

**AGENDA**

**WILSONVILLE CITY COUNCIL MEETING  
JULY 21, 2014**

**7:30 P.M.**

**CITY HALL  
29799 SW TOWN CENTER LOOP  
WILSONVILLE, OREGON**

Mayor Tim Knapp

Council President Scott Starr  
Councilor Susie Stevens

Councilor Richard Goddard  
Councilor Julie Fitzgerald

**CITY COUNCIL MISSION STATEMENT**

To protect and enhance Wilsonville's livability by providing quality service to ensure a safe, attractive, economically vital community while preserving our natural environment and heritage.

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**Executive Session is held in the Willamette River Room, City Hall, 2<sup>nd</sup> Floor**

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|------------------|---|-----------|
| <b>5:00 P.M.</b> | <b>EXECUTIVE SESSION</b>  | [15 min.] |
| A.               | Pursuant to ORS 192.660(2)(f) Exempt Public Records<br>ORS 192.660(2)(h) Litigation |           |
| <b>5:15 P.M.</b> | <b>REVIEW OF AGENDA</b>   | [5 min.]  |
| <b>5:20 P.M.</b> | <b>COUNCILORS' CONCERNS</b>   | [5 min.]  |
| <b>5:25 P.M.</b> | <b>PRE-COUNCIL WORK SESSION</b>   |           |
| A.               | Form Based Code – Neamtzu   | [30 min.] |
| B.               | ODOT Right of Way Sound Wall at Jory Trail<br>(Neamtzu)                             | [10 min.] |
| C.               | Charbonneau Consolidated Improvement Plan<br>(Kraushaar)                            | [40 min.] |
| D.               | Frog Pond Concept Plan Update (Mangle)  | [20 min.] |
| E.               | Development Code Section 4.022 Appeal and Call-up<br>Procedures (Neamtzu)           | [15 min.] |
| <b>7:25 P.M.</b> | <b>ADJOURN</b>  |           |
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## CITY COUNCIL MEETING

The following is a summary of the legislative and other matters to come before the Wilsonville City Council a regular session to be held, Monday, July 21, 2014 at City Hall. Legislative matters must have been filed in the office of the City Recorder by 10 a.m. on July 8, 2014. Remonstrances and other documents pertaining to any matters listed in said summary filed at or prior to the time of the meeting may be considered therewith except where a time limit for filing has been fixed.

### 7:30 P.M. CALL TO ORDER

- A. Roll Call
- B. Pledge of Allegiance
- C. Motion to approve the following order of the agenda and to remove items from the consent agenda.

### 7:35 P.M. MAYOR'S BUSINESS

- A. Proclamation Declaring the Month of August 2014 as "Wilsonville Rotary / End Polio Now Month" (Mary Stewart, President of Wilsonville Rotary)
- B. Upcoming Meetings

### 7:45 P.M. CITIZEN INPUT & COMMUNITY ANNOUNCEMENTS

This is an opportunity for visitors to address the City Council on items *not* on the agenda. It is also the time to address items that are on the agenda but not scheduled for a public hearing. Staff and the City Council will make every effort to respond to questions raised during citizens input before tonight's meeting ends or as quickly as possible thereafter. Please limit your comments to three minutes.

### 7:55 P.M. COUNCILOR COMMENTS, LIAISON REPORTS & MEETING ANNOUNCEMENTS

- A. Council President Starr – (*Park & Recreation Advisory Board Liaison*)
- B. Councilor Goddard – (*Library Board Liaison*)
- C. Councilor Fitzgerald – (*Development Review Panels A & B Liaison*)
- D. Councilor Stevens – (*Planning Commission; CCI; Wilsonville Seniors Liaison*)

### 8:15 P.M. CONSENT AGENDA

- A. Minutes of the June 2, 2014 and July 7, 2014 Council Meetings. (staff – King)

### 8:20 P.M. CITY MANAGER'S BUSINESS

### 8:25 P.M. LEGAL BUSINESS

### 8:30 P.M. ADJOURN

Time frames for agenda items are not time certain (i.e. Agenda items may be considered earlier than indicated. The Mayor will call for a majority vote of the Council before allotting more time than indicated for an agenda item.) Assistive Listening Devices (ALD) are available for persons with impaired hearing and can be scheduled for this meeting if required at least 48 hours prior to the meeting. The city will also endeavor to provide the following services, without cost, if requested at least 48 hours prior to the meeting: -Qualified sign language interpreters for persons with speech or hearing impairments. Qualified bilingual interpreters. To obtain services, please contact the City Recorder, (503)570-1506 or [king@ci.wilsonville.or.us](mailto:king@ci.wilsonville.or.us)

**CITY COUNCIL WORKSESSION  
STAFF REPORT**

<b>Meeting Date:</b> July 9, 2014	<b>Subject:</b> Coffee Creek Industrial Area Form Based Code and Pattern Book  <b>Staff Member:</b> Chris Neamtzu, Planning Director <b>Department:</b> Community Development	
<b>Action Required</b>	<b>Advisory Board/Commission Recommendation</b>	
<input type="checkbox"/> Motion <input type="checkbox"/> Public Hearing Date: <input type="checkbox"/> Ordinance 1 <sup>st</sup> Reading Date: <input type="checkbox"/> Ordinance 2 <sup>nd</sup> Reading Date: <input type="checkbox"/> Resolution <input checked="" type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input type="checkbox"/> Council Direction <input type="checkbox"/> Consent Agenda	<input type="checkbox"/> Approval <input type="checkbox"/> Denial <input type="checkbox"/> None Forwarded <input checked="" type="checkbox"/> Not Applicable  <b>Comments:</b>	
<b>Staff Recommendation:</b> Conduct the work session and provide staff with direction on revisions to the documents.		
<b>Recommended Language for Motion:</b> N/A		
<b>PROJECT / ISSUE RELATES TO:</b>		
<input checked="" type="checkbox"/> Council Goals/Priorities Clear vision and community design; Thoughtful land use; Multi-modal transportation network; Economic development.	<input checked="" type="checkbox"/> Adopted Master Plan(s) Coffee Creek Industrial Area Master Plan; Goal 9 EOA; Economic Development Strategy.	<input type="checkbox"/> Not Applicable

**ISSUE BEFORE COUNCIL:**

Receive consultant and staff presentation on efforts to develop a form-based code and pattern book for the Coffee Creek Industrial Area.

**EXECUTIVE SUMMARY:**

The City was awarded a grant from the Transportation and Growth Management (TGM) Program to create a Light-industrial Form-based Code (FBC) and Pattern Book that would apply to the Coffee Creek Industrial Area.

The City Council accepted the grant award and conducted a work session on the scope of work at the October 21, 2013 meeting. The Planning Commission received an orientation to Form-based Codes on February 19<sup>th</sup> and provided input to staff and the consultant team on both the Form-based Code and the Pattern Book. This project represents an opportunity to streamline the approval process for projects in the Coffee Creek Industrial Area while still maintaining the high quality design that is evident in the city's industrial zone.

The consultants have developed initial drafts of the Form-based Code and Pattern Book that have been reviewed by staff. The consultant team will provide an overview of the project, present the revised draft, summarize staff comments, and describe how the next steps in the project will address specific issues.

The purpose of the Form-based Code and Pattern Book is to support economic development and job creation through an integrated system of code elements that include:

1. A Form-based Code for the master plan area with clear and objective standards that provide a greater degree of certainty to applicants; and
2. A targeted set of specific adjustments to the FBC standards that offer flexibility to applicants and that can be administered by staff; and
3. A Pattern Book that details alternative design approaches that may be used instead of the clear and objective standards in the FBC.

Development applicants would have a choice of designing to meet either the clear and objective standards of the FBC, with compliance administered largely by staff, or meeting the flexible, subjective guidelines of the Pattern Book, with compliance administered by the Development Review Board. By providing an alternative path in which compliance with clear and objective standards is determined by staff, this project represents an opportunity to streamline the approval process for new industrial projects.

Connectivity is a crucial aspect of making walkable, bikeable places - a primary goal of the TGM program. This project seeks to create standards that lead to the development of industrial areas that accommodate all modes of transportation, including walking, biking, and transit, along with cars and trucks. A place with a high degree of transportation network (including streets, paths, trails, etc.) connectivity will have more direct connections and multiple routes from which to choose. The distance between origin and destination directly influences mode choice: the longer the distance, the less likely someone is to choose to walk or bike (or take transit, if it involves a long walk to/from the transit stop).

Thus, when increased connectivity leads to more direct (i.e., shorter) routes between points, the place becomes more walkable and bikeable. In the context of the Form-based Code project, this means that when connections (whether they be streets or walking/biking routes) are made at more frequent intervals, employees are more likely to walk or bike to access transit, lunch, or other destinations, and not see distance as a barrier to choosing that mode. Additionally, since increased connectivity offers more route choice, a walker or cyclist may have the option of choosing a route that is more comfortable to navigate and avoid routes with high speeds, trucks, and traffic that feel unsafe.

A challenge to the project has been planning for the implementation of a connected network of existing and new streets that add connectivity while also preserving large sites capable of

supporting large-scale industrial buildings. Form-based codes usually link regulations governing building and site design to streets. In the Coffee Creek Industrial Area, as in many industrial areas, there will be buildings that don't front streets; rather, they will front onto future streets or easements (or private street-like connections through sites or multi-use paths). Therefore, the Coffee Creek Form-based Code and Pattern Book propose to create a system of regulations that apply to buildings depending on what type of future connection they will be adjacent to.

The Council will also have the opportunity to discuss if the Form-based Code and Pattern Book should replace the Day Road Design Overlay District (Attachment B).

### **EXPECTED RESULTS:**

The project outcome will support economic development and job creation through regulations that provide the appropriate balance of certainty with a generous range of flexibility that result in high-quality design from the public realm to site design and landscaping to the buildings.

The Coffee Creek Industrial Area Master Plan (2007) could be amended to incorporate references to the connectivity standards of the Form-based Code. The Day Road Design Overlay Zone could be repealed and replaced by the new code.

The Coffee Creek Form-based Code and Pattern Book together establish regulations and guidelines for street design and connectivity, site design and circulation, building form and massing, and building design and architecture.

The intent is to create:

1. A multi-modal transportation network that accommodates pedestrians, bicyclists, transit riders, motorists, and freight in the context of a modern light industrial and employment district;
2. A complete network of existing and new streets, paths, and trails that will support a sense of place and identity; and
3. An industrial and employment district featuring cohesive and high-quality site, landscape and building design through an emphasis on the design of the public realm.

The Form-based Code uses clear and objective standards that are specific, discrete requirements and numerical standards, which substantially minimize judgment about compliance. Additional flexibility is built in to the Form-based Code with adjustment criteria for a limited set of standards that provide additional flexibility to applicants and can be administered by staff.

### **TIMELINE:**

- July 9<sup>th</sup> - Planning Commission Work Session
- July 21<sup>st</sup> - City Council Work Session
- Web page design/public input upcoming in Summer
- Neighborhood meeting date TBD
- Additional TAC input
- July through October - Final Draft Form-based Code and Pattern Book review and adoption.

### **CURRENT YEAR BUDGET IMPACTS:**

This is a grant funded project. Staff time is needed to manage the project, review the draft plans and facilitate the public review portions of the project.

**LEGAL REVIEW / COMMENT:**

Reviewed by: MEK\_\_\_\_\_ Date: July 10, 2014\_\_\_\_\_

This is a significant change to our land use development process. Working with the pattern books developed in conjunction with the Villebois Village Master Plan, which was a first at the time for the City, has proven successful with no litigation resulting. How the pattern book(s) relate to the clear and objective standards with visuals should assist in clarifying the process. The use of pattern book(s) may also assist in transitioning from the Day Road Overlay Zone standards which encompassed a great deal of Council and community input at the time.

**COMMUNITY INVOLVEMENT PROCESS:**

The scope of work outlines the details of the public involvement process. It involves work sessions with the Planning Commission and City Council, development of a project web site, engagement with a Technical Advisory Committee, and community engagement. The city's normal public review process is in addition to these other targeted outreach efforts.

**POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY** (businesses, neighborhoods, protected and other groups):

The potential for expedited review needs to be balanced with the expectations of community members to understand development proposals while ensuring appropriate timelines for review and comment on such proposals. Streamlined review would be beneficial to the development community saving time and money.

**ALTERNATIVES:**

There are a wide variety of alternatives possible on this project. The Council could accept it all, reject it all, or modify the proposal and direct staff to advance to the next phases of the project.

**CITY MANAGER COMMENT:****ATTACHMENTS:**

- A. Memo from the Consultant Team including:
  - July 2, 2014 Draft Form-based Code for Coffee Creek Industrial Design Overlay District
  - July 2, 2014 Draft Pattern Book for Coffee Creek Industrial Design Overlay District
- B. Day Road Design Overlay Zone
- C. February 19<sup>th</sup> Planning Commission work session minutes
- D. February 19<sup>th</sup> Technical Advisory Committee meeting minutes
- E. February 19<sup>th</sup> PC and TAC meeting PowerPoint slides

**Date** 02 July 2014  
**Project** TGM Code Assistance, Light Industrial Code Amendments, City of Wilsonville, Oregon  
**To** Chris Neamtzu  
**From** Consultant Team Marcy McInelly, Keith Liden and Joseph Readdy  
**Copy** Laura Buhl

## Introduction

The city of Wilsonville has been preparing for development of a new industrial area on its northern edge for several years. The Coffee Creek Industrial Area Master Plan was completed for this area in 2007 to guide the timing and character of future industrial development in an area of approximately 216 acres (Figure 1). Subsequently, the city adopted the Day Road Design Overlay District (Day Road DOD) as part of the Wilsonville Code (Section 4.134), which includes special design standards for properties fronting on Day Road.

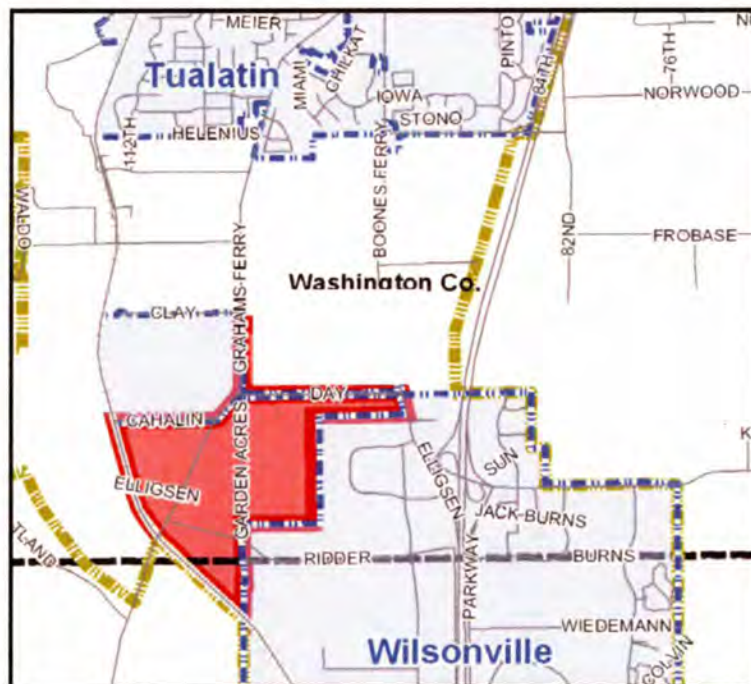


Figure 1. Coffee Creek Industrial Master Plan Area

The city currently uses a discretionary development review process to achieve its design goals, but it is interested in creating a quicker and more predictable development review process using a form-based code. The objectives of the project are to create code standards that will streamline light industrial development review while ensuring high quality design and a multi-modal transportation network that accommodates pedestrians, bicycles, transit, automobiles, and freight.

In order to achieve these objectives, three products (attached) have been prepared for Wilsonville Planning Commission review:

- A Master Table outlining the relationships between code standards, guidelines, and review procedure;
- A light industrial form-based code for the Coffee Creek Industrial Area (Section 4.135 Coffee Creek Design Overlay District); and
- A Pattern Book for the Coffee Creek Industrial Area.

The focus of the July 9<sup>th</sup> Planning Commission Work Session will be the relationship of the regulatory components to one another and the proposed review process.

### **Regulatory Components of the Two-Track System for Coffee Creek Industrial Design Overlay District**

The Coffee Creek Regulatory system has two primary components

- Development Standards in the proposed 4.134 Coffee Creek DOD; and
- Design Guidelines in the Pattern Book.

The Coffee Creek DOD, or Form Based Code, will consist of clear and objective Development Standards. The Development Standards, which are proposed to be administered largely by staff and provide a streamlined path of approval. The Pattern Book consists of discretionary Design Guidelines that are applied in the event that an applicant cannot or chooses not to meet the Development Standards. The Design Guidelines are proposed to be administered largely by the Development Review Board. The attached Master Table summarizes how the two components work together.

### **Two-Track System--Choosing the Right Track**

Below is a decision tree that summarizes the two-track system:

- Does your project comply with all of the Coffee Creek DOD Development Standards?
- If yes, proceed to staff review and approval (Track One).
- If no, does your project comply with all the development standards if the Adjustment Allowances are applied?
- If yes, proceed to staff review and approval.
- If no, proceed to DRB review (Track Two) and approval where:
  - Staff approves those aspects of the project that comply with the Coffee Creek DOD Development Standards.
  - Staff approves those aspects of the project that comply with the Coffee Creek DOD Development Standards with Adjustment Allowances applied.
  - Staff prepares a recommendation on all other aspects of the project and forwards recommendation to the Development Review Board. Staff uses the Design Guidelines to make their recommendations.
  - The Development Review Board reviews project aspects subject to the Design Guidelines and Intent Statements in the Pattern Book.

A variation to Track Two: Staff makes a tentative decision on the "non-DRB" items, but the DRB may reconsider to the extent it is relevant to reaching a suitable solution to the design "aspects" it's reviewing.



### **Relationship between the Wilsonville Code and the proposed Coffee Creek DOD and Pattern Book**

The Coffee Creek DOD will provide the standards for new industrial development within the Coffee Creek Industrial Area Master Plan area (Figure 1). The Coffee Creek DOD is proposed to be part of the Wilsonville Code, replacing the Day Road DOD (Section 4.134). While the Day Road DOD covers properties adjacent to Day Road, the Coffee Creek DOD would cover the entire Coffee Creek Industrial Area Master Plan area, including all of the existing Day Road DOD. The Coffee Creek DOD will function in a similar way to the Day Road DOD by providing development regulations that supplement the city's basic industrial zoning requirements.

Similar to the Day Road DOD, the Coffee Creek DOD will rely upon other zoning requirements found in the Wilsonville Code, such as landscaping and lighting. In addition to the Coffee Creek DOD, a Pattern Book for Coffee Creek Industrial Area will provide supporting design guidance to better illustrate how to implement the development standards in the Coffee Creek DOD and the Wilsonville Code. The Coffee Creek DOD and Pattern Book both follow a corresponding organization to facilitate a complete understanding between the DOD development standards and the corresponding guidelines to be considered to meet the standards.

### **Summary of the Proposed Light Industrial Form-Based Code Actions**

Proposed amendments to the Wilsonville Code include:

- Replacing the Day Road Design Overlay District (Wilsonville Code, §4.134) with the Coffee Creek Light Industrial Design Overlay District (Coffee Creek DOD).
- The Coffee Creek DOD will be an overlay zone similar to the Day Road DOD, but it will apply to the entire Coffee Creek Industrial Area Master Plan area shown in Figure 1.
- The PDI-RSIA zone (Wilsonville Code, §4.135.5) will remain as the intended base zone upon annexation of Coffee Creek Industrial Area Master Plan area properties into the city.
- Identification of the Wilsonville Code provisions that will remain in effect, be amended, or be superseded by the new Coffee Creek DOD.
- The Pattern Book for Coffee Creek Industrial Area to provide supplemental design guidance to complement the Wilsonville Code requirements.
- Supplementing the city's street design standards to enhance the pedestrian and cycling environment.

Date 2 July, 2014      Wilsonville Coffee Creek Light Industrial Area Code Amendments

**Master Organization Table for Sections of the Coffee Creek Industrial Design Overlay District - the Pattern Book and the Form Based Code**

Category - what is being regulated Chapter Heading	Pattern Book		Form-Based Code	
	Intent Statement Subheadings	Guideline Sub-subheadings Separate guideline or set of guidelines with text and illustration	Development Standards Row headings on the Development Standards Table (left side of table) Subsections of Section 4.134 Coffee Creek Industrial Area Design Overlay District (Coffee Creek DOD)	Adjustment Allowance
<b>Entire District – The Place of Coffee Creek</b>	<b>District-Wide Site Planning and Landscaping – Intent Statement</b>  (includes a general statement about the value of district-wide site planning and landscaping, followed by specific guidelines)	<b>Section A</b> District-wide Site Planning and Landscaping The Ice Age Landscape of Coffee Creek Develop and Emphasize the Themes of Coffee Creek Strengthening Gateways	<b>(.12) – A - District-wide Planning and Landscaping</b>  District-Wide Planning and Landscaping: tree removal, relocation or replacement – refers to 4.610.10  District-Wide Planning and Landscaping: tree removal, relocation or replacement – refers to 4.610.10(.01)	
<b>Achieving Connectivity</b>	<b>Achieving Connectivity - Intent Statement</b>  (includes a general statement about the value of connectivity, followed by specific statements about the character and function of Addressing, Supporting Streets and Through Connections)	<b>Section B</b> Addressing Streets Supporting Streets Through Connections	<b>(.11) Street Design and Connectivity</b>  Connection Spacing Connection Type	Adjustment for Connection Spacing (Y)  Adjustment for Connection Type (N)
<b>Site Design</b>	<b>Site Design- Intent Statement</b>  (includes a general statement about all parcels, followed by specific statements about parcels on Addressing, Supporting Streets and Through Connections)	<b>Section C</b> Parcel Access	<b>(.12) – B – Site Design – Parcel Access</b>  Site Design Applicability Parcel Driveway Access Parcel Driveway Width	

	Addressing Streets Supporting Streets Through Connections	<b>(.12) – B – Site Design – Parcel Pedestrian Access</b> General, Parcel Pedestrian Access Parcel Pedestrian Access per sections of 4.134, 4.154, 4.167 Parcel Pedestrian Access Spacing Parcel Pedestrian Access Width Parcel Pedestrian Access to Transit
		<b>(.12) – B – Site Design – Parcel Frontage</b> Parcel Frontage, Defined Parcel Frontage Occupied by a Building
<b>Parking Location and Design– Intent Statement</b>  (includes a general statement about all parcels, followed by specific statements about parcels on Addressing, Supporting Streets and Through Connections)	Parking Location and Design	<b>(.12) – B – Site Design – Parking Location and Design</b>  Parking Location and Design per sections of 4.134(.05) E parking at rear; 4.155 regarding loading, bicycle, etc; 4.176 regarding perimeter landscaping  Parking location and extent Parking setback Parking lot sidewalks Parking perimeter screening and landscaping Bicycle parking Off-street loading Carpool and Vanpool Parking
<b>Design that Contributes to the Site – Intent Statement</b>	Planting	<b>(.12) – B – Site Design – Grading and Retaining Walls</b>  Maximum height Required material Design (required offsets every 650 feet)

			<p><b>(.12) – B – Site Design – Planting</b></p> <p>General, reference to 4.176.02 Landscaping and Screening Standards</p> <p>Landscaping Standards permitted on different connection types</p> <p>Native Plant Materials</p>
	<b>Design that Contributes to the Building – Intent Statement</b>	<p>Primary Building Entries</p> <p>Location and Screening of Utilities and Services</p> <p>Exterior Lighting</p>	<p><b>(.12) – B – Site Design – Location and Screening of Utilities and Services</b></p> <p>General reference to 4.179 regarding Mixed Recyclables Section</p> <p>Location and Visibility</p> <p>Required screening on different connection types</p>
<b>Building Design</b>	<b>Designing the Building – Intent Statement</b>	Prominent Building Entrance	<p><b>(.12) – C – Building Design – Building Orientation</b></p> <p>Building orientation: Front façade</p>
			<p><b>(.12) – C – Building Design – Building Entrance</b></p> <p>Primary Building Entrance: ADA</p> <p>Primary Building Entrance: Location in relation to connection type</p> <p>Primary Building Entrance: visibility from connection type</p> <p>Primary Building Entrance: accessibility</p> <p>Primary Building Entrance: Required canopy</p> <p>Primary Building Entrance: Required amenity</p> <p>Primary Building Entrance: Transparency</p> <p>Primary Building Entrance: Lighting</p>

		<p>Overall Building Mass</p>	<p><b>(.12) – C – Building Design – Overall Building Mass</b>                  Front setback                  Allowance of primary building entrance (to extend into setback on an Addressing Street)                  Required Minimum height                  Ground floor height</p>	
		<p>Composition of Building Elevations</p>	<p><b>(.12) – C – Building Design – Building Elevations</b>                  Base, body and top dimensions                  Base design                  Top design                  Required Screening of Roof-mounted equipment</p>	
		<p>Roof Forms</p>		
		<p>Materials and Colors</p>		
		<p>Sustainable Building Design</p>		



**WILSONVILLE**  
**COFFEE CREEK INDUSTRIAL**  
**DESIGN OVERLAY DISTRICT (DOD)**

**DRAFT**

2 JULY 2014

CITY OF WILSONVILLE | TRANSPORTATION GROWTH MANAGEMENT (TGM)

URBSWORKS INC | JRA | BAINBRIDGE.

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This project is partially funded by a grant from the Transportation and Growth Management (TGM) Program, a joint program of the Oregon Department of Transportation and the Oregon Department of Land Conservation and Development. This TGM grant is financed, in part, by federal Moving Ahead for Progress in the 21st Century (MAP-21), local government, and the State of Oregon funds.

The contents of this document do not necessarily reflect views or policies of the State of Oregon.

## REVISION OF WILSONVILLE CODE SECTION 4.134

*The Day Road Design Overlay District in Section 4.134 of the Wilsonville Code is proposed to be repealed and replaced with a new Section 4.134 as drafted below.*

### Section 4.134 Coffee Creek Industrial Design Overlay District

- (.01) **Purpose.** The Coffee Creek Industrial Design Overlay District (Coffee Creek DOD) is an overlay district within the Planned Development Industrial - Regionally Significant Industrial Area (RSIA) Zone Section 4.135.5). It is the purpose of this Coffee Creek DOD is to implement the Coffee Creek Industrial Area Master Plan (2007) by establishing standards for street design and connectivity, site design and circulation, building form, and building architecture and landscape for all development located within the master plan area. These standards are intended to result in:
- A. A multi-modal transportation network that accommodates pedestrians, bicyclists, transit riders, motorists, and freight in the context of a modern light industrial district.
  - B. An industrial district featuring cohesive and high-quality site, landscape, and building design, which is well integrated with adjacent streetscapes and other public spaces.
  - C. Preservation of trees and natural features.
  - D. Minimization of adverse impacts on adjacent properties from development that detracts from the character and appearance of the area.
  - E. Minimization of the visibility of vehicular parking, circulation and loading areas.
  - F. Creation of a pleasant and functional industrial district for employees and visitors.
  - G. A more predictable and timely process for reviewing light industrial development applications.
- (.02) **Applicability.** The Coffee Creek DOD shall apply to all properties within the Coffee Creek Industrial Area Master Plan as shown in Figure CC-1 - Regulating Plan. The provisions of this section shall apply to:
- A. All new building construction.
  - B. Any exterior modifications to existing, non-residential buildings, subject to 4.134(.03) Exceptions.
  - C. All new paved parking lots.
  - D. All new outdoor storage and display areas.
  - E. All new signage.
  - F. All building expansions greater than 1,250 square feet.
- (.03) **Exceptions.** This section does not apply to the following:
- A. Maintenance of the exterior of an existing industrial/employment structure, such as painting to the approved color palette, reroofing, or residing with the same or similar materials.
  - B. Industrial/employment building expansions less than 1,250 square feet.
  - C. Interior remodeling.
  - D. Essential public facilities.
  - E. Maintenance of existing dwellings and accessory buildings.
  - F. Maintenance agricultural buildings.

- (.04) Uses that Are Typically Permitted. The uses permitted shall be governed by Section 4.135.5 (.03).
- (.05) Prohibited Uses. The uses prohibited shall be governed by Section 4.135.5 (.04).
- (.06) Overview of Coffee Creek DOD Standards.
- A. Section 4.134 (.09) - Regulating Plan. The Regulating Plan organizes all existing and future streets and shared-use paths within the Coffee Creek Industrial Area into a hierarchy of Addressing Streets, Supporting Streets and Through Connections.
  - B. Section 4.134 (.10)- Connectivity Standards. New Supporting Streets and Through Connections are required within the Coffee Creek Master Plan Area to meet Connectivity Requirements as shown on Figure CC-4 - - Connectivity Standards Diagram.
  - C. Section 4.134 (.11) - Development Standards. The Development Standards Table provides an overview of all applicable Development Standards. The development standards for any given parcel are determined by the existing or future street or shared-use path type on which the parcel fronts, as detailed in Table (.11) - 01 - Development Standards Table.
  - D. Section 4.134 (.10) - Street Types. The Street Types specify the cross sections for each of the street and shared-use path types within the Regulating Plan. These cross section specifications are applicable to both existing and proposed new streets. A range of cross sections for Supporting Streets and Through Connections is permitted and detailed in Figures CC-2 - Supporting Streets, All Types, and Cc-3 - Through Connections, All Types.
  - E. Section 4.134 (.11) - Coffee Creek FBC Development Standards. Areas bounded by new Supporting Streets and Through Connections are designated as Parcels and are required to comply with Development Standards governing site design, building orientation and frontage. The development standards for site design, building façade and landscape design are intended to work in tandem with the street types to create a cohesive and unified public realm.
  - F. Pattern Book. The Wilsonville Pattern Book for Coffee Creek Industrial Area provides supplemental design guidelines, which are intended to allow additional design flexibility than the Development Standards while satisfying the purpose of the Coffee Creek DOD.
- (.07) Review Process. Development applications shall follow the application review process described in:
- A. Section 4.197 Zone Changes and Amendments.
  - B. Section 4.140 Planned Development Regulations.
  - C. Section 4.035 (.03) Class II - Administrative Review.
- (.08) Coffee Creek DOD Regulating Plan, Figure CC-1 - Regulating Plan
- A. Components of the Regulating Plan Map
    - a. Addressing Streets. Existing and planned streets within the Regulating Plan Area are called Addressing Streets and include Cahalin Road, Day Road, Clutter Street, Ridder Road, Grahams Ferry Road, Garden Acres Road, and Kinsman Road, Java Road and Tonquin Road.



- b. Master Plan Area. Land area identified as "Master Plan Area" on Figure CC-1- Regulating Plan is subject to additional Connectivity Standards as detailed in Figure CC-4 - Connectivity Standards, and Table (.11) -1.
- c. Access Zone. New streets and new shared use paths shall intersect with Addressing Streets within the Access Zone. Outside of the Access Zone, only non-vehicular shared use paths may intersect with the Addressing Street.

*Note: with the change in numbering above the following sections will be renumbered in subsequent drafts.*

**(.10) Coffee Creek Connectivity Standards**

- A. Supporting Streets and Through Connections, Figure CC-1 - Regulating Plan. Within the land area bounded by Addressing Streets, called Master Plan Area, connectivity is required to be provided through new local streets and shared use paths. The location, alignment and cross-section of required local streets and shared-use paths is flexible, as long as they comply with spacing and minimum cross section standards. New connections may be one of the following two Types:
  - a. Supporting Streets. Supporting Streets are new local streets. They may be public rights-of-way or public easements. They shall meet the Development Standards set out in Figure CC -2 - Supporting Streets, All Types.
  - b. Through Connections. Through Connections are new streets or multi-use paths, or streets that combine characteristics of local streets and multi-use paths. They may be public rights-of-way or public easements. They shall meet the Development Standards set out in Figure CC -3 - Through Connections, All Types.
- B. Minimum spacing. See Figure CC -4 - Connectivity Standards Diagram and Table (.11) -1 - Development Standards.
- C. Access Zone. The Access Zone is defined by City access management spacing standards. Parcel Access is not permitted from any Addressing Street within 150 feet of an intersection of existing or planned Addressing Streets. See Figure CC -4 - Connectivity Standards.

**(.11) Development Standards Table - Table(.11) -1**

**A. Site Design**

- a. Applicability. Areas bounded by Addressing Streets, Supporting Streets and Through-Parcel Connections shall be designated as a Parcel and subject to the Development Standards for Site Design, Figure CC-5 - Parcel Standards.

<b>Table (.11) -1 - Development Standards</b>			
	<b>Addressing Streets</b>	<b>Supporting Streets</b>	<b>Through Connections</b>
<b>(.11) Street Design and Connectivity</b>			
Connection Spacing	Not applicable, Addressing Streets exist or are planned	600-feet, maximum, centerline to centerline  Exact location and alignment may vary by 50 feet maximum in either direction, provided that the Supporting Street or Through Connection intersects with Addressing Streets at nearest existing or planned	

<b>Table (.11) -1 - Development Standards</b>			
	<b>Addressing Streets</b>	<b>Supporting Streets</b>	<b>Through Connections</b>
		intersection	
Connection Type	Addressing Streets are Day Road, Boones Ferry Road, Grahams Ferry Road, Cahalin Road, Garden Acres Road, Clutter Street and Ridder Road, Java Road and Tonquin Road  Addressing Streets are multimodal	Supporting Street Specifications, see Figure CC - 2  Supporting Streets may be multimodal or exclusively for bike/ped access	Through Connection Specifications, see Figure CC - 3  Through Connections may be multimodal or exclusively for bike/ped access
<b>(.12) - Development Standards</b>			
<b>(.12)-A - District-Wide Planning and Landscaping</b>			
General	The following provisions apply: <ul style="list-style-type: none"> <li>• 4.610.10 for tree removal, relocation or replacement.</li> <li>• 4.610.10(.01) C for consideration of development alternatives to preserve wooded areas &amp; trees.</li> </ul>		
<b>(.12)-B - Site Design</b>			
Applicability	Areas bounded by Addressing Streets, Supporting Streets, and Through Connections shall be designated as a Parcel and subject to the Development Standards for Site Design.		
<b>Parcel Access</b>			
General	Unless noted otherwise below, the following provisions apply: <ul style="list-style-type: none"> <li>• 4.177(.02) for street design;</li> <li>• 4.177(.03) to (.10) for sidewalks, bike facilities, pathways, transit improvements, access drives &amp; intersection spacing.</li> </ul>		
Parcel Driveway Access	Not applicable	Limited by connection spacing standards  Parcel Driveway Access may be employed to meet required connectivity, if it	Limited by connection standards for motorized vehicle access.  Parcel Driveway Access may be employed to meet

<b>Table (.11) -1 - Development Standards</b>			
	<b>Addressing Streets</b>	<b>Supporting Streets</b>	<b>Through Connections</b>
		complies with Supporting Street Standards  Subject to approval by City Engineer	required connectivity, if it complies with Through Connection Standards  Subject to approval by City Engineer
Parcel Driveway Spacing	Not applicable	150-feet, minimum	150-feet, minimum
Parcel Driveway Width	Not applicable	24-foot, maximum or complies with Supporting Street Standards	24-foot, maximum or complies with Through Connection Standards
<b>Parcel Pedestrian Access</b>			
General	Unless noted otherwise below, the following provisions apply: <ul style="list-style-type: none"> <li>• 4.154(.01) for separated &amp; direct pedestrian connections between parking, entrances, street ROW &amp; open space</li> <li>• 4.167(.01) for points of access.</li> </ul>		
Parcel Pedestrian Access Spacing	No restriction		
Parcel Pedestrian Access Width	8 feet wide minimum		
Parcel Pedestrian Access to Transit	Provide separated & direct pedestrian connections between transit stops and parking, entrances, street ROW & open space		
<b>Parcel Frontage</b>			
Parcel Frontage, Defined	Parcel Frontage shall be defined by the linear distance between centerlines of the perpendicular Supporting Streets and Through-Parcel Connections.		
Parcel Frontage Occupied by a Building	A minimum of 50% of the Parcel Frontage shall be occupied by a building, see CC-5	No minimum	
<b>Parking Location and Design</b>			
General	Unless noted otherwise below, the following provisions apply: <ul style="list-style-type: none"> <li>• Section 4.155 (03) Minimum and Maximum Off-Street Parking Requirements:</li> <li>• Section 4.155 (04) Bicycle Parking</li> <li>• Section 4.155 (06) Carpool and Vanpool Parking Requirements</li> <li>• Section 4.176 for Parking Perimeter Screening and Landscaping - permits the parking landscaping and screening standards as multiple options</li> </ul>		

<b>Table (.11) -1 - Development Standards</b>			
	<b>Addressing Streets</b>	<b>Supporting Streets</b>	<b>Through Connections</b>
Parking Location and Extent	Limited to one double-loaded bay of parking, 16 spaces, maximum, designated for short-term (1 hour or less), visitor, and disabled parking only	Limited to double-loaded bay of parking between right-of-way of Supporting Street and building	Parking is permitted between right-of-way of Through Connection and building
Parking Setback	30-foot minimum from the right-of-way of an Addressing Street	20-foot minimum from the right-of-way of an Supporting Street	10-foot minimum from the right-of-way of an Through Connection
Parking Lot Sidewalks	Where off-street parking areas are designed for motor vehicles to overhang beyond curbs, sidewalks adjacent to the curbs shall be increased to a minimum of seven (7) feet in depth.	Where off-street parking areas are designed for motor vehicles to overhang beyond curbs, planted areas adjacent to the curbs shall be increased to a minimum of nine (9) feet in depth.	Where off-street parking areas are designed for motor vehicles to overhang beyond curbs, planted areas adjacent to the curbs shall be increased to a minimum of nine (9) feet in depth.
Parking Perimeter Screening and Landscaping	Screen parking area from view from Addressing Streets by means of one or more of the following: a) General Landscape Standard, 4.176 (.02) C b) High Berm Standard, 4.176 (.02) G, except within 50 feet of a perpendicular Supporting Street or Through Connection as measured from the centerline		Screen parking area from view from Through Connections by means of a) Low Screen Landscape Standard, 4.176 (.02) D, or b) High Wall Standard, 4.176 (.02) F, or c) High Screen Landscaping Standard, 4.176 (.02) H
Off-Street Loading	No off-street loading permitted facing Addressing Streets	No limitation	
Carpool and Vanpool Parking	Not permitted in parking areas facing Addressing Streets	No limitation	
<b>Grading and Retaining Walls</b>			
Maximum height	Where site topography requires adjustments to natural grades, landscape retaining walls shall be 48-inches tall maximum.  Where the grade differential is greater than 30-inches, retaining walls may be stepped.		
Required Materials	Materials for retaining walls shall be unpainted cast-in-place, exposed-aggregate,		

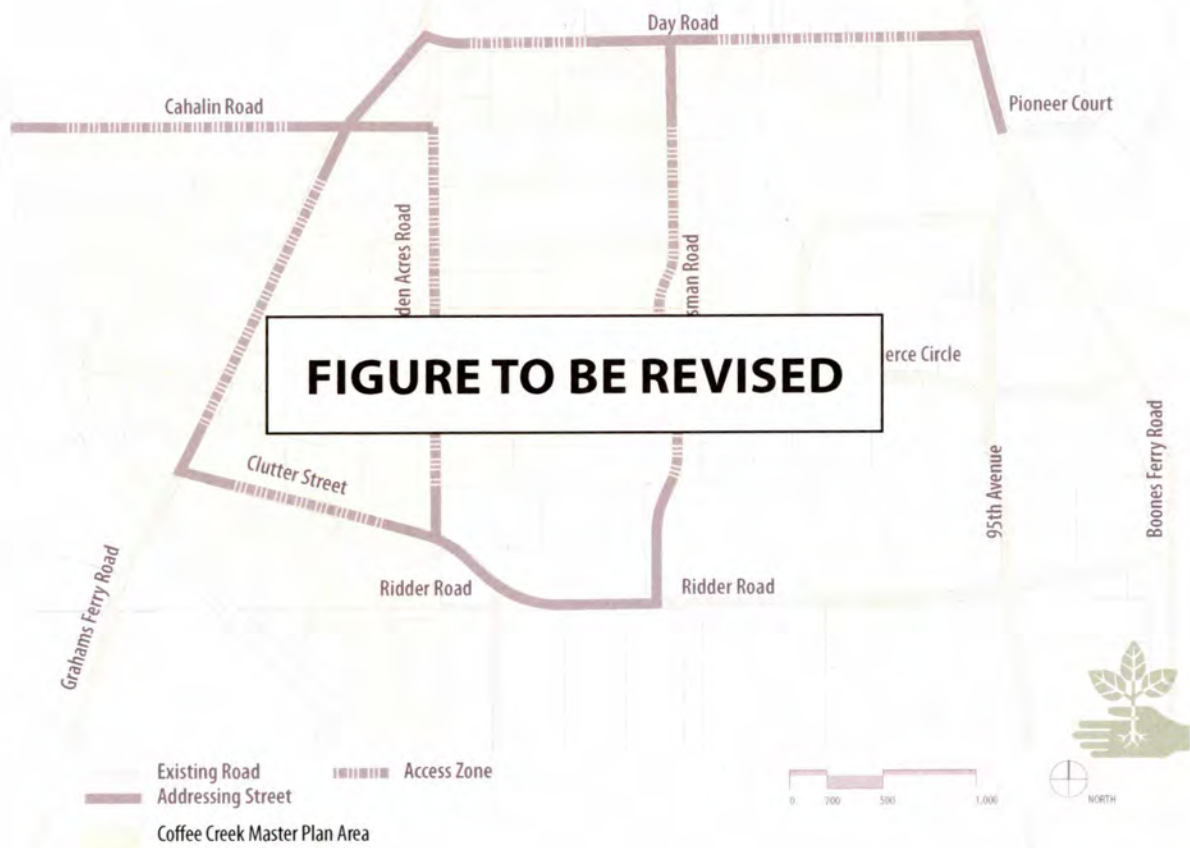
<b>Table (.11) -1 - Development Standards</b>			
	<b>Addressing Streets</b>	<b>Supporting Streets</b>	<b>Through Connections</b>
	or board-formed concrete; brick masonry; stone masonry; or Cor-ten steel.		
Design	Retaining walls longer than 50 linear feet shall introduce a 5-foot, minimum horizontal offset to reduce their apparent mass.		
<b>Planting</b>			
General	Unless noted otherwise below, the following provisions apply: <ul style="list-style-type: none"> <li>• 4.176.02 Landscaping and Screening Standards</li> </ul>		
Landscaping Standards Permitted	General Landscape Standard, 4.176 (.02) C  High Berm Standard, 4.176 (.02) G, except within 50 feet of a perpendicular Supporting Street or Through Connection as measured from the centerline	General Landscape Standard, 4.176 (.02) C  Low Screen Landscape Standard, 4.176 (.02) D  Screen loading areas with High Wall Standard, 4.176 (.02) F, and High Screen Landscaping Standard, 4.176 (.02) E	
Native Plant Materials	100 percent of plant materials used to meet the Landscape Standards shall be native plant materials	Not applicable	
<b>Location and Screening of Utilities and Services</b>			
General	Unless noted otherwise below, the following provisions apply: <ul style="list-style-type: none"> <li>• Section 4.179. Mixed Solid Waste and Recyclables Storage in New Multi-Unit Residential and Non-Residential Buildings</li> </ul>		
Location and Visibility	Site and building service, utility equipment, and outdoor storage of garbage, recycling, or landscape maintenance tools and equipment is not permitted	Site and building service, utility equipment, and outdoor storage of garbage, recycling, or landscape maintenance tools and equipment is not permitted within the setback	No limitation
Required Screening	Not permitted	High Wall Standard and High Screen Landscaping 100 percent native plant materials	
<b>(.12)-B - Building Design</b>			
<b>Building Orientation</b>			
Front Façade	Buildings shall have one designated front façade and two designated side façades		

