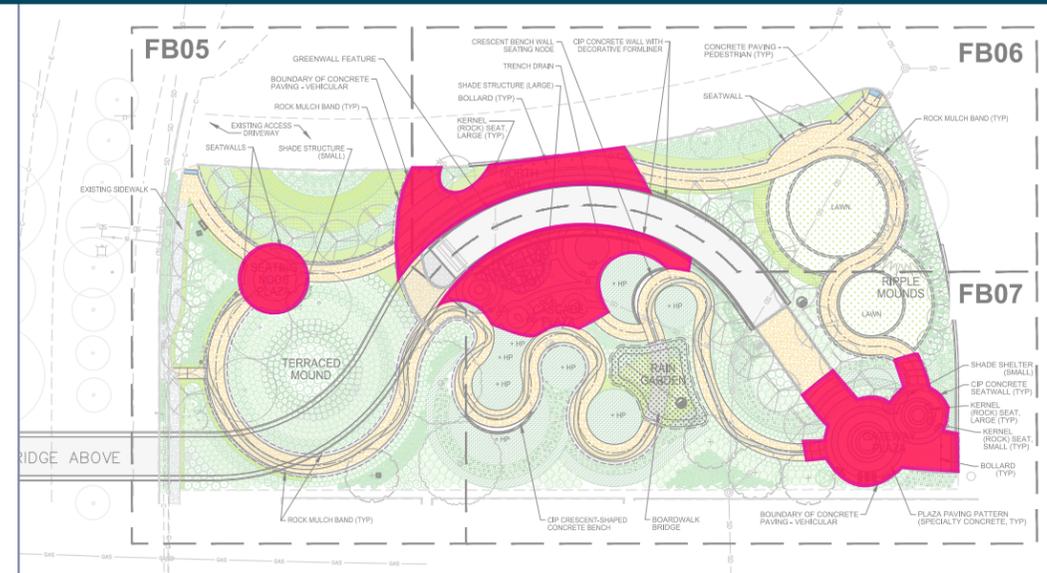
- Involves sourcing basalt



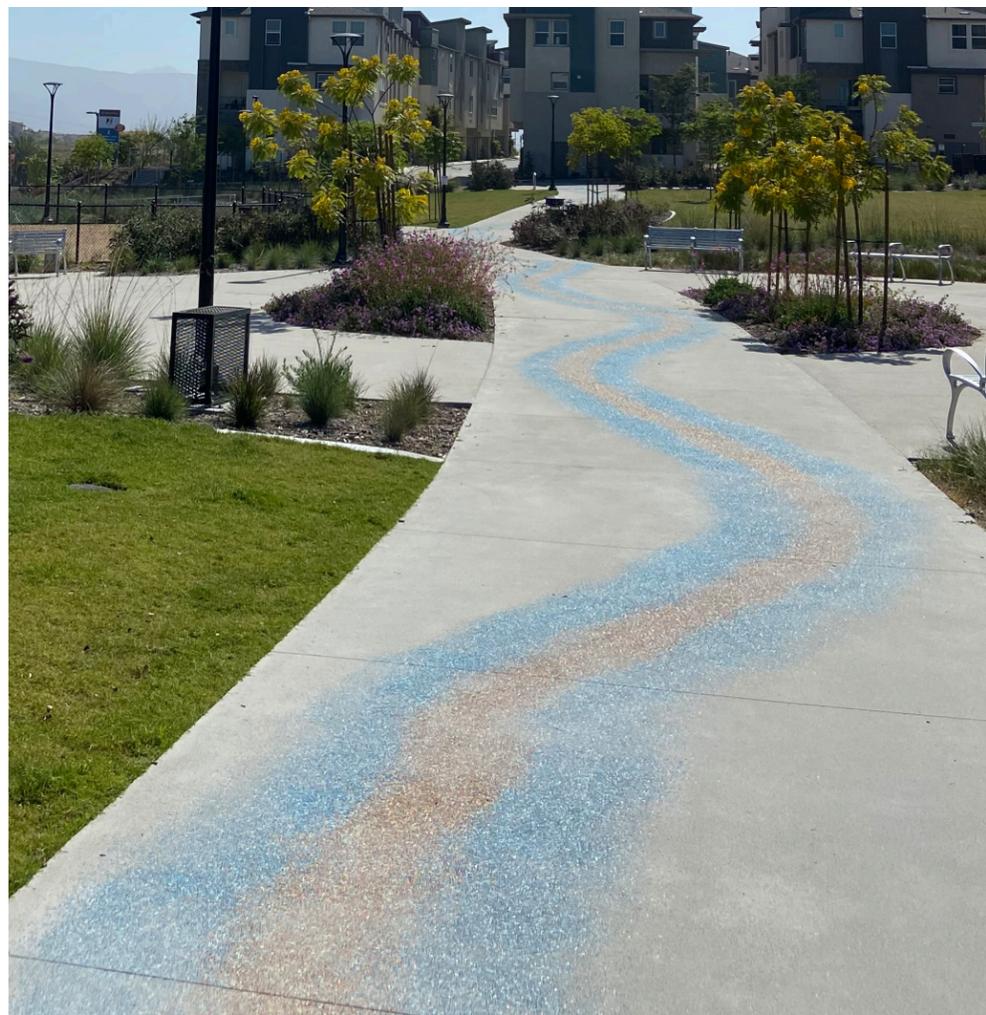
30% Design Features - SPECIALTY PAVING



60% Design Features - SPECIALTY PAVING

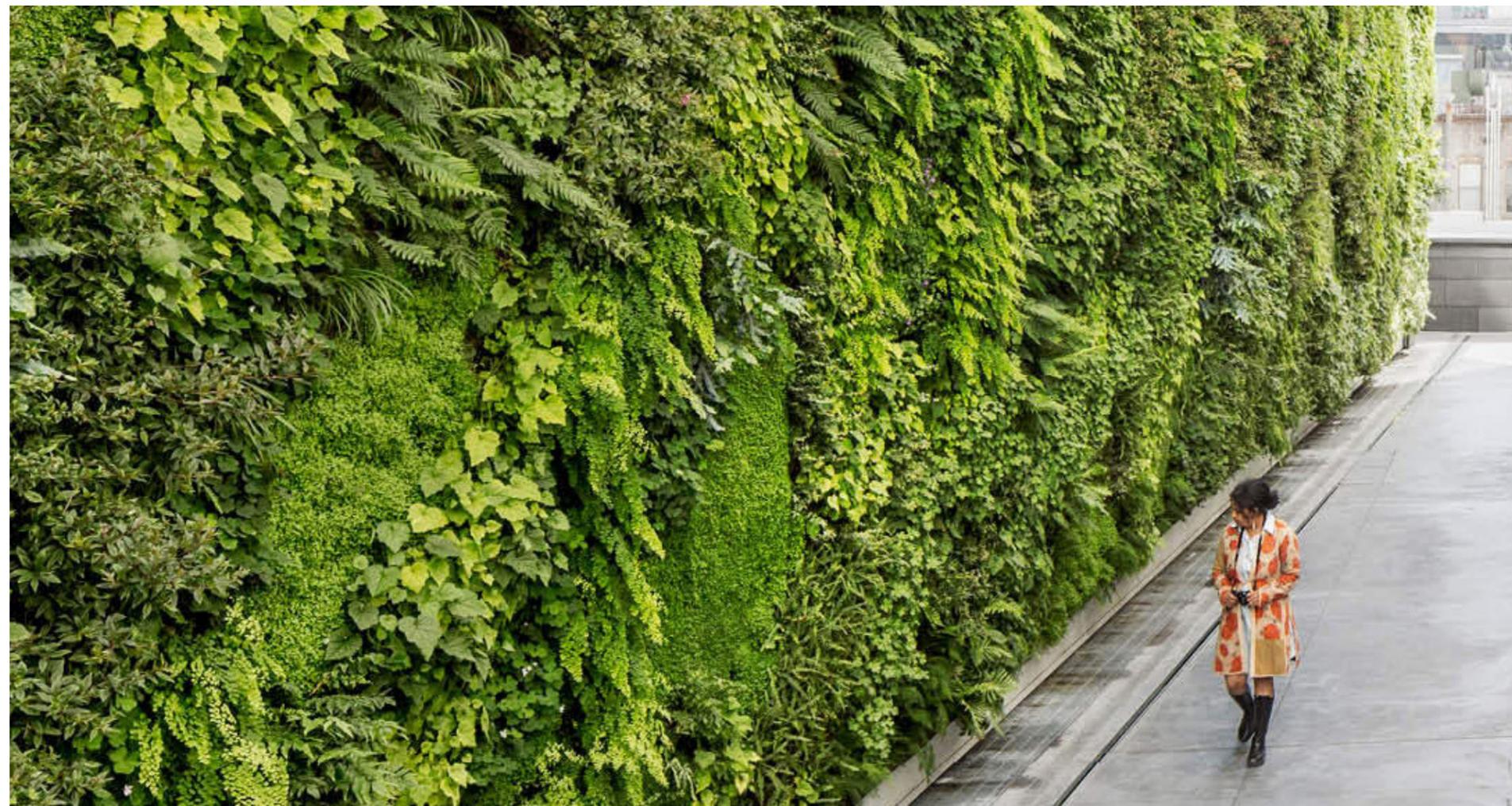
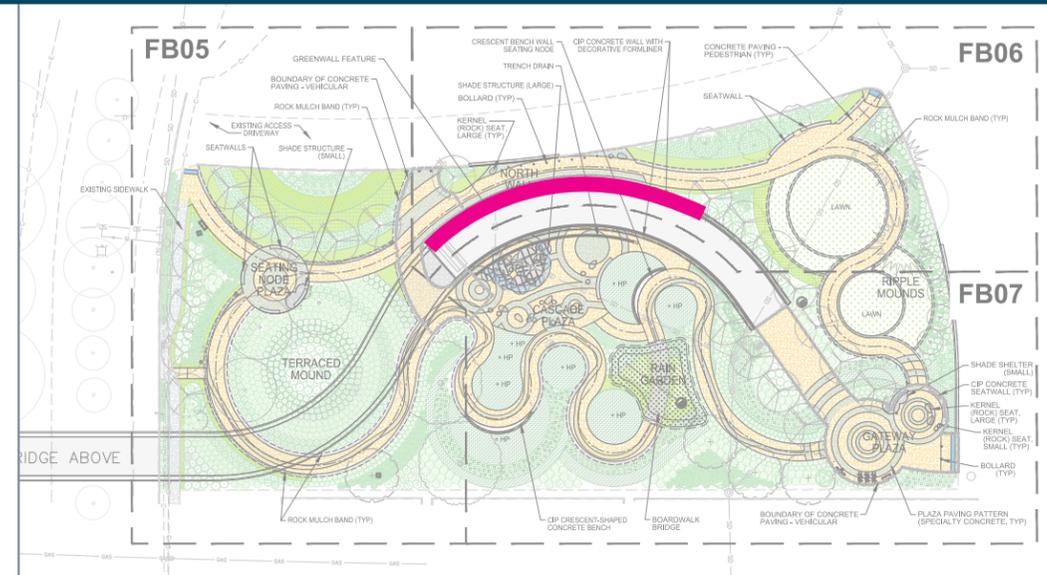