Table (.11) -1 - Development Standards			
	Addressing Streets	Supporting Streets	Through Connections
	<p>If one of the streets or connections bounding a parcel is an Addressing Street, the front façade of the building shall face the Addressing Street</p> <p>If none of the bounding streets or connections is an Addressing Street, the front façade shall face a Supporting Street</p>		
<b>Primary Building Entrance</b>			
Accessible Entrance	The Primary Building Entrance shall be visible from, and accessible to, an Addressing Street, or a Supporting Street if there is no Addressing Street Frontage. A continuous pedestrian pathway shall connect the Primary Building Entrance with a safe, direct, and convenient path of travel that is free from hazards and provides a reasonably smooth and consistent surface consistent with the requirements of Americans with Disabilities Act (ADA).		
Location	150-feet, maximum from right-of-way of an Addressing Street.	No limitation	
Visibility	Direct line of sight from an Addressing Street to the Primary Building Entrance.	No limitation	
Accessibility	Safe, direct, and convenient path from adjacent sidewalks.		
Required Canopy	Protect the Primary Building Entrance with a canopy with a minimum vertical clearance of 15-feet and an all-weather protection zone that is 8-feet deep, minimum and 12-feet wide, minimum.		
Required Amenity	The Primary Building Entrance shall be enhanced with one or more of the following: bench, 8-feet long, minimum; sculpture or artwork; fountain, or a planter(s).		
Transparency	Walls and doors of the Primary Building Entrance shall be a minimum of 65% transparent.		
Lighting	<p>The interior and exterior of the Primary Building Entrance shall be illuminated to extend the visual connection between the sidewalk and the building interior from day to night. Pathway lighting connecting the Primary Building Entrance to the adjacent sidewalk on an Addressing Street shall be scaled to the needs of the pedestrian.</p> <p>Comply with Outdoor Lighting, 4.199</p>		
<b>Overall Building Massing</b>			

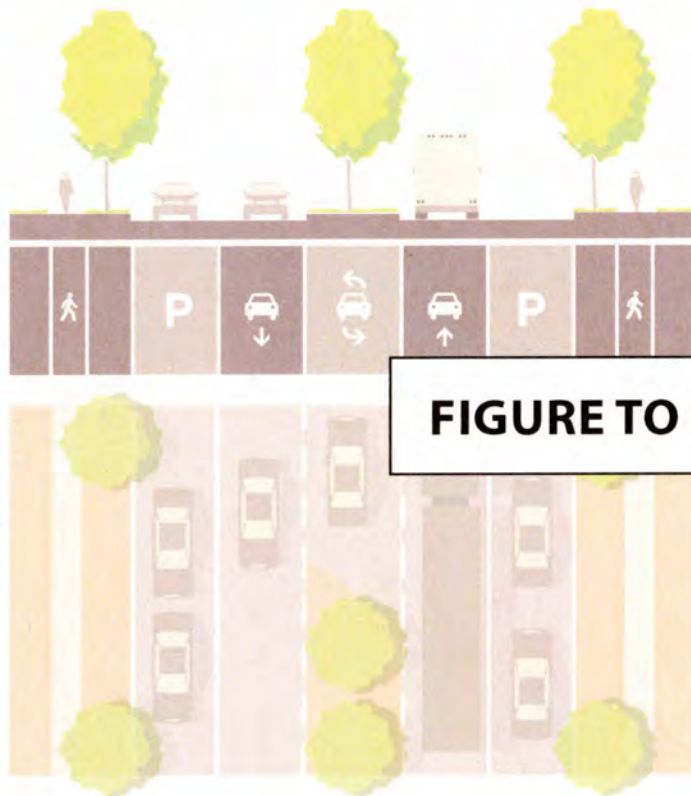
Table (.11) -1 - Development Standards			
	Addressing Streets	Supporting Streets	Through Connections
Front Setback	30-feet, minimum, except as provided below	30 feet maximum	30 feet maximum
Allowance of Primary Building Entrance	Where the Primary Building Entrance is located on an Addressing Street it may extend into the required front yard setback by 15-feet maximum provided that: a) It has a two-story massing with a minimum height of 24-feet; b) The Parcel Frontage on the Addressing Street is limited to 100-feet; c) The building extension is 60% transparent, minimum; d) The entrance is protected with a weather-protecting canopy with a minimum vertical clearance of 15-feet; and e) The standards for site design and accessibility are met.	Not applicable	Not applicable
Required Minimum Height	30-feet minimum.		
Ground Floor Height	The Ground Floor height shall measure 15-feet, minimum from finished floor to finished ceiling (or 17.5-feet from finished floor to any exposed structural member).		
Base, Body, and Top Dimensions	Buildings elevations shall be composed of a clearly demarcated base, body and top. A. For Buildings 30 feet in height: 1) The base shall be 30-inches, minimum; 1-story, maximum. 2) The body shall be equal to or greater than 75% of the overall height of the building. 3) The top of the building shall be 18-inches, minimum. B. For Buildings between 30 feet and 5-stories in height:		

<b>Table (.11) -1 - Development Standards</b>			
	<b>Addressing Streets</b>	<b>Supporting Streets</b>	<b>Through Connections</b>
	1) The base shall be 30-inches, minimum; 2-stories, maximum. 2) The body shall be equal to or greater than 75% of the overall height of the building. 3) The top of the building shall be 18-inches, minimum. C. For Buildings greater than 6-stories in height: 1) The base shall be 1-story, minimum, 3-stories, maximum. 2) The body shall be equal to or greater than 75% of the overall height of the building. 3) The top of the building shall be 18-inches, minimum.		
Base Design	The design of the building Base shall: A. Use a material with a distinctive appearance, easily distinguished from the building Body expressed by a change in material, a change in texture, a change in color or finish; B. Create a change in surface position where the Base projects beyond the Body of the building by 1 -1/2 -inches, minimum; and/ or C. High Berm Landscape Standard		
Top Design	Building Tops define the skyline. The design of the Building Top shall: A. Use a material with a distinctive appearance, easily distinguished from the building Body expressed by a change in material, a change in texture, a change in color or finish; and/ or B. Create a change in surface position where the Top projects beyond, or recesses behind, the Body of the building by 1 -1/2 -inches, minimum.		
Required Screening of Roof-mounted Equipment	Screen roof-mounted equipment with architectural enclosures using the materials and design of the building Body and/ or the building Top. No roof-mounted equipment shall be visible from an Addressing Street or Supporting Street.		





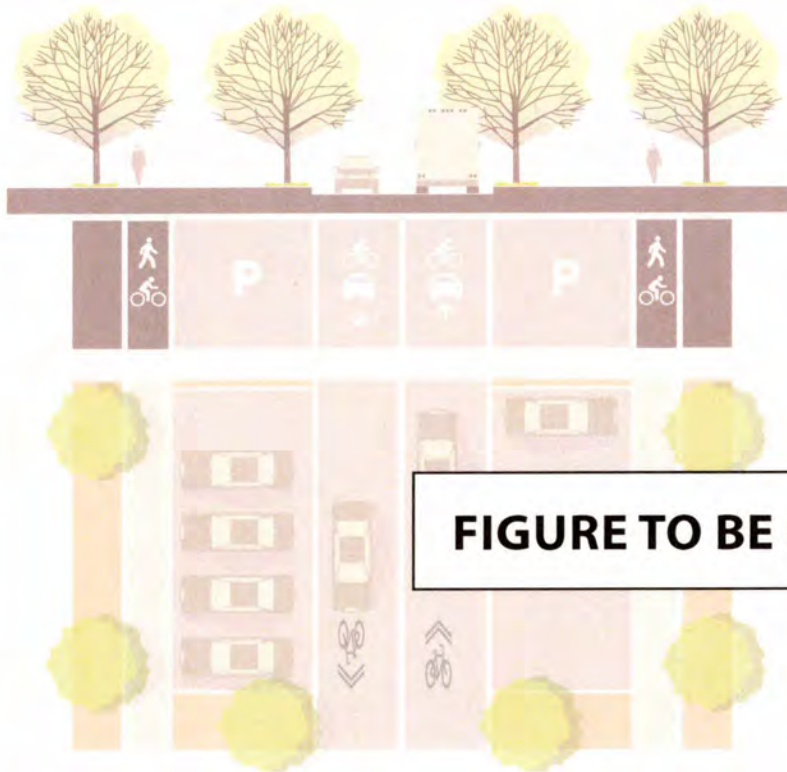
**FIGURE CC-1 – REGULATING PLAN**  
Refer to Section (.09) Coffee Creek FBC Regulating Plan



**Specifications for Supporting Streets**

Type	Local Street
Aesthetic Character / Identity	Minor Addressing Street
Role in Network	Bike, Pedestrian and Local Vehicular Connectivity
Design Speed	under 20 mph
Right-of-Way Width	Varies
Street Width	24-54 feet
Number of Lanes	2
Travel Lane Width	10-11 feet
Center Turn Lane Width	14 feet (maximum)
Parking Lane Width	8 feet (optional)
Bike Facilities	Shared Street
Sidewalk Width	6 feet (minimum)
Planting Strip Width	6 feet (minimum)
Planted Median Width	14 (minimum, optional)

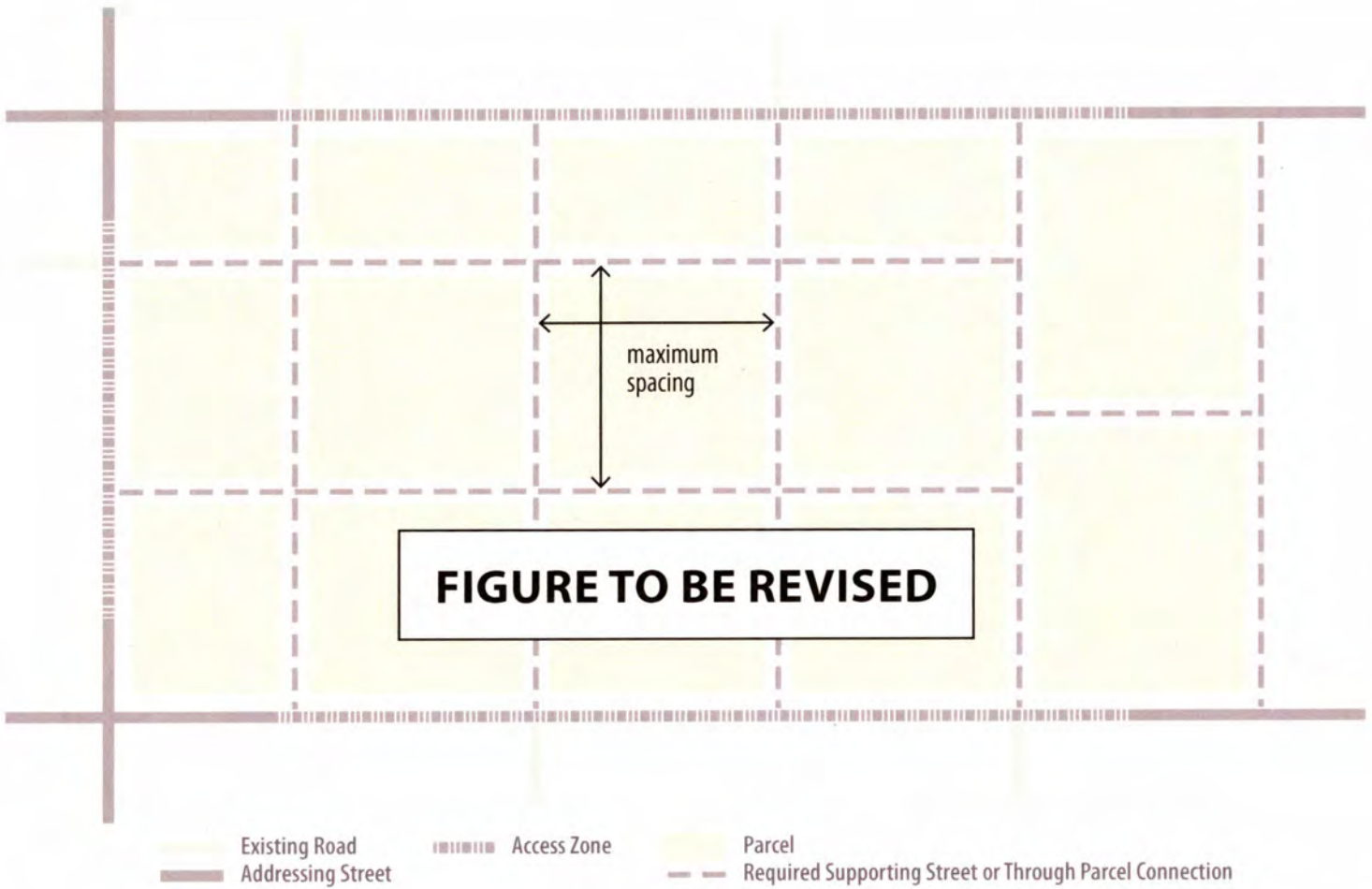
**FIGURE CC-2 - SUPPORTING STREETS, ALL TYPES**  
 Refer to Section (.10) Coffee Creek Connectivity Standards



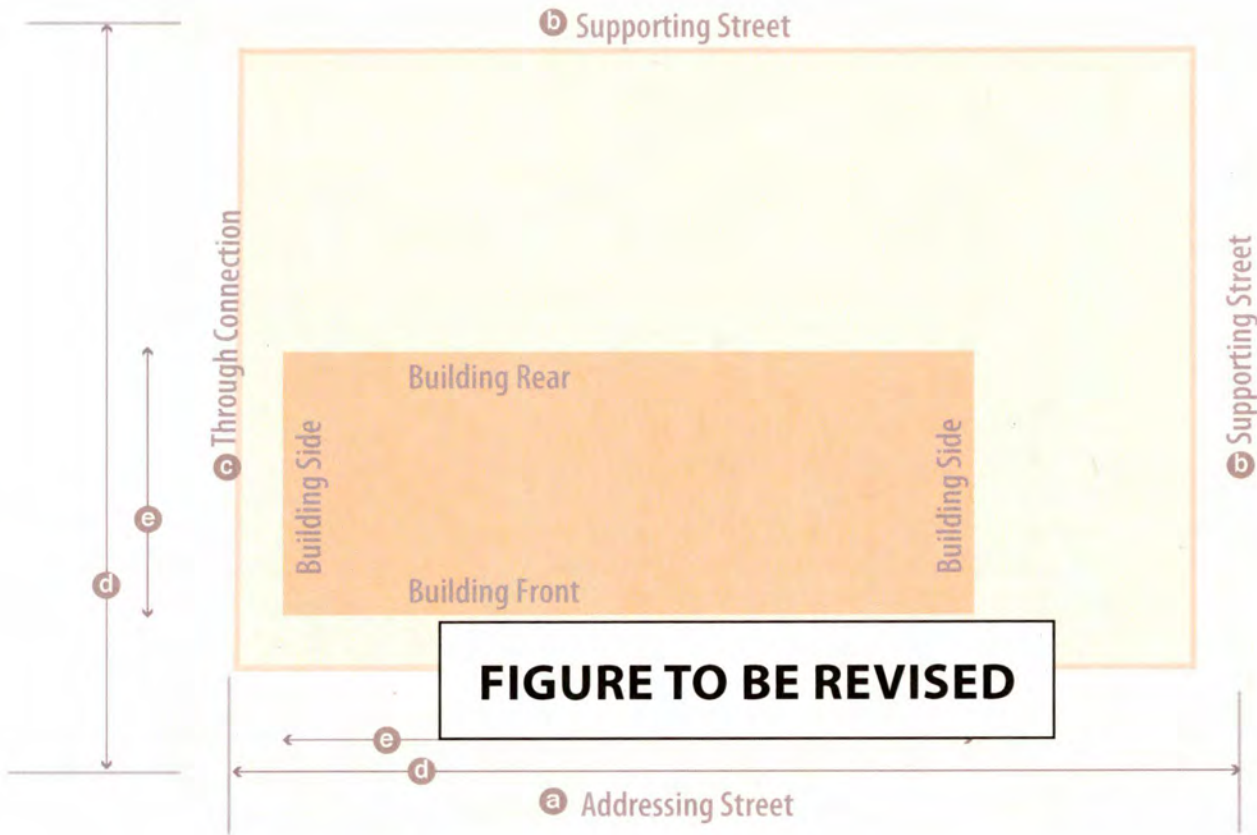
Specifications for Through Connections

Type	Local Street
Aesthetic Character / Identity	
Role in Network	Bike, Pedestrian, Local Vehicular Connectivity
Design Speed	under 20 mph
Right-of-Way Easement	Varies
Curb-to-Curb Width	Varies
Travel Lanes (number)	Optional
Travel Lane Width	12 feet (maximum)
Width	NA
	8-30 feet (optional; head-in, diagonal or parallel parking, or a combination, permitted.)
Bike Facilities	Shared Street or Shared-Use Path
Sidewalk Width	5 feet (minimum) each side or 10 feet (minimum)
Planting Strip Width	6 feet (minimum)
Planted Median Width	NA

**FIGURE CC-3 – THROUGH CONNECTIONS, ALL TYPES**  
 Refer to Section (.10) Coffee Creek Connectivity Standards




**FIGURE CC-4 - CONNECTIVITY STANDARDS**  
Refer to Section (.10) Coffee Creek Connectivity Standards



Site Design

- a** Addressing Street
- b** Supporting Street
- c** Through Connection
- d** Parcel Frontage
- e** Parcel Frontage Occupied by a Building

-  Parcel
-  Building

**FIGURE CC-5 - PARCEL STANDARDS**  
Refer to Section (.11) Development Standards

**WILSONVILLE  
PATTERN BOOK  
FOR COFFEE CREEK LIGHT INDUSTRIAL AREA**

**DRAFT**

02 JULY 2014

CITY OF WILSONVILLE | TRANSPORTATION GROWTH MANAGEMENT (TGM)

URBSWORKS INC | JRA | BAINBRIDGE.

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This project is partially funded by a grant from the Transportation and Growth Management (TGM) Program, a joint program of the Oregon Department of Transportation and the Oregon Department of Land Conservation and Development. This TGM grant is financed, in part, by federal Moving Ahead for Progress in the 21st Century (MAP-21), local government, and the State of Oregon funds.

The contents of this document do not necessarily reflect views or policies of the State of Oregon.

## PATTERN BOOK | INTRODUCTION

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### GOALS OF THE PATTERN BOOK

The Coffee Creek FBC and the Pattern Book together establish regulations and guidelines for street design and connectivity, site design and circulation, building form, and building architecture and landscape of all development located within the Coffee Creek Industrial Area Master Plan area. The guidelines are intended to result in:

- A multi-modal transportation network that accommodates pedestrians, bicyclists, transit riders, motorists, and freight in the context of a modern light industrial district.
- A complete network of existing and new streets, paths, and trails that will support a sense of place and identity and create a functional gateway to the City of Wilsonville.
- An industrial district featuring cohesive and high-quality site, landscape, and building design through a de-emphasis on building design and more appropriate emphasis on the design of the public realm.
- Minimization of the visibility of vehicular parking, circulation and loading areas.
- Public realm design that considers the contribution that landscape design has made to the design quality of other industrial lands in Wilsonville, where landscaping is effective at breaking down the scale of industrial development and providing a human scale to the public realm.
- Preservation of trees and natural features that supports the creation of a special place with a distinctive image and identity.
- Minimization of adverse impacts on adjacent properties from development that detracts from the character and appearance of the area.
- Connectivity requirements that achieve city policy objectives but are appropriate for industrial scale sites and buildings, establish connectivity between parcels and address challenges of shared site access between landlocked parcels.

### RELATIONSHIP TO THE WILSONVILLE CODE

The form-based code for industrial areas sets the standards for development and defines the essential determinants for design that are critical to development. These standards are clear, objective, and represent a baseline minimum for the sound development of employment uses in industrial areas. Because the community standards for design are high, the expectation for the design for all new buildings is correspondingly high. The design guidelines in this Pattern



Book encourage and promote the design of buildings and landscapes that exceed the minimum functional standards established in the Wilsonville Code. The design guidelines illustrate how the provisions of the form-based code can be practically applied with examples from specific context zones. However, no single illustration or series of illustrations is capable of representing the full, complete, and exhaustive range of possible design solutions. Rather than representing an ultimate design, the design guidelines and their illustrative examples are intended to promote a creative response to the development standards and foster a collaborative discussion of design that includes City staff and members of the Development Review Board.

The City of Wilsonville expects new development in industrial areas to be successful contributors to the quality of life in the city by:

- Constructing well-designed, high-quality buildings that serve current needs and are adaptable to future uses;
- Integrating industrial land uses with all modes of transportation including the active modes of transportation of walking, biking, and transit;
- Preserving existing trees and natural features and enhancing the character and qualities of a unique landscape with a distinctive image and identity; and
- Preserving existing jobs and creating new ones.
- Creating a quality workplace for employees.



**COFFEE CREEK IN CONTEXT**  
Employment and industry in the City of Wilsonville.

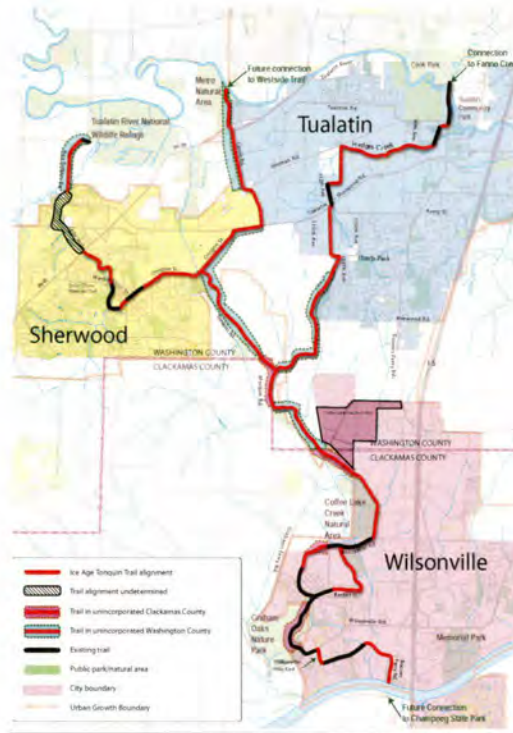
## SECTION A | THE PLACE OF COFFEE CREEK

### DISTRICT-WIDE SITE PLANNING AND LANDSCAPING – INTENT STATEMENT

The impact of the Ice Age floods on the Willamette Valley defies the imagination. The cataclysmic effects of the Missoula Floods created the modern-day landscape that includes Coffee Lake Wetlands and Coffee Lake Creek. These remnants of the geologic events of 12,000 years ago and the landscape that has emerged since that time are authentic elements that establish our sense of place and contribute to creating a distinctive image and identity that is unique to the City of Wilsonville.

The City's commitment to preserving and enhancing the heritage of this distinctive landscape is reflected in several of the patterns and guidelines. At the scale of the district, the City expects development to promote visual and physical connections from the industrial district to the Coffee Lake Creek Natural Area and the future Tonquin Ice Age Trail.

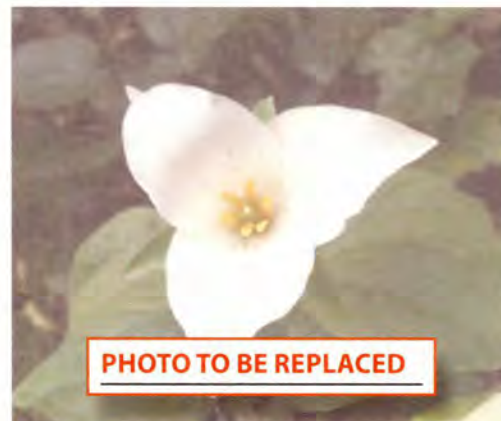
Design landscapes that acknowledge the Ice Age heritage of Coffee Creek. This guideline may be accomplished by orienting patterns of new landscape plantings to acknowledge and reflect the natural flows of water from the industrial district to Coffee Lake Creek.



#### THE ICE AGE LANDSCAPE

The landscape of Coffee Creek connects to a larger regional landscape.

hyperlink to be added



#### THE ICE AGE LANDSCAPE

Springtime Trillium rising out of the forest floor.

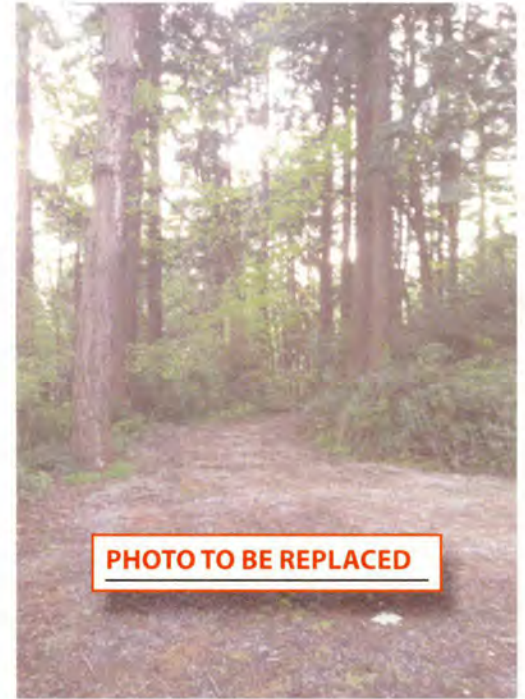
The themes that express the unique character, quality, and culture of Coffee Creek are still emerging as the district becomes fully-integrated with the larger, more established city. Existing stands of Douglas Fir both acknowledge the City's heritage as a Tree City USA and its commitment to maintaining its natural beauty. The City is also home to three water features by the celebrated Pacific Northwest landscape architect Bob Murase: water features are strongly encouraged as part of the Coffee Creek Industrial master plan.

Within the Coffee Creek industrial district the design of individual buildings should be linked by unifying elements. The public realm of Addressing Streets provides unity to the district by establishing a pastoral character of place with the regular planting of street trees, sidewalks, and front yard setbacks.

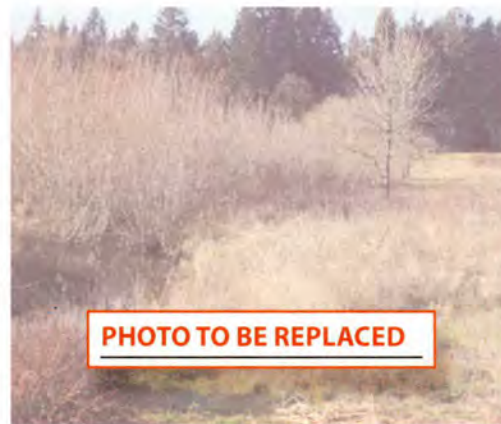
**GUIDELINES**

**District-wide Site Planning and Landscaping**

- Use the unifying elements of the natural landscape to visually connect and functionally integrate the industrial district. Promote a landscape that supports ecological function and habitat by using native species in a naturalized manner. This guideline may be accomplished by promoting and maintaining district-wide landscape through the consistent use of native plant materials as ground cover, ferns, shrubs, understory and canopy trees.



**THE ICE AGE LANDSCAPE**  
Groves of Douglas Fir.



**THE ICE AGE LANDSCAPE**

Define the edges of open areas with multiple layers of plantings at various scales.

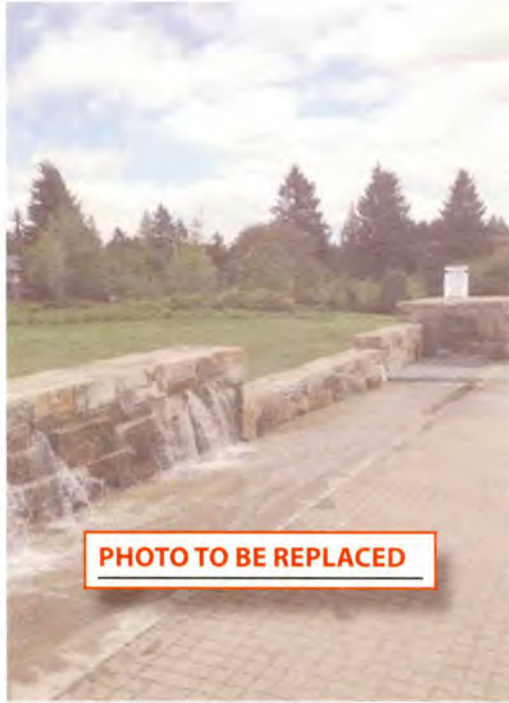


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- Pedestrian and bicycle connection is critical and incorporating public connections through large-scale industrial sites is encouraged. Access connections to the creek, natural areas, and greenway trails should be clearly marked and provide safe and convenient passage. Improving existing, and providing new pedestrian and bicycle connections, strengthens the sense of place by developing the character of place.

**The Ice Age Landscape of Coffee Creek**

- Identify, preserve, and enhance any Ice Age elements found on site, such as erratics –the foreign boulders carried to the site on ice rafts– as elements that influence site design and development.

**Develop and Emphasize the Themes of Coffee Creek**

- Integrate the themes related to the City of Wilsonville as unifying elements in the conceptual design for new development. Incorporating themes such as existing stands of native trees to emphasize points of site access and/ or building access. Integrate fountains and water features to emphasize important places, such as parcel access, building entries, and employee amenities. Integrate themes into the landscape design.
- The park-like character of the design of the Addressing Streets should be complemented by landscaping around buildings, parking lots, and open space that reflects the informal, natural, and original landscape that preceded development and persists in places across the site. Non-native, ornamental plants, shrubs, and trees should be used sparingly and strategically as elements that accent special elements of the site or building, such as entries.

**WILSONVILLE THEMES**

Water features that invite attention.

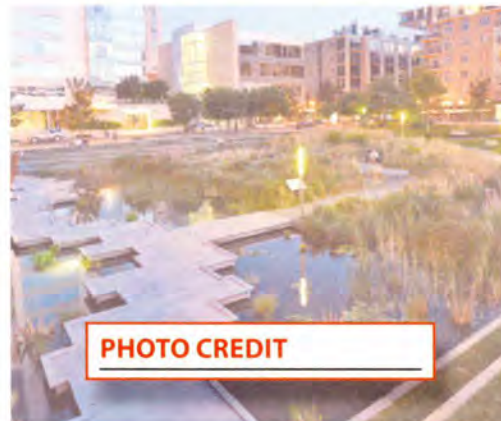


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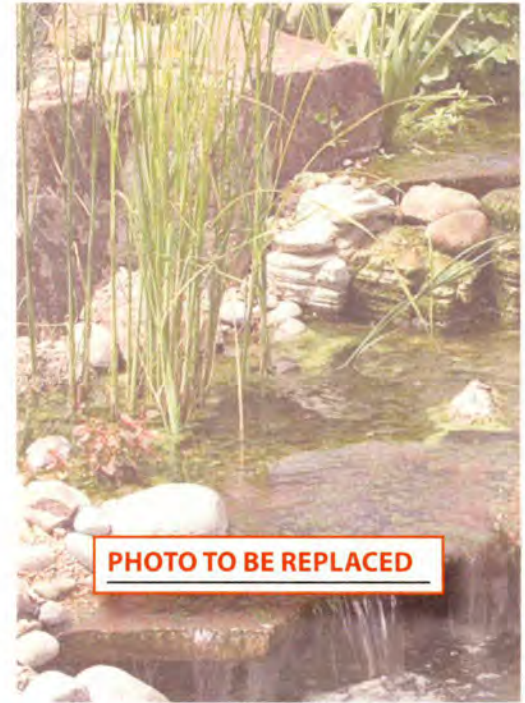
**WILSONVILLE THEMES**

An urban context that functions naturally.

- Integrate the materials of industry at an industrial scale. This guideline may be accomplished by designing buildings, enclosures, retaining walls with the simple, natural, unembellished materials common to industry. Use unfinished steel, raw aluminum, and plain concrete as the finish materials for the construction of site and building elements.

**Strengthen Gateways**

- Gateways reinforce a sense of arrival or departure and mark the transition from one precinct of the city to another.
- Design gateway locations to promote a sense of place and to reinforce the distinct identity of Coffee Creek. This guideline may be accomplished by placing new buildings strategically at areas that define boundaries and edges to create gateways in conjunction with other buildings or with significant landscape features. Develop gateway buildings at strategic intersections. Use freestanding monument signs to mark gateways. Install iconic elements within the right-of-way that serve as district-wide or site-specific gateways within the right-of-way.



**WILSONVILLE THEMES**

Water feature that recreates the natural qualities of a quiet stream.



**WILSONVILLE THEMES**

Water feature by Bob Murase.



**GATEWAYS**

Use freestanding monument signs to reinforce important district gateways.



**GATEWAYS**

Elements do not need to be large in scale to make a large contribution to the public realm.

## SECTION B | ACHIEVING CONNECTIVITY

### ACHIEVING CONNECTIVITY – INTENT STATEMENT

Streets do much more than provide access to buildings, blocks, parcels, and sites. Streets are the primary generators of urban form and their design determines the quality of the public realm and the character of our neighborhoods and cities. The Form-based Code sets standards for streets as well as standards for development of sites, parcels, and buildings. In the Form-based Code the Regulating Plan establishes an overall framework for access and mobility in the Coffee Creek industrial area by building upon those roads that already exist. Existing rural roads and new streets will become the major streets of Coffee Creek and will set the character for its development. The Regulating Plan sets forth only a rough framework for new development with standards for Connection Spacing that are appropriate to the large scale of industrial development, and to ensure that connectivity to, and through, all sites is supported. The Form-based Code sets minimum standards for connectivity, and establishes a hierarchy of Addressing Streets, Supporting Streets, and Through Connections. The nature of many of these connections, their function, and their typology is flexible so that their ultimate design can be a reflection of their unique context. Supporting Streets and Through Connections should work together to provide a complete network that serves people getting around no matter what form of transportation they use. It's not necessary for every connection to serve everyone, but the network should make it possible to get to, through and around parcels and the district.



### REGULATING PLAN

The Regulating Plan sets the overall framework for development in Coffee Creek by identifying the location of Addressing Streets and setting standards for the location of Supporting Streets and Through Connections.

### CONNECTIVITY

Multiple options for connections of many scales from the intimate pedestrian path to roads with parkway character.



## GUIDELINES

### Addressing Streets

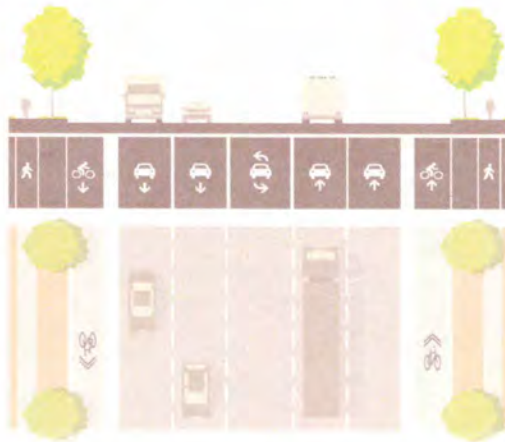
- Design Addressing Streets to establish and support a park-like character of the public realm. Addressing Streets should support pedestrian, bicycle, automobile, freight, and transit access and mobility equally. Design Addressing Streets to serve as the “front door” or “address” for new buildings and development. Orient building massing, form, architecture, and programmatic function along Addressing Streets to help define the public realm, create a distinctive frame for the streets, and support the sense of place in Coffee Creek.

### Supporting Streets

- Design Supporting Streets to establish and support the extension of the public realm established by the network of Addressing Streets. In some instances Supporting Streets should be developed to the same standards as Addressing Streets and serve as the “front door” or “address” for new buildings and development. This is especially true for those development sites or parcels that are sufficiently large in scale to support multiple building development. In other instances Supporting Streets should be developed as secondary service connections from Addressing Streets.

### Through Connections

- Design Through Connections to fully support the extension of the public realm while responding to a wide range of functions. Through Connections may serve only pedestrians and function as an extension of the sidewalk network in Coffee Creek; or they may serve as multi-use paths and support the effective use of bicycles for transportation; or they may serve as drive aisles for linked surface parking lots within a parcel. Install a system of signage that serves to orient people to their location and assist them in wayfinding to their destination.



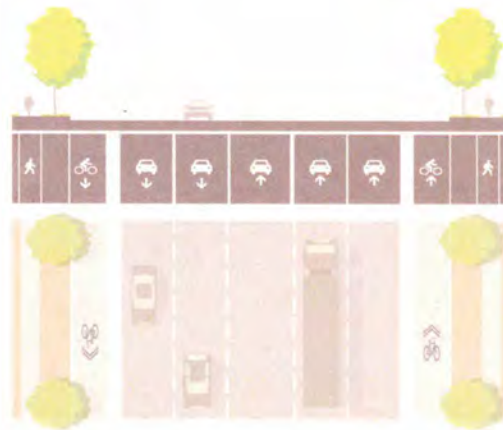
Specifications for Day Road

Specifications for Cahalin Road

Type	Major Arterial
Aesthetic Character / Identity	Addressing Street
Role in Network	
Design Speed	
Right-of-Way Easement	
Curb-to-Curb Width	
Travel Lanes (number)	
Travel Lane Width	
Center Turn Lane Width	
Parking Lane Width	
Bike Facilities Width	
Sidewalk Width	
Planting Strip Width	
Planted Median Width	

Type	Local Street
Aesthetic Character / Identity	Addressing Street

**TO BE REVISED AND RELOCATED TO PUBLIC WORK STANDARDS**



Specifications for Grahams Ferry Road



Specifications for Garden Acres Road

Type	Minor Arterial
Aesthetic Character / Identity	Addressing Street
Role in Network	
Design Speed	
Right-of-Way Easement	
Curb-to-Curb Width	
Travel Lanes (number)	
Travel Lane Width	
Center Turn Lane Width	
Parking Lane Width	
Bike Facilities Width	
Sidewalk Width	
Planting Strip Width	
Planted Median Width	

Type	Local Street
Aesthetic Character / Identity	Addressing Street

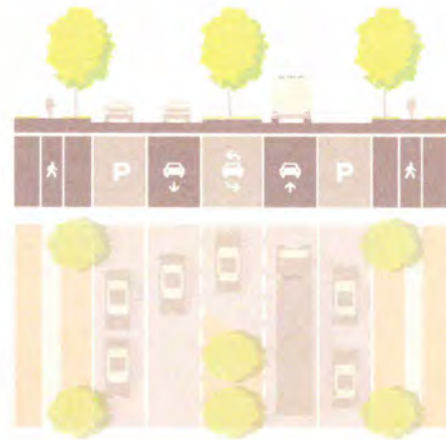
**TO BE REVISED AND RELOCATED TO PUBLIC WORK STANDARDS**



Specifications for Clutter Road

Specifications for Kinsman Road

Type	Collector	Type	Minor Arterial
Aesthetic Character / Identity	Addressing Street	Aesthetic Character / Identity	Addressing Street
Role in Network	<b>TO BE REVISED AND RELOCATED TO PUBLIC WORK STANDARDS</b>		
Design Speed			
Right-of-Way Easement			
Curb-to-Curb Width			
Travel Lanes (number)			
Travel Lane Width			
Center Turn Lane Width			
Parking Lane Width			
Bike Facilities Width			
Sidewalk Width			
Planting Strip Width			
Planted Median Width			



Specifications for Supporting Streets

Specifications for Through Connections

Type	Local Street
Aesthetic Character / Identity	Minor Addressing Street
Role in Network	
Design Speed	
Right-of-Way Easement	
Curb-to-Curb Width	
Travel Lanes (number)	
Travel Lane Width	
Center Turn Lane Width	
Parking Lane Width	
Bike Facilities Width	
Sidewalk Width	
Planting Strip Width	
Planted Median Width	

Type	Local Street
Aesthetic Character / Identity	

**TO BE REVISED AND RELOCATED TO PUBLIC WORK STANDARDS**

optional)      Planted Median Width      NA



**THROUGH CONNECTIONS**

Through Connections may serve only pedestrians and function as an extension of the sidewalk network in Coffee Creek; or they may serve as multi-use paths and support the effective use of bicycles for transportation.

## SECTION C | SITE DESIGN



### THROUGH CONNECTIONS

Also intended to link Coffee Creek to other regional destinations.

### SITE DESIGN – INTENT STATEMENT

Access and mobility are essential elements of successful industrial development. We tend to think of tractor-trailer rigs as essential to industry –and they are, but equally essential to industry is an educated work force that can get to their shifts with a full range of transportation options: options that offer employees real choices that include driving alone, but also support transit, walking, and biking.

Automobile and freight access from Addressing Streets and Supporting Streets to a parcel should be obvious, clear, simple, and safe. Parcel access provides an opportunity to create a gateway and reinforce a strong sense of place.

Bicycle and pedestrian access to a parcel from Addressing Streets and Supporting Streets can also reinforce the sense of place in Coffee Creek. Bicycle and pedestrian access from an Addressing Street to a parcel should be convenient, direct, and complete. Bike riders and walkers should be able to clearly perceive their ultimate destination from the Addressing Street.

Through Connections offer a broad range of design possibilities that support specific needs for access and mobility. Through Connections may look like Addressing Streets or Supporting Streets and function like any other street; they may look and function like a drive aisle in a surface parking lot; they serve as a multi-use path for bicycles and pedestrians; or they may look and function like a sidewalk. Regardless of their ultimate appearance or configuration, Through Connections are intended to link the Coffee Creek industrial area together as a network of streets, routes, and paths that support multi-modal transportation. Through Connections are also intended to link Coffee Creek to other local and regional destinations, such as the Ice Age Tonquin Trail or Coffee Lake Creek Natural Area.