- Maintenance staff expressed concerns with pavers. Concrete with mixed colored glass is suggested alternative

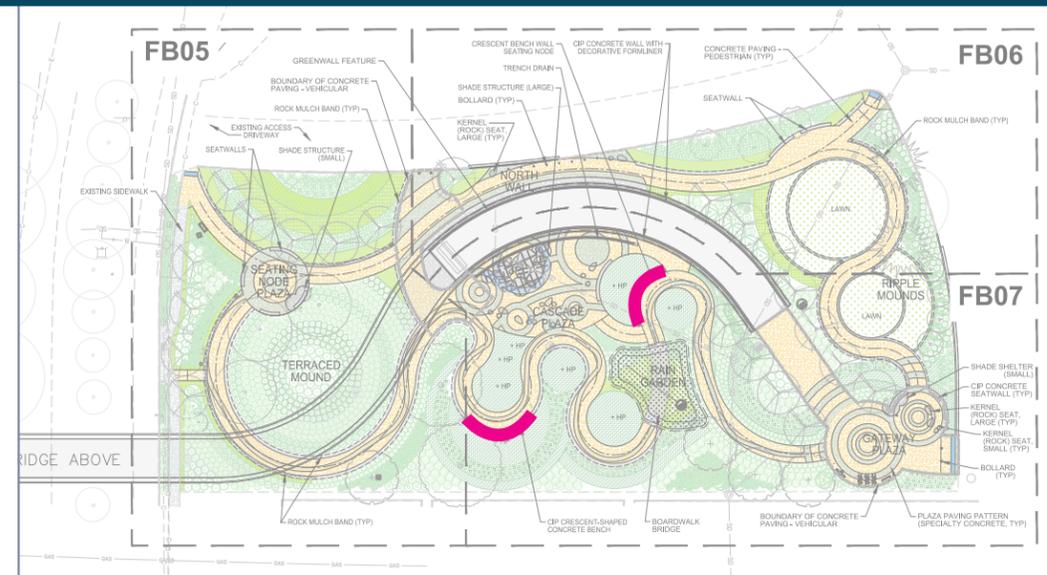


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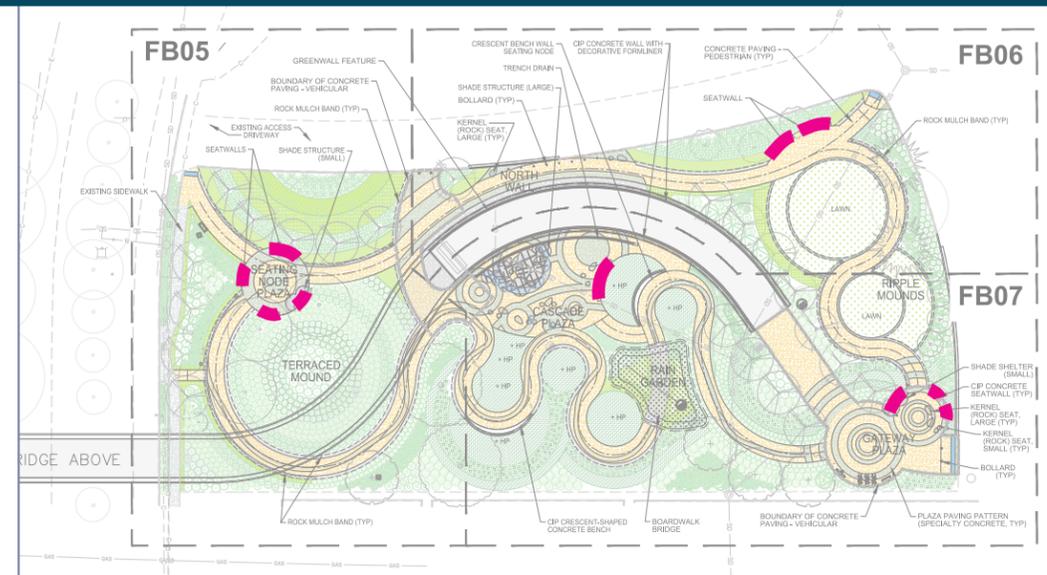
60% Design Features - GREEN/FERN WALL



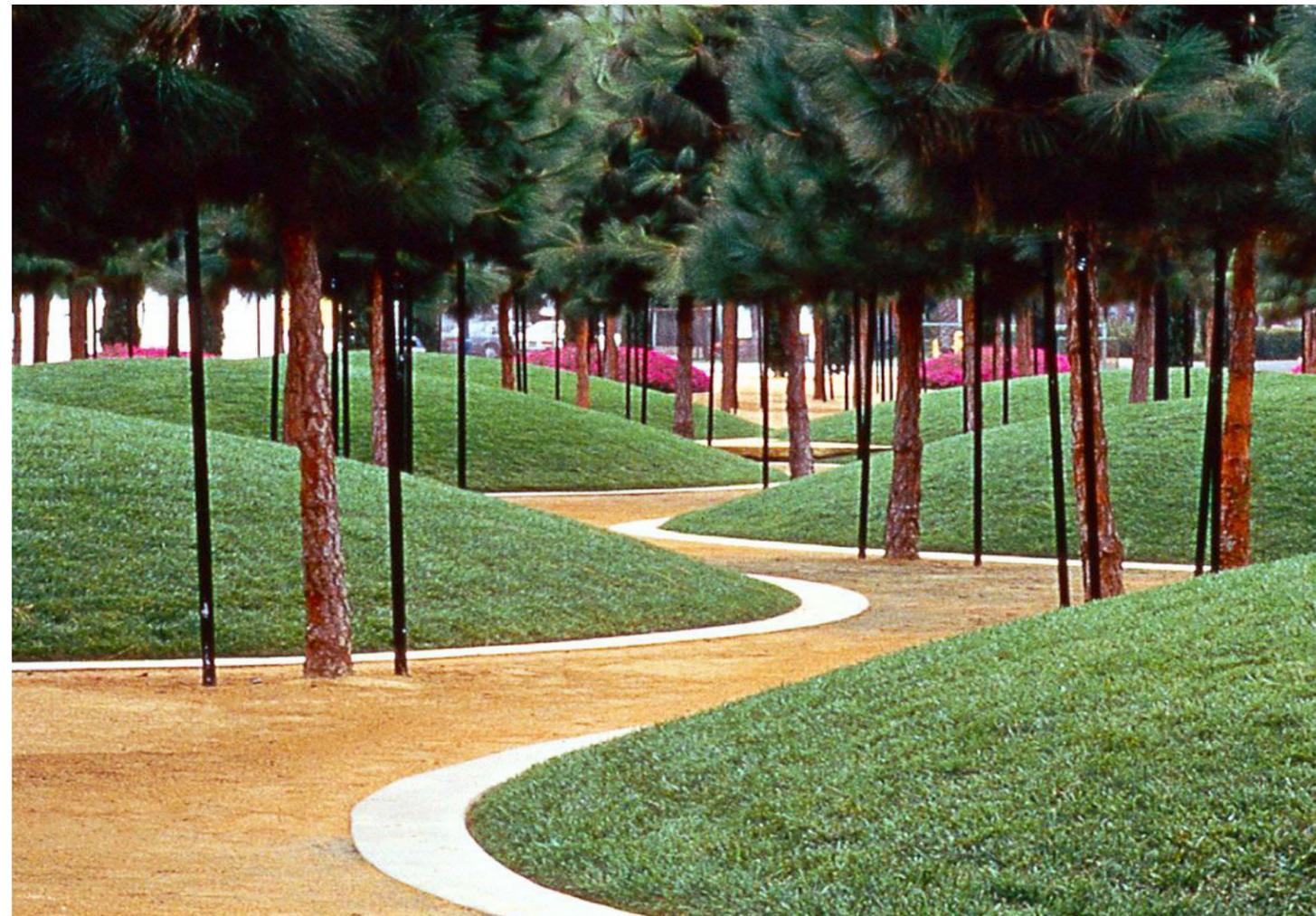
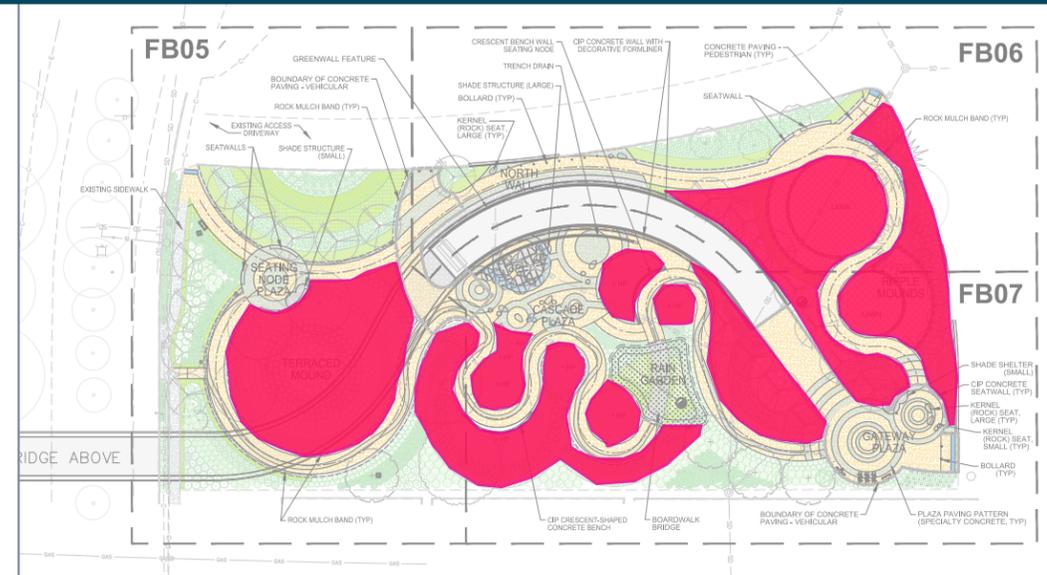
60% Design Features - CRESENT SHAPED CONCRETE BENCH