The design of the landscape in yards along Through Connections is intended primarily as a visual relief from the large-scale industrial development. A simple, natural landscape of native plant materials will result in an attractive contribution to the quality of this limited part of the public realm.

Access and mobility are for all people. The pedestrian system is successful only when all people can conveniently reach their destinations. Universal and equitable barrier-free design is most successful when designed and developed systematically from initial site design through final building design and construction.

## GUIDELINES

### Parcel Access

- Where parcel access is also the primary automobile access to a building for visitors use landscaping, signage, to create a distinctive sense of arrival.
- Use routes providing parcel access to build active intersections where pedestrians, bicyclists, and motorists come together.
- The Form-based Code and design guidelines for the landscape of front yards along Addressing Streets encourages a natural, irregular pattern of native plant materials; consider breaking this informal character of the landscape frontage with design and plant materials that are more formal, regular, and ornamental where parcel access occurs.

### Addressing Streets

- Where parcel access connects the primary building entrance to the Addressing Street extend the design, character, scale, and materials of the entry to the public sidewalk. Where parcel access from a Supporting Street is the primary automobile access to a building for visitors use landscaping, signage, to create a distinctive sense of arrival.

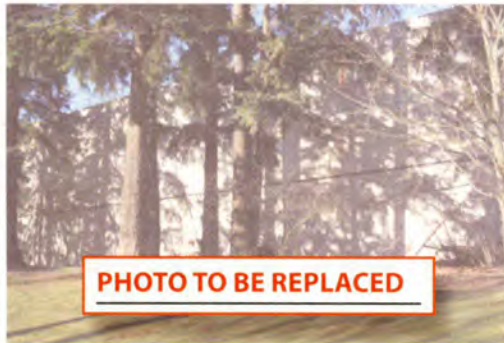
### Supporting Streets

- Where appropriate to the master plan for large development sites, design Supporting Streets to the same standards as Addressing Streets. Match street design standards for Addressing Streets, including street profiles, street trees, and sidewalks.

### Through Connections

- Where parcel access connects only a pedestrian walkway or multi-use path with the Coffee Creek pedestrian and bicycle network, design the walkway or multi-use path for safety, comfort, and convenience.
- Develop an integrated system for pedestrian and bicycles that includes good connections to other adjacent parts of the Coffee Creek Industrial District and to the larger City beyond.

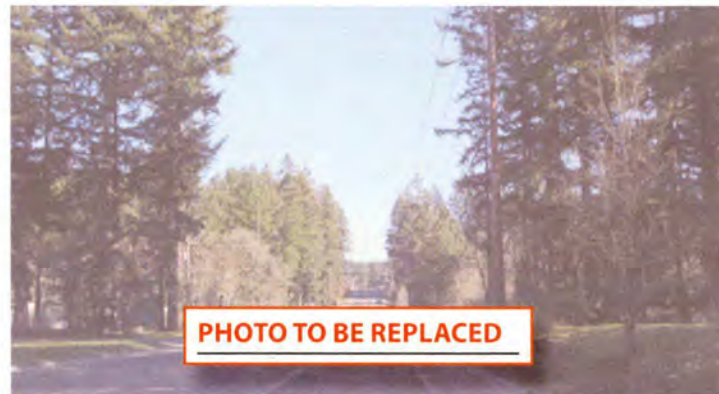




**PARCEL FRONTAGE**

Landscape supports and enhances building design.

- Make paths accessible for all. Adjust paths to accommodate existing features of the site that add character and interest.
- Adjust the alignment of Through Connections to accommodate natural features and resources.
- Increase the horizontal dimension of a Through Connection designed as multi-use paths to incorporate amenities such as benches, lighting, or trash receptacles and to create visual interest.
- The design of the landscape in front yards along Addressing Streets is carefully regulated by the Form-based Code and will result in an attractive contribution to the quality of the public realm.
- Ensure that the landscape is planned, installed, and maintained to promote the informal design character associated with each landscape frontage type.
- Plan the size and location of signs and their structure so that they do not detract from the natural quality of the native landscape.
- Establish and maintain a sense of the public realm as an outdoor room where building elevations serve as walls and the streets, sidewalks, and landscape serve as the floor. Use buildings to create and maintain a sense of urban enclosure.



### PARKING LOCATION AND DESIGN – INTENT STATEMENT

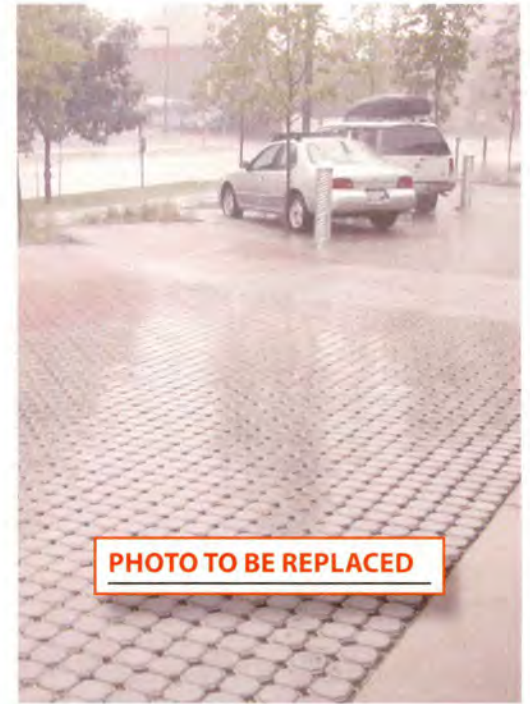
Surface parking is permitted in the front yard setback for development along Addressing Streets with limitations. Surface parking lots are limited in scale and designated for short-term parking for visitors, people with disabilities, and deliveries only. The design guidelines are intended to establish the character for surface parking lots that supports the City’s goals for pedestrian convenience, comfort, and safety.

Ensure that the landscape is planned, installed, and maintained to promote the informal design character associated with each landscape frontage type.

### GUIDELINES

#### Parking Location and Design – General

- The design of parking lots is carefully regulated by the Form-based Code and will result in an attractive and functional experience for staff arriving by car. Surface parking is permitted in the front yard setback for development along Addressing Streets and Supporting Streets with limitations. Through Connections can support a wide range of parking options including parallel, diagonal, or perpendicular parking. To enhance the design quality of parking lots in front yards along Addressing Streets consider increasing the quality of the materials used and treating the surface of the parking lot and walkway system as a plaza that connects to, and integrates with, the primary building entrance.



#### PARKING

Make pedestrian routes through parking areas clear to pedestrians and motorists.



#### PARKING LOCATION

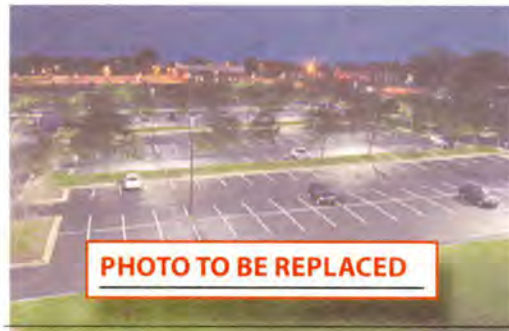
Link parking areas and buildings with a continuous network of pedestrian routes.



- Choose the type of parking most appropriate to the context: consider natural features and resources as well as programmatic needs associated with building use. Adjust the layout of parking lot to accommodate natural features and resources.
- Vary the scale of parking lots, the pattern of landscape elements and lighting to add visual interest and reduce the monotonous effect of large extents of surface parking.
- Design parking lots for the comfort and convenience of visitors and the disabled. The accessible route from one's parking spot to the primary building entrances should be clear, obvious, and unobstructed.
- Consider the integration of permeable paving to reduce stormwater run-off.

**PARKING DESIGN**

Use landscaping to define discrete parking areas.



**PARKING DESIGN**

Integrate lighting with parking lot layout and design.

**DESIGN THAT CONTRIBUTES TO THE SITE – INTENT STATEMENT**

Site grading, retaining walls, and walls & fences. Minimize site grading to preserve the natural character of the site. Contoured slopes are generally preferred to the installation of retaining walls. Where retaining walls are necessary to support site development, ensure that they facilitate surface drainage, limit soil erosion, and avoid increasing instability of native soils. Integrate retaining walls with other site design features, such as stairs, ramps, and planters wherever possible.

To the extent possible, site development should maintain and enhance natural drainage patterns. Incorporate features for the storage, cleaning, transport, and re-infiltration of stormwater into site design and landscaping. Stormwater facilities such as swales should be designed to reinforce the natural quality and visual continuity of the landscape at the scale of the site and the district.

Trees help to define place. Whether individually, or in groves of native species, trees enhance the public realm by giving context and scale to the Coffee Creek Industrial Area. Landscape planting in front, side, and rear yards and as screening for parking lots, service drives, and service enclosures gives form and defines the public realm and parcels. The landscape design, installation, and maintenance help to define the Coffee Creek industrial district and to diminish the large-scale industrial buildings. It also helps to define and direct people to building entries. The native plant material are climate adaptive, have low water and maintenance requirements, and visually blend with adjacent, undisturbed landscapes. Native trees should be preserved and employed as the visual anchors of new landscapes.

The building types in Coffee Creek need extensive, relatively flat surfaces for buildings, parking lots, service yards, access lanes, and truck maneuvering areas. It may still be possible to fit a multistory building into the terrain of Coffee Creek. Integrating buildings with their sites is strongly encouraged.

### GUIDELINES

#### Planting

- Newly planted landscapes should be substantially completed and covered with plant materials within three years of installation.
- Plant materials shall be selected from the list supplied by the City of Wilsonville.
- Design and install new landscapes with plantings grouped in natural, irregular masses to establish and support a continuous, integrated, and natural district-wide appearance. Landscapes and plant materials shall be maintained throughout the year.

#### FITTING INTO THE TERRAIN

Where grading is necessary, provide a series of stepped retaining walls that merge into the landscape.



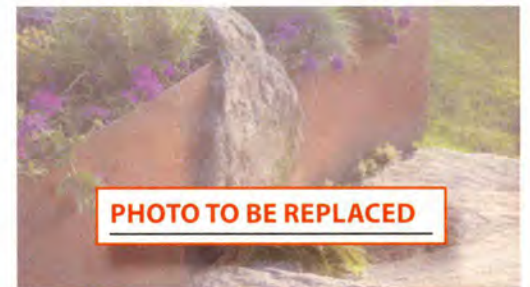
#### BUILDING ENTRANCE

Make entries clear and inviting.



#### BUILDING ENTRANCE

Make accessible routes interesting.

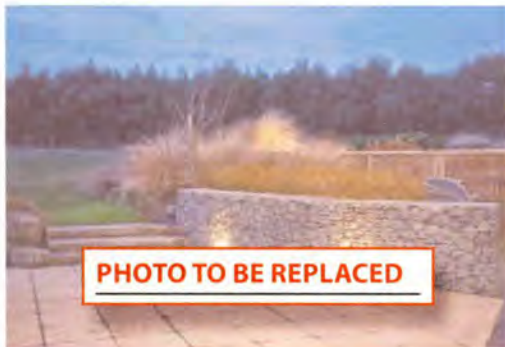


**DESIGNS THAT CONTRIBUTE TO THE BUILDING – INTENT STATEMENT**

Building design should acknowledge and respect the natural character of its site. The Coffee Creek industrial area has a strong character that derives from context, topography, and native vegetation. New site development, landscaping, and building design can reinforce this distinctive character.

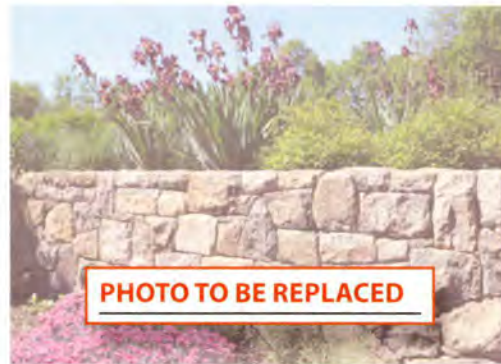
Provide a consistent and high-quality environment for the Coffee Creek industrial area by obscuring views of loading areas, work yards, above-grade utilities and services, and recycling and refuse areas from Addressing Streets, Supporting Streets and Through Connections. Whenever possible, group utilities and services to minimize visual clutter.

The primary building entry is a significant element of building design in Coffee Creek. The design guidelines recommend that the primary entrance for all buildings front on an Addressing Street. This is not a requirement of the Form-based Code; an entrance on a Supporting Street or Through Connection is acceptable provided the entry is clearly visible from the Addressing Street and a clear public route to the entry is provided. Emphasize the importance of the primary building entry with glass, canopies, signage, public art, landscaping, and lighting.



**EXTERIOR LIGHTING**

Integrate pathway lighting into both buildings and landscapes.



**FITTING INTO THE TERRAIN**

Use materials that complement the colors, textures, and patterns of the landscape.

## GUIDELINES

### Primary Building Entries

Building elevations fronting Supporting Streets deserve design attention. The development standards set only limited criteria for regulation including articulation of a base, body, and top for all buildings. Articulate façades with a sense of depth by including design elements that create shadow lines, change color or materials, or incorporate other details that –together with the required landscape– breakdown large expanses of flat, unembellished surfaces.

- Make the primary building entry a significant element of building design in Coffee Creek. Emphasize the importance of the primary building entry with glass, canopies, signage, public art, landscaping, and lighting.

### Location and Screening of Utilities and Services

Organize above-grade services elements, such as transformers, with the geometry of the adjacent streets or nearby site elements and buildings.

- Walls used for screening may be constructed from stone, Cor-ten steel, or smooth-finished cast-in-place or board-formed concrete. Long extents of such fencing should be modulated with the use of reveals and other techniques. Where required, service access gates and doors should be constructed of high-quality, durable materials that complement the design of screening walls and receive regular maintenance.
- Where appropriate, screening walls should be enhanced with native plant material to diminish the visual mass and integrate with the landscape.

### Exterior Lighting-

Exterior lighting should support safe access and use of sites in the evening and nighttime.

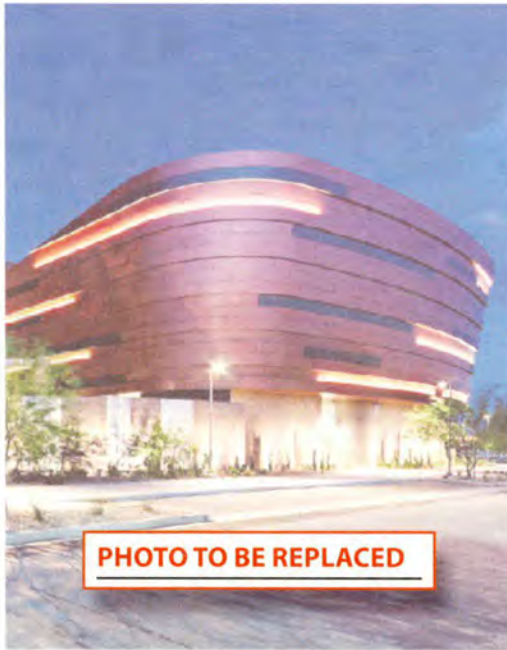
The selective highlighting of significant architectural elements, such as building entries and circulation to those entries from the street and/ or landscape elements such as sculpture or other featured elements in the landscape will contribute to the high-quality design of the Coffee Creek industrial area. Surface parking lots, building entries and courtyards, and loading areas and service yards should be illuminated, but the use of flood lighting is discouraged. Exterior lighting should be selected for maximum energy-efficiency, durability, and maintainability.

- Lighting plays a significant role in supporting the design character of Addressing Streets and Supporting Streets in the evening and nighttime by encouraging the selective highlighting of significant architectural elements, such as building entries and circulation to those entries from the street and/ or landscape elements such as sculpture or other featured elements in the



### SCREEN UTILITIES

Combine walls, fences, and landscaping to screen utilities and services.



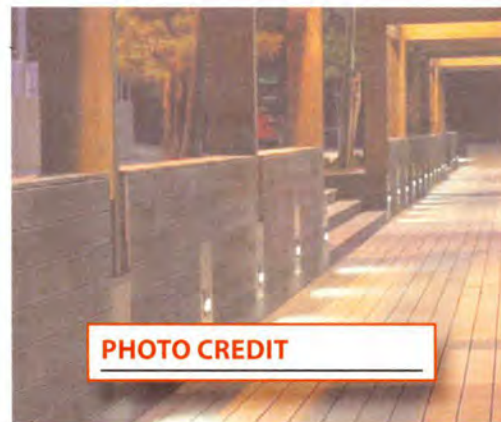
landscape frontages required along Addressing Streets.

- Lighting plays a supporting role in the design character of Through Connections in the evening and nighttime by promoting safety and security along routes of pedestrian access as well as the selective highlighting of significant architectural elements, such as building entries and circulation to those entries from the street and/ or landscape elements.
- Fixture heights between 15-20' are preferred for surface parking lots and loading areas and service yards. Through connections, internal walks, courtyards, and paths should be illuminated with pedestrian-scaled lighting.
- Lighting shall protect night skies, and not extend beyond site boundaries. Light fixtures shall be cast downward with full cut-off shades. In-ground up-lighting should be avoided.

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**LIGHTING**

Use lighting to draw attention to significant elements of the building.



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**LIGHTING**

Provide lighting at a variety of levels and intensities.



PHOTO CREDIT



PHOTO CREDIT

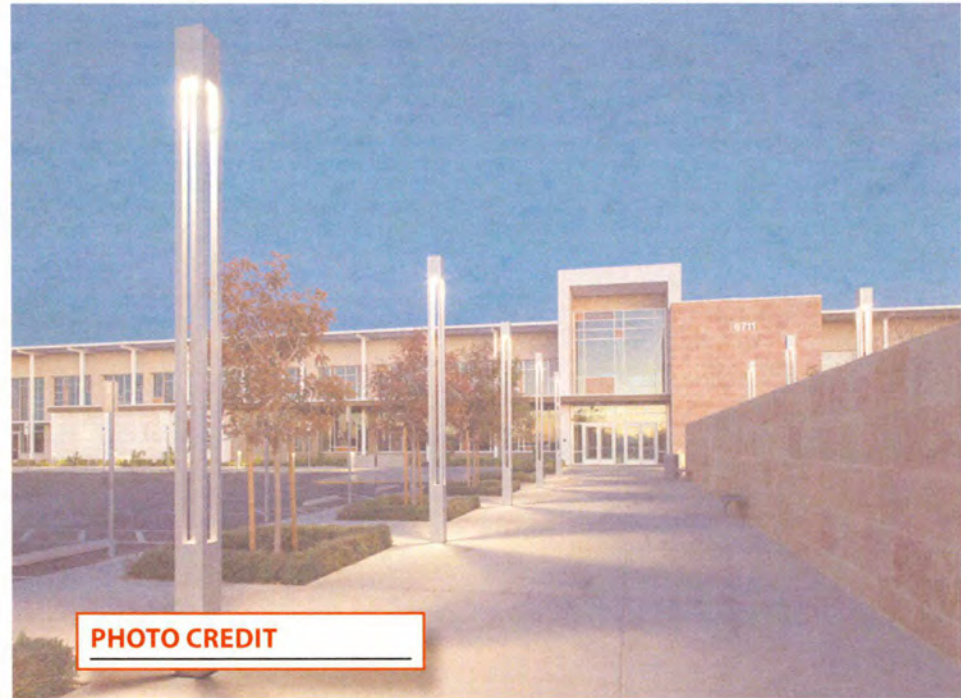


PHOTO CREDIT



PHOTO CREDIT

**LIGHTING**

Use lighting to draw attention to the primary building entrance as day turns to night.



## SECTION D | BUILDING DESIGN

### DESIGNING THE BUILDING – INTENT STATEMENT

Building massing and the architectural expression of building design elements define the scale, quality, and character of the built environment. The design guidelines for buildings focus on the following elements:

- Prominent building entrance visible from an Addressing Street
- Overall building mass and bulk
- Composition of building elevations
- Roof forms
- Materials and colors
- Sustainable building design

Every address, business, and destination in Coffee Creek deserves a good entrance. Every destination is ultimately reached on foot, so making every building entrance clearly visible and fully accessible is a fundamental pattern. The regulations of the Form-based Code require that the primary building entrance shall be visible to and accessible from an Addressing Street. The intent of the design guidelines is that every primary entrance of every building will contribute to the quality and vitality of the public realm by creating a clear sense of entry.

### GUIDELINES

#### Prominent Building Entrance

Enhance the minimum standards required by the development code with:

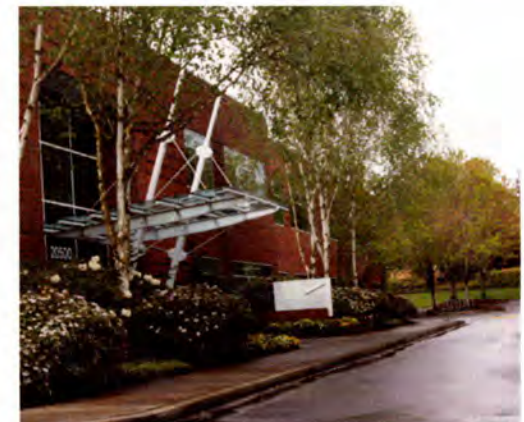
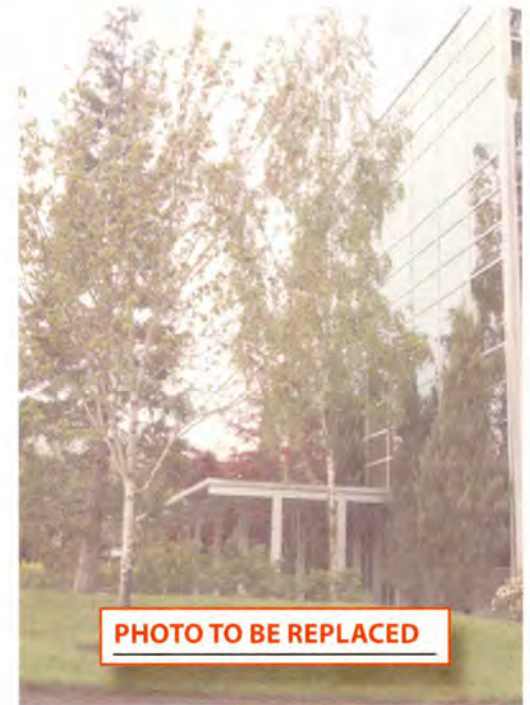
- A landscaped forecourt with decorative paving;
- a prominent roof form at the building's entrance;
- A generous canopy of metal or glass that offers protection from the elements;
- A major recess in the façade;
- A projecting glass vestibule;
- Accent and pathway lighting;



#### BARRIER-FREE DESIGN

Make the path to the primary building entrance universally accessible.

- Plantings that reinforce the qualities and sense of building entry; and
- Seating elements such as benches, ledges, and movable chairs.
- Place these functional elements on an Addressing Street or Supporting Street and make their function visible from the streets and sidewalks. The interaction of private enterprise inside building with the public contributes to the vitality of the streets and adds a subtle message that behavior in the public realm is being observed, enhancing its safety. When passersby can sense activity that occurs inside of a building, they get a sense of people participating in their community.
- Locate the office and support spaces for warehouse and industrial buildings on the Addressing Street or Supporting Street instead of burying these functions in the interior of a large monolithic structure. The lower forms can help diminish the bulk of large building and add visual interest and a human scale to the public realm. This guideline may be accomplished by wrapping the high-bay industrial form with lower-scaled structures on the street; extending a discrete element of the building that contains these functions and giving this element a distinctive, contrasting architectural expression; or providing a visual break in the building mass and structure that creates an impression of two separate buildings.
- Office buildings windows and doors offer opportunities to decrease apparent building mass and promote a sense of human scale. Programmatic elements of office buildings including lobbies, conference rooms, lunch rooms, and fitness centers can be expressed as distinct elements. Multi-story office buildings in Coffee Creek should consider incorporating elements such as jogs or offsets in street-facing building elevations; building step-backs at upper floor levels; projections that create shadow lines; deep roof overhangs; major recesses in the building elevation to mark



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**BUILDING FORM**

Make all building entrances distinctive.

Protect the Pedestrian at entries.

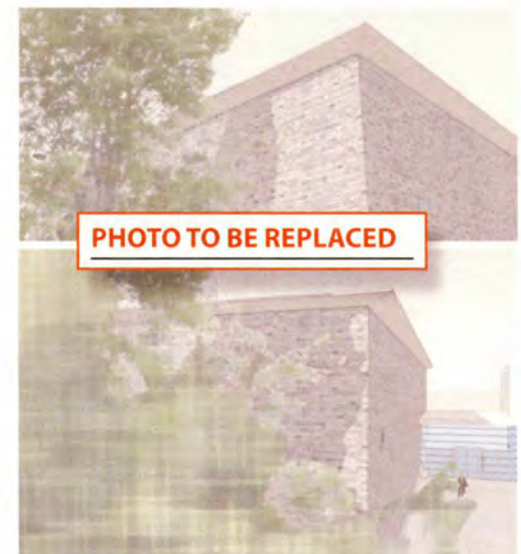
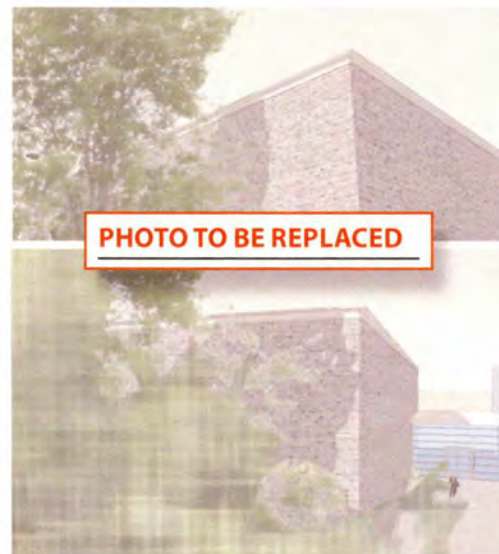


entries; or the bold expression of the building's structural system.

- Building elevations fronting Addressing Streets offer an initial impression of design quality and deserve special design attention. The development standards set only limited criteria for regulation including articulation of a base, body, and top for all buildings. The design guidelines encourage a rich articulation of design and creation of façades with a sense of depth by including design elements that create shadow lines, change color or materials, or incorporate other details that –together with the required landscape– breakdown large expanses of flat, unembellished surfaces.
- Articulate façades with a sense of depth by including design elements that create shadow lines, change color or materials, or incorporate other details that –together with the required landscape– breakdown large expanses of flat, unembellished surfaces.

**MASSING**

Pay special attention to the top of buildings where it meets the sky.



**Overall Building Mass**

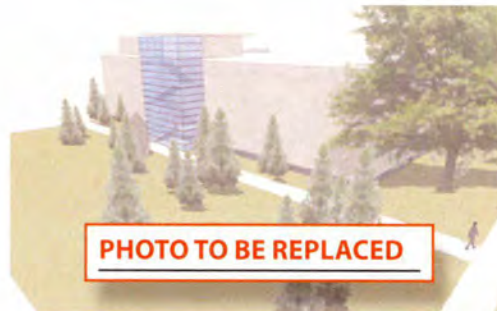
- The massive size, enormous bulk, and large surface areas of many industrial buildings represent design challenges and opportunities. Not all of the buildings developed in the Coffee Creek will be warehouses or factories. Some will be office buildings or industrial hybrid buildings that incorporate office, research, assembly, manufacturing, distribution and/ or warehousing. Buildings designed to support industrial or warehouse functions should have strong, simple forms and use windows and doors to create visual interest. Office buildings may have more varied forms that emphasize windows into, and views from, the office floors. While methods for reducing building bulk, mass, and scale will differ, the design for all buildings should consider architectural techniques that reduce their perceived scale along streets and adjacent to public spaces and help them blend into the district-wide landscape context for the aesthetic benefit of motorists, bicyclists, and pedestrians.

**Composition of Building Elevations**

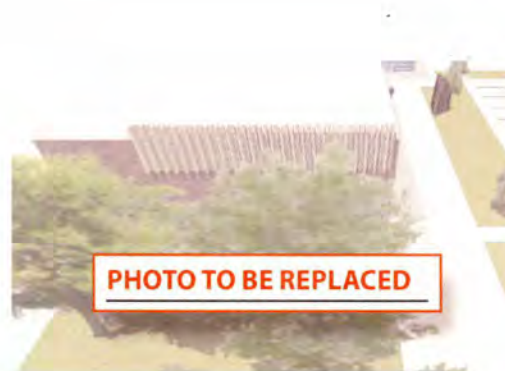
- The Form-based Code requires 50% of the building elevation on an Addressing Street to be constructed at the line of the front yard setback and a minimum height 30-feet. The combination of these two required design elements supports a uniform street wall of buildings along Addressing Streets frames the public realm and supports a unified streetscape.
- Building elevations fronting Through Connections deserve design attention. The development standards set only limited criteria for regulation including articulation of a base, body, and top for all buildings.
- Many types of businesses incorporate programmatic functions that require and benefit from daylighting. These functions include dining areas, lobbies, lounges, fitness centers, waiting rooms, conference rooms, lunch/break rooms, as well as related outdoor seating areas.
- The Form-based Code sets minimum standards for building elevation design that specify a clear division between a building's base, body, and top.

**Roof Forms**

- The roof forms of office buildings and industrial buildings in the Coffee Creek industrial area should be considered as the “fifth elevation” of the building and their design should be fully considered as one element in the overall design of any building. For manufacturing or warehousing facilities, the design guidelines strongly encourage the historic “saw tooth” roof form with integrated north-facing clerestory windows.
- At a minimum, the roof edge of all buildings will create a distinctive profile against the sky when seen from the public realm. In the case of warehouse or factory buildings with large floor plates, the roof may not be visible from grade and other elements of the building –the primary building entry, landscape plantings, signage, or elements of the building façade– will be the prominent design features.



- Buildings in the Coffee Creek industrial area are encouraged to include prominent roof forms. This guideline may be accomplished by accentuating the required building top with upturned eaves or projections; using sloped roofs; extending roofs beyond the building elevation to create deep overhangs; adding architectural elements like braces or brackets; and prominent vertical features such as towers or vertical circulation. Design roof forms to incorporate a building's mechanical systems and screen roof-mounted equipment is fully from view from the public realm. Minimize any visual clutter of multiple, isolated roof-top equipment by grouping such elements and screening them from view with architectural elements.
- Collection, storage, and discharge of stormwater from building roofs should be expressed as distinct architectural features, integrated into building design using the design of sloping roofs, gutters, scuppers, downspouts, and cisterns that collect and store rain water. Green roofs technologies could be appropriate in new buildings in Coffee Creek as an integrated element in stormwater management.



**ROOF FORMS**

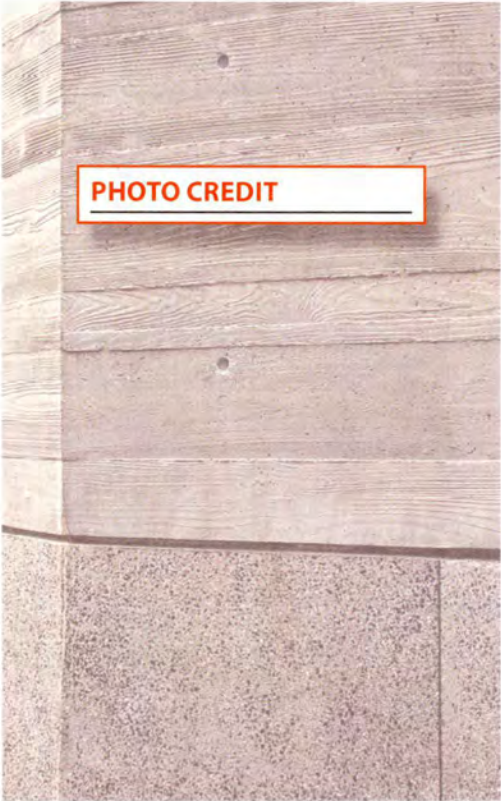
Use roof forms to express the industrial quality of the building and to provide opportunities for natural daylighting.

**Materials and Colors**

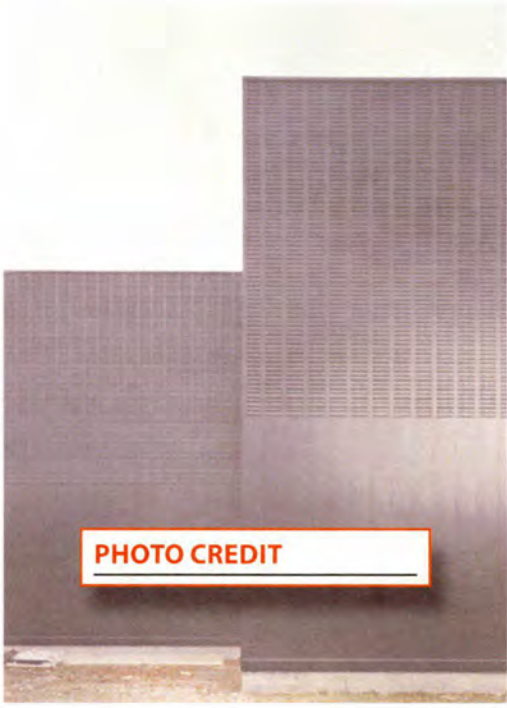
- Use of authentic, durable, and sustainable materials that derive their color from the natural setting of Coffee Creek support a consistent image and identity of the industrial area as a high-quality employment hub of the City of Wilsonville. Simple, basic, industrial materials, such as board-formed or cast-in-place concrete, architectural metal panels, corrugated steel, brick masonry, and architecturally-finished concrete masonry units are encouraged. Materials should be organized on each building elevation to emphasize the three required zones of base, body, and top and to highlight important features such as entrances.
- Site features and buildings should incorporate the subtle color palette derived from the natural landscape. Larger building forms can be made less prominent by employing a muted color palette drawn from the colors prevalent on the site.

**Sustainable Building Design**

- Sustainable building practices help to create a healthy communities and ecosystems.
- Where possible, buildings, surface parking lots, drive aisles, service yards, and loading focus should merge with the existing grades, rather than significantly altering them.
- To the extent possible, building orientation should consider solar exposure and capture the energy of the sun in a passive manner. Entries and public spaces should be sited where they can benefit from daylight. Building elevations facing south and west should incorporate deep roof overhangs, projections, or sun shading devices.
- Introduce natural light into buildings using clerestory windows and skylights. In those areas, such as offices, operable windows and natural ventilation is encouraged.



**MATERIALS**  
 Simple industrial materials carefully detailed, installed, and maintained are the most appropriate for Coffee Creek. .



**MATERIALS**  
 Opaque sheet metal walls can be elegant.





## Attachment B

### Section 4.134. Day Road Design Overlay District

(.01) Purpose. The Day Road Design Overlay District (DOD) is an overlay district within the larger Planned Development Industrial - Regionally Significant Industrial Area (RSIA) Zone. It is the purpose of the Day Road DOD to establish standards for site design and exterior architecture of all structures located in the Day Road DOD in order to ensure high quality design of development and redevelopment at the Day Road gateway to the City of Wilsonville. These standards are intended to create an aesthetically pleasing aspect for properties abutting Day Road by ensuring:

- A. Coordinated design of building exteriors, additions and accessory structure exteriors
- B. Preservation of trees and natural features
- C. Minimization of adverse impacts on adjacent properties from development that detracts from the character and appearance of the area
- D. Integration of the design of signage into architectural and site design, and
- E. Minimization of the visibility of vehicular parking, circulation and loading areas.

It is the intent to create improved pedestrian linkages and to provide for public transit. It is also the intent of this section to encourage architectural design in relationship to the proposed land use, site characteristics and interior building layout.

(.02) Applicability. The Day Road DOD shall apply to all properties abutting Day Road. The provisions of this section shall apply to:

- A. All new building construction
- B. Any exterior modifications to existing, non-residential buildings
- C. All new parking lots
- D. All outdoor storage and display areas
- E. All new signage
- F. All building expansions greater than 1,250 square feet.

(.03) Exceptions. This section does not apply to the following activities:

- A. Maintenance of the exterior of an existing industrial/employment structure such as painting to the approved color palette, reroofing, or residing with the same or similar materials
- B. Industrial/employment building expansions less than 1,250 square feet

- C. Interior remodeling
- D. Essential public facilities
- E. Existing dwellings and accessory buildings
- F. Agricultural buildings

(.04) Review Process.

- A. Compliance with the Day Road DOD shall be reviewed as part of Stage One – Preliminary Plan, Stage Two - Final Approval and Site Design Review. Such review shall be by the Development Review Board. Building expansions less than 2500 square feet and exterior building modifications less than 2500 square feet may be reviewed under Class II Administrative procedures.
- B. Waivers. Under City Code [4.118(.03)], waivers to several development standards may be approved, including waivers to height and yard requirements, and architectural design standards, provided that the proposed development is equal to or better than that proposed under the standards to be waived. For example, a height waiver might be granted on a smaller site if the façade presentation was significantly enhanced, additional landscaping or open space is provided and site modifications are necessary to preserve significant trees. Waivers to the additional front yard setback for future improvements on Day Road may not be granted. [4.134(.05)(C)(1)]

(.05) Design Review Standards. The DRB shall use the standards in this section together with the standards in Sections 4.400 – 4.421 to ensure compliance with the purpose of the Day Road DOD. These standards shall apply on all Day Road frontages, and on the frontage of corner lots abutting both Day Road and either Boones Ferry Road, Kinsman Road, Garden Acres Road or Grahams Ferry Road.

- A. Natural Features. Buildings shall be sited in compliance with WC 4.171, Protection of Natural Features and Other Resources and with WC 4.600, Tree Preservation and Protection.
- B. Building Location and Orientation: New buildings shall have at least one principal building entrance oriented towards the Day Road frontage. All building elevations fronting on Day Road or on the frontage on corner lots as described in (.05) above, shall have at least 20% glazing.
- C. Setbacks:
  - 1. Front Yard: For public health and safety reasons, the front yard setback shall be 30' plus additional setback (15' minimum) to accommodate future improvements to Day Road.
  - 2. Side and rear setbacks shall be 30'. Side and rear yard setbacks may be reduced from the 30' minimum setback requirement where the

setback is adjacent to industrial development subject to meeting other requirements of this section and Building Code requirements.

- D. **Building Height:** A minimum building height of three stories, 48' is required, on the Day Road frontage and on frontages described in (.05) above. Sites may contain a combination of taller building space abutting the identified street frontages together with 1 or 2-story lab, R&D, and/or manufacturing building space on the remainder of the site. The 1 and 2-story portions of the buildings will be designed to be compatible with the taller structure's design, building materials and colors. Increased building height is encouraged, particularly in combination with site amenities such as under-structure parking, preservation of significant trees rated good or better in the arborist's report, and/or provision of trail segments or of open space areas open to the public.
- E. **Building Design:**
1. Buildings shall be planned and designed to incorporate green building techniques wherever possible.
  2. **Exterior Building Design:** Buildings with exterior walls greater than 50 feet in horizontal length shall be constructed using a combination of architectural features and a variety of building materials and landscaping near the walls. Walls that can be viewed from public streets or public spaces shall be designed using architectural features for at least 60% of the wall. Other walls shall incorporate architectural features and landscaping for at least 30% of the wall. Possible techniques include:
    - a. Vary the planes of the exterior walls in depth and/or direction.
    - b. Vary the height of the building, so that it appears to be divided into distinct massing elements.
    - c. Articulate the different parts of a building's facade by use of color, arrangement of facade elements, or a change in materials.
    - d. Avoid blank walls at the ground-floor levels. Utilize windows, trellises, wall articulation, arcades, change in materials—textured and/or colored block or similar finished surface, landscape, or other features to lessen the impact of an otherwise bulky building.
    - e. Define entries within the architecture of the building.
    - f. Incorporate, if at all possible, some of the key architectural elements used in the front of the building into rear and side elevations where seen from a main street or residential district.
  3. **Building Color:** All colors shall be harmonious and compatible with colors of other structures in the development and the natural surroundings. Concrete finishes must be painted. The general overall atmosphere of color must be natural tones. Stained wood, natural stone, brick, dark aluminum finishes, etc. shall be used as background colors. The use of corporate colors is permitted provided that such colors are not patterned so as to compete for visual attention. The use

of corporate colors shall not create an advertisement of the building itself. Corporate colors shall not violate any other color or design limitations within the Code.

4. Building façade articulation: Both vertical and horizontal articulation is e required. If a building is at a corner, all facades must meet the requirement. Incorporation of several of the techniques is the preferred option. The purpose is not to create a standard rigid solution but rather to break up the mass in creative ways.

- a. Horizontal articulation: Horizontal facades shall be articulated into smaller units. Appropriate methods of horizontal façade articulation include two or more of the following elements:

- i. change of façade materials
- ii. change of color
- iii. façade planes that are vertical in proportion
- iv. bays and recesses
- v. breaks in roof elevation, or other methods as approved

Building facades shall incorporate design features such as offsets, projections, reveals, and/or similar elements to preclude large expanses of uninterrupted building surfaces. Articulation shall extend to the roof.

- b. Vertical Facade Articulation: The purpose is to provide articulation, interest in design and human scale to the façade of buildings through a variety of building techniques. Multi-story buildings shall express a division between base and top. Appropriate methods of vertical façade articulation for all buildings include two or more of the following elements:

- i. Change of material
- ii. Change of color, texture, or pattern of similar materials
- iii. Change of structural expression (for example, pilasters with storefronts spanning between at the base and punched openings above)
- iv. Belt course
- v. The division between base and top shall occur at or near the floor level of programmatic division
- vi. Base design shall incorporate design features such as recessed entries, shielded lighting, and/or similar elements to preclude long expanses of undistinguished ground level use
- vii. Differentiation of a building's base shall extend to a building's corners but may vary in height

5. Building Materials:

- a. No less than 50% of the exterior exposed walls of any new building, or any expansion over 1,250 square feet, shall be constructed of noncombustible, non-degradable and low maintenance construction materials such as face brick,

architectural or decorative block, natural stone, specially designed pre-cast concrete panels, concrete masonry units, concrete tilt panels, or other similar materials. Metal roofs may be allowed if compatible with the overall architectural design of the building. Where an elevation of the building is not currently, or will not likely in the future, be exposed to public view, the above standard does not apply.

- b. Accessory structures visible to the public shall be constructed of materials similar to or the same as the principal building(s) on the site.
6. Roof Design:
- a. Roofs shall be designed to reduce the apparent exterior mass of a building, add visual interest and be appropriate for the architectural design of the building. Variations within an architectural style are highly encouraged. Visible rooflines and roofs that project over the exterior wall of buildings, and especially over entrances, are highly encouraged.
  - b. Mechanical Equipment and Service Areas: Mechanical equipment and service areas shall be screened from adjacent properties, from Day Road and on Day Road corner properties abutting SW Boones Ferry Road, Kinsman Road, Garden Acres Road and Grahams Ferry Road. The architectural design of the building shall incorporate design features which screen, contain and conceal all heating, ventilation, air conditioning units, trash enclosures, dumpsters, loading docks and service yards. Such screening shall blend visually with the related structure.
7. Pedestrian Walkways:
- a. A continuous pedestrian walkway shall be provided from the primary entrance to the sidewalk along Day Road for access to building entrances and to transit facilities.
  - b. Walkways from parking areas to building entrances shall be at least six (6) feet in width, and shall be separated from moving vehicles. Walkways shall be distinguished from vehicular areas through the use of special pavers, bricks, scored concrete or similar materials providing a clear demarcation between pedestrian and vehicular traffic.
  - c. Buildings shall be connected with onsite walkways at least six (6) feet in width.
8. Community Amenities: Community amenities such as patio seating, water features, art work or sculpture, clock towers, pedestrian plazas with park benches, connections to area trails, parks and open spaces, and similar amenities are strongly encouraged.
9. Lighting and Flag Poles: All lighting shall be shielded and directed interior to the site, including parking lot lighting. Lighting shall not

spill over onto adjacent properties. Light poles, light fixtures and flagpoles shall conform to the City's Outdoor Lighting Standards. Flagpoles shall not exceed 40' in height.

10. Signage: Signage shall include a monument sign on the Day Road frontage identifying the industrial/business park and buildings therein. Each building may have wall signage, and such other directional and informational signage as allowed by WC 4.156.05, 4.156.08, and 4.156.09. Pole signs are prohibited. The design of signage must be integrated into the overall architectural and site design for the project. [Amended by Ord. No. 704, 6/18/12]

11. Parking: Employee parking shall be located at the rear of the building, or in courtyard parking areas between buildings. If no other option is available due to site limitations, then employee parking may be located to the side of buildings. Time and number limited visitor parking is allowed at the front of the building. Within a Stage I master plan, employee parking may be combined in a shared facility or facilities with mutual use agreements. Any parking areas visible from Day Road shall be screened from view with broadleaf evergreen or coniferous shrubbery and/or architectural walls or berms.

- (.06) Infill construction. The following general rules shall be followed when constructing a new building adjacent to existing industrial/employment buildings built under the Day Road DOD. Adjacent includes buildings north of Day Road built under the Day Road DOD.
- A. Proportions and Façade: The average height and width of the surrounding buildings determines a general set of proportions for an infill structure or the bays of a larger structure. The infill building shall fill the entire space and reflect the characteristic rhythm of facades along Day Road. If the site is large, the mass of the façade must be broken into a number of smaller bays to maintain a rhythm similar to the surrounding buildings.
  - B. Composition: The composition of the infill façade (i.e. the organization of its parts) shall be similar to surrounding buildings. Rhythms that carry throughout the block, such as window and door spacing, shall be similar to those on surrounding facades.
  - C. Detailing/Textures: Infill architecture shall reflect some of the detailing of surrounding buildings in window shapes, cornice lines, brick or stone work, etc. Textures of exterior surfaces shall be reflected in the design of new buildings.
  - D. Materials: An infill façade shall be composed of materials similar to adjacent facades. The new building(s) shall not stand out from existing buildings.
  - E. Color: All colors shall be harmonious and compatible with colors of other structures in the development and the natural surroundings.

- F. Setbacks: Setbacks for new buildings shall be an average of the setbacks of the two adjacent buildings built under the Day Road DOD, or if none exist, shall meet the setback requirements of the Day Road DOD. Rear yard setbacks may be reduced from the 30' minimum setback requirement in Section 4.135(.06)(D) where the setback is adjacent to industrial development subject to meeting Building Code requirements. Front yard setbacks must include additional setback (15' minimum) to accommodate future improvements to Day Road.
- G. Building Height: A minimum building height of three stories, 48' is required on the Day Road frontage and on frontages described in (.05) above. Sites may contain a combination of taller building space abutting the identified street frontages together with 1 or 2-story lab, R&D, and/or manufacturing building space on the remainder of the site. The 1 and 2-story portions of the buildings will be designed to be compatible with the taller structure's design, building materials and colors. Increased building height is encouraged, particularly in combination with site amenities such as under-structure parking, preservation of significant trees rated good or better in the arborist's report, and/or provision of trail segments or of open space areas open to the public.
- H. Lighting and Flag Poles: All lighting shall be shielded and directed interior to the site, including parking lot lighting. Lighting shall not spill over onto adjacent properties. Light poles, light fixtures and flagpoles shall conform to the City's Outdoor Lighting Standards. Flagpoles shall not exceed 40' in height.



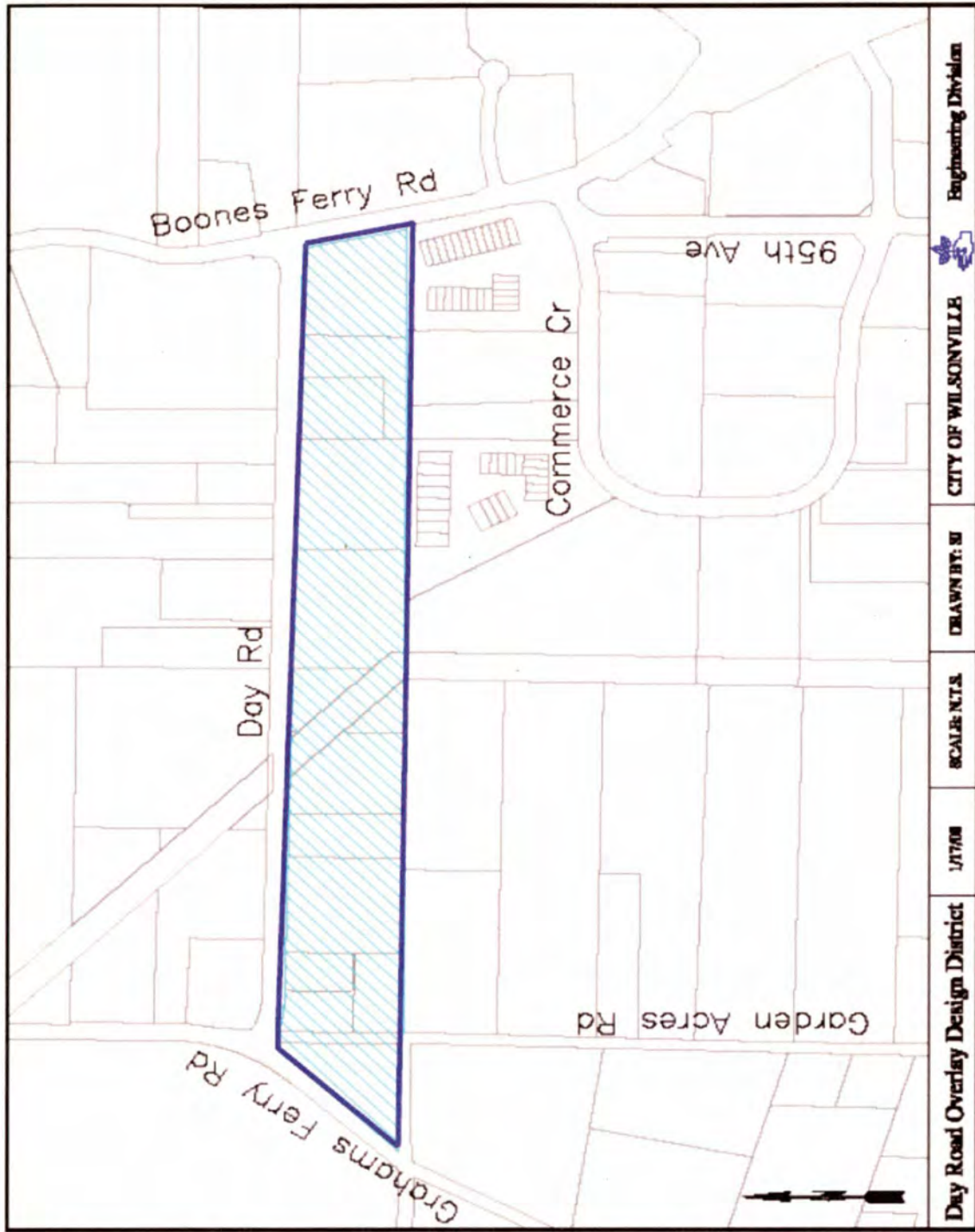


Figure D-1: Day Road Design Overlay District Area Map

## ATTACHMENT C

### PLANNING COMMISSION WEDNESDAY, FEBRUARY 19, 2014 6:00 P.M.

Wilsonville City Hall  
29799 SW Town Center Loop East  
Wilsonville, Oregon

Approved  
April 9, 2014

#### Minutes

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#### I. CALL TO ORDER - ROLL CALL

Chair Altman called the meeting to order at 6:04 p.m. Those present:

Planning Commission: Ben Altman, Peter Hurley, Al Levit, Phyllis Millan, Jerry Greenfield, and City Councilor Susie Stevens. Eric Postma and Marta McGuire were absent.

City Staff: Chris Neamtzu, Barbara Jacobson, and Katie Mangle

#### VII. WORK SESSIONS

##### B. Industrial Form-Based Code (Neamtzu)

Chris Neamtzu, Planning Director, introduced the Industrial Form-Based Code (FBC) project, noting the City had aspired to create this new tool for industrial development, specifically in the Coffee Creek Industrial Area. His key comments were as follows:

- Chamber of Commerce members and City Staff had been discussing for some time whether creating a FBC for industrial development was possible that would allow for a few primary objectives, which included having more certainty in the review process; ensuring the high-quality design expected of industrial development in the Wilsonville community; and saving time on that review process. With these primary objectives, Staff applied to the DLCDC's Code Assistance Program which was through the Transportation and Growth Management (TGM) Program. The Code Assistance Program focused on Code work and Code updates and involved many objectives, many of which involved promoting multi-modal, access, and connectivity standards, essentially some of the Smart Growth principles that the Commission and Staff had been studying. He noted the Planning Commissioners and City Staff member who had just returned from the Smart Growth Conference, where they learned that the cities of Denver and Miami had adopted citywide FBCs. The Commission's meeting packet included the first technical memorandums that would lead to the first draft of FBC.
- A Technical Advisory Committee (TAC) meeting was held today with developer and broker interests as well as Chamber of Commerce representatives present. The conversation was great as two hours were spent discussing expectations and outcomes, and reflecting upon the past and the history of the existing Development Code with regard to the Day Road Design Overlay District. The meeting was very helpful in introducing the consultant team to that history and getting feedback from outside stakeholders.
- He introduced Laura Buhl of the TGM program who was from Salem, Oregon and part of the project management team. Also present were Marcy McInnelly and Joseph Readdy of Urbsworks, urban designers and architects who were the primary lead on the project and experts in the FBC field, along with Keith Liden of Bainbridge Planning. Included in tonight's packet as background information was the Sacramento Area Council of Governments (SACOG) FBC Handbook, which was created by Ms. McInnelly and Mr. Liden and was an extensive FBC document that provided a foundation for FBC and

how it could be applied. He noted the internal project management team consisted of Assistant City Attorney Barbara Jacobson, Ms. Mangle, Mr. Neamtzu, Manager of Current Planning Blaise Edmonds and City Transit Director Stephan Lashbrook.

- He noted the three memos attached to the Staff Report and the PowerPoint presentation that was also distributed to the Commission provided an overview of the FBC and an opportunity to establish some baseline assumptions to ensure everyone was on the same page with regard to the Form-Based Code.
  - He added this FBC would be unique, as few examples were available for pure industrial development. The Code would be the first of its kind in the Portland Metro area, and possibly in Oregon as a whole. The team had discovered sections of existing FBCs in Bend, Oregon. One memo before the Commission tonight was a case study featuring four applicable case studies.
- He reiterated that the presentation would lay a foundation, adding the project would move quickly due to the short timelines and finite amounts of money involved with TGM grants. The team would work within those parameters, but building community consensus would be a challenge with the short timelines. Regardless of the grant funds, the City would do what was needed to complete the project. Tonight was a good opportunity for the Commission to learn, ask questions, and articulate their expectations about what they would like to see achieved in its final form.

**Marcy McInnelly, President, Urbsworks, Inc.,** stated she was excited to work on the first FBC in a light industrial area. She briefly described her professional background, including dealing with the zoning code in the City of Portland as a former Portland Planning Commissioner for five years. During that time, she was an architect and came to realize there had to be a better way of obtaining an urban form than through the typical land use focused zoning codes. Portland had one of the better zoning codes at the time, but there were still issues that placed her on the front line of battles with neighborhoods regarding compatibility issues. As a result of her experience, she became very interested in FBCs and began studying, taking many courses and subsequently got employment and gained a lot of experience with FBC. She and Mr. Liden were commissioned by the SACOG to write a book, which she believed was one of the best books written about FBCs. She was very excited about this project because it would allow them to engage a new frontier, the industrial district FBC, which had never been done before.

Ms. McInnelly briefly presented an overview of the project and issues related to FBC via PowerPoint, which were also discussed in the attachments included in the meeting packet. She reviewed general questions and specific areas which they sought input about from the Planning Commission noting that:

- The consultant team was biased toward making the Industrial FBC mandatory because it would achieve the desired urban form more consistently and might be easier to administer.
- FBCs are subjective and do not rely on much discretionary judgment when used in making urban form approval decisions. Sometimes, a menu of various options was provided to meet a regulation; applicants still had to meet the requirement, but could choose one of five different ways to meet it.

Commissioner comments and discussion points regarding the FBC concept was as follows with responses from the consultant team and Staff as noted:

- The focus on Day Road was wrong; it was not a gateway or entry corridor, but an industrial area.
- The 3-story requirement was also questioned. It seemed FBC could address multiple story options while giving direction. As an example, a single-story warehouse or manufacturing building with an office versus a research and development building, like Mentor Graphics, allowed for three different types of use functions that did not have the same building need and never would. A 3-story building was more of a traditional downtown frame for a commercial center. Even there, a wall of 3-story buildings was not preferred; a variety of heights was desired.
  - Flexibility that provided various users direction on how they interfaced with the street and created a public interface should be considered; a specific height should not be dictated.
- Did the TAC have a vision of the type of businesses they desired to attract? For example, the needs of a metal fabrication shop would be very different than those of lab related business. What was the TAC's strategy for the types of industries they were looking to attract? As discussed, light manufacturing jobs would be created because that was desired and fit the City's housing development plan.

- The TAC wanted flexibility and the ability to build something that was light industrial with the option to build something with more intense employment. The main concern was that nothing stood in the way of a building being built that had jobs.
  - Interest also existed in the ability to build buildings able to successfully accommodate a number of uses; so some buildings would be built speculatively for an unknown end user.
  - Parking flexibility was discussed. If a building were built speculatively, initially it might have light industrial, but later, it could have many more employees and need more parking. Concern existed about the possibilities for future adaptation of buildings being precluded by an overly restrictive Code.
- It appeared that a Kruse Way design was being created, but the buildings were moved closer to the street for Day Road. Day Road was not a Kruse Way environment. If that was desired, it should be created on Boones Ferry Road near the freeway, where the space was available. Day Road seemed like a more logical place for a Class A type environment.
  - Based on discussions with various builders, flexibility of the Code was the question. In discussions at the Chamber originally, they were unsure how to work with the Day Road Design Overlay District. Part of the issue was being clear about what the City was trying to accomplish and the Code delivering that, as well as providing the flexibility.
- The summary that applied FBC to the way the overlay had been written and identified pluses and minuses was good. It would be helpful to consider how FBC might have provided a different result on other industrial buildings, particularly along the freeway; such as how the new code could help the City achieve a better design without fouling up the function. There had been no design focus on the interface with the street; the buildings were more functionally designed.
  - The Day Road Overlay District put a lot of emphasis on what the building should do in terms of architectural treatment. The more effective way to create a strong, somewhat unified character to the public realm was to make sure some regulations applied to the relationship between the building and street, and the street and building.
    - While the regulations would be very flexible, minimum standards regarding where buildings were placed in relationship to the street should exist. TAC strongly believed that parking should be allowed between the back of the sidewalk and the building; they wanted that flexibility. This could be accomplished, but then the emphasis should be on the landscape. Parking should possibly be limited between the fronts of buildings and important streets, like Day Road and Kinsman Road. The City could still allow parking, but require it to be more landscaped than a typical parking lot. It was also possible that the City's landscaping standards were adequate and could be used instead.
    - With regard to the building, less emphasis should be on the wall of the building itself and more on the treatment of the landscape. A simple base, middle and top of the building could be enough to create some articulation. The City should not go overboard requiring recesses in the façade that do not offer much influence when looking at a row of buildings on a street, the full length of the street.
- Concern existed regarding the heavy emphasis on citizen input in the documents the Commission had been provided with from Sacramento. The way the process was structured, citizen input would be minimal at the end because the TAC represented the developer's side.
- Would Day Road be developed at the same time as or sequentially with Coffee Creek, and would it be a stretch for FBC to be done for the entire area as opposed to just Day Road?
  - Mr. Neamtzu thought that the development sequencing would begin in the southern area and move north. However, the geography of Day Road was highly desirable so it was possible that infrastructure could be advanced in an effort to make Day Road happen. The logical progression was that development would advance from Clutter Road and Ridder Road toward Day Road. In the logical sequencing of infrastructure, Day Road looked to be in the later phases.
    - When Basalt Creek planning was completed, the possibility of jumping Day Road did exist.
  - Regardless of how it developed, it should all be one concept; otherwise, one area would look different than the rest or it would eventually have to be expanded to the rest.

- Agreeing that Day Road was not the gateway would lead to a certain conclusion how development should occur. If it was agreed that Day Road was a gateway, that concept needed to be carried into the Basalt Creek planning to ensure it ended up on the north side of Day Road as well.
- The FBC would apply to all of Coffee Creek, not just the Overlay District, and Day Road was on the north edge of Coffee Creek. It would allow for the option of two standards within the FBC, but not force one thing.
- A pseudo 3-story building was not environmentally friendly and was a waste of resources and energy in the long run. Architecturally, more height did look better, but a mix of heights was preferred.

The Commission discussed and provided feedback specific issues presented in the PowerPoint as follows with comments from Staff and the consultant team as noted:

- Multi-Modal Connections. What were different ways connectivity could be achieved? Were larger spacing standards necessary? Should an exemption or different standard be in place in the industrial area? How could pedestrian, bicycle and transit circulation and access be accommodated?
  - Ms. McInelly noted consideration of the employees in the industrial district and how they travel was important in terms of how they got around at lunch, travel to work and from work, etc. but especially, that they might not have to use a car every time they wanted to leave to run errands, grab a sandwich, etc.
  - Pedestrian and bicycle crossings should be frequent as long as they were not at corners.
    - To get workers in the industrial development to use these facilities, they would need to go to lunch, for example, within walking distance. Currently, no establishments existed within walking distance on Day Road, so clusters of shops would also need to be added to the development.
  - Connectivity should be a requirement for pedestrians. The street spacing standard of 530 feet was not an issue given it was not a residential area.
  - For bicycles, FBC would have to be reworked, unless it referred to existing Code which mandated many items with regard to bike parking and circulation in parking lots.
  - If the business had customers coming and going, visitor parking up front made sense but other cars should be located in the rear.
  - Focusing more on regulating the landscaping in the front of the building than being so prescriptive on the building itself made more sense when trying to encourage this type and a variety of development. Aesthetically, a row of 3-story buildings would not fit well in the area. A much more uniform look could be achieved if the landscaping approach in the front was emphasized, even if the buildings were different heights and configured differently to fit the business. The key was to consider what the business need; what would attract them to the area.
    - One example involved the buildings located on the west side of Kinsman Road. Many trees were located in front of the buildings, so they were hardly seen. On the other hand, on 95<sup>th</sup> Ave where the buildings were set back from the road, even though there were many trees, the facades of the buildings and all parking in front could be seen, which was much less appealing visually, if a more sophisticated look was desired.
  - At the Smart conference, pedestrian focus versus auto focus was discussed and involved a whole different design context. The auto focus involved getting from A to B in the quickest way possible. To encourage pedestrian activity, an attraction and experience beyond the A to B must be created to attract people to the area. The situation was a bit different because the location was industrial, but to get people to walk and bike, one had to get to that level to see the experience.
  - The City's street standards design the street side, but ignore the property side, leaving it to the individual property owner so there was no continuity of activity. Framing both sides of the sidewalk with landscaping would be very important. A section of sidewalk on Boones Ferry Road near Tonkin Audi was framed on both sides with shrubbery, but one side screened the parking lot so nothing could be seen; it was a wall of shrubbery. That was not a pedestrian environment. The pedestrian component required reconsideration because the design had been looked at from the driver's perspective of not wanting to see vehicles.

- Moving employee vehicles to the rear would be okay, but not all of the parking lots had to be hidden; instead visual interest should be created. Nothing was uniform in nature, but people have a propensity to line things up in nice even rows and symmetrically spaced out.
- It would be good to keep industrial employees in the industrial area. Otherwise, they would flood other locations in town at lunch or break time. If the area was purely light industrial, with zero commercial, it would generate more and more traffic problems.
- Another issue with the Industrial Code was that it severely limited retail.
- In the minutes of the December meeting, a comment was made that a park could not be in an industrial area. If that was true, what would the impact be on the FBC.
  - Mr. Neamtzu believed that issue involved Metro's functional plan requirements. There was litigation regarding the Tualatin TSP and Tonquin Trail, and it appeared that parks in industrial areas were precluded. The City's Bicycle & Pedestrian and Parks & Recreation Master Plans identified industrial waysides in generalized locations throughout the industrial zone. The waysides were close to creeks or a pocket of trees and provided opportunities for some active recreation, like a basketball court. Some of the more progressive developers would provide that type of amenity for their employees onsite. Simple design elements such as outdoor plazas with tables could enable people to have lunch on a large industrial site.
  - These industrial sites were huge and a 20-minute walk would not get one very far. Having to get back to one's shift in 45 minutes would force one to drive as fast as they could to the nearest outlet; these were real challenges.
  - Xerox's campus had a lot of open green space and people could be seen playing ultimate Frisbee at lunch, which made a big impact on employee morale and productivity.
- One interesting twist was that Coffee Creek was a Regionally Significant Industrial Area (RSIA) which involved square-foot limitation caps on development. The limitation caps did not recognize the extensive size of some of the industrial buildings and the City's Code was not exactly perfect in regards to that. With 500,000 sq ft single building, only about 5,000 sq ft would be allowed for support services in that building.
  - According to the Code, two 20,000 sq ft buildings would be allowed 20,000 sq ft of commercial and half could be retail because in a multi-building complex, up to 20,000 sq ft of commercial use is allowed. The Code did not scale or slide well as buildings get larger.
- Support services were wanted in industrial development, but it was difficult to get and retain them. A small deli or restaurant in an industrial area struggled because they had no traffic after 6 pm, and no evening or weekend business. The businesses tend to provide the service and many provide a cafeteria, lunch break experience, rely on food trucks, etc. Even businesses in a commercial zone adjacent to an industrial zone tend to struggle to make a profit in off peak hours.
  - Support services that make the work environment a vibrant place should be identified. The team is considering the employee experience as it related to large employment centers.
- At City Council, the 3-story requirement was debated at length because the City wanted to identify that type of employment or a Mentor Graphics-type complex with a lot of employees per square foot in a building. At the time, the City was working on the Goal 9 Economic Development of Industrial and Commercial Lands and Council had a vision to have more employees per building. The density of employees was a target and a focus, so multiple stories could be dictated and that a Mentor Graphics-type development the community had aspired to could be identified.
  - At the time, Council believed there had to be a vision; otherwise the project would never happen. A fair amount of testimony was taken stating that Day Road was not market feasible, would take forever and was not cost effective. Council made the intentional decision to stick with the vision they had for the project. The minutes could be provided to the Commission and had already been provided to the consultant team because it was very important for them to read through the testimony. Many of the people invited to be on the TAC had testified in opposition to the 3-story requirement at the time.
  - That was seven years ago and the City was in a different place now. The City had developed an economic task force, incentivized single-parcel TIF zones, and now had a large call center with many employees. New times warranted a new look, but the work that was

previously done would not be dismissed. The TAC looked at the FBC with fresh eyes, and tried to apply it and it had flaws. The City could do better and should seize the opportunity to do so. Passion and vision drove the decisions about Day Road. Unfortunately, the Code was poorly implemented.

- Weeks had been spent discussing the percent of building glazing on Day Road as part of the public hearing process. The amount of glass required around a building was driven by the Energy Code and, through the desire to obtain a certain look on Day Road, all the building glass was being forced on one elevation, the north side. A lot of nuances were involved in building design and site function.
- The 3-story requirement regarded three functional stories. Mr. Readdy had noted that as written, the Code would allow a fake western storefront of three stories to be built that was six inches deep because the Code did not discuss how far it should wrap around the corners of the building. Many flaws existed and some of the issues had been identified in the memo in the packet.
- Some City Code that required specific spacing on building articulation caused a lot of difficulty in making the inside work.
- Administration. Having a new Code for the Coffee Creek Industrial Area that applied a different process with non-discretionary standards implied that Staff would be given more ability to approve and the Development Review Board (DRB) and other bodies would have less. FBC did a good job of incorporating non-discretionary standards, but could offer choices to provide flexibility. Streamlining the approval process for industrial buildings seemed to be of great interest to most everyone. How did the Commission feel about non-discretionary standards making up the majority of regulations in the new FBC?
  - Non-discretionary standards would be good because they would allow for more certainty. At DRB, one design was approved for a building but it was not actually built the way it had been presented originally. The explanation provided was that the project, as designed, was too expensive to be completed. The whole process was very deceptive. The changes were made by Staff and that was within their purview.
  - Non-discretionary meant specific, discreet requirements and numerical standards, so no judgment was required to dictate compliance.
  - If the FBC allowed Staff to make more decisions, taking away potential for DRB approvals, there should be less discretion.
  - Having more non-discretionary, discreet standards and numerical standards in a Code, did require more work upfront. More involvement would be required by the TAC and Commission to ensure the standards were right. Once adopted, Staff or the DRB would use those standards to determine whether a project complied.
    - This was another fork in the road. If the path of more objective standards were taken, a more thorough review of the standards would be required as they were developed, and there would be no chance for second-guessing them after adoption.
  - FBCs could be constraining with regard to results, but they could also be freeing with regard to process. If a certain quality of landscaping was desired between the building and street, there might be three different ways to achieve that through different development standards. It could be accomplished via a perimeter parking lot landscaping of a certain height, material, etc., or through an architectural treatment, or something else, for example. The developer would then have three different ways to meet the requirement. The standards were all discrete, but they provided the developer with a choice.
  - A recent DRB case involved the terminology "safe and convenient", which left a lot of room for judgment. The DRB found the application was not safe or convenient, applying a different standard apparently than Staff. Defining safety in the Code would be very complex. The definition would have provided a much clearer meaning of what "safe" meant in terms other than one having good driving manners. The case required that it be safe for people to respect pedestrian access by leaving space for crossing traffic in two queue lanes, but the Code did not provide for that, except in this kind of discretionary characterization of safe and efficient. Some degree of discretion should

not be lost in such regard. Having to be so detailed as to make that a check box would be wearisome because one could not determine every instance in which safety and efficiency might come into play.

- Specific language regarding cross traffic, and particularly cross traffic involving queuing at a drive-thru, parking lot behavior, etc. could be required. It would be difficult, but determining what the Code meant by “safe and efficient” would be helpful to developers and the DRB. Judgment should not be excluded altogether, and that was what the DRB step did. If Staff ironed out virtually all the issues, and they did for the most part, the last step of citizen overview was of great importance.
- The team was not envisioning that any Code would be strictly clear and objective criteria from one end to the other. Places would exist in which judgment would have to be exercised. The master planning at Coffee Creek would be a four-step process involving a zone change; the planned development, Stage I and Stage II; and then the site plan review. A portion could be more discretionary, similar to the way it was done currently, and other portions could be clearer and more objective as the review process occurred. The tough question was how the Commission viewed the two different types of criteria.
- One inclination was to avoid discretion to the greatest degree possible, but in terms of design some level of discretion always existed.
- In terms of the Stage I Master Plan and Stage 2 Final Development Plan, good FBC guidance within the master planning would address 80 percent of the issues. During specific site design, a more refined review would take place and 95 percent of the issues should be addressed.
  - With regard to the DRB project previously discussed, the gap was in the Code. The Code did not provide good guidance on how to design pedestrian versus auto circulation and it still leaned most likely toward the auto orientation. Once that was in place, pedestrian connectivity was fit in wherever possible. Design should begin with pedestrian orientation in the street and then address the site. If design was done on that basis, especially if with master planning, global circulation issues were already considered.
- Discussion returned to Multi-Modal Connections.
  - The 330 ft crossing requirement was a problem with a 600 ft long building. Convenient pedestrian circulation needed to be considered and the options available, such as turning a building sideways to create proper circulation. Block spacing requirements came from Metro and perhaps that could be discussed with them. With a good TSP and Pedestrian Plan, the City should be able to dance around that requirement to some degree to maintain good circulation. Pedestrian circulation was the weakest part of the TSP.
    - A 660-ft spacing was being considered for connections of all kinds, so two different standards would not exist for streets versus shared use paths. The 660-foot spacing would apply on center for everything from shared use paths that could be public easements on private land to something totally private, such as a parking lot drive aisle, to an actual street. A number of streets would need to lead into the large areas because a lot of property could not be accessed from the streets due to access spacing standards. At this time, the system of FBC regulations would include a spacing standard, but there would also be a wide range of ways in which the standard could be met.
  - Pedestrians would not walk 660 ft if they wanted to cross the street; jaywalking was being encouraged. People would always walk the shortest distance.
    - Mr. Readdy displayed the Project Site (Slide 3). He clarified the issue was less about crossing new and existing streets at appropriate locations and more about ensuring a pedestrian could get from Kinsman Road to Garden Acres Road without walking all the way to Day Road or Ridder Road. They also wanted to institute standards that allow for the industrial scale buildings needed for employment without eliminating the ability for someone to get from one destination to another without getting in a car.
      - He indicated where a pedestrian path connected a sandwich shop site through to Kinsman Road and on to Garden Acres Road. Someone working in that area might be able to bike or walk over to eat lunch once or twice a week. However, if they had to get in their



car, would they drive to Commerce Cir, someplace closer to downtown or someplace even out of Wilsonville entirely? Choices are made based on where one could reasonably travel. The team was working to create a network of pedestrian, bicycle and some road access points through the whole district.

- If the access ways were spaced out too far, the shortest distance was not being created which was a concern.
  - The balance that needed to be achieved was to find a spacing distance that was great for pedestrian/bicycle connectivity and supported the scale of industrial development desired.
  - Two issues were being discussed: the block size due to the size of the buildings and sites being developed; and opportunities to get across a street, both could be different distances. Crossing opportunities could occur more often than 660 ft.
  - New crossing signals allowed for safer mid-block crossings.
- Lot sizes would be up to the developer, but the team was working to develop a regulatory system in which certain spacing standards would have to be met, which would help dictate some of the lot sizes.
  - The 660 ft was actually based on the width of two of the parcels between Garden Acres Road and Kinsman Road, which was indicated on the map. Existing property ownership was one generator of possibly creating the intermediate network, rather than resorting to picking a number out of the air.
    - From the bottom of the study area, between Garden Acres Road and Kinsman Road, there would be at least two or three intermediate roads between Ridder Road and Day Road. Hopefully there would be at least one or two additional pedestrian/bicycle connections through there as well. Much of this depended on when infrastructure gets implemented, at what rate and how property was assembled or consolidated for development, which made the project a real fun challenge.
- An incentive approach was very concerning and not appropriate with FBC. The City could elect to establish something that worked as best and most prescriptive it possibly could. Trying to incentivize people to follow FBC was not in Wilsonville's best interest, in terms of trying to meet goals [inaudible].
  - Staff and TAC seemed to be reserving judgment about whether the system could work, which was why the alternative track was being carried through. Ms. McInelly was confident the City would get an actual Code that was workable, could streamline the process, and be good enough to become the only single mandatory system that everybody else was waiting to see.
- The decision was whether a FBC should be adopted or not. Having an alternative was not favored, because it was like having the option to do a regular development subdivision or a planned development, and the Planned Development Code was not used because it was easier to go the other route. The whole reason to change to a FBC was the City was trying to accomplish something it had not been able to do yet. In order to really test that, everything currently available needed to be put toward that effort.
  - The only way to compare it in reality was to compare the FBC in the Coffee Creek location to the PDI zone elsewhere; two different systems were needed to know which was doing what.
    - Some TAC members had said the double system would be confusing to administer and a unified character might not be achieved. A coherent streetscape would not be obtained if one property followed the old system and the next followed the new FBC system.
- One item at issue was determining what role waivers played in a FBC process if that was the only path. The concern was that the second the FBC was adopted, someone would come in with something totally unanticipated that was not in the Code, and no opportunity existed to accommodate it because the other alternative had been shut down. In that situation, the person might have to be sent to Hillsboro because the City had no way to allow that to happen in Wilsonville.
  - That could be where the discretionary element came back into play and a process could exist where 95 percent of what was desired could be obtained through the FBC and, if Staff got stuck, it could be sent to design review for discretionary issues. General guidelines would still exist about what was trying to be accomplished via the FBC. By the time the FBC is completed, the Commission would be very clear about what they were working to accomplish. The current

problem with implementation of Code was that the City did not know what they were trying to accomplish. Instead, they were working to match the Code standard to "safe and convenient" circulation or setback standards or use criteria, but the net result was still not clear. This process forced thinking about the outcome which would then get worked it into the FBC structure and defined so that everyone could judge it.

- Waivers could exist for some regulations and not others. In some instances, regulations should be required and no waivers would be allowed, such as for connectivity, but perhaps the building façade could have a waiver.
- Waivers were thought of as having to do with very specific requirements. The overall system applied, but certain things could not be done and forgiveness was requested.
- The process where a waiver could be requested existed and it would be justified by returning to the design objectives in the Code, stating that the objectives were being met but one thing was desired instead of another. That was where the current waiver process was used. It was unsure whether the FBC could address that in terms of options built into it, rather than as a waiver. In other words, the objectives could be met in a number of different ways. An option might still exist, but the sense was that led to the discretionary part that would remove it from Staff's hand. A limit existed to the discretion that could be applied.
- The team's work consisted of two components: the FBC and the Pattern Book, which was intended to provide much more guidance regarding the design objectives and could provide more of a basis for waivers, if needed. If a waiver from the non-discretionary standard would be useful, the Pattern Book would be burdened to provide guidance about how that waiver could be allowed.
  - The FBC would be very simple with its numerical standards and the way that it works was sort of stark. It did not offer much information, except in an intent statement upfront. Through the numerical standards, one could not necessarily see the desired result. The FBC was better than conventional Codes as far as providing an idea of the objectives.
  - The Pattern Book would complement the FBC because it could show all different ways that the non-discretionary standards in the FBC could be met, using photos and illustrations. For example, minimum standards would be provided in the FBC regarding the landscaping and amounts of pavement needed for bikes and pedestrians on a shared use path. The pictures in the Pattern Book could show different ways that the shared use path requirement could be met. It was intended to be much more visually informative.
  - The Pattern Book would illustrate multiple options and would not offer a single illustration for any one standard. The Pattern Book did not seem to stifle variety, but seemed to create quite a lot of creative opportunity.
    - For example, in the FBC handbook written for SACOG, the northwest crossing FBC was used for a residential mixed-use district in Bend, Oregon. The FBC stated houses on a certain street must meet certain setback and height requirements, setting the building envelope and the more dry numerical standards. The Pattern Book displayed how a house could be designed to have a modern look, bungalow look or a colonial look; it showed a range of different house styles that could be emulated. It did not dictate any style. The styles could not be mixed, but one style could be chosen and then the characteristics described in the Pattern Book for that one style would be followed.
  - Villebois was the closest the City had come to FBC and, in reality, most of the construction there was pretty similar. Most of the industrial or commercial light industrial space around Wilsonville was fairly similar. It would be nice to see some variation in style.
  - Christopher Alexander was considered to be the grandfather of the *Pattern Language* concept. His other book, *Timeless Way of Building*, begins with a chapter entitled, "Quality Without a Name". The book is almost mystical in its language, but the bottom line was that a bottom line exists and that was something that could not be nailed down precisely. That was overreaching everything in that the whole Pattern Language takes place within that quality. If you do not achieve that quality, you have not achieved your objective. This is something that Wilsonville has driven for, something like that quality, and the Commission needed to be very careful not to do anything which constrained the City out of that quality, but instead allowed for working within it and fulfilling it.

- The consultants had had been worked on FBCs for many mixed-use or residential-focused districts. When considering the industrial district, that kind of quality might not exist or be achievable in some areas, like the loading dock areas, for example. The focus was on the streets where connections would be made, and where that pedestrian scale and quality was wanted. The goal was to have a system where the Commission was very careful about how that quality was achieved in the targeted areas while allowing flexibility for the developer in other areas that are not regulated very much, such as the backs of the buildings, for instance.
  - Referencing Alexander's sensitivity, the Commission needed to be mindful of the experience of the person who was in this case, working in that space; they were the main focus. The experience of people working in that district was even more important than people driving past and thru into Wilsonville.
    - That point was consistent with earlier comments regarding the pedestrian environment. Much of the City's focus had been on the street's appearance and users were being ignored in the process. If the FBC process worked right, the site would be designed to support the users, while interfacing with the public piece for the overall circulation. As much of a focus should be put on the overall site design, making it functional, not only for the warehouse function but also the employees.
- The current industrial Code required a landscaped strip down the entire property line and a 30-foot setback on all property lines regardless of what was there. The Code also had a landscape requirement of at least five feet on all perimeters, resulting in having five feet on the other side of adjoining properties with completely different landscaping and no continuity or cross flow between them. Other than the idea that things should be screened, what that was to accomplish had never been considered.
  - Uncertainty existed about whether the backyard should be ignored, but the desired environment needed to be created so that the front was not a showcase and the back a war zone. Criteria were needed regarding how the entire site would function.
  - Given the desire to have pedestrian/bike pathways run through the development, the pathways must go behind the facilities. The backs of the facilities would be visible, so some treatment would be needed to make the experience a pleasant one.
- First, connectivity requirements must be met. A network of connections must be laid out which could include a variety of different types of connections; the choice would be left up to the developer. However, in each circumstance where the property met the connection had to be treated in a pedestrian friendly manner. Priority could be given to the fronts and sides of the buildings, and less would be given to the opposite side and the rear. The pathways would still have to be human scaled and landscaped with quality. In the system currently being tested, the developer would decide which was the front and side of the building and would have to meet certain higher standards for those sides of the building.
  - A developer would not be able to place loading docks street side. Visitor parking could be placed street side, but it would be limited and the parking would have to meet a higher standard of landscaping than if it were on the rear side of the building.
  - One notion discussed amongst the project management team was the idea of requiring some sort of interior plaza to be developed along the lines of pedestrian and bicycle connectivity. Certain standards would have to be met, but these were the same things the City had been pressing industrial clients for: a workplace that provided amenities for employees that are on the site day in and day out.
    - Foremost in the team's mind was that the development was a place where nothing would be left undesigned, but the design would be regulated based upon Christopher Alexander's *Pattern Language* hierarchy that worked from the large scale to the small scale, and the things that counted the most were regulated. The developer and architects would be able to develop things that were more discretionary without being heavy handed with regulation.
- If one goal was to obtain high-quality employment, the design had to be employee-focused. If the City wanted to bring in high-quality people, a high-quality design was required. If a row of factory buildings were desired, that would be a whole different story, but instead, the goal was to attract better businesses.

- Day Road should not look like the Tualatin-Sherwood Hwy or 95<sup>th</sup> Ave in Wilsonville. If the project was going to be done, it should look nice.
- Mr. Neamtzu assured Staff would do everything needed to ensure citizen involvement was appropriate and proper for the process. Nothing was ever done short of that and he would make sure that was taken care of. Property owners were being engaged as well, so there would be specific outreach for the people who owned land in that area when the right point was reached.
  - Getting the TAC's feedback would be useful at some point as well.
- Some comments had focused on the Pattern Book providing information rather than the more prescriptive pieces of FBC. The Pattern Book was where many of the design options would come from and would force the Commission into defining what they wanted to accomplish.
  - The current Code required a 30-foot setback, specific building height, a certain number of required parking spaces and a minimum landscaping requirement, and that was it. All of those could be met and the result would still be nothing.
- One huge area for job growth would be the new entrepreneur. Portland had a creative class and "if you built it, they would come" attitude. Wilsonville should build that sense of place, where the creative class wanted to be.
  - For example, the Rockwell Collins building and its sister building could be developed to go from five tenants to one tenant, and to have one, two or three stories inside. Having an extremely well lit meandering bike path with seating situated between two concrete, 3-story tilt-up buildings was another idea. Enhancing the rear building walls in loading dock areas was also suggested. Hillman Court at 95<sup>th</sup> Ave, where the Nike building was across from what will be World of Speed, had one little corner with a grove of trees. The sidewalk was not against the street, but meandered through the property and at 5:30 am people were walking and cycling throughout that industrial area. Such areas might support a little sandwich shop.
  - Creating a development where someone wanted to come to work should be considered. The idea was to provide flexibility, while creating areas that offered a strong sense of place and character at the same time.
    - In the SACOG document, at least two Starbucks were pictured.
- Boones Ferry Road was not considered to be an entranceway to Wilsonville, even though it was being rebuilt because it came from a residential area. If the entranceway were not Day Road, it would have to be somewhere in that location. Grahams Ferry Road would be a better option, but uncertainty existed about where it would land.
  - When the overlay was created, Day Road was being thought of for the 124<sup>th</sup> Ave extension from Tualatin to I-5, so the idea was that the major traffic flow would be that way. Through the Basalt Creek Plan, it looked more and more like the connection would be farther north, so Day Road would no longer be the key corridor, which the Commission needed to keep in mind.
  - It was suggested that Boones Ferry Road would be a better option to that east/west corridor and 124<sup>th</sup> Ave/Tonquin Rd was a more logical corridor than Day Road. An arterial street with three 90-degree turns had never been seen as being functional.