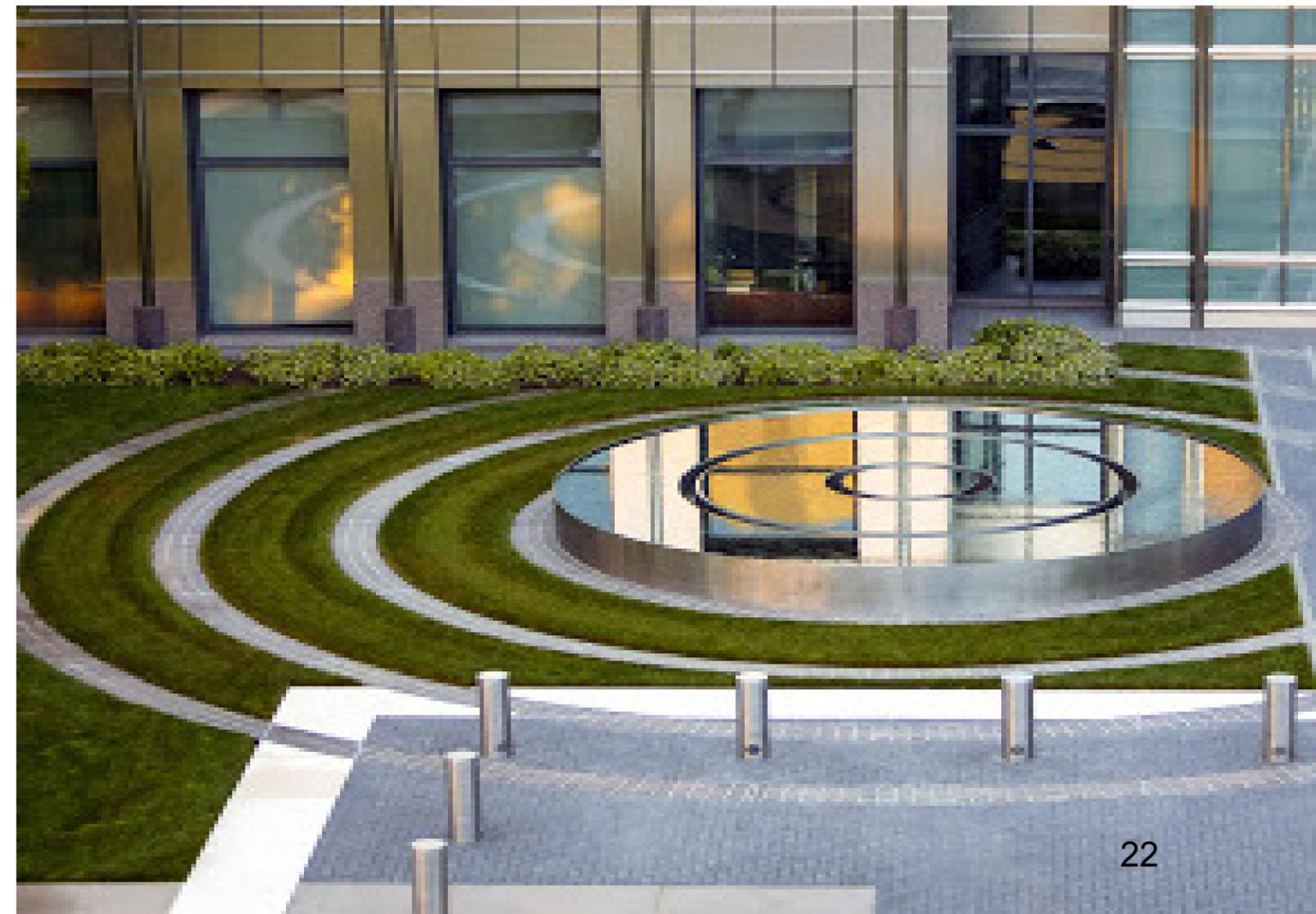
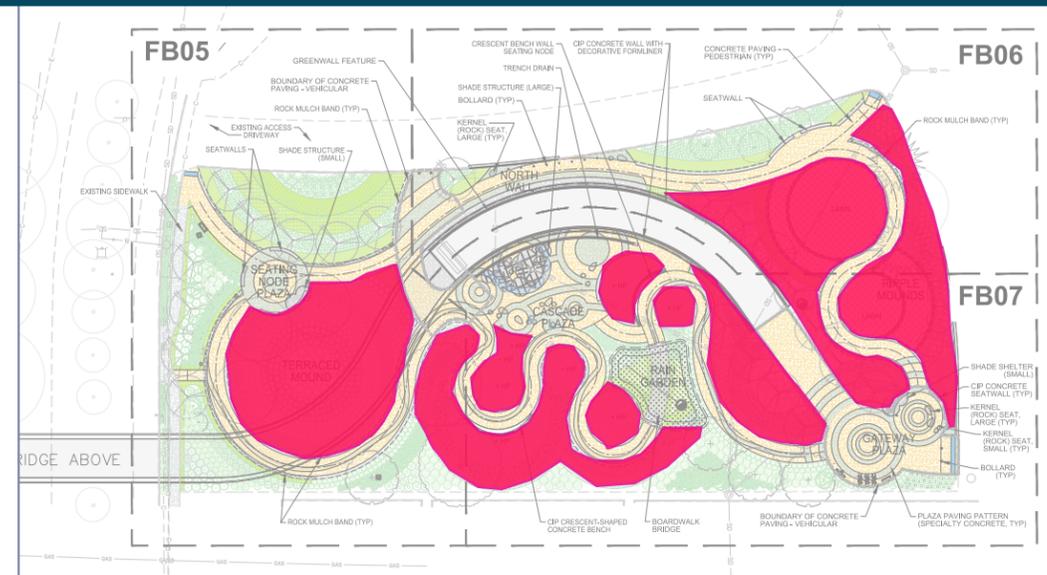
60% Design Features - SEATWALLS AND SIDE TABLES