Ms. McInelly thanked the Commission for their time adding she looked forward to returning with something more specific to discuss and she hoped the Commission would see how they had influenced the result. She understood it would be best to not talk in the abstract and that understanding certain concepts without specifics was difficult.

**Light Industrial Form-Based Code TAC Meeting**  
**February 19, 2014**  
**1:00 p.m**  
**Meeting Notes**

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**Attachment D**

The meeting commenced at 1:05 p.m. Those Present:

Marcy McInelly and Joseph Readdy, Urbsworks	Laura Buhl, DLCD
Chris Neamtzu and Linda Straessle, City of Wilsonville	Stu Peterson, Macadam Forbes
Cheryl Dorman, Wilsonville Development Review Board	Greg Specht, Specht Development
Eric Postma, Wilsonville Planning Commission	Gene Mildren
Steve Gilmore, Wilsonville Chamber of Commerce	Ernie Platt
Keith Liden, Bainbridge Planning	

The following documents were distributed:

- Wilsonville Planning Commission Staff Report with attachments:
  - A. Sacramento Area Council of Governments Form Based Code Handbook(SACOG) (The link is <http://www.sacog.org/projects/form-based-codes.cfm>. (Large document – only 3 were available at the meeting)
  - B. Case study research on projects that have innovative design standards and review processes.
  - C. City Code evaluation memorandum and sketches
  - D. Regulation memorandum
- Paper copy of PowerPoint presented during the meeting.

Chris Neamtzu gave an overview of the Light Industrial Form-Based Code and the objective of today's Technical Advisory Committee meeting.

- He cited the City's planning history of the Coffee Creek and Day Road areas. He discussed some of the problems with the Day Road Design Overlay District and suggested this tool could be an exciting replacement for parts or all of the existing Code. .
- Wilsonville is proposing a new tool that could be used for those areas that would accomplish the objective of high-quality industrial development in less time and with greater certainty.
  - \* A Light Industrial Form-Base Code will include discretionary and nondiscretionary processes. It is envisioned at this point of the process as an optional path; not a required path.
- The main outcomes of this project would be the adoption of a Light Industrial Form-Based Code (FBC) into the Development Code and a Pattern Book to apply to the Coffee Creek Industrial Area.
- While form-base codes are traditionally done in downtown areas, Wilsonville is proposing it for a light industrial area.
- A \$60,000 TGM Code Assistance Program Grant is funding this project. Laura Buhl of DLCD manages the Code Assistance Program for the TGM Program, a joint program between DLCD and ODOT.
- The Planning Commission staff report that was distributed at the meeting includes early, preliminary technical memorandums.
- The consultant team includes two firms, Urbsworks and Bainbridge Planning.
  - \* They were selected by the State.

- \* They have extensive experience in developing form-base codes in other parts in the Country. He cited the Sacramento Area Council of Governments (SACOG) Form Based Code Handbook as an example of Urbsworks work.
- City staff and the Project Management Team have already met a few times. This committee, today, is the Technical Advisory Committee. The Planning Commission is the official public body for public review when that time comes.
  - \* Once the consultants present the proposal, the rest of the meeting is for the TAC members' response to the proposal; the Project Management Team is interested in their comments and concerns.

He thanked those at the table for their willingness to help with this code work.

Those at the table introduced themselves and briefly related their interest in Wilsonville and experience in development.

Marcy McInelly stated that she is the project manager of this effort and related her background on doing FBCs. Using a PowerPoint, Marcy presented an overview, goals, framework, site, and schedule of the project as presented in the Planning Commission staff report.

- She is an advocate for Form-Base Codes because they implement more predictability, nondiscretionary standards so that the process can be more streamlined, predictable, and more transparent in what urban form is desired. As far as they can tell, this project is the first industrial form-based code of its kind in the United States, and would be the first form-base code of any kind in the Portland-Metro area.
- She asked that those at the table think about the following as the project was being presented:
  - \* Did they perceive that there are problems with the current Code for the Coffee Creek and Day Road areas?
  - \* If they do, offer suggestions as to how they think those problems might be solved.
  - \* How can they be solved within the context of this project?
- Laura corrected that the final end date of the project timeline is August 2014.
- There are not that many projects around the country where multi-modal transportation is a priority in an industrial area. Moving people, and cars, around truck traffic can be a challenge.
- A lot of the FBC will come from Wilsonville's existing Code. They may simply be reorganized, some language added, or amended to make them stronger.
- Marcy highlighted different areas of the SACOG that could further explain FBCs and issues that could be applied to this project.
- The SmartCode, El Paso, Texas Case Study's airport district could be used as a template for customizing a local municipality's large industrial area.

Joseph Readdy, using the PowerPoint reviewed the issues listed in Attachment C.

- Three major components of the Day Road Design Overlay District (DRDOD):
  - \* Creating a Gateway to Wilsonville.
    - He understood that the design elements of the DRDOD reflected a desire to create a Kruse Way-type area along Day Road.
  - \* Regulate design to ensure height and design quality.
    - There is a lot of information in the DRDOD about arterials, finishes and variations in building facades, but it doesn't have a hierarchy of elements of most important to least important.

- It is trying to get high design quality from the building out rather than from the street into the site. He thought that working from the street back into the site would be more effective in accomplishing a higher design.
- The DRDOD requires the building entrances to be accessible from Day Road but a concern is that this could be detrimental to the function of that building once it has been constructed, and to the adaptability to a future tenant.
- \* Connection, context, & the capacity to drive urban form in the area.
  - A PowerPoint slide was shown that included 4 site plans to demonstrate access drives, building entrances and parking. Joseph pointed out the issues of the site plans.
  - The Coffee Creek Master Plan shows the new Kinsman Road as well a pair of accesses to the south side of Day Road between Kinsman Road and Boones Ferry Road, and another pair of access points between Kinsman Road, going west to Grahams Ferry Road. Garden Acres Road is not projected to continue north to Day Road. He listed challenges that these access points present. He and Marcy believe that if these access points were more street-like in character, and less driveway-like, it would create a stronger urban form rather than letting driveways happen wherever they want.
  - Marcy stated that one of the key problems that they have identified is that there is a built-in contradiction between requiring the principal entrance to face the street when you can't have vehicular access on that street.

Keith Liden reviewed Attachment D. *Regulation Memorandum* of the staff report via the PowerPoint.

- He explained that the proposed standards in the memo are preliminary ideas to start the conversation.
- He reviewed the General Questions slides.

Marcy finished up the presentation by reviewing the final PowerPoint slides starting *with Issues, Challenges, Opportunities*.

She then opened up the discussion to the TAC members who offered the following questions and comments and suggestions:

- Stu Peterson, Gene Mildren and Greg Specht participated in the DRDOD planning process and all felt that their input on the standards for the area had been disregarded and they were accused of being biased. They all agreed that basing the Day Road design on Kruse Way, including the 3-story building requirement, was unpalatable and unmarketable.
  - \* There are too many differences between Day Road and Kruse Way such as the heavy truck traffic on Day Road and that Day Road ends at the prison. Kruse Way leads into a residential area surrounded by high end residential homes.
  - \* Several disagreed with the consultants' assertion that there were areas along Kruse Way and Meadows Road where buildings could not be seen.
  - \* Three-story industrial office buildings are very rare.
  - \* It was noted that at the time that the three-story building was adopted as part of the DRDOD, City Council discussed the desire for many more employees per parcel. Because of the recent recession, we are in a different place. We should be talking about what do we want now and that is what this process is largely about.
    - There needs to be flexibility for the market to figure out what should be built in this area. Focus on something that responds to the demand and the marketplace today; that draws tenants to Wilsonville rather than to Vancouver, Portland, or elsewhere.

- Focus on allowing a product to be designed that meets the needs of the marketplace.
  - Correct the deficiencies in the Code to allow business to come to Wilsonville and make an investment in the Day Road area or elsewhere in Wilsonville. Let's fix it, not just change it.
- \* Joseph stated that if this area of Wilsonville is being held until it could develop according to a vision, this is fine if that is what the city wants to invest in, but if what you want is an employment district, then the Code needs to be written for that.
  - If you want to allow jobs to be located in this area, the Code ought to allow one end of the spectrum to the other whether it is a 10-story office building or a manufacturing facility or flex deal; they can co-exist.
  - The goal is to get to "yes" as quickly as possible if a development fits within the parameters of the FBC.
- Stu stated that he thought that Wilsonville's PDI zoning standards were very flexible. Wilsonville has a more germane industrial Code than Tualatin.
  - \* One of the first reforms that the FBC will address is simplifying the way industrial uses are classified in the Code. Marcy stated that the first goal of a FBC is to be more flexible about land use. The long classification lists in codes are outdated and don't reflect modern mixed uses. The land use side of Wilsonville's industrial district code is already form-based in terms of being flexible and modern; there is no intent of changing this.
  - \* There was a concern that businesses don't always fit the zone.
- Making the FBC mandatory versus an alternative option.
  - \* Some developers using the traditional PDI path and others using the FBC path along Day Road could create potential problems – this is something that still needs to be sorted out.
  - \* This could make it more difficult for the DRB to sort out during their review of a development.
  - \* The current DRDOD creates a situation where the first developer gets to set the color theme and everyone else then has to follow that pattern
- All disagreed that Day Road could be perceived as the entrance to Wilsonville and offered various reasons as to why it is **not** a "gateway".
- Spacing of 560 ft. between accesses is too tight.
  - \* It was noted that 560 ft. and 330 ft. for block face distances comes from the TSP update and mostly apply in commercial and residential areas.
  - \* The scale is different for industrial areas. The distance standards need to be realistic. The team is looking at how to make it work.
  - \* 20,000–40,000 sq. ft. buildings are no longer the norm; larger 100,000 to 1 million sq. ft. buildings are becoming more common. The City's new Economic Development Strategy was cited.
- This is an industrial area and not a place for multi-modal traffic. People/bicyclists are not compatible with heavy trucking.
  - \* The current political environment is going to push for extreme sensitivity to multi-modal transportation. If these standards are not workable, then find another way to incorporate the bicycle/pedestrian/public transportation concepts into the FBC.
- There are great examples of industrial buildings around Wilsonville that have already been through a development review process; use them as models.
  - \* The Rockwell and DW Fritz buildings were cited as good buildings to use as examples of the FBC's building design. They have manufacturing, research and some office space.
- Whether a development could bypass a DRB review if they meet the FBC was discussed.



- \* It was suggested that if a development meets the FBC, they it should be able to bypass planning review and go straight to applying for building permits.
- \* The Chamber's perspective is that speed saves money which in turn gets people working sooner.
- \* It was noted that historically, Wilsonville's process is citizen driven where citizens have most of the authority and City Staff has limited authority. Wilsonville relies on the DRB to make the best decisions in the broadest interest of the community.
  - The recent Sign Code revisions were the first time in recent history that the community/Council entrusted City Staff with ability to make decisions in the best interest of the community.
  - There is a delicate balance because the bigger decisions deserve public conversations.
- \* This FBC is going to go through a public process; it will go through the Planning Commission and City Council, ending with a determination about how the Coffee Creek and Day Road areas are to be developed. Then let the development happen without having the delays that additional public review at the DRB level will create. Whatever speeds up development in Wilsonville, we should do it.
- \* It might be possible to achieve a greater shift. toward more administrative decision-making because these industrial areas are not next to residential and do not have the visibility that can cause public concerns and comments.
  - It was noted that there are some homes in the Coffee Creek area that will be significantly impacted by industrial development around them.
- \* It was questioned if once the public involvement for the FBC takes place, and when an individual applicant comes forward and complies with the FBC, then it is no longer a land use decision that could be appealed – at that point it is simply applying for a building permit? It was stated that this was true.
  - It was suggested that if you can't accomplish this with the FBC, then nothing has been accomplished.
- \* Developers conforming to the FBC will be conforming to the Comprehensive Plan and the Zoning for the area.
- \* Portland's IG2 zoning was cited as an area where as long as the objectives are met, then a developer can go straight to getting building permits – there is no discretion. The consultants were asked if they could support that kind of process.
  - Marcy responded that they like FBCs because they provide upfront work to develop the standards and the community is allowed to weigh in during that process. Once that process is done, those are the adopted standards. They are not discretionary and a developer can meet them much more easily. The adoption of the FBC for this area is a good possibility. The highest hope is that if it works well here, then it could be transferrable to other land use districts within the City of Wilsonville.
- \* The Consultants were asked what they thought about taking Portland's IG2 zoning requirements, renaming them FBC and applying to the Day Road area, and calling it good.
  - Chris said that he is not familiar with it but he could take a look at it.
  - Marcy stated that City of Wilsonville's land use mix for the industrial area is more progressive than the City of Portland's IG2 as it allows commercial/retail uses in industrial zones; Portland's IG2 does not. It was noted that the IG2 zone is limiting and not a good model.
- The FBC process is seen as creating predictability and speed with which industrial applications can be considered and go through the process.
  - \* There still can be aesthetic standards, but still have flexibility for its use and construction.

- The length of time to amend the Comprehensive Plan and Zoning Map were cited.
  - \* Wilsonville's land use process currently goes from conceptual to specific, then the last step is City Council's review.
    - This is backwards; if you flip the process around and have Council making its Comprehensive Plan Map & Zoning Map decision first, then the DRB process should go through more nimbly with more clear criteria.
  - \* Wilsonville's application process is quite lengthy; it can be seven to eight months and that does not include the construction time. It was noted that Hillsboro's application process is 45 days.
    - Chris noted that during the last five to six years, Wilsonville has been moving quickly through the land use process. Stu agreed with this assessment.
  - \* With Wilsonville's current discretionary process, an appeal can add even more time.
- Several expressed support for the FBC if the FBC gets rid of the subjectivity making development as objective as you can.
  - \* If you really want to solve a problem, then get rid of the discretion. This FBC seems to do that.
  - \* Establish a set of guidelines that allows a variety of product type whether it is a 20,000 sq. ft.. building or smaller, or a 250,000 sq. ft.. building, without the potential for an appeal.
- What does "light" industry mean?
  - \* The Coffee Creek area is zoned as a Regionally Significant Industrial Area (RSIA); it is an industrial area, not necessarily "smoke and sparks" but it is manufacturing/storage/distribution.
  - \* It was stated that 39% of Wilsonville's employee base is industrial/manufacturing/ high technical. There is an expectation that this will go up due to OIT moving to Wilsonville and some of the businesses that are just now starting to locate here.
  - \* Chris stated that calling it "light" was an attempt to clarify that it is not "smoke and sparks". "Light" is a general term that was used loosely to define everything that is out there today in Wilsonville's industrial area.
  - \* "Light" may lead to a misperception about what could be built.
  - \* Laura noted that this is the way that it was proposed in the TGM Grant. She stated that one of the purposes of the project is to serve as an example for other communities and it helps define it better when other people are looking at the result.
- Parking:
  - \* There was a lengthy discussion about why the DRDOD parking standards are not feasible. The current Code requiring a principal entrance on Day Road that is not accessible from any parking areas is not realistic. The parking is behind the building requiring people to walk around the building to get to the main entrance.
  - \* Industrial development has to be designed to "over park" it to allow for future changes.
    - After discussing reasons why the parking locations and dimensions in Attachment C and on the PowerPoint slides were not realistic for an industrial area, it was noted that the sketches did not include the full site – they are only showing how the buildings sit on a lot. There is a lot more space that can be used for parking that is not shown on them.
    - It was noted that parking minimums and maximums are a Metro issue.
  - \* Currently in Wilsonville, warehouse buildings are being transformed into higher density employment areas with some manufacturing components. Loading docks are turning into striped parking areas with higher percentages of compact car parking.
  - \* Parking areas as shown in Attachment C and on the slides versus the DRDOD parking standards were discussed:
    - A FBC typically allows buildings next to a street with parking on the sides and behind the buildings.

- If there is landscaped screening between the parking and road, then parking in front of building is not that objectionable. You deal with aesthetics with building design, reveals, colors, and landscaping. A building along 95th Avenue that has a double row of parking in front and landscaping in front was cited.
- If you are trying to generate jobs along Day Road, then get to “yes” as quickly as possible with the proper qualifications.
- Parking and truck maneuvering dominate an industrial site.
- There was some agreement that having visitor parking in front and sides of the building, employee parking in the rear, and a turnabout in the front center, results in the best of both worlds. The slide showing the building along Kruse Way was referred to during this discussion.
- In an office building, you want the front lobby entrance to be double loaded where access to it comes from two different directions for the sake of efficiency.
- On an industrial building, some tenants want a grade-level door. A grade-level door facing the street ought not to be prohibited simply because parking is not allowed between the building and the street.
- Loading docks on the front side of a building are not appropriate.
- It was suggested that Kruse Way office buildings are not going to happen on Day Road. They will be industrial buildings, with some office components.
- From an office building perspective, having the principal entrance on a corner does not happen; the entrance is usually located in the center of the building. Industrial buildings do have entrances on the corner.
- Create a Code to allow for that, but don’t require it.
- Joseph stated that based on today’s discussion, they will be tweaking the sketches.

Marcy explained that the Project Management Team has only reviewed a rough draft. FBC version and has not made any decisions because they want the TAC’s feedback on what has been presented at this meeting. There are three main categories in the system of regulations that they are thinking about:

- How do you achieve the multi-modal connectivity through these really large blocks of land? They are trying to be flexible by achieving a pattern that allows pedestrians and bicyclists to move through the large sites from building to building, from parking to building, from other districts through to regional trail systems.
  - \* They are talking about a standard of 660 feet between connections, but they could be a wide range of connection types such as a shared use path, a parking lot drive aisle, or an access street. It would have to meet certain requirements for pedestrian walkability and bicycle-ability which in most cases would be a paved surface.
    - If you wanted to subdivide a large property, you would have to meet the 660-ft. minimum spacing standard but you could do it by having a parking lot and simply embellishing one of the sidewalks along the parking lot, put in a shared-use path, or put in the access street and make that your new addressing street.
  - \* Since for most of these lots, the perimeter streets that are either built or planned can’t have access. They are not going to be the addressing streets. There are going to have to be some new streets built to be able to address the buildings off of them.
- What might be the front or the side of the building will have to be established.
  - \* The sides have some regulations for aesthetic appearance. Beyond those two sides, and you decide which they are, there is not a whole lot of regulation.

- \* There is an assumption that there will be loading docks and a large amount of blank facades, that can be mitigated aesthetically through landscaping or architectural treatment, but we are thinking that it should be a broad venue of choices.
- The parking should be allowed along the main street and the fronts of buildings.
  - \* There will probably be a lot of regulation about how that is landscaped; they will probably use Wilsonville's existing parking lot standards, but there might be some additional ones about the way the perimeter is landscaped. This is mostly to provide a unified appearance on these streets that is created by the relationship of the building and the landscape together whether there is parking there or not; there is some aesthetic unification.

The group responded to Marcy's regulation system proposal:

- Intermittent multi-modal connectivity through a large industrial parking lot will create safety problems. This is private property and this is talking about allowing the public to move through it.
  - \* A public easement will be needed.
- Day Road currently has a lot of little parcels so can every parcel get its own driveway?
  - \* Chris stated that they would not; they would have to share an access which is where reciprocal access easements will be needed, so there may have to be cooperation amongst the entities to allow access.
  - \* A public access has a lot higher standards than a private street or driveway does. You get into cost issues there.
  - \* Required joint accesses or easements will have legal issues that may impact the marketability of a deal. This has to well thought-through to a conclusion that you can live with.
  - \* There are going to be negative repercussions of a public easement across private property that allows anybody to drive, park, walk, bicycle along it.
- Day Road is currently a race track now. A detailed traffic study was not done as part of the Coffee Creek Master Plan. Many of the parcels in the area are supposed to access onto Kinsman Road that is to connect to Day Road.
  - \* The eastern segment of Day Road from Kinsman Road to Boones Ferry Road is to be a five-lane road according to Washington County's TSP. It is in Wilsonville's TSP as a five-lane road as well.
  - \* The north side of Day Road is not in the city; it is the southern boundary of the Basalt Creek Planning Area.
  - \* The Coffee Creek Industrial Area is just under 200 acres in size.
- Joseph questioned if the vast, extensive roofs in industrial areas should be regulated, should people be encouraged to design these roofs in some way, or just accept that these large roofs come with the territory of industrial areas. He asked the group to think about different treatments that could be done with roofs and forward their ideas to the Team.

The TAC was asked to review the handouts and forward their comments to Chris.

Chris has not yet given his feedback to the consultants on the draft. FBC yet as he was waiting for the TAC's comments regarding what was presented today. Once the first draft. FBC is ready, it will be forwarded to the TAC and a second TAC meeting will be scheduled.

Comments, feedback, and questions are to be forwarded to Chris at [Neamtzu@ci.wilsonville.or.us](mailto:Neamtzu@ci.wilsonville.or.us) or call him 503-570-1571. He thanked everyone for their help.

The meeting ended at 3:05 p.m.

A large, stylized tree graphic in a light tan color serves as the background. The tree has a thick trunk and several large, rounded branches. The leaves are represented by white, vein-like patterns within the tan-colored areas. The tree is centered and extends across most of the page.

Attachment E

# City Of Wilsonville Light-Industrial Form-Based Code & Pattern Book

Technical Advisory Committee Work Session  
& Planning Commission Consultation

February 19, 2014

# Agenda

- **Introduction of project and team**
- **Project overview and issues**
- **Discussion**

*We'd like to hear from you.*

*Do you perceive that there are problems with the current code?*

*If so, how do you think they should be solved, and what can be done within the context of this project?*

# Project Overview

- **Transportation Growth Management (TGM)-funded project**
- **Project timeline: November 2013 to June 2014**
- **End product: Adopted code amendments to Coffee Creek and Day Road Overlay industrial areas**

# Project Goals

- **Enable multimodal transportation in industrial areas**
- **Ensure high design quality**
- **Streamline process for light industrial development**
- **Implement the 2007 adopted master plan for Coffee Creek Industrial Area**



# Organizing Framework

## Major Category

## FBC Element

**Multi-modal connections**

Street design & connectivity

Site design & circulation

**High design standards**

Building form

Architecture and landscape

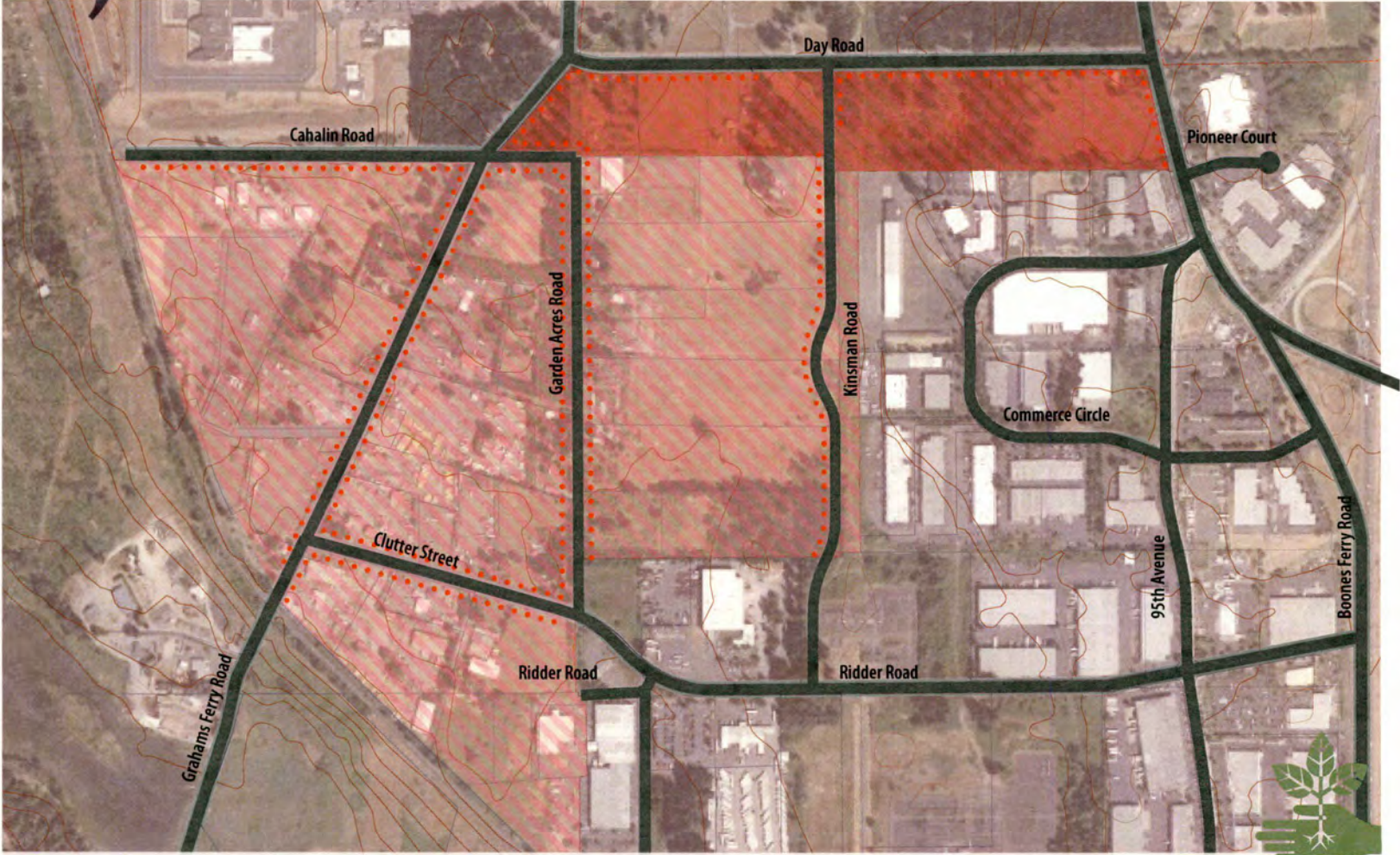
**Efficient public review**

Use

Administration



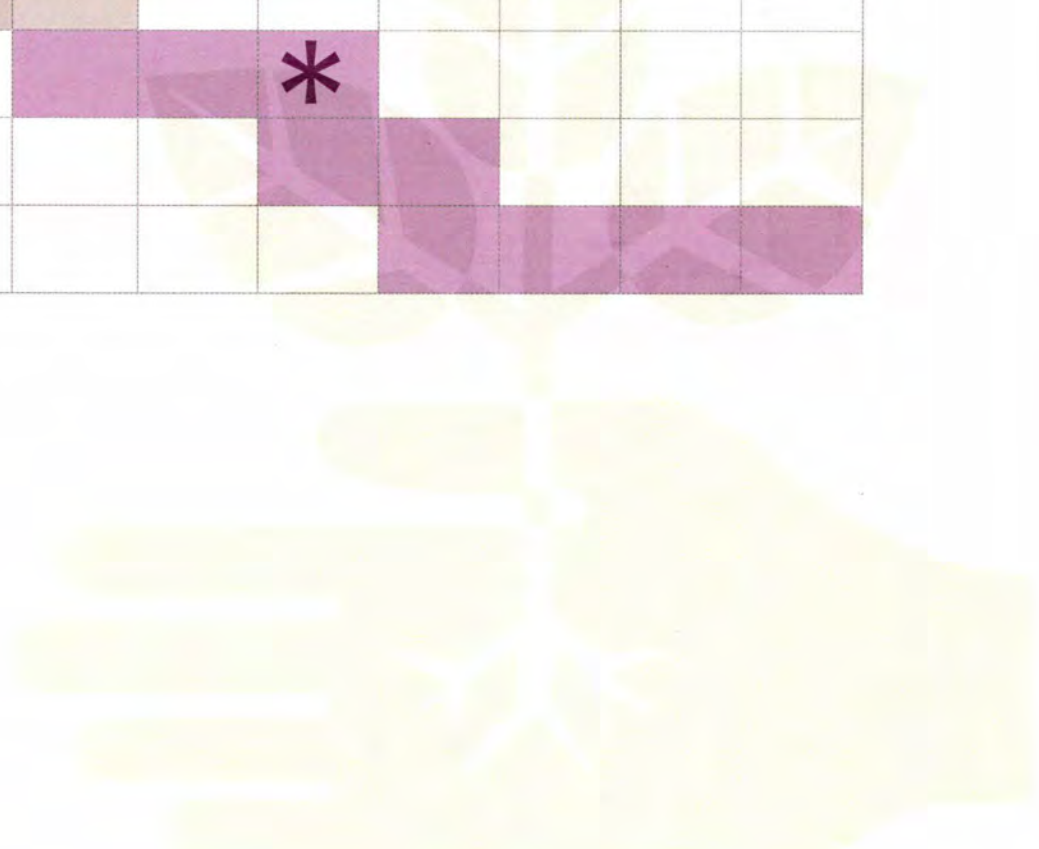
# Project Site



Coffee Creek Industrial  
& Day Road Design Overlay District

# Project Schedule

Milestones and Deliverables	2013		2014						
	OCT	NOV	DEC	JAN	FEB	MAR	APR	MAY	JUN
Project Kick-off									
Evaluate Existing Regulations									
First Draft Form Based Code					*				
Second Draft Form Based Code									
Final Draft and Adoption									



# Explaining The Package

- **Form-based Code Handbook for Sacramento Area Council of Governments (SACOG)**
- **Industrial Case Studies**
- **Evaluation Memo & Sketches**
- **Regulation Memo**

# Form-Based Code Handbook

- Educational handbook about form-based codes
- What they are, basic components and advantages, examples and analyses of different approaches
- In use and adopted in US municipalities
- Form-based approach for four “context types”
- Addresses multimodal street design; site design; building form; land use and architecture

# Industrial Form-Based Codes

## Four Case Studies

- Juniper Ridge, Bend Oregon
- City-wide Form-based Code, Denver, Colorado
- Miami 21 City-wide FBC, Florida
- SmartCode, El Paso, Texas



# Evaluation Memo & Sketches

- Gateway to Wilsonville
- Regulating design
- Connection, context, & capacity drive urban form



# Gateway

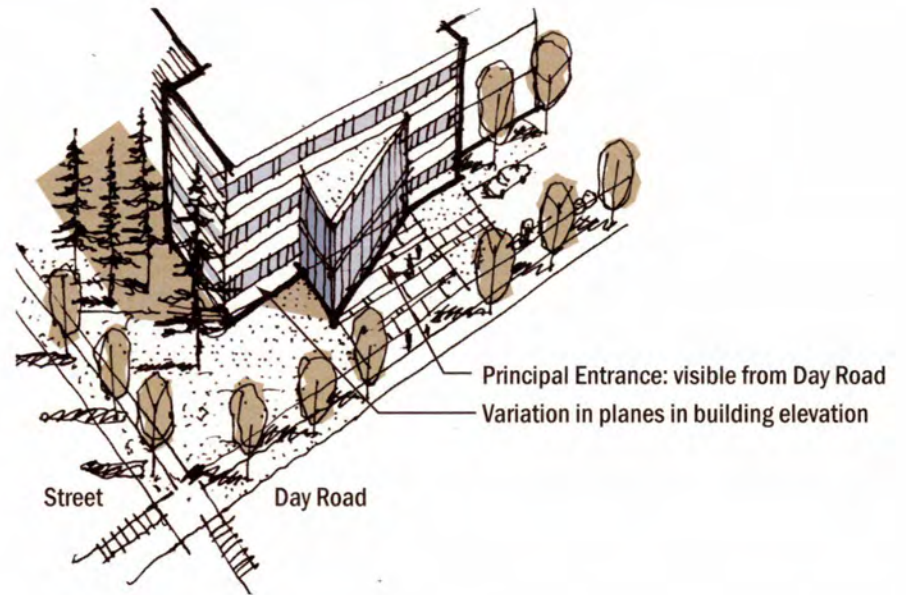
Vision | Reality





# Regulating Design

## High Design Standards

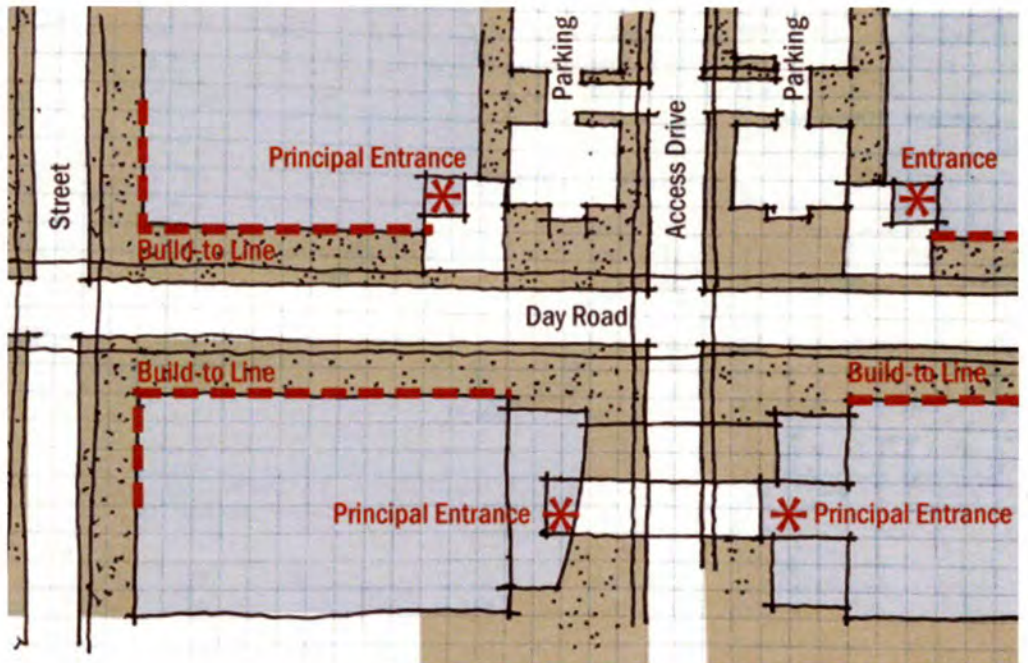
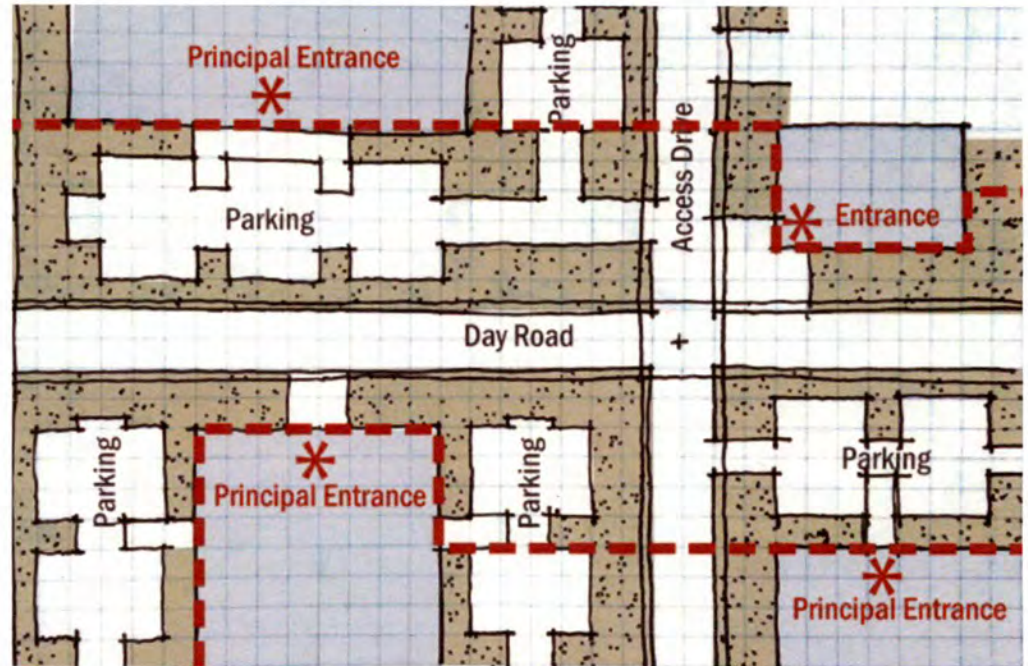


# Urban Form

Access Drives

Building Entrances

Parking: site location & extent



# **Regulation Evaluation**

## **– Purpose**

**Review and understand existing city requirements  
& application process**

- Provide preliminary amendment recommendations
- General recommendations
- Specific amendment ideas

**Raises questions for further discussion/evaluation**

# General Questions

## Affected Area

- Should the Day Road DOD be replaced with the new Coffee Creek Master Plan district?

## Code Organization

- Should industrial form-based code (FBC) district be self-contained or rely on cross referencing?

# General Questions

## Approval process

- Should following the Industrial FBC be mandatory or an alternative track for approvals?

## FBC Alternative track

- What incentives would be attractive and effective?

## Standards: discretionary versus objective

- How should these be used in the FBC?

# Issues, Challenges, Opportunities

- Multimodal connections
- Administration
- Building appearance
- District definition and character

# Multi-Modal Connections

## Background

- §4.135(.04) PDI block and access standards, §4.131(.02) PDC zone require a max. 530' spacing between local streets, max. 330' for pedestrian & bicycle crossings.

## Considerations

- Should the code amendments take into consideration the scale of larger buildings associated with industrial employment?
- How might we accommodate pedestrian, bicycle, and transit circulation and access?

# Administration

## Background

- The Day Road Design Overlay District requires a discretionary design review process that is perceived to add time and uncertainty.

## Considerations

- Should the new code for the Coffee Creek Industrial Area apply a different process with non-discretionary standards?



# Building Appearance

## Background

- Day Road Design Overlay requires min. 3-story frontage with office building appearance.

## Considerations

- Should the code amendments continue to treat Day Road differently than the rest of the Coffee Creek Industrial district?
- Should we pursue an alternative approach that keeps high design standards, allows 3-stories, but doesn't require office building appearance?

# District Definition & Character

## Background

- Day Road Design Overlay assumed new gateway to the City of Wilsonville.

## Considerations

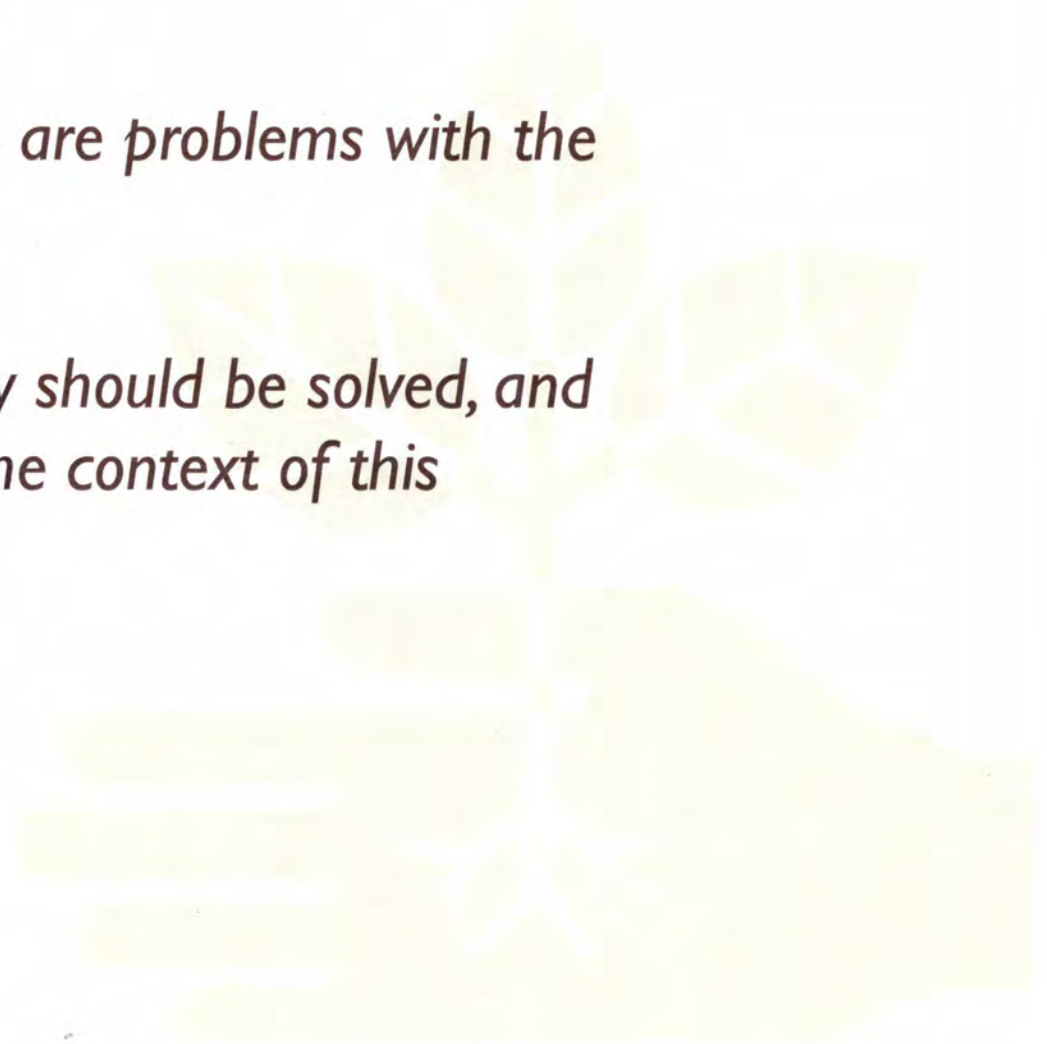
- Day Road will continue to carry high volumes of heavy truck traffic associated with nearby quarry; should we consider other streets, like Boones Ferry Road for potential as gateways?

# Questions

**We'd like to hear from you**

*Do you perceive that there are problems with the current code?*

*If so, how do you think they should be solved, and what can be done within the context of this project?*



# Coffee Creek Industrial Area Form-Based Code

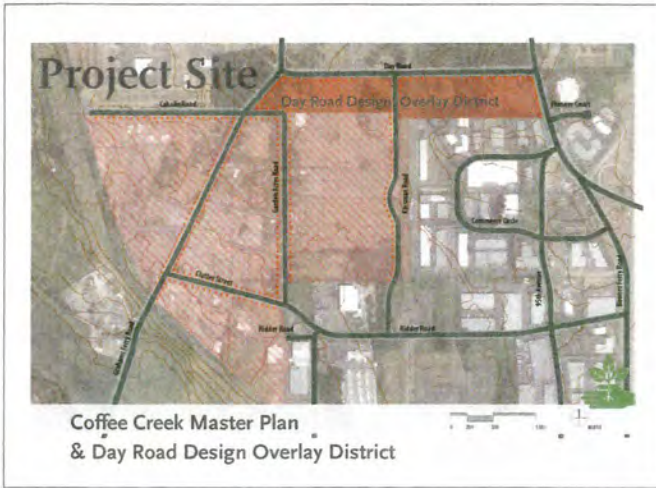
City Council Consultation

21 July 2014

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## Focus of the Project

- High-quality design for employment
- Balance certainty of results with flexibility for developers
- Develop a streamlined track as part of a two-track process
- Expand the Day Road Design Overlay District to the full Coffee Creek Master Plan Area, with refinements



## Day Road Design Overlay Refinement

Recommendations from our evaluation

- The Day Road Design Overlay District does not ensure quality of development inside large sites
- Does not address public realm
- Does not address connections and multiple modes of travel
- Requires building massing, architectural design and construction types which may not be feasible

## Day Road Design Overlay Refinement

### Proposed fixes

- Encourage, but don't require, three story buildings
- Encourage, but don't require, design and construction types which may not be feasible
- Address the gaps—develop regulations that apply to the district, the site and connections (not just to the building)
- Address quality of design through clear and objective standards that are easy to administer and understand

## Day Road Design Overlay Refinement

- Clear and objective standards that provide certainty;
- Targeted set of specific adjustments to the standards that offer flexibility and can be administered by staff, and
- A Pattern Book that details alternative approaches that may be used instead of the standards.

## Better support for the Day Road Design Overlay District

- Clarify roles of the Development Review Board and Staff
- Shift in level of authority to Staff for the streamlined review track (Track oOne)
- Continued authority for Development Review Board Track Two

## Technical Advisory Committee

*Feedback received 19 February 2014*

- Form-based Code, Pattern Book, and review process should respond to market forces;
- The goal must be to get to “yes” as quickly as possible;
- Give staff the authority and flexibility to make decisions, and
- Allow the principal building entrance to be where it needs to be (the Day Road Design Overlay District got it wrong).

## Next Steps

Next tasks are set up to address specific issues:

**"Road Test"** - Walk through the different options for applicants and approval to ensure that applicants achieve streamlining and staff and DRB can administer proposed amendments

**Urban form testing** - 3D model the development standards and design guidelines to ensure the City achieves desired urban form and public realm design

**Code graphics testing** - Test illustrations that provide clear and objective standards and can be incorporated into Chapter 4 and City's code publishing process

## Expected Results

- Support economic development and job creation;
- A streamlined process and new industrial projects;
- A multi-modal transportation network that accommodates pedestrian, bicycles, transit, motorists, and freight;
- A complete network of existing and new streets, paths, and trails;
- Industrial employment district with high-quality site, landscape, and building design, and
- A district with a sense of place and identity.



## Current Schedule

- 09 July 2014: Planning Commission Work Session
- 21 July 2014: City Council Meeting
- Summer 2014: Web page design and public input
- Summer 2014: Neighborhood meeting
- Summer 2014: Technical Advisory Committee meeting
- July – November: Final draft Form-based Code and Pattern Book

## The Two Tracks

### Track One

Your project complies with all Development Standards, or

Your project complies with all Development Standards and all needed Adjustment Allowances

Your project is approved by staff

### Track Two

Your project does not comply with any Development Standards, or

Your project complies with some Development Standards

Your project must comply with some or all Design Guidelines in the Pattern Book

Staff prepares a recommendation for applicable Design Guidelines

Your project is approved by the Development Review Board

## The Regulatory System

Four components to the regulatory system:

- Development Standards
  - Adjustment Allowance
  - Intent Statement
  - Design Guideline
- } FORM-BASED CODE
- } PATTERN BOOK

## Planning Commission

*Feedback received 19 February 2014*

- Support services that foster a vibrant work environment;
- Integrate new regulatory components into current Code;
- Incorporate two track system into current administrative procedures;
- Incentives are not appropriate for the FBC, and
- High-quality design, nothing left “undesigned.”

## **Planning Commission**

*Feedback received 19 February 2014*

- **Acknowledge need for citizen input into the process;**
- **Connectivity;**
- **Pedestrian and bicycle mobility, and**
- **The design of the public realm.**

---

## **Discussion**

**Thank You**

**CITY COUNCIL WORKSESSION  
STAFF REPORT**

<b>Meeting Date:</b> July 21, 2014	<b>Subject:</b> I-5 Right-of-Way Landscaping at the Terrene at the Grove  <b>Staff Member:</b> Chris Neamtzu, Planning Director <b>Department:</b> Community Development	
<b>Action Required</b>	<b>Advisory Board/Commission Recommendation</b>	
<input type="checkbox"/> Motion <input type="checkbox"/> Public Hearing Date: <input type="checkbox"/> Ordinance 1 <sup>st</sup> Reading Date: <input type="checkbox"/> Ordinance 2 <sup>nd</sup> Reading Date: <input type="checkbox"/> Resolution <input type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input checked="" type="checkbox"/> Council Direction <input type="checkbox"/> Consent Agenda	<input type="checkbox"/> Approval <input type="checkbox"/> Denial <input type="checkbox"/> None Forwarded <input checked="" type="checkbox"/> Not Applicable  <b>Comments:</b>	
<b>Staff Recommendation:</b> Discuss the proposal and provide staff with direction on the next steps regarding project implementation.		
<b>Recommended Language for Motion:</b> N/A		
<b>PROJECT / ISSUE RELATES TO:</b>		
<input checked="" type="checkbox"/> Council Goals/Priorities Environmental stewardship; Clear vision and community design; Safe, healthy and aesthetically pleasing community.	<input type="checkbox"/> Adopted Master Plan(s)	<input type="checkbox"/> Not Applicable

**ISSUE BEFORE COUNCIL:**

Staff is seeking Council direction whether to pursue a tree planting project along the I-5 corridor adjacent to the Terrene and Grove apartments.

### **EXECUTIVE SUMMARY:**

At a recent City Council work session, Mayor Knapp requested that city staff investigate the feasibility of installing evergreen trees in the ODOT right-of-way (ROW) along the private sound wall adjacent to the Terrene at the Grove apartments (Attachment A) immediately south of Boeckman Road to reduce the scale and soften the visual impact of the wall on the passing I-5 traffic. This work session provides the Council with the opportunity to discuss the project and determine if the project is worth pursuing.

Installation of a sound wall was included by Holland Partner Group (HPG) along I-5 with the construction of the Jory Trail and the Terrene apartment communities. This wall was installed on private property for the benefit of the residents to mitigate the sound impacts of I-5 traffic.

At the south end of the project, large mature native forest remnants provide screening and buffering to the sound wall. At the north end, the forest begins to thin with several sections of the wall being highly visible to the motorists on I-5. Recently, the wall has become the canvas for vandals who have been painting graffiti on the I-5 side of the wall. The plantings will serve multiple purposes; to enhance the aesthetics of the edge of the project; to reduce the scale of the wall; to soften visual impacts of the wall on the community; and long term as the trees mature, to provide additional screening and noise and potential air quality mitigation to residents. The purpose of the project would be to fill in the gaps in the forest creating a green edge to the sound wall along I-5.

As is evidenced by the photographs contained in Attachment A, a number of European white birch trees have died presenting a potential hazard. In different parts of the community, there has been a recent outbreak of bronze birch borer insects killing birch trees. The loss of these trees has further opened up the visibility of sections of the wall.

In a recent meeting, ODOT, HPG, OTAK (landscape architect) and city staff discussed the project specifics. The group collaborated and showed a willingness amongst the meeting participants to implement the project. In the meeting, ODOT outlined their permit requirements to conduct planting in the I-5 ROW. They provided specifics regarding permit issuance and expectations for maintenance. ODOT agreed to schedule the removal of the dead and dying birch trees as part of their normal maintenance routine. HPG agreed to have their consultant, OTAK, design a planting plan (Attachment B) with evergreen trees and Boston ivy along the sound wall and provided cost estimates for installation and maintenance (Attachment C). If the City Council decides to pursue the project, the city would pay for purchase and installation of the plant material and would be expected to perform maintenance of the newly planted areas, potentially in partnership with HPG who has expressed a willingness to assist in covering maintenance costs for several years until the plants are established. Additional discussion with their equity partners is needed prior to finalization of commitments.

### **EXPECTED RESULTS:**

Installation of evergreen plantings along the I-5 sound wall would result in an enhanced visual environment benefiting the traveling public and community at large.

### **TIMELINE:**

If Council directs staff to proceed with project implementation, staff would obtain the

appropriate permits from ODOT and once funds were available via the budget process, target a planting date. A contractor would be used to install the plant material and temporary irrigation due to safety concerns of working with volunteers along I-5. The trees would require maintenance in the form of weeding and spraying the mulched area around the trees three times a year.

**CURRENT YEAR BUDGET IMPACTS:**

HPG had TruGreen Land Care prepare a project proposal that can be found in Attachment C.

- The cost of plant material and installation is \$18,840
- The cost of installation of temporary irrigation is \$4,314
- The cost of maintenance is \$3,555/annually (3X \$1,185)

Water to the temporary irrigation system would be provided by HPG via their existing irrigation system.

**FINANCIAL REVIEW / COMMENTS:**

Reviewed by: \_\_\_\_\_ Date: \_\_\_\_\_

**LEGAL REVIEW / COMMENT:**

Reviewed by: MEK \_\_\_\_\_ Date: July 10, 2014 \_\_\_\_\_

Upon council the providing consensus direction, legal is prepared to assist Planning with the necessary documents for City Manager approval as the sums are within the Manager's authority.

**COMMUNITY INVOLVEMENT PROCESS:**

The adjacent development has been through a public review process before both the Development Review Board and the City Council. No additional community involvement is anticipated or needed regarding the tree planting project.

**POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY** (businesses, neighborhoods, protected and other groups):

The benefits to the community include an enhanced I-5 frontage and diminished sound wall appearance. Long-term, large evergreen trees will screen the tops of the buildings providing a benefit to the residents of the Terrene at the Grove. There are no business signs that would be negatively impacted by the plantings.

**ALTERNATIVES:** The Council can amend the proposal, or do nothing.

**CITY MANAGER COMMENT:**

**ATTACHMENTS**

- A. Photos of the sound wall from the Boeckman Road bridge over I-5
- B. Planting plan, installation and maintenance costs prepared by OTAK
- C. TruGreen installation and maintenance cost proposal

Attachment A



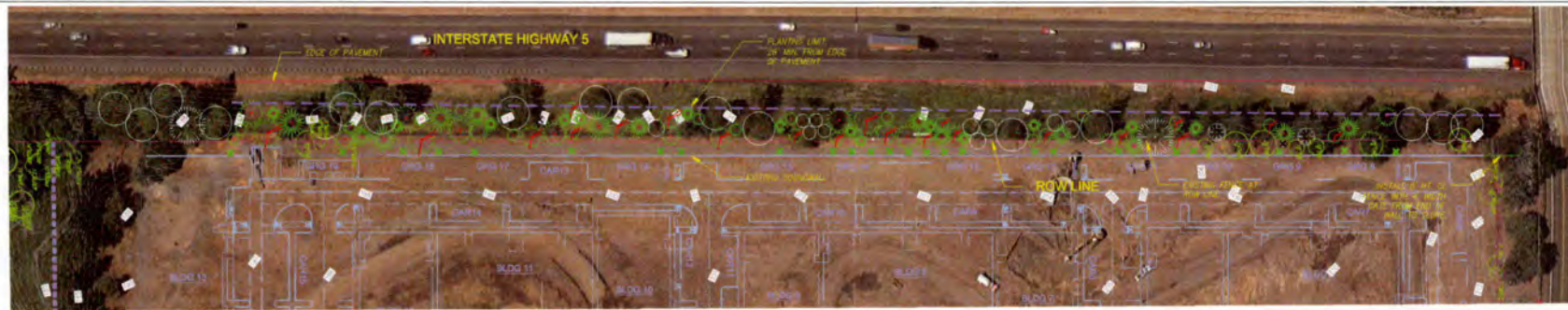










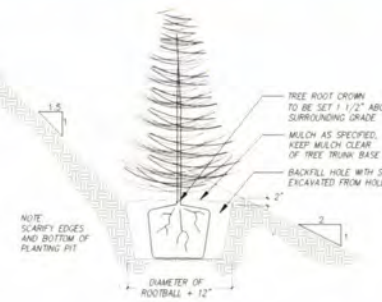


7/16/2014  
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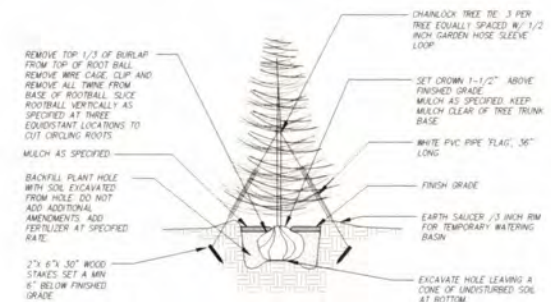
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25	10' x 10' x 10'

REGISTERED  
 263  
 LANDSCAPE ARCHITECT

**HOLLAND PARTNER GROUP**  
 1111 Main St #500  
 Vancouver, WA 98660  
 Phone: 360.649.7886



**TREE/SHRUB PLANTING ON SLOPE** 2  
 L.I.O.



**EVERGREEN TREE PLANTING** 1  
 L.I.O.



**UTILITY NOTE:**

CONTRACTOR IS CAUTIONED THAT EXISTING UNDERGROUND FACILITIES OCCUR THROUGHOUT THE WORK AREAS INCLUDING BUT NOT LIMITED TO POWER, GAS, TELEPHONE, WATER SUPPLY, AND IRRIGATION. CONTRACTOR SHALL MARK THE FACILITIES BEFORE WORK, POTHOLES WHERE NECESSARY, AND PROTECT DURING CONSTRUCTION. IMMEDIATELY NOTIFY OWNER'S REPRESENTATIVE IF ANY CONFLICTS ARE FOUND.

**LANDSCAPE NOTES:**

- GENERAL: SEVERAL OF THE FOLLOWING NOTES SUMMARIZE THE PROJECT SPECIFICATIONS FOR THE CONTRACTOR'S CONVENIENCE. IF A DISCREPANCY EXISTS BETWEEN THESE NOTES AND THE EXISTING CONDITIONS THE OWNERS REPRESENTATIVE IS TO BE NOTIFIED IMMEDIATELY.
- THE LANDSCAPE CONTRACTOR IS TO THOROUGHLY REVIEW THE SITE. IF THERE ARE ANY DISCREPANCIES BETWEEN THE PLAN AND THE EXISTING CONDITIONS THE OWNERS REPRESENTATIVE IS TO BE NOTIFIED IMMEDIATELY.
- IF THE LANDSCAPE CONTRACTOR STARTS WORK BEFORE SITE CONDITIONS ARE READY OR CONTINUES WORK IN ADVERSE CONDITIONS WITHOUT PRIOR APPROVAL THEY WILL BE RESPONSIBLE FOR ANY ADDITIONAL COSTS RELATING TO THE CONDITION.
- IMMEDIATELY NOTIFY OWNERS REPRESENTATIVE CONCERNING ANY CONDITION AT ANY TIME DURING CONSTRUCTION THAT IS DESTRUCTIVE TO THE HEALTH AND VIGOROUS GROWTH OF THE SPECIFIED PLANT MATERIAL.
- PROVIDE QUANTITY OF PLANT MATERIAL INDICATED IN PLANT LIST OR THE QUANTITY REQUIRED TO COVER AREAS INDICATED AT SPECIFIED SPACING, WHICHEVER IS GREATER.
- IF AN AREA DIFFERS SIGNIFICANTLY IN SIZE FROM THAT SCALED ON DRAWING AND REQUIRES MORE OR LESS MATERIAL THE OWNERS REPRESENTATIVE IS TO BE NOTIFIED.
- SOIL ANALYSIS: CONTRACTOR SHALL OBTAIN A COPY OF OWNER- PROVIDED SOIL TEST AND AMEND AND FERTILIZE IN CONFORMANCE WITH RECOMMENDATIONS INDICATED IN THE REPORT.
- FERTILIZER: APPLY FERTILIZER TO ALL PLANT HOLES AND TURF AREAS OF THE TYPE, QUANTITY, APPLICATION METHOD, AND TIMING NOTED IN THE SPECIFICATIONS.
- BARK MULCH: SPREAD 3 INCH DEPTH FINE-MEDIUM GRADE FIRMWOOD BARK IN 6\"/>

**TREE AND VINE LEGEND**

- 12 4'-6\"/>

- EXISTING TREES**
- EXISTING DECIDUOUS TREE NOT ON SURVEY
  - EXISTING CONIFER TREE NOT ON SURVEY
  - ✗ EXISTING SNAG NOT ON SURVEY REMOVE
  - EXISTING CONIFER TREE ON SURVEY
  - EXISTING DECIDUOUS TREE ON SURVEY
- OTHER**
- INSTALL 8\"/>

**PROPOSED SOUND WALL LANDSCAPING**  
**BRENCHLEY ESTATES PARTNERS PHASE II**  
**LOT 1 SITE DEVELOPMENT**  
 SOUND WALL TREE PLAN

**otak**  
 Home/Global Partner  
 4018 3rd Ave. Ste. 330  
 Portland, OR 97224  
 Phone: 503.527.4822  
 Fax: 503.413.2394  
 www.otak.com

16339 16339-0101-01  
 Project No. Drawing No.  
**L1.0**  
 Sheet No.  
 © 2012 Otak, Inc.

Project: 16339-0101-01 Date: 07/14/2014 11:11 AM

## PROPOSAL

Property: <b>Jory Trail</b>	Regarding: <b>Sound Wall Landscape</b>	
Submitted To: <b>Ben Hawkins</b>	Submitted By: <b>Shawn Tripp</b>	Date <b>7/2/14</b>

**Attachment C**

Project Summary:

Install landscape per plans provided by Holland Residential via email on 6/20/14. The proposal is based off of the landscape plans L1.0 dated July, 3 2014. This price is based on private wages

Project Specifications:

- Plant the following trees:
  - 12 pcs 4'-6' California Incense Cedar
  - 12 pcs 8'-10' California Incense Cedar
  - 6 pcs 12' California Incense Cedar
  - 12 pcs 4'-6' Leyland Cypress
  - 6 pcs 8'-10' Leyland Cypress
  - 3 pcs 12' Leyland Cypress
  - 14 pcs 4'-6' Doug Fir
  - 9 pcs 8'-10' Doug Fir
  - 4 pcs 12' Doug Fir
- Install drip irrigation for establishment

**Total \$23,154.00**

- Maintain the area after installation. Recommended 3 times a year @ \$1,185.00 per occurrence.

We propose hereby to furnish materials and labor – complete in accordance with above specifications, for the sum of:

**See Above**

Payment to be made as follows: **In Full Upon Completion**

All material is guaranteed to be as specified. All work to be completed in a workmanlike manner according to standard practices. Any alteration or deviation from above specifications involving extra costs will be executed only upon written orders, and will become an extra charge over and above the estimate. All agreements contingent upon strikes, accidents or delays beyond our control. Owner to carry fire, flood and other necessary insurance. Our workers are fully covered by Workman's Compensation Insurance.

TruGreen LandCare Representative

AUTHORIZED SIGNATURE

NOTE: This proposal may be withdrawn if not accepted within 30 days.

**ACCEPTANCE OF PROPOSAL**

The above prices, specifications and conditions are satisfactory and are hereby accepted.  
You are authorized to do the work as specified. Payment will be made as outlined above.

Signature: \_\_\_\_\_

Date Accepted: \_\_\_\_\_

City of



**WILSONVILLE**  
in OREGON

**CITY COUNCIL WORK SESSION  
STAFF REPORT**

<b>Meeting Date:</b> July 21, 2014		<b>Subject:</b> Charbonneau Consolidated Improvement Plan (Resolution No. 2481)	
		<b>Staff Member:</b> Nancy Kraushaar, PE, Community Development Director & Zachary Weigel, PE, Civil Engineer	
		<b>Department:</b> Community Development	
<b>Action Required</b>		<b>Advisory Board/Commission Recommendation</b>	
<input type="checkbox"/> Motion <input type="checkbox"/> Public Hearing Date: <input type="checkbox"/> Ordinance 1 <sup>st</sup> Reading Date: <input type="checkbox"/> Ordinance 2 <sup>nd</sup> Reading Date: <input type="checkbox"/> Resolution <input checked="" type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input type="checkbox"/> Council Direction <input type="checkbox"/> Consent Agenda		<input type="checkbox"/> Approval <input type="checkbox"/> Denial <input type="checkbox"/> None Forwarded <input checked="" type="checkbox"/> Not Applicable	
		<b>Comments:</b>	
<b>Staff Recommendation:</b>			
The City Council discusses the Charbonneau Consolidated Improvement Plan (Plan) that was introduced at their July 7, 2014 work session. Council will also preview the draft resolution to acknowledge the Plan and include the recommended projects in the existing Stormwater and Water utility master plans, the Transportation System Plan, and the Pavement Management Program.			
<b>Recommended Language for Motion:</b>			
N/A			
<b>PROJECT / ISSUE RELATES TO:</b> <i>[Identify which goal(s), master plans(s) issue relates to.]</i>			
<input checked="" type="checkbox"/> Council Goals/Priorities Goal 6 – Well Maintained Infrastructure	<input type="checkbox"/> Adopted Master Plan(s)	<input type="checkbox"/> Not Applicable	

**ISSUE BEFORE COUNCIL:**

Continue City Council review and discussion of the Charbonneau Consolidated Improvement Plan (Plan) findings and project list. Introduce draft resolution to add the Plan Spot Repair and



Complete Project Repair lists to the City's applicable adopted master plans and pavement management program.

**EXECUTIVE SUMMARY:**

The Charbonneau District was developed in the 1970s and 1980s as one of the first master planned communities in Oregon. While much of the infrastructure has been in service for 30 to 40 years, the infrastructure is wearing out and is in need of repair or replacement. This is a shorter service life than would be expected using Wilsonville's current and more robust design and construction standards. Due to the number of utility deficiencies in Charbonneau, individual utility repairs are not practical. A comprehensive analysis of the Charbonneau District was needed to develop an efficient and economically viable asset repair and replacement program.

The Charbonneau Consolidated Improvement Plan identifies a 20-year infrastructure repair program that provides three key functions.

1. Clearly identifies and prioritizes the infrastructure deficiencies within the District.
2. Devises an efficient infrastructure repair and replacement plan aimed at reducing costs to ratepayers and impacts to residents.
3. Provides infrastructure repair costs to guide future analysis of utility rates and fees.

The Plan includes 15 spot utility repair and 38 large infrastructure repair projects. These projects have been prioritized based on the length and severity of utility deficiencies within each project area. As a result, the highest priority repairs are completed first. The Plan also includes short-term and long-term proposals for the French Prairie Drive pathway.

Planning level cost estimates for the projects were developed to use in funding analysis and future inclusion in the City's Capital Improvement Program and annual budgets. An investment of approximately \$46.2 million is needed to correct deficiencies identified in the Plan.

During the July 21, 2014 work session Staff is seeking Council discussion, comments, and questions about the Plan in preparation for adopting the list of projects that can be used in future rate and fee studies, the City-wide Capital Improvement Program, and future budgets. The stormwater needs identified in the Plan will be considered and incorporated in the Stormwater Rate Study that is underway.

**EXPECTED RESULTS:**

The Plan lays out a program for maintaining reliable infrastructure services in Charbonneau. After work sessions schedule in July, the City Council will be asked to acknowledge the Charbonneau Consolidated Improvement Plan and adopt the project list by resolution. This action will allow for the identified projects to be added to each of the associated utility master plans.

**TIMELINE:**

The Plan's project list is expected to be adopted by City Council in August or September 2014.

Inclusion of the Charbonneau District stormwater repair projects in the Stormwater Master Plan

is needed to guide the upcoming Stormwater Utility Fee update. The stormwater fee update is scheduled for City Council update and consideration in August or September 2014.

The repair projects for sewer will be included in the ongoing Sanitary Sewer Collection System Master Plan update (adoption scheduled for late 2014). Sewer utility rates will be reviewed upon master plan completion.

**CURRENT YEAR BUDGET IMPACTS:**

The adopted 2013-14 Wilsonville Budget includes \$24,500 for the Charbonneau Consolidated Improvement Plan. The document was created using in-house resources for which the budget has been sufficient. Funding to implement the Plan was not included in the adopted 2014-15 budget because the Plan is just now being considered by the City Council.

Please note that the Plan is primarily a technical document identifying infrastructure improvement needs. It is not intended to identify funding sources or future budget impacts, but is meant to lay the groundwork for future analysis in determining utility rates or other funding mechanisms. As a result, there are no current fiscal year budget impacts resulting from the acceptance of the Charbonneau Consolidated Improvement Plan.

**FINANCIAL REVIEW / COMMENTS:** *[Item must be sent to Finance for review.]*

Reviewed by: \_\_\_\_\_ Date: \_\_\_\_\_

**LEGAL REVIEW / COMMENT:** *[Item must be sent to City Attorney for review.]*

Reviewed by: \_\_\_\_\_ Date: \_\_\_\_\_

**COMMUNITY INVOLVEMENT PROCESS:**

Staff met with two Charbonneau representatives on June 23, 2014. A meeting with Charbonneau residents is scheduled for July 30, 2014. Council input is requested regarding other desired public outreach.

**POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY:**

Acceptance of the Charbonneau Consolidated Improvement Plan provides a blueprint for an asset repair and replacement program for use in providing quality urban utility service in Wilsonville. Inclusion of the recommended infrastructure improvement projects into the utility master plans will guide scheduling of construction projects as part of the City's Capital Improvement Program. The Charbonneau District will experience an increase in construction activity as the infrastructure improvement projects are built. However, the Plan has prioritized the projects in such a way as to reduce construction impacts to Charbonneau residents as much as possible.

The Wilsonville community will benefit from the Plan by implementing an infrastructure rehabilitation program that can be expected to remain in good working condition for the next 75+ years.

**ALTERNATIVES:**

City staff considered a number of alternatives regarding what utilities and associated deficiencies

should be included within the 20-year time frame of the Charbonneau Consolidated Improvement Plan. The Plan represents the improvements necessary to maintain the Charbonneau District infrastructure in good working condition. As an affordable funding strategy is developed for the plan, staff will present alternative (longer) time frames for Council consideration.

**CITY MANAGER COMMENT:**

**ATTACHMENTS**

DRAFT Resolution No. 2481

(Please note that the Charbonneau Consolidated Plan is considered an attachment to the DRAFT Resolution, and was provided to the Council at the July 7 work session, and has not been reprinted for the Council packets.)

**DRAFT RESOLUTION NO. 2481**

**A RESOLUTION OF THE CITY OF WILSONVILLE ACKNOWLEDGING THE FINDINGS OF THE CHARBONNEAU CONSOLIDATED IMPROVEMENT PLAN (PLAN) AND AMENDING THE CAPITAL IMPROVEMENT PROJECT LISTS OF THE 2012 STORMWATER MASTER PLAN, 2012 WATER SYSTEM MASTER PLAN, 2013 TRANSPORTATION SYSTEM PLAN, AND 2013 PAVEMENT MANAGEMENT PROGRAM BY INCLUDING THE SPOT REPAIR AND COMPLETE REPAIR PROJECTS DOCUMENTED IN THE PLAN.**

WHEREAS, the Charbonneau District was developed in the 1970s and 1980s; and

WHEREAS, a significant portion of Charbonneau District infrastructure has been in service for more than 30 to 40 years which approaches or exceeds the service life of many of the materials and methods used for their construction at that time;

WHEREAS, the infrastructure conditions have been comprehensively inventoried through inspections and reviewing the adopted City of Wilsonville Stormwater and Water Master Plans and Transportation System Plan and the Pavement Management Program; and

WHEREAS, the inventory has confirmed that deterioration has occurred and structural and capacity deficiencies are present throughout the aging infrastructure; and

WHEREAS, the level of infrastructure deterioration make utility improvements on an individual basis impractical and created the need for the Charbonneau Consolidated Improvement Plan (Plan), attached and included as reference herein, that provides a strategic plan to repair and replace the utilities and retain reliable public works services; and

WHEREAS, the Plan provides a clear understanding of the infrastructure repair and replacement program to be implemented over the next 20 years; and

WHEREAS, the Plan provides for utility improvements to be completed efficiently at the least cost and impact to residents; and

WHEREAS, the Plan identifies utility improvement costs to guide future analysis of utility rates and fees; and

WHEREAS, the Plan concludes that an investment of approximately \$46.2 million is needed to improve the deficient infrastructure in the Charbonneau District; and

WHEREAS, the City currently has a Stormwater Master Plan that was adopted by City Council (Ordinance No. 700) on February 6, 2012 that includes a Capital Improvement Program; and

WHEREAS, the City currently has a Water System Master Plan that was adopted by City Council (Ordinance No. 707) on September 7, 2012 that includes a Capital Improvement Plan; and

WHEREAS, the City currently has a Transportation System Plan that was adopted by City Council (Ordinance No. 718 on June 17, 2013) that includes a Higher Priority Projects List.

WHEREAS, the Community Development Department implements a Pavement Management Program to evaluate and track future pavement rehabilitation needs; and

WHEREAS, the existing Stormwater Capital Improvement Program, the Water Capital Improvement Plan, and the Pavement Management Program need to be amended to include the projects identified in the Plan, specifically those documented in Table 4 – Charbonneau Project Cost Summary, High Priority Spot Repair Scenario (see Exhibit A); and

WHEREAS, the existing Transportation System Plan Higher Priority Projects List needs to be amended to include the short term and long term projects identified in the Plan for the French Prairie Pathway; and

WHEREAS, such amendments may result in replacement or refinement of similar projects that are already included in those existing master plans or programs.

NOW, THEREFORE, THE CITY OF WILSONVILLE RESOLVES AS FOLLOWS:

1. The City Council incorporates herein the above recitals and acknowledges the findings and conclusions of the Charbonneau Consolidated Improvement Plan.
2. The City Council finds and concludes that the Spot Repair and associated infrastructure repair projects are necessary to help protect the public health, safety and welfare of the Charbonneau community by extending the reliability and serviceability of the infrastructure.
3. The Capital Improvement Program included in the City's current Stormwater Master Plan is hereby amended to include the stormwater improvement projects identified in the Plan.

4. Capital Improvement Plan included in the City's current Water System Master Plan is hereby amended to include the water system improvement projects identified in the Plan
5. The Capital Improvement Plan included in the City's current Transportation System Plan is hereby amended to include the short term repair and long term replacement projects for the French Prairie Drive Pathway identified in the Plan.
6. The identified street system improvement projects are hereby added to the City's Pavement Management Program.
7. This resolution is effective upon adoption.

ADOPTED by the Wilsonville City Council at a regular meeting thereof this XX day of XXX, 2014, and filed with the Wilsonville City Recorder this date.

\_\_\_\_\_  
TIM KNAPP, MAYOR

ATTEST:

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder

**SUMMARY OF VOTES:**

Mayor Knapp \_\_\_\_\_  
Council President Starr \_\_\_\_\_  
Councilor Goddard \_\_\_\_\_  
Councilor Fitzgerald \_\_\_\_\_  
Councilor Stevens \_\_\_\_\_

Table 4 - Charbonneau Project Cost Summary  
 High Priority Spot Repair Scenario  
 June 20, 2014

Exhibit A to Resolution  
 No. 2481

**Spot Repair Projects**

CIP	Project Rank	Project Location	Project Costs		
			Sewer	Storm	Total
YEAR 0 - 5	SR-1	8000 Block of Fairway Drive	\$87,000	\$174,000	\$261,000
	SR-2	Estates Post Road	\$70,000	\$112,000	\$182,000
	SR-3	Mollala Bend SE	\$43,000	\$56,000	\$99,000
	SR-4	French Prairie Dr. Near Del Monte Dr.	\$242,000	-	\$242,000
	SR-5	7300 & 7800 Block of Fairway Drive	-	\$300,000	\$300,000
	SR-6	Armitage Road - South	-	\$173,000	\$173,000
	SR-7	Middle Greens Road	-	\$190,000	\$190,000
	SR-8	Country View Loop	-	\$157,000	\$157,000
	SR-9	Boones Bend Road	-	\$158,000	\$158,000
	SR-10	Arbor Lake Drive	-	\$314,000	\$314,000
	SR-11	Armitage Road - North	-	\$213,000	\$213,000
	SR-12	Lake Drive	-	\$107,000	\$107,000
	SR-13	Country View Lane	-	\$226,000	\$226,000
	SR-14	Juliette Drive	-	\$224,000	\$224,000
	SR-15	Louvonne Drive	-	\$105,000	\$105,000
Spot Repair Project Total			\$442,000	\$2,509,000	\$2,951,000

**Complete Repair Projects (Re-ranked based on addition of Spot Repair projects)**

CIP	Project Rank	Project Name	Project Costs				Total
			Sewer	Storm	Streets	Water	
YEAR 0 - 5	1	French Prairie Drive Phase II	\$491,000	\$1,319,000	\$670,000	-	\$2,480,000
	2	Old Farm Road Phase I	\$342,000	\$900,000	\$448,000	\$191,000	\$1,881,000
	3	Village Greens Circle	\$243,000	\$662,000	\$323,000	-	\$1,228,000
	4	Edgewater Lane	\$551,000	\$785,000	\$376,000	\$81,000	\$1,793,000
	5	French Prairie Drive Phase III	\$182,000	\$1,148,000	\$462,000	\$313,000	\$2,105,000
Year 0-5 Total (Includes SR Projects)			\$2,251,000	\$7,323,000	\$2,279,000	\$585,000	\$12,438,000
YEAR 6 - 10	6	Boones Bend Road Phase II	\$399,000	\$621,000	\$375,000	\$515,000	\$1,910,000
	7	Mollala Bend Road	\$67,000	\$338,000	\$227,000	\$442,000	\$1,074,000
	8	Country View Loop	\$179,000	\$541,000	\$260,000	\$32,000	\$1,012,000
	9	Country View Lane Phase II	\$145,000	\$474,000	\$195,000	\$33,000	\$847,000
	10	French Prairie Drive Phase V	\$138,000	\$294,000	\$146,000	\$87,000	\$665,000
	11	French Prairie Drive Phase IV	-	\$641,000	\$185,000	\$62,000	\$888,000
	12	Sacajawea Lane	\$249,000	\$306,000	\$355,000	\$452,000	\$1,362,000
	13	French Prairie Drive Phase I	\$98,000	\$970,000	\$785,000	\$548,000	\$2,401,000
Year 6 - 10 Total			\$1,275,000	\$4,185,000	\$2,528,000	\$2,171,000	\$10,159,000

**Complete Repair Projects Continued**

CIP	Project Rank	Project Name	Project Costs				
			Sewer	Storm	Streets	Water	Total
YEAR 11 - 15	14	Old Farm Road Phase II	\$119,000	\$737,000	\$304,000	\$18,000	\$1,178,000
	15	Lafayette Way	-	\$271,000	\$196,000	-	\$467,000
	16	Curry Drive	\$171,000	\$381,000	\$73,000	-	\$625,000
	17	Arbor Lake Drive Phase I	\$342,000	\$180,000	\$364,000	\$481,000	\$1,367,000
	18	East Lake Court	\$384,000	\$718,000	\$367,000	\$394,000	\$1,863,000
	19	Armitage Road Phase I	\$227,000	\$207,000	\$400,000	\$292,000	\$1,126,000
	20	Arbor Lake Drive Phase II	\$414,000	\$171,000	\$301,000	\$556,000	\$1,442,000
	21	Country View Lane Phase I	\$144,000	\$177,000	\$165,000	-	\$486,000
	22	Lake Drive	\$118,000	\$287,000	\$134,000	-	\$539,000
	23	Illahee Drive	\$418,000	-	\$52,000	\$289,000	\$759,000
24	Middle Greens Road	\$121,000	\$230,000	\$318,000	\$362,000	\$1,031,000	
Year 11 -15 Total			\$2,458,000	\$3,359,000	\$2,674,000	\$2,392,000	\$10,883,000
YEAR 16 - 20	25	Boones Bend Road Phase I	\$215,000	\$640,000	\$370,000	\$483,000	\$1,708,000
	26	Fairway Drive Phase I	\$73,000	\$178,000	\$414,000	\$550,000	\$1,215,000
	27	Fairway Drive Phase II	\$116,000	\$638,000	\$175,000	-	\$929,000
	28	Armitage Road Phase II	\$70,000	-	\$369,000	\$355,000	\$794,000
	29	Lake Bluff Court	-	\$419,000	\$206,000	\$355,000	\$980,000
	30	Del Monte Drive	-	-	\$50,000	\$228,000	\$278,000
	31	Lakeside Loop & Village Green Court	\$167,000	\$525,000	\$172,000	\$34,000	\$898,000
	32	French Prairie Drive Phase VI	\$73,000	\$884,000	\$320,000	-	\$1,277,000
	33	Arbor Lake Drive Phase III	\$121,000	\$545,000	\$134,000	-	\$800,000
	34	Estates Post Road	-	\$51,000	\$247,000	\$307,000	\$605,000
35	Charbonneau Storm Improve. Phase I	-	\$307,000	-	-	\$307,000	
36	Charbonneau Storm Improve. Phase II	-	\$529,000	-	-	\$529,000	
37	Mariners Drive Water Improvements	-	-	\$89,000	\$486,000	\$575,000	
38	Louvolle & Juliette Street	-	-	\$32,000	-	\$32,000	
Year 16 - 20 Total			\$835,000	\$4,716,000	\$2,578,000	\$2,798,000	\$10,927,000
Complete Repair Project Total			\$6,377,000	\$17,074,000	\$10,059,000	\$7,946,000	\$41,456,000
20 Year Total			\$6,819,000	\$19,583,000	\$10,059,000	\$7,946,000	\$44,407,000



## CITY COUNCIL MEETING STAFF REPORT

<b>Meeting Date:</b> July 21, 2014		<b>Subject:</b> Frog Pond Area Plan	
		<b>Staff Member:</b> Katie Mangle <b>Department:</b> Community Development	
<b>Action Required</b>		<b>Advisory Board/Commission Recommendation</b>	
<input type="checkbox"/> Motion <input type="checkbox"/> Public Hearing Date: <input type="checkbox"/> Ordinance 1 <sup>st</sup> Reading Date: <input type="checkbox"/> Ordinance 2 <sup>nd</sup> Reading Date: <input type="checkbox"/> Resolution <input checked="" type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input type="checkbox"/> Council Direction <input type="checkbox"/> Consent Agenda		<input type="checkbox"/> Approval <input type="checkbox"/> Denial <input type="checkbox"/> None Forwarded <input checked="" type="checkbox"/> Not Applicable  <b>Comments:</b> Planning Commission reviewed this material at its July 9 <sup>th</sup> 2014 meeting.	
<b>Staff Recommendation:</b> Provide feedback on the draft Vision Statement and Guiding Principles for the Frog Pond Area Plan.			
<b>Recommended Language for Motion:</b> N/A			
<b>PROJECT / ISSUE RELATES TO:</b>			
<input checked="" type="checkbox"/> Council Goals/Priorities Thoughtful Land Use 5.a. Complete a formal concept plan for Advance Road and Frog Pond Residential Areas.		<input type="checkbox"/> Adopted Master Plan(s)	<input type="checkbox"/> Not Applicable

### ISSUE BEFORE COUNCIL:

The purpose of this Council briefing is to share the draft Vision Statement and Guiding Principles for development in the Frog Pond Area.

### EXECUTIVE SUMMARY:

Attached for Council review and discussion is a draft of the Vision Statement and Guiding Principles for the Frog Pond Area Plan (see Attachment A). The Vision Statement and Guiding Principles were derived from a two-step process: (1) An initial visioning discussion exercise held at the Joint Technical Advisory Committee/Task Force meeting on May 12, 2014; and (2) a review and refinement of a preliminary draft of the Vision and Guiding Principles by the Task Force at their meeting on June 12, 2014.



**LEGAL REVIEW / COMMENT:**

Reviewed by: MEK \_\_\_\_\_ Date: 7/10/2014 \_\_\_\_\_

N/A

**COMMUNITY INVOLVEMENT PROCESS:**

On June 12, 2014, the second meeting of the Task Force included presentations on Market Analysis and Land Use Program by Leland Consulting, and discussion of the Vision Statement and Guiding Principles by Angelo Planning Group. Following the presentations, Task Force members and visitors were given an opportunity for discussion and to provide comments to Planning staff.

Property owners and others in the community will be invited to participate in the planning process. There will be many different ways to engage with the project, including:

- Task Force and Planning Commission meetings
- Two public open house meetings, each with a corresponding online open house
- Individual and small group meetings with project staff
- Online surveys and commenting
- Website information

**POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY**

By leading the planning effort, the City will balance community interests with feasibility of implementation. The project will resolve questions related to design, infrastructure, and land uses that will make it more likely that the Frog Pond area will be ready to construct when the market is ready to support it.

**ALTERNATIVES:**

None at this time.

**CITY MANAGER COMMENT:**

**ATTACHMENT**

- A. Frog Pond Area Plan Vision Statement and Guiding Principles, draft dated July 9, 2014



## A VISION FOR FROG POND IN 2035

The Frog Pond Area in 2035 is a Wilsonville community with attractive and connected neighborhoods. The community's hallmarks are its safe, walkable, and active streets, variety of quality homes, and trails and open spaces. Frog Pond's excellent schools and parks are focal points of the community. Frog Pond is a valued and integral part of the larger city, just a convenient bike, walk, or bus trip to all parts of Wilsonville.

### GUIDING PRINCIPLES FOR THE FROG POND AREA PLAN

#### Create great neighborhoods

Frog Pond's homes, streets, open spaces, neighborhood-scale retail, and other uses fit together into walkable, cohesive, and connected neighborhoods. Frog Pond is a fun place to live.

#### Create a complete streets and trails network

Streets are designed for safe and enjoyable travel by bike, on foot, or by car. A great network of trails is provided. Safe crossings and connections are provided throughout the street and trail network.

#### Provide access to nature

The creeks and natural areas provide opportunities to see and interact with nature close to home.

#### Create community gathering spaces

Beautiful parks, quality schools, and other public spaces serve as community centers and gathering places. The land uses, transportation, and open space around the Advance Road school and park sites support a compatible neighborhood plan in that area. The Frog Pond Grange, and adjacent uses, fit together as a focal point of the community.

#### Provide for Wilsonville's housing needs

A variety of attractive homes are provided to fulfill the City's housing needs and align with the market. Single-family homes are an important part of the mix, and neighborhoods are designed to be multi-generational and offer a diversity of attractive housing options at a variety of prices.

#### Create a feasible implementation strategy

A realistic funding plan for infrastructure, smart and flexible regulations, and other strategies promote successful implementation of the plan.