60% Design Features - MOUNDS



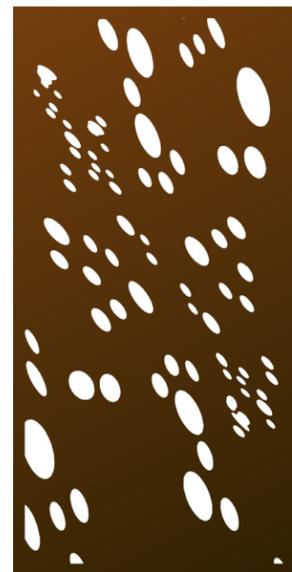
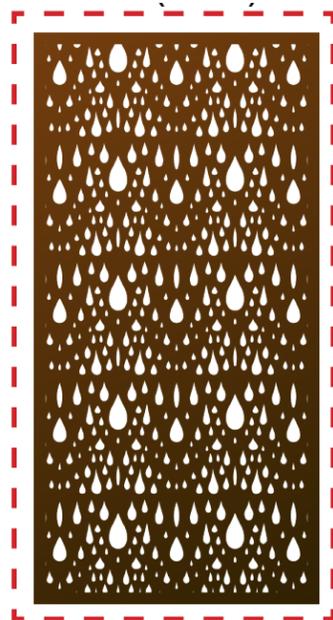
60% Design Features - RIPPLE DETAILS