### Frog Pond is an extension of Wilsonville

Frog Pond is truly connected – it is an easy and safe walk, drive, bike trip, or bus ride to other parts of Wilsonville, and Frog Pond feels like a well-planned extension of the city.

### Retain trees

Mature native trees are integrated into the community to enhance the area's character and value.

### Honor Frog Pond's history

A sense of history is retained, recognized, and celebrated.

### Provide compatible transitions to surrounding areas

New urban land uses are good neighbors to adjacent rural land uses, future developable areas, and existing neighborhoods. The plan provides for future growth of the City into adjacent urban reserves.

### Promote healthy, active lifestyles

Extensive walkways, community gardens, recreational facilities, and other elements support active and healthy lifestyles.

### Integrate sustainability

The plan integrates solutions which address economic, environmental and social needs. Frog Pond is a sustainable community over the long term.

### Coordinate with Wilsonville's transportation network

The plan is consistent with the Wilsonville Transportation System Plan for all modes of travel: trails, bikeways, SMART, and vehicles. Traffic impacts are managed for key streets and intersections, including the I-5 interchanges.

## PROCESS PRINCIPLES

- Provide early and ongoing opportunities for stakeholders to raise issues and concerns.
- Facilitate equitable and constructive communication between the public and project team.
- Empower residents to become involved with the project.
- Provide the public with balanced and objective information to help the public understand issues, alternatives, opportunities, and solutions.
- Aim to create the best product, a model that could be used in other communities.

**CITY COUNCIL WORKSESSION  
STAFF REPORT**

<b>Meeting Date:</b> July 21, 2014	<b>Subject:</b> Proposal to amend the Wilsonville Development Code Section 4.022 <u>Appeal and Call-up Procedures</u>  <b>Staff Member:</b> Chris Neamtzu, Planning Director <b>Department:</b> Community Development
<b>Action Required</b> <input type="checkbox"/> Motion <input type="checkbox"/> Public Hearing Date: <input type="checkbox"/> Ordinance 1 <sup>st</sup> Reading Date: <input type="checkbox"/> Ordinance 2 <sup>nd</sup> Reading Date: <input type="checkbox"/> Resolution <input type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input checked="" type="checkbox"/> Council Direction <input type="checkbox"/> Consent Agenda	<b>Advisory Board/Commission Recommendation</b> <input type="checkbox"/> Approval <input type="checkbox"/> Denial <input type="checkbox"/> None Forwarded <input checked="" type="checkbox"/> Not Applicable  <b>Comments:</b> For Work Session Discussion

<b>Staff Recommendation:</b> Direct staff to prepare Ordinance amendment reflecting the proposed modification, as described in this report.
<b>Recommended Language for Motion:</b> N/A

<b>PROJECT / ISSUE RELATES TO:</b> <i>[Identify which goal(s), master plans(s) issue relates to.]</i>		
<input type="checkbox"/> Council Goals/Priorities	<input type="checkbox"/> Adopted Master Plan(s)	<input checked="" type="checkbox"/> Not Applicable

**ISSUE BEFORE COUNCIL:**  
 Whether revising Section 4.022 (.03) of the Wilsonville Municipal Code, pertaining to the City Council call-up period, as outlined below, will sufficiently address Councilor concerns with respect to the time frame allowed for City Council call-up while still meeting statutory time frames and applicant expectations for timely issuance of a final decision.

**EXECUTIVE SUMMARY:**

The foregoing Code Section is proposed to be revised to clarify and nominally extend the duration of time that the City Council has to cause any final action taken by the Development Review Board ("DRB") to be called up for review by the full City Council.

Recently, due to a holiday, a DRB hearing was moved from its normal Monday date to Thursday. As a result of that rare date change, the City Council had less time than normal to consider whether to call-up a DRB decision. This situation caused Council members some concern. Staff was asked to look at the feasibility of giving the City Council more time to call-up a DRB decision while still keeping the City process timely for the applicant and other impacted parties.

Under the Code, the applicant or any impacted party of a DRB decision has 14 calendar days from the date of the posting of the notice of the decision to appeal the decision to the City Council. The time frame for the City Council to call-up the decision on its own, however, is measured from the shorter duration of the date the DRB actually made the decision, as opposed to the date notice of the decision was posted.

To give the City Council more time to consider a call-up, while still keeping the application on track to allow a timely final decision to be made within the statutory 120-day time period, staff is recommending that the Code be amended to give the City Council the same 14 calendar days from the date of the posting of the notice of decision, as opposed to the date the decision was actually rendered, to call it up. This change will make the time frame equal to that afforded to applicants and other impacted parties for appeal. With this change, if there is any delay in the posting of the notice, such delay cannot impact the time frame the City Council has to consider and call-up a DRB decision. It will remain incumbent on staff to post the notice of the decision, from which the call-up time period will be measured, as soon as reasonably possible.

**EXPECTED RESULTS:**

The proposed changes will give City Council more certainty of time during which to determine whether a matter should be called up, while still keeping the current time lines for applicants or other impacted parties as already codified under the current Code language.

**ALTERNATIVES:**

Keep the Code language as currently written.

Propose an alternative extension of time, which is not recommended due to the statutory 120-day requirement for the local jurisdiction to render a final decision.

## PROCLAMATION

### Declaring the Month of August 2014 as “Wilsonville Rotary / End Polio Now Month”



WHEREAS, the Rotary Club of Wilsonville was founded in 1975 and has consistently been a major asset to the community; and

WHEREAS, Wilsonville Rotary conducts several omelet breakfasts each year to raise funds for Wilsonville schools and feed volunteers working on community-improvement projects; and

WHEREAS, Wilsonville Rotary provides \$7,500 in scholarships annually to Wilsonville-area high school graduates; and

WHEREAS, Wilsonville Rotary started and leads a chess program in local schools to help teach students critical-thinking skills and self-discipline; and

WHEREAS, Wilsonville Rotary conducts the “Through A Child’s Eyes Program” at the Coffee Creek Correctional Facility in partnership with the Oregon Department of Corrections to promote healthy parent-child relationships and help break the cycle of criminality; and

WHEREAS, Wilsonville Rotary produces the Wilsonville Rotary Summer Concerts as a benefit to the community, and as a way to promote friendship and interaction between neighbors; and

WHEREAS, the eradication worldwide of the debilitating disease polio is one of the top priorities of Rotary International, and is just three endemic countries away from being reality; and

WHEREAS, Wilsonville Rotary has raised more than \$31,000 in local contributions toward the goal of ending polio since 2007 by hosting a series of free, donation-only community Summer Concerts;

NOW THEREFORE, I, Tim Knapp, as Mayor of the City of Wilsonville hereby do proclaim August 2014 as

“WILSONVILLE ROTARY / END POLIO NOW MONTH”

in the City of Wilsonville and urge my fellow citizens to attend the Wilsonville Rotary Summer Concerts in July and August, and donate as you are able so we together can end polio once and for all.

A handwritten signature in blue ink that reads "Tim Knapp". The signature is written in a cursive, flowing style.

Tim Knapp, Mayor, City of Wilsonville

Dated this 21<sup>st</sup> day of July 2014.



# WHY MORE FUNDING?

Although polio cases are down 99% worldwide from 350,000 cases in 1988 to 223 cases — an all-time low — in 2012, our work is still not finished.

## THE RISK

Lack of funding will decrease immunization rates in polio-affected countries. Until polio is eradicated **everywhere**, no unvaccinated child is safe **anywhere**. If polio rebounds, more than 200,000 children worldwide could be paralyzed each year within a decade.

## THE CHALLENGE

2013-18 Polio Eradication and Endgame Strategic Plan ([polioeradication.org](http://polioeradication.org))

- Estimated cost: us\$5.5 billion
- Funding commitments: us\$4 billion
- Funding gap (if commitments are met): us\$1.5 billion

## THE SOLUTION

Secure support from governments, nongovernmental organizations, corporations, and the public to fully fund the polio endgame plan.

# YOU CAN HELP NOW

This is a massive effort that Rotary and its partners cannot do alone. Only one other human disease, smallpox, has ever been eradicated. With your help, we can make history today by ending polio.

## MAKE A CONTRIBUTION

Your support can protect a child for life.

➔ [rotary.org/contribute](http://rotary.org/contribute)

## HOLD COMMUNITY FUNDRAISING EVENTS

Find tools to spur support.

➔ [endpolionow.org/resources](http://endpolionow.org/resources)

## ASK BUSINESSES TO CONTRIBUTE

Find articles, graphics, and other tools.

➔ [endpolionow.org/resources](http://endpolionow.org/resources)

## BE AN ADVOCATE

Contact elected officials, business leaders, friends, and the media to promote polio eradication.

➔ [endpolionow.org/resources](http://endpolionow.org/resources)

## SHARE YOUR VOICE

Visit [endpolionow.org](http://endpolionow.org) and share this site with the people on your social networks, and ask them to do the same.

➔ [endpolionow.org](http://endpolionow.org)

## FOR ROTARIANS

Your district can contribute from its District Designated Fund (DDF) — the suggested minimum is 20%. Every dollar will be matched 50% by The Rotary Foundation's World Fund.

### Recognition

Districts giving 20% or more in DDF to PolioPlus will receive a special certificate each year they qualify. Districts that qualify from 2013-14 until eradication is achieved will be recognized on a plaque displayed at RI World Headquarters. Contributors will receive Paul Harris Fellow credit.

# END POLIO NOW



944-EN—(713)



BILL & MELINDA GATES foundation

TAKE ACTION: [rotary.org/contribute](http://rotary.org/contribute)

# MAKE YOUR MONEY WORK 3 TIMES AS HARD

Rotary and the Bill & Melinda Gates Foundation are extending their partnership during the critical endgame phase of the **Global Polio Eradication Initiative**. Find out how your donation can **triple** your impact!

## LEARN

→ [ENDPOLIONOW.ORG](http://ENDPOLIONOW.ORG)

## DONATE

→ [ROTARY.ORG/CONTRIBUTE](http://ROTARY.ORG/CONTRIBUTE)



**"ROTARY CONTINUES TO BE THE HEART AND SOUL OF POLIO ERADICATION."**

— Bill Gates, Cochair, Bill & Melinda Gates Foundation

**"FAILURE TO ERADICATE POLIO IS UNFORGIVABLE, FOREVER."**

— Margaret Chan, Director-General, World Health Organization

**"WE'RE WORKING TO ENSURE THAT THE POLIOVIRUS WILL BE FOUND ONLY IN HISTORY BOOKS AND NOT IN CHILDREN."**

— Robert S. Scott, Chair, International PolioPlus Committee, Rotary International

**"WE HAVE THE ABILITY TO PROTECT EVERY LAST PERSON, ESPECIALLY CHILDREN, FROM THIS ENTIRELY PREVENTABLE DISEASE."**

— Anthony Lake, Executive Director, UNICEF

**"POLIO ANYWHERE IN THE WORLD IS A RISK EVERYWHERE IN THE WORLD."**

— Thomas Frieden, Director, U.S. Centers for Disease Control and Prevention

## THE PROMISE

From 2013 to 2018, every US\$1 Rotary commits to WHO and UNICEF in direct support for polio immunization will be **matched 2 to 1** (up to US\$35 million per year) by the Bill & Melinda Gates Foundation.

YOUR DONATION

US\$25

+

BILL & MELINDA GATES FOUNDATION

US\$50

TOTAL CONTRIBUTION

US\$75



OR



OR



150

**VESTS FOR VOLUNTEERS**

Bright yellow vests help those seeking vaccination to recognize health workers and volunteers easily.

75

**VACCINE CARRIERS**

Oral polio vaccine must be kept cool to remain effective.

600

**PURPLE FINGER MARKERS**

Children's pinky fingers are marked with purple ink to show they've been immunized.

## WE'RE THIS CLOSE

Only **three** countries are polio-endemic:



THE REMAINING

1%

of polio cases are the **most difficult** to prevent because the virus persists in the hardest-to-reach areas.

YOU CAN HELP NOW →

## CITY COUNCIL ROLLING SCHEDULE Board and Commission Meetings 2014-15

### July

DATE	DAY	TIME	MEETING	LOCATION
7/21	Monday	7 p.m.	Council Meeting	Council Chambers
7/23	Wednesday	6:30 p.m.	Library Board	Library
7/28	Monday		DRB Panel B <i>Cancelled</i>	

### August

DATE	DAY	TIME	MEETING	LOCATION
8/4	Monday	7 p.m.	Council Meeting	Council Chambers
8/11	Monday	6:30 p.m.	DRB Panel A	Council Chambers
8/13	Wednesday	1 p.m.	Wilsonville Community Seniors Inc. Advisory Board	Community Center
8/13	Wednesday	6 p.m.	Planning Commission	Council Chambers
8/18			Council Meeting <i>Cancelled</i>	
8/25	Monday	6:30 p.m.	DRB Panel B	Council Chambers
8/27	Wednesday	6:30 p.m.	Library Board	Library

## COMMUNITY EVENTS

July	August
<p><b>Wilsonville Farmers Market every Thursday</b> July 24 and 31 – 4-8 p.m. Villebois, 28836 SW Costa Circle West</p> <p><b>Movie in the Park: Memorial Park River Shelter</b> July 25 – Gravity – Edited Version. All movies are free and will be shown on an inflatable big screen. Lawn opens at 8:15 p.m., movie starts at dusk. Bring a blanket to sit on.</p> <p><b>Rotary Concert – Johnny Limbo and the Lugnuts</b> July 24<sup>th</sup> 6:30 pm at Town Center Park</p> <p><b>Wilsonville Kiwanis Kids Fun Run</b> July 26 – 9 a.m. Wood Middle School</p> <p><b>Library Party in the Park</b> July 28 – 4 p.m. – 6 p.m. Murase Plaza</p> <p><b>Rotary Concerts: Aaron Meyer</b> July 31 – 6:30 p.m. Town Center Park</p>	<p><b>Fun in the Park</b> August 2 – 10 a.m. – 5 p.m. Town Center Park</p> <p><b>Wilsonville Farmers Market</b> August 7 -- 4-8 p.m. Villebois, 28836 SW Costa Circle West</p> <p><b>Rotary Concert – Big Night Out</b> August 7 -- 6:30 pm at Town Center Park</p> <p><b>Movies in the Park, Memorial Park River Shelter:</b> August 8 - The Lego Movie All movies are free and will be shown on an inflatable big screen. Lawn opens at 8:15 p.m., movie starts at dusk. Bring a blanket to sit on.</p> <div style="text-align: center;">  </div>

# Heart of the City

Volunteer Service Award 2014!

## Nominations Now Open

We invite you to nominate deserving individuals who have made outstanding contributions to our City through volunteer service.

Please note that qualifications for receiving this award have been changed to involve more of our volunteer community. We highly encourage you to think about those around you who inspire, lead, and devote their own time to making the City of Wilsonville the amazing community that it is.

Here in Wilsonville, we acknowledge these exemplary individuals by presenting them with the distinguished *Heart of the City Volunteer Service Award*.

Nominees must have a history of volunteering for the City of Wilsonville, and embody one or more of the following:

- Demonstrates exceptional commitment to volunteerism by helping with a special project or ongoing activities
- Demonstrates exemplary leadership, creativity, cooperation, and hard work in service to others
- Serves as a role model by inspiring others

In 300 words or less, please explain who you think should receive this year's *Heart of the City Volunteer Service Award*, and why. Submit your nomination in person, by fax, or by e-mail by 4:00 pm on July 31st to: **Patty Brescia, Recreation Program Manager, Wilsonville Parks and Recreation, 7965 SW Wilsonville Road, Wilsonville, OR 97070, [brescia@ci.wilsonville.or.us](mailto:brescia@ci.wilsonville.or.us), 503.570.1525.**

A committee will be appointed to review all applications and will make a formal recommendation for the recipient of the award.

***Date for Event: Thursday, August 7 from 5:30 to 7:00pm at the Rotary Concert in Town Center Park***



Planning Division  
29799 SW Town Center Loop E  
Wilsonville, OR 97070  
503-682-4960  
503-682-7025 fax

## Notice of Cancelled Meeting Wilsonville DRB Panel B July 28, 2014

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Please be advised that the regularly scheduled DRB Panel B meeting for Monday, July 28, 2014 has been cancelled. The next DRB Panel B meeting is scheduled for August 25, 2014.

We apologize for any inconvenience this may have caused. Questions about this notice may be directed to Shelley White, Planning Administrative Assistant, Wilsonville Planning Division, 29799 SW Town Center Loop E., Wilsonville Oregon, 97070, (503) 570-1575.

**CITY OF WILSONVILLE**  
**CITY COUNCIL MEETING MINUTES**

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A regular meeting of the Wilsonville City Council was held at the Wilsonville City Hall beginning at 7:00 p.m. on Monday, June 2, 2014. Mayor Knapp called the meeting to order at 7:10 p.m., followed by roll call and the Pledge of Allegiance.

The following City Council members were present:

Mayor Knapp  
Councilor President Starr  
Councilor Fitzgerald  
Councilor Stevens  
Councilor Goddard – Excused

Staff present included:

Bryan Cosgrove, City Manager  
Jeanna Troha, Assistant City Manager  
Mike Kohlhoff, City Attorney  
Nancy Kraushaar, Community Development Director  
Cathy Rodocker, Assistant Finance Director  
Sandra King, City Recorder  
Mark Ottenad, Public Affairs Director  
Blaise Edmonds, Manager of Current Planning  
Daniel Pauly, Associate Planner  
Keith Katko, Finance Operations Manager  
Jon Gail, Community Relations Coordinator  
Stan Sherer, Parks and Recreation Director  
Kristin Retherford, Economic Development Manager

**Motion:** Councilor Starr moved to approve the order of the agenda. Councilor Fitzgerald seconded the motion.

**Vote:** Motion carried 4-0.

## **MAYOR'S BUSINESS**

### **A. Upcoming Meetings.**

Mayor Knapp announced the next City Council meeting scheduled for June 16<sup>th</sup>. He reported on the second meeting held on Climate Smart Communities, an ongoing effort to develop strategies to reduce emissions from light vehicles in the region. He also noted the recent events that had been held including the Memorial Day celebration, observance at the Korean War Memorial and Annual Festival of Arts.

## **COMMUNICATIONS**

### **A. Korean War Memorial Foundation of Oregon Presentation, Korean War Veterans Association**

Mayor Knapp introduced tonight's presenters, briefly noting their work history and current positions with the Korean War Memorial Foundation of Oregon and Korean War Veterans Association. He thanked them all for coming and hoped anyone listening or watching rebroadcasts of the presentation would learn more about the Korean War Memorial.

**CITY OF WILSONVILLE  
CITY COUNCIL MEETING MINUTES**

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Jim Park, Chair, Korean War Memorial Foundation of Oregon (KWMFO), briefly reviewed the history of the Foundation, which began about three years ago after John Lim, then a former Oregon senator, approached him. The Foundation was not for Korean-Americans alone, but to make sure veterans of the Korean War were honored. The Foundation gifts the City \$5,000 each year to ensure maintenance of the Korean War Memorial.

John Lim Founder, Korean War Memorial Foundation of Oregon, said the Foundation was fortunate to have worked with the City, which donated the land for the Korean War Memorial in Wilsonville. He briefly described the Foundations five-year plan, which included building five statues in honor of those people who were significant in the Korean War and the country's recovery. Mr. Lim thanked City Council for the proclamation honoring James Lee, one of the founders of the Korean War Memorial Foundation who passed away April 16, 2014.

Chuck Lusardi President, Korean War Veterans Association (KWVA), Oregon Trail Chapter, explained that this was a transition year as the KWMFO would be taking the leadership role in putting future programming together. The Korean War veterans were interested in seeing the memorial be used to express the cultural relationship with the Korean community.

Mayor Knapp read the proclamation honoring James Lee, who was so instrumental in creating the Oregon Korean War Memorial in Wilsonville.

Mr. Lusardi presented a check for \$1,000 to the City of Wilsonville from the KWVA, Oregon Trail Chapter as a token of their appreciation for providing and maintaining the site of the Oregon Korean War Memorial in Town Center Park. They requested the funds be used to help support the cost of installing the electrical and audio circuitry for the audio and video equipment in the newly remodeled interpretive center.

**COUNCILOR COMMENTS, LIAISON REPORTS & MEETING ANNOUNCEMENTS**

Council President Starr – (*Parks & Recreation Advisory Board Liaison*) announced that Todd Blankenship had been hired as the City's new park supervisor and reviewed his background. He briefly reported on the Festival of the Arts, Tonquin for the Love of Schools Race and the Memorial Day celebration. He announced the upcoming meeting dates for the Community Center Task Force and Frog Pond Task Force. He noted the fountains were turned on in the main parks this weekend and that the Wilsonville farmers' market would begin June 12th.

Councilor Fitzgerald – (*Development Review Panels A & B Liaison*) discussed how the Aurora Squadron Civil Air Patrol performed the fly over at the Memorial Day Ceremony since the Blue Angels were no longer funded. She announced the upcoming Boy Scout event, which would be held the same day as the Korean War Memorial Remembrance Ceremony. She announced the upcoming DRB June meeting dates and briefly reported on the DRB-Panel B approval of a new maintenance building and new container storage area at Republic Services, noting that Phase 2 would be addressed at the June 23 meeting.

Councilor Stevens – (*Planning Commission; CCI; Wilsonville Seniors Liaison*) announced that the June Planning Commission meeting was cancelled, so the Commission would meet again on July 9. She reviewed the June 12<sup>th</sup> agenda of the Frog Pond Task Force meeting and announced the upcoming Basalt Creek Community Workshop. The Councilor encouraged the public to become involved as this project would change the shape of that area and create jobs for Wilsonville.

**CITY OF WILSONVILLE  
CITY COUNCIL MEETING MINUTES**

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This Friday, the Wilsonville Seniors would host a special lunch and musical tribute at the community center to honor all veterans of all ages. Other announcements included information about The Relay for Life, SMART's scenic trolley tours, and many summer programs hosted by the library.

**CONSENT AGENDA**

Mike Kohlhoff, City Attorney read the titles of the Consent Agenda items into the record.

- A.     **Resolution No. 2472**  
A Resolution Of The City Of Wilsonville Amending Resolution No. 2416 Which Authorized Acquisition Of Property Interest Related To Construction Of An Infill Segment Of The Ice Age Tonquin Trail Along Willamette Way East.
  
- B.     Minutes of the May 5, 2014 Council Meetings.

**Motion:**     Councilor Fitzgerald moved to approve the Consent Agenda as presented. Councilor Starr seconded the motion.

**Vote:**        Motion carried 4-0.

**PUBLIC HEARING**

Mr. Kohlhoff read the titles of Resolution No. 2474 and 2475 into the record.

- A.     **Resolution No. 2474**  
A Resolution Declaring The City's Eligibility To Receive State Shared Revenues.
  
- B.     **Resolution No. 2475**  
A Resolution Declaring The City's Election To Receive State Revenues.

Mayor Knapp read the public hearing format into the record and opened the public hearing for Resolution No. 2474 and Resolution No. 2475 at 8:00 p.m.

Ms. Rodocker stated that as required by the State of Oregon, the City must hold a state shared public hearing to receive state shared revenues. State revenues allow, in part, funding for the road operations program, facilities and parks maintenance, law enforcement, community services and library operations. To qualify for state shared revenues, the City must provide four of seven specific services and the City currently provided six of those seven.

Mayor Knapp invited public testimony, hearing none he closed the hearing at 8:03 p.m.

**Motion:**        Councilor Starr moved to approve Resolution No. 2474. Councilor Stevens seconded the motion.

**Vote:**        Motion carried 4-0.



**CITY OF WILSONVILLE  
CITY COUNCIL MEETING MINUTES**

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**Motion:** Councilor Starr moved to approve Resolution No. 2475. Councilor Fitzgerald seconded the motion.

**Vote:** Motion carried 4-0.

**C. Resolution No. 2476**

A Resolution Of The City Of Wilsonville Adopting The Budget, Making Appropriations, Declaring The Ad Valorem Tax Levy, And Classifying The Levy As Provided By ORS 310.060(2) For Fiscal Year 2014-15.

Mr. Kohlhoff read the title of Resolution No. 2476 into the record.

Mayor Knapp read the public hearing format into the record and opened the public hearing for Resolution No. 2476 at 8:08 p.m.

Cathy Rodocker presented the staff report, noting that the approved budget for FY 2014-15 totaling \$122,496,664 had been presented to City Council. Of that budget, \$114.7 million would be Appropriated Funding and \$7.7 million would be Unappropriated Ending Fund balances. Unappropriated Ending Fund Balances was made up of all the contingencies approved by the finance management policy, which included 15 percent of the City's current operating expenditures for the General Fund, along with the \$2 million Sustainability Fund, and the 20 percent operating expenses for all other operating funds with the exception of the Stormwater Fund, which fell short.

The Budget Committee amended the proposed budget, directing staff to reduce Personnel Services and/or Materials and Services by \$375,000 and offset the contingencies. She explained that Personal Services were reduced by \$162,000, and Materials and Services were reduced \$215,000 which increased the Ending Fund balance. The approved budget was the exact same dollar amount, but the different categories had been changed. Looking at the reduction by fund, the biggest change was \$101,000 to the General Fund, with the CD fund and Water and Sewer Operating Funds being the next largest contributors to the reduction.

When starting the budgeting process in January, staff estimated an 8 percent increase in Personnel Services for insurance, but learned in April that it would only be 5 percent; therefore, all the funds associated with employees reflected that reduction to the insurance in Personnel Services. The resolution included setting the General Fund tax rate at \$2.5206 per \$1,000 and their debt service of \$360,000, which would be paid in full by January 2016, and this levy would meet the Debt Service needs for this fiscal year.

Mayor Knapp called for questions from the Council.

Council Starr said that with regard to the Debt Fund, he assumed there was an early retirement fee if the City were to pay that debt off faster than 2016.

Ms. Rodocker added that it worked out to .156 per \$1,000.

Mayor Knapp confirmed there were no further questions from Council and invited public testimony.

Alan Kirk, 7926 SW Edgewater E. Wilsonville, OR 97070, said it was good to see that the debt bond measure approved by the voters 15 years ago for the library expansion would be completely paid off a

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year from now. Staff said that would reduce people's taxes by \$40 if they had a \$250,000 house. The city manager and his staff had once again presented a new budget that he would classify as status quo, while at the same time maintaining services, reserves, and the City's very strong financial position. He asked that his comments be included in the Urban Renewal Agency public hearing as well.

With regards to urban renewal, the West Side Plan started in 2003 with an assessed value of \$16.5 million. Today that had increased by \$248 million to now over \$264 million. This increase would not have occurred without the urban renewal district. With the proposed development later on the agenda, he assumed the City had a plan to sell the excess land of the school district back to the developer and asked if the sales price was public. If not, he asked that it be provided later on and if it would go back into the Urban Renewal Fund. The citizens of Wilsonville voted with 79 percent in favor of forming the West Side Urban Renewal District. The Year 2000 Plan was started in 1992. The City had removed about 298 acres in an attempt to return excess revenues back to the other taxing districts. The State changed the law, so a few years ago the City limited what the urban renewal district collected, which was about \$4.2 million with any excess going to the taxing districts. As seen in the West Side Plan, the Year 2000 Plan today went from \$44 million to \$396 million with what was remaining. He agreed with the recommendation made last year by Urban Renewal Task Force to close the Plan as soon as possible and pay the debt off early. He did not believe the closure would have a material compression impact upon the school district. Once closed and the outstanding bonds paid, 100 percent of the revenue would flow to the other taxing districts at about an additional \$4.2 million per year. Two district bonds would remain, one of which would be paid off June 1. Once that was paid, he suggested using that payment to pay against the other bond to get it paid earlier. The district also had a Program Reserve Fund of about \$1.65 million. He suggested using that to help fund the Canyon Creek South project and then fund the rest with a short-term loan instead of another bond measure, as was somewhat indicated in the minutes from the last Council meeting. The Council would have two upcoming urban renewal actions that they might want to ask the voters about. One was the new Coffee Creek District, and the second was an increase in the West Side Plan. The City had a limit on the number of acres that could be had in the plan, so it needed to be reduced. Showing the voters the history of increased revenue to taxing districts would bode well for both if the Year 2000 District was closed as soon as possible.

Mr. Kohlhoff confirmed the City had not sold the excess land of the initial school site in the north SAP nor was it currently on the market, but it was gaining value every day.

Mayor Knapp confirmed there were other questions from the Council and closed the public hearing at 8:18 p.m.

Mr. Kohlhoff provided a summary of a letter Mayor Knapp received from Budget Committee Chair Alan Steiger requesting that City Council reinstate the amendments passed by the Budget Committee that reduced the Personnel Services and/or Material and Services funds.

Mayor Knapp confirmed there was no further comment and requested a motion.

**Motion:** Council Starr moved to approve Resolution No. 2476. Councilor Stevens seconded the motion.

Councilor Fitzgerald appreciated the Budget Committee Chair's comments, and added that there was interest among various members of the Budget Committee about wanting additional information about why variances existed and what led to them. The city manager agreed that more notes would be included in next year's budget to that effect. She was disappointed the Budget Committee had not taken advantage of the opportunity to discuss the budget in depth, noting that 20 percent of their members were absent at

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that first meeting. Committee members were given notice of the meeting dates and staff sent several notices to check if the dates worked for people's calendars. She understood unexpected situations come up to prevent people from attending. However, she hoped that as people are recruited to these committees they would be encouraged to come in for a second meeting or ask questions in advance of the Committee's decision making time if they were unable to attend so the Committee could hear and respond to those questions.

Councilor Stevens noted Mr. Steiger's letter asked for the budget to be restored to the original budgeted amount. Given the limitations on what the Council could do once the Budget Committee made a decision, she asked if Council could do what he requesting.

Mr. Cosgrove replied yes, because it was by fund and the changes would be less than the 10 percent.

**Vote:** Motion carried 4-0.

Mayor Knapp noted he had some sympathy with Chair Steiger's position. He was also troubled by the late arrival of discussion at the second Budget Committee meeting which hindered staff from responding to questions and concerns that came up late. He believed staff had done a very fine job and the performance of the City overall was indicative of the efficient and effective use of public funds. He saw no indication to suggest that staff needed tighter guidance in order to continue producing that result. He hoped next year's discussion would be more forthright and take place earlier in the Budget Committee dynamic. He looked forward to the Budget Committee responding to concerns and having discussion about actual concerns of how the City budget was working as opposed to philosophy about governmental approach. He trusted Mr. Cosgrove would do the same fine work with the budget that he had done in the last few years.

Mr. Cosgrove assured staff made an effort to both orient Budget Committee members and made themselves available to meet with folks individually to answer any questions prior to the budget deliberations, but they could not make people come in. He was open to hearing about other ways to approach the situation. Having people provide staff questions in advance served the process better because staff could be more prepared and respond in more detail. He thanked the Council for setting the service level for the City.

- D. **Ordinance No. 743** - 1<sup>st</sup> reading  
An Ordinance Of The City Of Wilsonville Annexing Approximately 15.16 Acres Of Land Located Southeast Of The Intersection Of SW Grahams Ferry Road And SW Tooze Road Into The City Limits Of The City Of Wilsonville, Oregon; The Land Is More Particularly Described As Tax Lots 1200, 1202, And 1205 Of Section 15 T3S-R1W, Clackamas County, Oregon, Polygon Northwest Company, Applicant.

Mr. Kohlhoff read the title of Ordinance No. 743 into the record on first reading.

Mr. Kohlhoff advised that Council receive amendments to Ordinance No. 743 during work session, which primarily incorporated the attachments and renumbered them accordingly.

Mayor Knapp called to order the public hearing on Ordinance No. 743 first reading and asked if any members of City Council wished to abstain, declare a conflict of interest, or report any ex parte contact or information gained outside of that hearing.

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Councilor Fitzgerald did not believe she had any ex parte contact. She had attended the DRB meetings, but stayed only for the introductions and liaison report, especially for this meeting. She had the same level of information as her colleagues.

Mayor Knapp confirmed that all members had familiarized themselves with the application. No member of the audience wished to challenge the participation of any member of the Council. He read the public hearing protocol and called for staff's presentation on the application.

Dan Pauly, Associate Planner, announced that the criteria applicable to the application were stated on Pages 1 and 2 of 7 of the annexation findings which were attached to the ordinance. He presented the staff report, noting that the subject property was the southeast corner of Grahams Ferry Road and Tooze Road, which had been master planned as part of the Villebois development. Out of all the area within the Villebois Master Plan, the area between Grahams Ferry Road and 110<sup>th</sup> Avenue, which was part of Specific Area Plan (SAP) North, was the only part of Villebois not currently within the city limits. The two property owners involved were registered voters and as required by state statute, all of the owners and electors consented to the annexation allowing for a quasi-judicial process, rather an election.

Councilor Fitzgerald noted in the findings regarding the petition for annexation, Goal 8 under Exhibit B, stated, "Recreational Needs: To satisfy the recreational needs of the citizens of the state and visitors and where appropriate to provide for the siting of necessary recreational facilities including destination resorts..." She realized no destination resort was being sited, but asked if staff could clarify the findings for that goal.

Mr. Pauly replied the findings essentially discussed the overall park experience in the Villebois Master Plan, including a 10-acre forested preserve located just south of the subject property that had been planned with some additional programming together with the subdivision as well as a pocket park with some recreational amenities.

Mr. Pauly explained that recreational needs did not necessarily refer to the trees, but the fact that there were trails, playgrounds and picnic tables in that forested area, which was programmed for recreational activities.

Councilor Stevens confirmed the location of the forested area, which would remain forested, and that trails would be put in.

Mr. Pauly added that it was a great forested area. The existing trail was actually approved as part of a previous phase, but the approval of some of the park programming in the forested area was left until this phase because it would directly serve the homes in this phase.

Mayor Knapp asked what the property status would be once the property was annexed without further formal action.

Mr. Pauly replied the area would be left in the city with a Comprehensive Plan designation but would still have the county zoning until rezoning occurred. He noted same scenario has occurred elsewhere, particularly in Villebois, where an area that already had a Comprehensive Plan designation for Residential Village was annexed. A number of areas were still zoned EFU prior to development, including the school property.

Mayor Knapp confirmed there were no further questions from Council and called for comments from the Applicant and for public testimony.

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Alayna Robertson, Polygon Northwest, stated the annexation piece complied with the master plan and Polygon wanted to bring the area within the city limits in order to request the zoning change that was previously approved in the DRB hearing.

Mayor Knapp confirmed there were no questions for the Applicant and no public comment. He closed the public hearing at 8:39 p.m.

**Motion:** Councilor Fitzgerald moved to approve Ordinance No 743 as amended on first reading. Councilor Stevens seconded the motion.

**Vote:** Motion carried 4-0.

**E. Ordinance No. 744 – 1<sup>st</sup> reading**

An Ordinance Of The City Of Wilsonville Approving A Zone Map Amendment From The Clackamas County Rural Residential Farm Forest 5 (RRFF5) Zone To The Village (V) Zone On Approximately 15.16 - Acres Comprising Tax Lots 1200, 1202, And 1205 Of Section 15, T3S, R1W, Clackamas County, Oregon, Polygon Northwest Company, Applicant.

Mr. Kohlhoff read the title of Ordinance No. 744 into the record.

Mayor Knapp read the public hearing protocol and called the public hearing to order at 8:40 p.m. He asked if any members of Council wished to abstain, declare a conflict of interest, or report any ex parte contact or information gained outside of that hearing. There was none. All members had familiarized themselves with the application. No member of the audience wished to challenge the participation of any member of Council. He called for the presentation of the staff report.

Mr. Pauly announced the applicable criteria were stated on Pages 2 and 3 of 10, Attachment B to Exhibit A. The staff report had been entered into the record and copies were available on the table at the side of the room. He presented the Staff report, noting he had been before Council a number of times regarding zone changes in Villebois and this was similar. As a Residential Village Comprehensive Plan designation, there was one menu option when land was being rezoned to Village, which was what was proposed.

He noted a review criterion on Page 10 of 10 of Attachment B, Subsection 4.197.2(G) of the Development Code stated, "The proposed development and uses can be developed in compliance with the applicable development standards or appropriate conditions are attached to ensure that the project development substantially conforms to the applicable development standards." This Zone Map Amendment was reviewed by DRB, including some amendments to the Specific Area Plan (SAP), a Preliminary Development Plan (PDP), Final Development Plan (FDP) and Type C Tree Plan; all of which were approved with conditions by DRB Panel A on May 12, 2014. The DRB found the requests complied with the applicable standards.

One modification regarded a change to the mix of unit types from the Master Plan, generally, a reduction in the number of small and standard lots, and an increase in the number of large and medium lots. There was also an addition of a park area at the corner of Tooze Road and Grahams Ferry Road, which would be a nice focal point coming into the community. Retaining the wooded wetland at the southwest portion of the site was also a change from the Master Plan.

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He described the trees related to the proposed project, noting that two properties were involved and most of the trees being discussed were along the edges on the Tabor property which were planted as part of the landscaping for the property when that home was built. The sidewalk was being designed to preserve some good-sized trees along Tooze Road, including a Ginkgo tree, Douglas fir, Ponderosa pine, and a red cedar. The lotting pattern had been changed to retain a 25-inch Douglas fir in one yard, and a tree maintenance easement was also provided to allow the HOA to maintain the tree as an amenity for the community.

The giant Sequoia was likely planted in the 1970s as the project arborist estimated the tree to be about 40 years old. Giant sequoias tend to grow fast and have a larger diameter at breast height (DBH), the diameter of the tree 4.5 feet above ground. He reviewed the Development Code standards used when considering the preservation of such trees, noting that part of the process involved considering options, especially when weighing other design alternatives, such as connectivity and urban design, as well street networks and lotting patterns that made sense. After considering the options, the Sequoia tree was in the middle of a street. Staff concurred with the Applicant's recommendation that it must be removed.

In this particular case, Polygon researched many design alternatives before the lot layout design was brought to staff. Mr. Pauly deferred to Polygon to discuss the alternatives they considered before bringing this iteration forward.

Councilor Starr expressed concern about density, which had increased over time as Villebois expanded. He wondered when the density would be relaxed to make up for the higher densities that had been previously approved and wondered when the DRB would require more large and standard sized lots. He asked if the DRB was taking into account that Villebois was getting closer to build out and the densities needed to be lower to balance out what was originally platted.

Mr. Pauly understood density was a concern. Unlike the last project, this parcel was shown with quite a bit of density in the Villebois Master Plan and the Applicant's changes fell within the limits set for refinements in the Development Code. They increased number of medium lots and the number of large lots was also increased so the proposal had bigger lots and less density than the Master Plan, yet it was consistent with the Master Plan.

Councilor Starr noted over the last three years, Council had seen the density raised, but were told it would be lower in other parts of Villebois to reflect how it was planned originally. If the DRBs were not considering whether Villebois was in line with the densities originally projected, he believed it fell on the Council.

Mr. Edmonds said the overall Villebois Master Plan was originally approved for 2,400 units. Over time, the density increased, and there were more small lots. The property just east of the city-owned property was heavily treed and heavily sloped, and lots there were likely to be larger because of the conditions.

Mr. Pauly confirmed that the Master Plan, which was currently at about 2,600 units, was amended to allow for the increased density. Of the original 2,400 units, 100 were in Grand Pointe, which was not included with any units originally. There had been about a 100-unit increase from the original master plan.

Mr. Kohlhoff stated said the 2004 development agreements had 2,600 units originally. It increased from the first plan.

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Councilor Starr said he was concerned about having only a few parcels left to play catch up making it not necessarily equitable for that developer as opposed to making sure the City made thoughtful decisions the whole way. He did not know if the DRBs were actively engaged in these discussions.

Mr. Edmonds replied that the DRBs were actively engaged with refinements. A 10 percent adjustment or refinement was allowed within the buckets of the residential types that could be applied for without changing the Master Plan. Through those 10 percent refinements, there had been an increase over the past 10 years from 2,400 to 2,600 units.

Councilor Stevens noted one item to consider was the number of different lot sizes. She believed a lot of families would want to live in the smaller houses with other amenities close by. The large lots would have a fenced backyard up against two very busy roads. Everyone had different lifestyles and preferences. She believed the development provided more options for how people wanted to live and where they chose to buy a house. When crunching numbers, sometimes they lost sight of how people would actually live in those homes. She believed the variety of options in the plan would be desirable for many different types of families and households.

Mayor Knapp confirmed that the criteria that applied to this action in Exhibit B, starting at the bottom of Page 1 and continuing on page 2 of 10, and that Exhibit A1, shown on Page 9 of 135 of tonight's staff report, was the DRB staff report.

Mr. Pauly explained it was the standards that he read from Development Code Section 4.197.

Mayor Knapp read the top paragraph, "Staff has reviewed the site plan and has not identified any reasonable design alternatives to retain the good trees being removed for construction; notably a 61-inch diameter Giant Sequoia tree, #10478, in excellent health that sits in the middle of a planned street that cannot be realigned sufficiently to avoid the tree without significantly changing the layout of the development." It was not clear from that statement whether this was a good, excellent or important tree.

Mr. Pauly replied the categories included good or important trees. The standards were referenced in the Community Elements Book as noted in the Villebois Master Plan. He did not have a copy of the book with him, but could discuss the tree categorization. Primarily, native white oaks in good health would have been deemed important trees in the past.

Mayor Knapp noted that was an observation; the criterion did not specify only white oak trees. The criterion stated, "Important classification is comprised of trees that rated high in the following categories: health, structure and vigor of the tree." The Staff report stated this tree was in excellent condition meaning it would meet that criterion. He confirmed with Staff that the Giant Sequoia, at 61-inches in DBH, was probably one of the biggest trees in Villebois, so the tree appeared to comply under the second category, "Age and/or DBH." He asked what "Species rating per ISA standards for the Pacific Northwest region" meant.

Mr. Pauly replied that was essentially one of the main reasons the tree was not important. It was introduced as an ornamental tree, so it did not rate highly in this category. Often arborists did not like it from a natural resource standpoint because it provided extra habitat for wildlife.

Mayor Knapp continued reviewing the standards related to how the Giant Sequoia was categorized with Staff and noted that the tree met all the important categories, yet the Giant Sequoia was not categorized as an important tree, but a good tree. He reviewed the categories for good trees, noting some ambiguity existed between the good and important classifications. The tree qualified under all but maybe two of the

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important categories described. He was confused how it could be rated as good, when it complied with that many of the criteria, unless people just wanted to remove it.

Mr. Pauly explained that the ratings were provided by the project arborist, Ms. Holan. Her arborist's report was part of the record. The important and good ratings were not determined by Staff, but were based on a well-respected professional that had rated trees throughout Villebois.