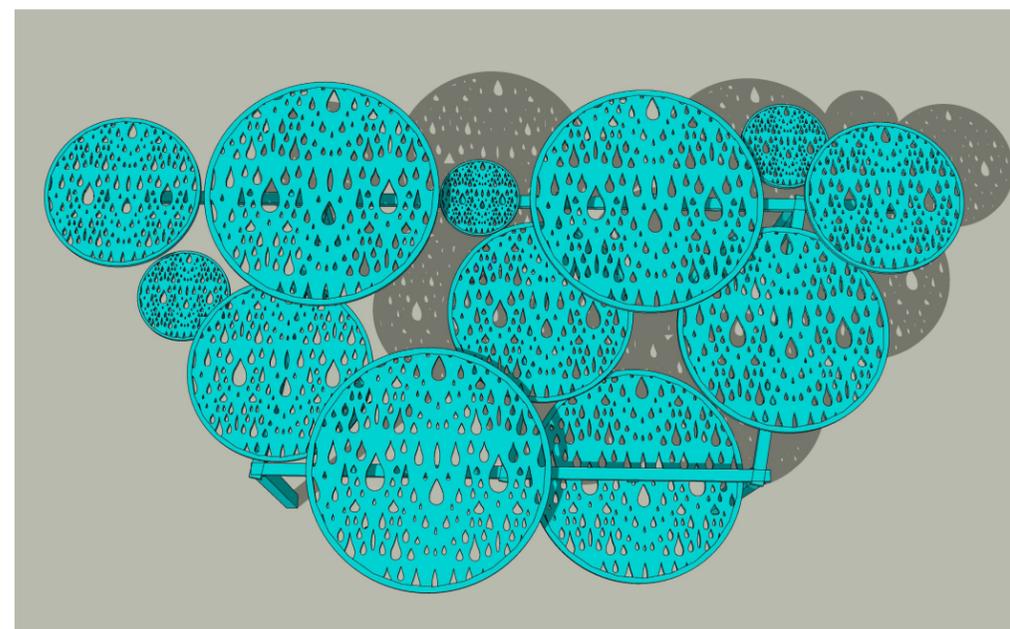
60% Design Features - RAINGARDEN



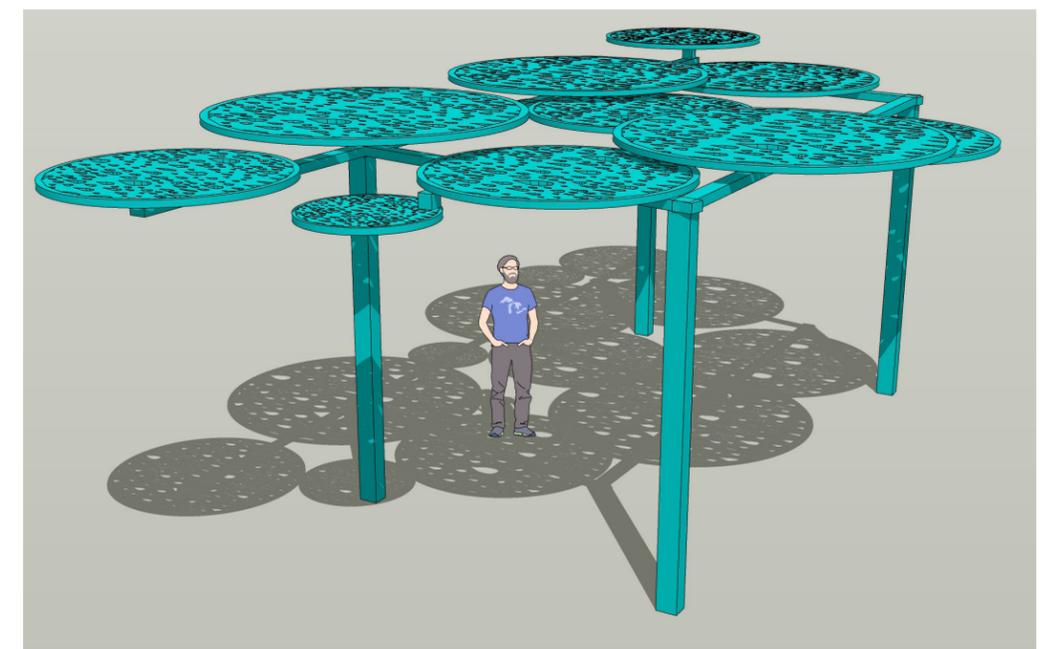
60% Design Features - SHADE STRUCTURE



City of Wilsonville
custom color



Plan View



Perspective View

60% Preliminary Cost Estimate

A) BICYCLE-PEDESTRIAN BRIDGE AND RAMP:
\$14.2M-\$18.8M

B) PLAZA COST: \$2.7M-\$3.7M*

*COST ASSUMES CONCRETE FORMLINER, UPCHARGE WILL BE ADDED FOR SOURCED BASALT COLUMNS

TOTAL: \$16.9M-\$22.5M

COST CONSIDERATIONS

- Range of options for each key element that vary in cost.
- Revised the west approach alignment so that the wall would start south of Peyton.
- Using retaining walls to the extent practical to reduce the bridge cost.
- Approached ODOT to use cable barrier in the median rather than concrete barrier eliminating widening work on mainline I-5.

WILSONVILLE TOWN CENTER I-5 PEDESTRIAN AND BICYCLE BRIDGE: BARBER STREET TO WILSONVILLE TOWN CENTER



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WILSONVILLE TOWN CENTER
I-5 PEDESTRIAN BRIDGE