Mayor Knapp noted Criterion C22 on Page 46 of 135 of Exhibit A1 stated, "Parks and open space areas shall incorporate existing trees where feasible, and large shade trees shall be planted." As an existing tree, the Applicant should try to incorporate it. He also cited Criterion C25, "Park design shall encourage opportunities for wildlife habitat such as plantings for wildlife foraging and/or habitat bird and/or bat boxes and other like elements, though not mentioned, including raccoons."

Mr. Pauly replied that the Applicant would discuss the alternatives they considered. He agreed it met Criteria C25, but tree preservation was weighed with all the other design criteria.

Mayor Knapp stated the second paragraph on Page 112 of 135, Subsection 4.610.00(.03)(B) read, "Where the site is proposed for development necessitating site plan review or plat approval by the Development Review Board (DRB), the DRB shall be responsible for granting or denying application for a tree removal permit, and that decision may be subject to affirmance, reversal, or modification by City Council, if subsequently reviewed by Council." He asked if the Council was subsequently reviewing. He clarified this was one of the criteria Mr. Pauly listed as applicable to this action.

Mr. Kohlhoff stated that by incorporation from Page 10 that Mayor Knapp had referred to before, it would be there.

Mayor Knapp continued, noting Page 113, Subsection F7, Subsection 4.610.10(.01)(B) stated, "No development application shall be denied solely because trees grow on the site. Nevertheless, tree preservation and conservation as a principle shall be equal in concern and importance to other design principles." On Page 114, the last sentence of the top paragraph, said, "No trees with a rating of important are present within the subject site." He could not accept that as accurate given the review they had just taken through the criteria used to determine important trees. On Page 115, Finding F13, Subsection 4.610.01(.01)(H) said, "Tree removal or transplanting shall be limited to instances where the applicant has provided completed information as required by this chapter, and the reviewing authority determines that removal or transplanting is necessary based on the criteria in this subsection." He understood this to mean that Council had authority.

Mr. Pauly stated all of these were the Development Standards listed on Page 10.

Mayor Knapp cited Finding F23 on Page 117 stating, "The City shall review tree relocation or replacement plans in order to provide optimum enhancement, preservation, and protection of wooded areas. To the extent feasible and desirable, trees shall be relocated or replaced..." All of these suggested that the question of preservation of a major tree that possessed a very aesthetic quality was a significant factor in deciding whether the proposal was appropriate. He was concerned about removing a tree that should live for 1,000 years in order to build houses that would last for 100 years. He was not sure he saw the logic or stewardship in that and would ask the Applicant to respond when they came forward.

Mayor Knapp confirmed there were no further questions for staff and invited the Applicant's comments.

Jim Lange, Pacific Community Design, 12564 SW Main Street, Tigard, introduced himself.



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Alayna Robertson, Polygon Northwest, 109 E 13<sup>th</sup> Street, Vancouver, WA 98660, thanked staff and the Council for taking the time to review the application for the zoning change. She displayed two maps. The Master Plan SAP originally showed there would be more small lots and less of the large lots. According to the numbers, the original Master Plan called for 101 units to be built in this section of Villebois. In response to requests from Council, as well as other Villebois and Wilsonville residents, Polygon took steps to introduce larger lots. The refinement limitations allowed a 10 percent reduction to the small lots allowed within that density requirement. Polygon reduced the number of small lots in this particular application by 10 percent from the Master Plan, and increased the number of medium and large lots. Although they looked to be the same depth, the medium lots were actually wider than the small lots. The proposal did introduce more large lots, which was in line with the Villebois Master Plan. By the numbers, the density had decreased as well as the number of small lots.

Because of the reduced density, Polygon was able to increase the wetland and open park spaces in the area. The park in the northwest corner would be the developed park with a play structure, where the lovely oak once stood. An oak would be planted there to reestablish a shade tree in that park. On the Master Plan, a road was originally intended to punch through a wetland space. However, Polygon's application looked to preserve the existing trees and wetland space while still maintaining the connectivity, which was so integral to the Villebois Master Plan.

There had been some definite changes to the responses received from previous applications. Villebois was a project Polygon strongly believed in, as indicated by the number of applications they had submitted. They were excited about the opportunity to bring large lots to Villebois. They had been very fortunate to have success with smaller lots, but were introducing bigger lots with this and the Grand Pointe applications to help with the density that had shifted since the original planning.

She reviewed the park system, noting the south boundary showed the trail system that would go into the treed area that currently existed. They did not plan to remove trees from that area, but would clean up some of the trails to make them walkable. She indicated the wetland area was preserved by relocating the street that connected to Grahams Ferry, thereby adding a significant area that was not originally part of the Master Plan. In the northwest corner she noted the developed park facility would be well-maintained with fencing appropriate to the Master Plan to make it a safe area for families to enjoy. Polygon knew their customers liked to see trees and be around green space, so they absolutely saw the trees as a benefit to their customers and homeowners.

She displayed conceptual elevations, noting the medium plans featured wider lots and a variety of architecture styles. The small lot sizes ranged from 2,000-3,600 square feet, which was Polygon's current alley-loaded product; medium lots ranged from 2,900-4,500 square feet; standard lots were 4,500-6,500 square feet. To meet easement requirements, homes on the small lots would be 1,700 or 1,800 square feet; the medium homes would be 1,900 to 2,200 square feet with houses sized incrementally as the lot sizes increased. Polygon was cognizant that having good-sized side yards and back yards was a benefit to their customers. The small lots and medium lots would be an alley-loaded product. The large homes could be about 3,500-4,000 square feet. They were significantly larger, front-loaded plans with driveways leading to two-car garages..

Polygon responded to the need for single-story homes due to the aging populations. The front-loaded lots provided an opportunity for master bedrooms on the man floor. No townhomes or multifamily connected units would be built, all the homes were single-family, detached homes.

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Mr. Lange added if the number of small lots were reduced any more, the Master Plan would have had to be revised. He described key discussion items of the Master Plan, which included intersection spacing and focusing on this grove of trees. There had been a progression of thinking about trees, because the decision to move the street connection preserved a number of significant trees. Early on, some of the work between staff and the Applicant involved started on the design of Grahams Ferry Road, Tooze Road, and its intersection, and preserving that oak tree. After the application was submitted, the tree split and fell.

The Sequoia in question was a beautiful tree, and they had gone through some exercises to try to figure out what could be done to save the tree after receiving the arborist's report. He reviewed several ideas that illustrated for the Council. The tree was in the middle of the road and a little toward the north side. One alternative they considered was to pull the road south, but that violated street design criteria and having that movement close to an intersection was not very safe. The other problem was the road was still significantly underneath the canopy of the tree. Earlier photos showed the massive, pyramid-shaped base of the tree and they did not think this alternative would help the tree, nor was it safe, so they discarded the idea. Another alternative was to terminate the street in some fashion. From a fire safety standpoint, the street could have been turned into an acceptable hammerhead for fire trucks, but the issue was the very long block which was unavoidable due to the intersection spacing. This alternative would not meet the connectivity requirements, which was one of the three goals of the Master Plan, so that alternative was eliminated as well.

In Phase 2 of SAP South there was a tree in an island, and the next alternative displayed was superimposed that same idea on the Sequoia. The developer was fighting four feet of grade change in that area. Such a tree island would meet fire safety requirements and it would not break the block the Applicant believed needed to be connected. However, it would impose an interesting grading challenge to center the island and do the grading needed to make the ground flat and having those turning movements that close to an intersection were also a problem. The Applicant could not eliminate more units without modifying the Master Plan, and the island would put them back in the position of doing some smaller lots, which seemed contrary to what they had heard and to Polygon's desire to have some large lots. Based on the combination of all those factors, this alternative was eliminated as well. The Applicant did look at the Sequoia tree and did go through an evaluation, which was how they reached their decision. Perhaps the error they made was not sharing the alternatives resulting from the evaluation process in the application.

Mayor Knapp was unconvinced by the Applicant's decision criteria. He felt the tree was an iconic, visual identifier for the entire neighborhood and he did not believe the concerns cited with the alternatives were fatal to the idea of saving the tree. The Mayor suggested splitting the road around the Sequoia, and using permeable pavement under the tree.

Ms. Robertson said their understanding from the arborist was that the Sequoia, not being a native tree, grew faster than trees native to the region, and offered to plant a Sequoia in another location.

Ms. Robertson noted the significant character of this particular area of Villebois needed to be taken into account as three areas, two corners, and the full southern border was trees. Of the entire tree inventory of 41 trees, seven would be retained, not including the Sequoia, based on the arborist's evaluation of the health and importance of those trees. The other trees being preserved within that area would absolutely be defining features for the community, which was unique in that it did not have the in-Village connectivity seen farther south on the opposite side of the trees where it was clearly a part of Villebois. This community would be very much defined in its own space because of the trees in the south section set it off to itself on the north end. She agreed the Sequoia was a beautiful tree, but did not believe it was the

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last defining tree to set the neighborhood apart. There were so many other trees that would define this space and neighborhood from the surrounding sections of Villebois.

Councilor Starr had two observations. First, to keep the tree, the developer and Council might need to meet halfway to keep the tree. If the Council wanted to keep the tree that bad, perhaps the Master Plan should be altered to help make it work. Council might need to come up with some type of joint idea. Second, they needed to think about what the tree would look like 30 years from now. Would the tree take over the road? He was concerned that even if they did something to save the tree, they might be creating a problem for the future. The tree was massive and he did not know how much bigger it would get. Although this was just first reading, he was open to some sort of compromise; however what the tree would be like many years down the road should be considered to make sure the plan would be workable in the future.

Mr. Lange said it was a big tree, but it was still a baby relative to Giant Sequoias. The 80 or 90 year old Sequoias around the Hillsboro courthouse were twice as big and twice as tall.

Ms. Robertson said there were always concerns with maintenance and as the root system continued to grow, it would require maintenance. As noted earlier, the Sequoia was able to thrive because it was an open space with plenty of rainfall able to get to it. The introduction of homes, irrigation systems and planter strips would limit the ability of water to reach the tree, which would affect its strength. They also had to consider how the introduction of concrete around the tree would impact the root system, even if permeable pavers were used. The pavers were relatively new in design and their long-term impact was unknown.

Councilor Fitzgerald agreed with everything that had been said. She loved the tree and its look, and having an arborist present would be helpful, but she was sensitive to what Councilor Starr brought up. One of the trees planted in a lot of plantings out in Graham Oaks was the white oak, which grew much slower. It was a tough reality, because no one liked to be the one to say they should take the tree out.

Mayor Knapp said in his view, they were not paid to be arborists or civil engineers, so the question was what could the Applicant and staff bring back as some kind of proposal or alternative. He knew that if the Council did not ask for viable alternatives, they would not get any and the decision would be made and the tree would be gone. He had not seen enough information to suggest that was the responsible course of action.

Ms. Robertson replied Polygon would need a clear understanding of the goals, would the tree be preserved at the expense of the density or Master Plan. The goal was to get back in line with introducing larger lots and decreasing the density, which was the message the Applicant had received and wanted to be aware of. She understood saving the tree was of the utmost importance, but requested some clear direction about what Council was willing to compromise on so they could move forward.

Mayor Knapp suggested reducing the size of Lots 12, 13, 51, and 52 sufficiently to allow the road to go around the tree, giving it sufficient growth space to thrive.

Councilor Starr asked if making some of those lots smaller at the expense of adding a corner lot instead of a pocket park, giving the Applicant one more lot in that area and providing more ground around the tree to protect it.

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Mr. Cosgrove believed the key issue, notwithstanding connectivity and safety, involved the tree and the arborist. He asked what the tree would look like 50 years from now, and what the root system would look like with that impacting the infrastructure.

Mr. Lange agreed to provide that information and project what the Giant Sequoia would look like in 100 years, including the canopy and how the tree would impact the street and homeowners. He confirmed the Applicant was working on a timeline and hoped to get the second reading, because the annexation had to be complete 90 days before the general election.

Mr. Cosgrove interjected that the annexation had been approved. Council could do first reading on the zone change with some direction for staff to work with the applicant before the second reading of the ordinance.

Mayor Knapp agreed they could, but he had not heard enough specificity to be comfortable.

Mr. Kohlhoff said Staff projected having the development agreement at the next meeting. There were significant road improvements to Grahams Ferry as dedications were involved to improve Grahams Ferry, especially at the intersection where a right turn had been set up. There had been some balancing for when the City went forward with Tooze Road that also involved some storm drainage issues. He believed the Applicant would go forward with Grahams Ferry before the City scheduled moving forward with Tooze Road. It was a complicated document, but he had been negotiating working with their attorney, and he believed the agreement should be ready for the June 19<sup>th</sup> meeting.

Mr. Cosgrove suggested Staff had enough direction to work with the Applicant to come up with some alternatives for the tree and project what the impact of leaving the tree would be on the infrastructure and homes. They might return with no good alternatives or a possible solution.

Councilor Fitzgerald said sometimes people forget to look at what certain new trees would do or look like in 15 years. She would like to hear how all those new trees would look in 15 years.

Mr. Lange noted Council was very familiar with the proposed street trees. The reason the arborist had classified the oak tree as poor was that it had rot in the trunk and was going to fall apart, and when the wind storm came it did. That corner was a very high visibility area, being on the edge of the development and on the high ground. The Applicant had proposed to plant another oak there, which was on the approved plan. He believed it would be a good place to plant a Giant Sequoia, because it would stick up in the air a lot higher being on the hill and would be a very visible landmark.

Mr. Kohlhoff asked how many additional trees were being added, including the street trees.

Mr. Lange said he was not certain but it was about 200 trees. He indicated where significant mitigation would be done.

Councilor Starr suggested approving the ordinance on first reading, because he wanted to be sensitive to everyone's time. He was totally open to tweaking the Master Plan if it meant the tree could be saved. He believed Council needed to hear from the arborist and get some more information before making that decision.

Ms. Robertson noted the Arborist Morgan Holan was available by phone and willing to go on record to answer some questions if Council would allow.

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Mayor Knapp believed more information was needed to get Council to a decision point.

**Motion:** Councilor Starr moved to approve Ordinance No. 744 on first reading and leave the public hearing open. Councilor Stevens seconded the motion.

Mayor Knapp did not agree with the current motion, and wanted to know if a motion to continue under Roberts' Rules of Order would take precedence over a motion to pass on first reading and leave the public hearing open.

Mr. Kohlhoff answered yes, it could.

**Motion:** Mayor Knapp moved to not vote on first reading and continue the entire discussion of Ordinance No. 744 to June 16, 2014, the next regularly scheduled City Council meeting. Councilor Fitzgerald seconded the motion.

Mayor Knapp did not think the matter was ready for a first vote and was not confident in the subject matter of staff's response, nor did he know how well it would address the options needed. He believed there was plenty of time to extend the discussion given the timeframe described.

Councilor Stevens confirmed that if the ordinance was passed on first reading keeping the public hearing open and it went to second reading and the Council was not satisfied and did not pass it on second reading, the ordinance would fail.

Ms. Robertson asked that the Applicant be able work with staff to get the ordinance passed today leaving the public hearing open so they could work on a solution for the tree, either that provides a solution entirely or that provides satisfactory information as to why the tree could not be preserved. Due to the property being foreclosed, part of that option was that the application be approved in order for the Applicant to close on that property and they did not want to miss that window of time.

Mayor Knapp stated he had heard no testimony about the time limitation, adding she had just introduced a subject that he did not find in the record.

Mr. Kohlhoff responded he did not want to necessarily introduce information into the record, but since it was open, he noted he and the Applicant's attorney had discussed trying to meet on June 19 because they were trying to place their closing. [1047 1:40] The Applicant had a purchase and sales agreement and wanted to have these items in place right after that. The Applicant's closing was very close after the next City Council meeting on June 16.

Mayor Knapp asked what was harmful about having the Applicant and staff bring the requested information on June 16 for the Council to make a decision.

Ms. Robertson understood the approval at first reading was needed now in order to proceed and get approval at second reading.

Mr. Kohlhoff noted the Applicant had a purchase and sales agreement and were trying to set up the closing based on the fact that they had approval for the annexation. They had approvals for the zone change and development agreement. They had thought the second reading would be June 16 and they would be producing the development agreement on the 16<sup>th</sup> for the Council's approval as well. That was the timeline with which he was working with the Applicant. He did not know the Applicant's situation or

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how a two-week delay would change the timeline. They were purchasing property from a couple different people and he was not sure what the economic situations were of the people selling the property.

Mayor Knapp stated he was uncomfortable moving forward with approving the ordinance on first reading without knowing about staff's alternatives or recommendations. He had not heard anything suggesting urgency in making the decision. He needed more specifics and had not heard a good reason why delaying it to the next meeting would change anything other than allowing time to get the answers in front of the Council in order to make an informed decision.

Councilor Starr was not trying to minimize the Mayor's concerns, but he believed it would be helpful to the Applicant to move forward tonight with an approval on the first reading because Council still had the opportunity to stop it on the second reading, which would address the Mayor's concerns while at the same time, set the Applicant up for success. He recommended passing the ordinance based on its ability to bring value to both concerns and still get it done on time.

Councilor Fitzgerald called the question.

Mayor Knapp restated his motion to continue the entire matter to the next Council meeting.

**Vote:** Motion failed 1-3.

Mayor Knapp restated the motion to approve Ordinance No. 744 on first reading, leaving the hearing open with direction to staff.

**Vote:** Motion passed 4-0.

Mayor Knapp said the appropriateness of the decision would depend on the quality of what came back to City Council. He challenged both staff and the applicant to make that happen.

Ms. Robertson thanked the Council and assured the Applicant would definitely put in the time and provide the Council with all the requested information.

Mayor Knapp noted the time was past 10:00 pm and asked if the Council wanted to continue the meeting.

**Motion:** Councilor Fitzgerald moved to proceed with the meeting and subsequent Urban Renewal meeting after a short break. Councilor Starr seconded the motion.

**Vote:** Motion carried 4-0.

Following a brief recess, Mayor Knapp called the meeting back to order at 10:22 pm.

**NEW BUSINESS**

A. **Resolution No. 2473**

A Resolution Of The City Of Authorizing Support Grant Agreement With Wilsonville Community Sharing (staff- Mr. Kohlhoff)

Mr. Kohlhoff read the title of Resolution No. 2473 into the record.

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Ms. Rodocker stated the resolution was an extension of the Community Service Grant to Wilsonville Community Sharing (WCS), noting \$30,677 of the grant was for the general use of their program, which could be used for anything including administrative cost, but not capital construction, and \$16,000 was for the Renters' Utility Assistance Program, which could be used to pay City water, sewer, natural gas, and electricity bills. The general program had been in effect for more than 15 years, and the renters' assistance program had been in effect since 2010. Each December the City looked at the unemployment rate for the Portland area and if it went under 7 percent, the renters' program would not be available the following year. This December, the rate was at 7.3 percent.

Councilor Fitzgerald favored supporting the disadvantaged in the community in the most effective way possible, but inquired about process. During the procedure to approve the grant, Council had a presentation by some of the WCS Board members several months ago, and there was very limited enrollment on their board. Since then, more board members had been elected and Council had talked about the importance of adopting certain best practices in management of a board and rotating terms and officers. She asked where that discussion fit in with the decision being made tonight.

Mr. Cosgrove replied Staff had changed the agreement, after being given direction by the city attorney to tighten up some of the language for financial accountability. The City did not have any control over the Board other than in the agreement. The Board was given some strong hints about branching out and bringing on more members, and they were moving in that direction.

Mr. Kohlhoff confirmed the Board had done that and adopted new bylaws. Legally, their authority was now set and they could proceed. Mr. Kohlhoff clarified the grant was strictly operational funds, not capital.

Councilor Fitzgerald appreciated that, and clarified she did not want to micromanage the WCS Board, but rather revisit the discussion. She asked if anyone else had any other thoughts. She believed the Board had made some good improvements, but did not want to completely disregard that aspect of their last meeting.

Mayor Knapp noted according to Page 4 of 6, Items 7.1, 7.2, reporting was required within three weeks following the end of a quarter and reporting in January on annuals. As of about March of this year, he understood the City had not received any reporting regarding the year-end 2013.

Ms. Rodocker was sure all of the required reports had been received, but she would have to check whether the annual report had been received.

Mr. Cosgrove noted Ms. Rodocker had said the City was receiving the reporting, but was uncertain whether the annual report had been received. The annual report was a required report under the contract, and something their accountant would have to prepare for them. If tied to the tax timeline, it should have been done in April.

Councilor Fitzgerald read that 4.6 under 7.2 called for an annual report each January. She asked if the report was for the business year ending December 31.

Staff responded the annual report was a rollup of the actual monthly information sent to the City as well as a budget projection for the next year. The monthly reports were being received, but Staff was uncertain about the formal annual report.

Mayor Knapp did not want to micromanage either, but given some of the difficulties the City had with long-term commitments to groups in the community without auditing, follow up, or robust supervision, he

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was not sure how to proceed. In early March, the finance director said the City had not received it and she had no information to give him at that point.

Mr. Cosgrove recommended the item be continued to the next meeting. He confirmed there was no pressing reason to approve the contract tonight. WCS would still be providing the same services; the question was whether or not they were meeting the reporting requirements for the annual report. Conversely, Council could make a motion to approve it, subject to WCS submitting the annual report before he was authorized to sign the agreement.

Mayor Knapp said he would like to see the reports the City had been receiving. He was interesting in the long term trending with the quarterly reports, which was part of the fiduciary kind of supervision he believed Council was obligated to provide.

Mr. Cosgrove confirmed the reports were to be submitted to the finance department, so staff was responsible for making sure WCS was meeting those requirements. He asked if the reports were typically shared with Council.

Mr. Kohlhoff stated the reports were usually seen during the budget process. He reminded that the WSC had testified at the budget hearing.

**Motion:** Councilor Fitzgerald moved to continue Resolution No. 2473 to June 16, 2014 to receive additional information from staff. Councilor Stevens seconded the motion.

Councilor Fitzgerald wanted to continue the discussion because in addition to seeing the financial reports, Council could determine if the City was actually allocating sufficient funds to address the need.

Mr. Cosgrove commented the issue was whether the funds provided for the utility relief was being utilized to the maximum extent it should. From the report WCS submitted, they were using all the money the City sent for utility relief. He would bring the report back to Council and have WCS present to discuss their programs.

**Vote:** Motion carried 4-0.

### **CITY MANAGER'S BUSINESS**

Mr. Cosgrove explained the Community Survey surveyed citizens about the services and programs the City provided to the community. The first survey was in 2012, so this second survey would give the City a benchmark to see how it was performing over time in terms of the services being provided.

### **LEGAL BUSINESS**

Mr. Kohlhoff stated that last Friday the City filed and served a complaint for imminent domain on property in connection with the Barber Street extension west.



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**ADJOURN**

The Council meeting adjourned at 10:37 p.m.

Respectfully submitted,

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Sandra C. King, MMC, City Recorder

ATTEST:

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TIM KNAPP, MAYOR

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A regular meeting of the Wilsonville City Council was held at the Wilsonville City Hall beginning at 7:30 p.m. on Monday, July 7, 2014. Mayor Knapp called the meeting to order at 7:38 p.m., followed by roll call and the Pledge of Allegiance.

The following City Council members were present:

Mayor Knapp  
Council President Starr  
Councilor Goddard  
Councilor Fitzgerald  
Councilor Stevens

Staff present included:

Bryan Cosgrove, City Manager  
Jeanna Troha, Assistant City Manager  
Mike Kohlhoff, City Attorney  
Sandra King, City Recorder  
Nancy Kraushaar, Community Development Director  
Mark Ottenad, Government Affairs Director  
Jon Gail, Community Relations Coordinator  
John Smith, CCSD Police Chief  
Pat Duke, Library Director

Motion to approve the order of the agenda.

**Motion:** Councilor Starr moved to approve the order of the agenda. Councilor Fitzgerald seconded the motion.

**Vote:** Motion carried 5-0.

## **MAYOR'S BUSINESS**

### **A. Renewal of City Manager Employment Contract**

Mayor Knapp commented the City Manager's employment contract is renewed annually. The Council members believe Mr. Cosgrove has done a fine job of moving the City forward, and the City is making significant progress on numerous fronts. They felt City is well positioned for growth in infrastructure and capabilities for the coming years and expressed their appreciation for his work during the past year. Mayor Knapp noted Council President Starr lead the review and discussions with Councilors.

Council President Starr stated the Council was pleased with the direction the City was going, and Mr. Cosgrove's leadership from customer service through financial responsibilities has been recognized and appreciated.

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**Motion:** Councilor Stevens moved to approve the City Managers Employment Contract for an additional year with an annual salary of \$140,905.47 and direct staff to make changes to the contract consistent with the motion. Councilor Fitzgerald seconded the motion.

**Vote:** Motion carried 5-0.

B. Library Board Appointments

Mayor Knapp interviewed the additional applicants; however, both Ms. Chuinard and Mr. Gaines wish to continue on the Library Board and they are eligible to continue. The Mayor stated he would be pleased to appoint any of the other individuals and asked staff to keep them notified of any board or commission vacancies.

**Motion:** Councilor Fitzgerald moved to appoint Megan Chuinard and Reggie Gaines to the Library Board for a term beginning July 1, 2014 and ending June 30, 2018 Councilor Goddard seconded the motion.

**Vote:** Motion carried 5-0.

C. The Mayor announced upcoming meetings and reported on the meetings he attended on behalf of the City.

**CITIZEN INPUT & COMMUNITY ANNOUNCEMENTS**

This is an opportunity for visitors to address the City Council on items *not* on the agenda. It is also the time to address items that are on the agenda but not scheduled for a public hearing. Staff and the City Council will make every effort to respond to questions raised during citizens input before tonight's meeting ends or as quickly as possible thereafter. Please limit your comments to three minutes.

Dorothy Von Eggers indicated she had read the staff report about Wilsonville Road traffic slowing solutions. It appeared the installation of the medians will be an interim solution. She thanked Council for paying attention to the concerns of the residents and asked Council to keep the Wilsonville Road residents' concerns into consideration when the Frog Pond area develops..

**COUNCILOR COMMENTS, LIAISON REPORTS & MEETING ANNOUNCEMENTS**

Council President Starr – (*Park & Recreation Advisory Board Liaison*) announced the next meeting date for the Parks and Recreation Board; and noted he had attended Chamber of Commerce meeting where they discussed the financial stability of the Chamber. The Chamber is also in the midst of conducting a business retention survey. The Councilor noted the Community Center Task Force meeting scheduled for July 15<sup>th</sup> and reminded the community of the farmers market every Thursday in Villebois.

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Councilor Goddard – (*Library Board Liaison*) reported on the Library summer reading program and announced upcoming Library youth activities and book club events. He invited the community to attend the movies in the park Friday the 11<sup>th</sup>.

Councilor Fitzgerald – (*Development Review Panels A & B Liaison*) reported on the next meeting dates of the DRB boards, and announced the results of the last DRB Panel B meeting decisions. She announced the SMART scenic trolley tours and the Rotary the 24<sup>th</sup>.

Councilor Stevens – (*Planning Commission; CCI; Wilsonville Seniors Liaison*) described the work the Planning Commission will address at their next meeting. She announced the joint Tualatin and Wilsonville City Council meeting July 16<sup>th</sup> where Basalt Creek area development will be discussed and reported on the activities of the French Prairie Forum.

**CONSENT AGENDA**

Mr. Kohlhoff read the Consent Agenda items into the record.

A. Minutes of the June 16, 2014 Council Meeting.

**Motion:** Councilor Fitzgerald moved to approve the Consent Agenda. Councilor Starr seconded the motion.

**Vote:** Motion carried 5-0.

**NEW BUSINESS**

A. Wilsonville Road Improvements – Meadows Loop/Ashland Drive to Boeckman Road.

Nancy Kraushaar, Community Development Director, presented the staff report. This agenda item is brought forward per Council direction to identify possible solutions to traffic concerns raised by residents who live adjacent to Wilsonville Road. Staff has identified a traffic calming project to address speeding concerns. Council will also consider adding a project to the Adopted 2014-15 Wilsonville Budget to construct landscaped medians on Wilsonville Road between Meadows Loop/Ashland Drive and Boeckman Road.

At the June 2, 2014 City Council Work Session, staff presented the results of the speed and truck volume study for Wilsonville Road in the Landover area. At the conclusion of the presentation, the City Council asked to learn more about cost and funding sources for a project that would add landscaped medians to Wilsonville Road (between Meadows Loop/Ashland Loop and Boeckman Road). The City Council received input from citizens in the Landover area via email and Citizen Comment at the June 16, 2014 Council meeting.

The subject project is in response to neighborhood reports of vehicles traveling at excessive speeds in the area. The posted speed is 35 mph and speed data show that the 85<sup>th</sup> percentile speed

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is 38 to 39 mph. Landscaped medians were suggested for this section of Wilsonville Road to slow traffic. They are considered a traffic calming tool that can influence vehicle speed by narrowing the overall unencumbered roadway width.

The preliminary cost estimate for the project is \$175,000 to \$200,000. The cost was developed using the Canyon Creek Road Extension bid tabs adjusted for project magnitude and location.

The following funding sources could be used for the project:

- Year 2000 Urban Renewal Plan. The 2000 Plan scenarios developed during the ongoing strategic plan all include this landscaped median project with an allocation of \$289,400.
- Gas Tax. State gas tax revenues can be used. However, the project would use a disproportionate share of the fund that is otherwise needed for annual Road Operations.
- System Development Charges (SDCs). SDCs include two elements – the reimbursement fee and the improvement fee. The reimbursement fee could be used to fund at least a portion of the project.
- General Fund. The general fund can be used for the project.

Staff recommends that one or a combination of the above sources, except for gas tax, be used to fund the project.

The landscaped medians are expected to serve several purposes. If successful as a traffic calming tool, the community will benefit from moderated vehicle speeds on an arterial adjacent to residential neighborhoods. Benefits include a more friendly pedestrian and bicycle environment and higher safety level for all travel modes.

The landscaping will add an aesthetic treatment to the roadway cross-section and complete the “boulevard” treatment that is found along most of Wilsonville Road throughout the city.

The outcome of the City Council’s decision will determine budget impacts. Should the project move forward, a supplemental budget adjustment will be presented to Council for budget appropriation. Staff has identified four possible funding sources for this project. These funding sources are not mutually exclusive, that is, Council may determine that using a combination of the funding sources is necessary and prudent to complete the project.

Mayor Knapp asked if the Council thought moving forward with the installation of the medians would benefit the community. Councilors indicated they thought the medians will slow traffic, and convey the sense to drivers that they were entering a residential area. They asked that signs be posted not allowing the use of air brakes in the neighborhood.

Mr. Cosgrove said staff would return with the funding elements once a project was identified.

**Motion:** Councilor Stevens moved to add the Wilsonville Road (Meadows Loop/Ashland Drive to Boeckman Road) median installation project to the Adopted 2014-15 Wilsonville Budget. Councilor Fitzgerald seconded the motion.

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Councilor Starr wanted the project funded by the General Fund rather than Urban Renewal funding.

**Vote:** Motion carried 5-0.

**CITY MANAGER'S BUSINESS**

Mr. Cosgrove presented an update on the status of City Council Goals:

- The Tourism Implementation Plan would be before Council in the fall
- Frog Pond and Advance Road is an ongoing planning process, with completion of a major component on tract for April 2015
- The Aquatic Recreation Center Task Force is formed with the second meeting set for July 15<sup>th</sup>
- Successfully connecting live and work goal is scheduled to come before Council in the fall in several sessions.
- The Filling of vacant store fronts survey study has been funded in the current budget and firms specializing in that work have been contacted. The Chamber will be involved as well. This will return to Council in the fall.
- Coffee Creek annexation strategy is on hold until the strategic plan for Urban Renewal is completed.
- A joint meeting with Tualatin is scheduled to discuss the Basalt Creek Planning Area
- A request for proposals for the Branding and Marketing study will be on the street very soon.

In other updates:

- The City Manger stated he would be attending the OCCMA Conference the rest of the week.
- The fiber project is wrapping up and will connect all public facilities and have interface with the school district and Clackamas Community College.
- He walked through the remodeled visitor and information center building; citizens are thankful with the change in restroom location which allows parents to keep an eye on their children using the facilities.
- A report on the Spring Training was distributed and will be useful at the next goal setting session and to facilitate better communication with Boards and Commissions.
- To help senior citizens attend the weekly Farmers Market in Villebois, SMART will be providing a small bus to transport senior citizens to and from the Market.
- Data aggregation on the community survey is taking place now and results should be coming soon.

**LEGAL BUSINESS**

Mr. Kohlhoff was working with the Planning Department to analyze the call-up provision in the Code and hoped to have a report to Council at the next meeting.

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**ADJOURN**

Mayor Knapp adjourned the Council meeting at 8:31 p.m. The City Council reconvened their Work Session.

Respectfully submitted,

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Sandra C. King, MMC, City Recorder

ATTEST:

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Tim Knapp, Mayor

# Monthly Report

## FROM THE DIRECTOR'S OFFICE

Happy summer! My June activities focused on many Community Development work program items and citizen concerns.

We held two meetings to review preliminary work on the Stormwater Rate Study and toured project sites to develop a good understanding of how they fit into the stormwater capital improvement plan (CIP). Before we fine tune the rate model, the City needs to have confidence in the CIP.

We received a planning level report from OBEC Consulting on two bridge alternatives for a possible future Boeckman Dip bridge. It provides excellent information about bridge costs and sewer routing for both the Frog Pond Plan and the Sanitary Sewer Master Plan update.

I have been working on citizens' traffic concerns—primarily on Wilsonville Road, Day Road, and in Villebois. As part of our work with the Villebois community, we will be striping some pilot bike buffers on Barber Street to emphasize "bike friendly" and slow traffic with narrower travel lanes.

The Frog Pond and Basalt Creek Plan staff technical teams have been reviewing a variety of technical memos pertaining to existing conditions, infrastructure, transportation, and market analysis. We also attended task force meetings and a public workshop for Basalt Creek.

The Oregon Passenger Rail project staff met with our staff experts as a follow up to their last presentation with the City Council.

Enjoy your summer! - Nancy Kraushaar, PE

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## Building Division

**Single Family Dwelling Permits YTD:** 123

**Major Projects Under Review:**

- West Chiropractic 30485 SW Boones Ferry Rd
- Oregon Hot Tub 25725 SW Gwen Dr
- W3 I-5 Corporate Park Shell Building 27100 SW Parkway

**Temporary or Certificates of Occupancy Issued:**

- North 76 Station
- Café Yumm
- Coca Cola Lab Addition





## Engineering Division, Capital Projects

**Waste Water Treatment Plant (2082):** Staff is pursuing an expedited permitting process to allow an interim repair of the damaged outfall pipe this summer, followed by a full replacement in 2016/2017.

**Barber Street Extension (4116):** Property acquisition and final plans were completed, and the project remains on schedule for an August 7th bid date. Construction will take approximately 14 months.

**Street Maintenance (4014):** Contract is in place, and activities will begin in early July.

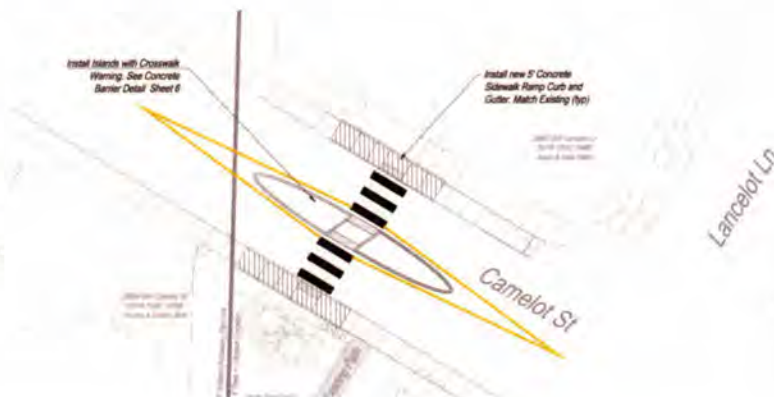
**Tonquin Trail (9135):** A contract was awarded to GSE Inc. for \$94,448 for the Willamette Way East section of the trail. Work will begin approximately August 1, 2014 and be completed by September 30, 2014.

**Canyon Creek Road Extension (4184)** Canby Excavating, Inc. was given a Notice to Proceed on June 26, 2014. Construction is expected to start in early July 2014.

**Streetlight Infill (4696)** The contractor recently installed five new cobra style streetlights at the intersection of Wilsonville Road and Grahams Ferry Road. These should provide for a dramatically better lighted intersection during the evening hours. Additional street lighting will be replaced along Grahams Ferry Road and Tooze Road with black cobra poles. The existing Villebois acorn lights will be sold as surplus. Some of the existing aluminum cobra lights will be re-installed on Burns Way, the remaining lights will be stored and used on the Barber Street extension project.

### Annual Pedestrian Enhancements

**(4717):** Work will soon begin on adding a new, protected crosswalk in the Park at Merryfield neighborhood to provide a safer crossing for children walking to Wood Middle School or Boones Ferry Primary.



## Engineering Division, Private Development

**Villebois SAP East PDP 3—Meadows 1:** The contractor, Northwest Earthmovers Inc. will start construction work in early July, 2014. This will involve closure of 110th Avenue and rerouting of traffic.

**Lowrie Primary School - Safe Routes to School:** In July, City staff will assist parents and volunteers from Lowrie Primary in refreshing the white dots painted on sidewalks which help guide children on the preferred routes to the school.

## Planning Division, Current

On June 9<sup>th</sup> DRB Panel 'A' voted to approve the application for a re-plat for the Charbonneau Village Center Condominiums. Charbonneau Village Center, Owner. Application Number: DB013-0058. Staff: Mike Wheeler.

On June 9<sup>th</sup> DRB Panel 'A' voted to approve a 4,995 sq. ft. building addition to the Artistic Auto Body building located at 27975 SW Parkway Avenue. Application Number: DB014-0017 - 0020. Staff: Blaise Edmonds.

On June 9<sup>th</sup> DRB Panel 'A' voted to approve a remodel of the north side of the former Hollywood Video warehouse building for a Chrysler, Jeep, Ram and Dodge dealership located at 25600 SW Parkway Avenue. Findlay Automotive Group, Owner. Application Numbers: DB14-0036 - 0040. Staff: Blaise Edmonds



On June 9<sup>th</sup> DRB Panel 'A' voted to approve a remodel of the north side of the former Hollywood Video warehouse building for a Chrysler, Jeep, Ram and Dodge dealership located at 25600 SW Parkway Avenue. Findlay Automotive Group, Owner. Application Numbers: DB14-0036 - 0040. Staff: Blaise Edmonds

On June 9<sup>th</sup> DRB Panel 'A' voted to approve a 34,500 sq. ft., 2 story office, manufacturing and warehouse building (W3 building) located at 27200 SW Parkway Avenue. Jack Martin, Martin Real Estate Development, Owner. Application Numbers: DB14-0042 - 0043. Staff: Dan Pauly, AICP.



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## Planning Division, Long Range

**Form Based Code:** Project staff have provided the consultant team detailed comments on the first draft of the Coffee Creek Industrial Area Form Based Code and Pattern Book. The consultant team will be incorporating the changes and will be presenting the revised draft to the Planning Commission and City Council at regular meetings in July.

**Regional Park 4 (Edelweiss Park):** Regional Park 4 in Villebois (now being called Edelweiss Park) is under construction with underground utilities, concrete flat work, irrigation and plant material being installed. This park will contain a significant section of the Ice Age Tonquin Trail, a covered picnic area, playground, full court basketball, and a pickle ball court (a small version of a tennis court played with paddles and specialized balls). The construction is anticipated to be complete by the end of August.



### **Basalt Creek Community Workshop**

On Tuesday, June 17, 6:00-8:30 p.m., the cities of Wilsonville and Tualatin held a public planning workshop to begin discussing the future of the Basalt Creek area, located between the two cities. Approximately 60 people attended, including a mix of study area property owners, Tualatin residents, interested developers (residential and industrial), and Wilsonville residents. Representation from Wilsonville was limited.

The outcomes of the workshop and the existing conditions inventory will be presented at the next Joint City Council meeting with the City of Tualatin on July 16th in Tualatin.

**Frog Pond Area Plan:** As its first task, the Frog Pond Area Plan Task Force helped staff create a draft vision statement for the future of the study area. Staff will present this statement for discussion and endorsement to the Planning Commission and City Council in July.

### A VISION FOR FROG POND IN 2035

The Frog Pond Area in 2035 is a Wilsonville community with attractive and connected neighborhoods. The community's hallmarks are its safe, walkable, and active streets, variety of quality homes, and connected trails and open spaces. Frog Pond's excellent schools and parks are focal points of the community. Frog Pond is "just a short bike, walk, or bus trip" from all parts of Wilsonville – a highly valued and connected part of the larger city.



Creating a great community

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## Economic Development Division

This month we have launched a new economic development website, that you can visit at [www.wilsonvilleecdev.com](http://www.wilsonvilleecdev.com). Additionally, this month's activities included submitting two RFI responses for Business Oregon and Greater Portland, Inc. One of these led to a site tour of Xerox Building 83. I also facilitated a separate tour and information packet in response to a site selector request that came directly to the City. I had the opportunity to serve as a group facilitator at the NW Supply Chain Conference in Portland this month and also conducted a site visit and outreach meeting with Machine Sciences Corporation and one with Flir this last month. Kristin Retherford, Economic Development Manager.

# Parks and Recreation

## June 2014 Report



### Summer Activities Kick Off!

June marked the beginning of summer programs and included a number of large community special events, as well as the opening of water features at Town Center Park and Murase Plaza.

#### Week of June 1:

- \* Festival of Arts at Town Center Park
- \* Tonkin for the Love of Schools - 5k, 10k, 1/2 Marathon
- \* Water Features open for the season
- \* Beginning Yoga (12 participants)

#### Week of June 15:

- \* Summer Camps begin with Babysitters Bootcamp (7 attendees) and Horse Camp (5 campers)
- \* Korean War Remembrance Ceremony at Town Center Park
- \* Wilsonville Relay for Life at Town Center Park
- \* Tap Dancing (12 participants)

#### Week of June 22th:

- \* Camps continue with Coach Nics Hoop Camp (23 campers), Skyhawks Tennis Camp (15 campers) and Mini Hawk Camp (20 campers)
- \* Soccer Shots Saturday program begins (22 participants)
- \* Body Sculpt Fitness Class begins (10 participants)
- \* Annual Girl Scout day camp over 5 days (350 kids daily) at the River and Forest Shelters with a sleepover/camp out occurring on the last night

#### Week of June 30th:

- \* Horse Camp (6 campers)
- \* Coach Nic's Hoop Camp (23 campers)

#### June Shelter Rentals:

- \* River Shelter: 11 days which included all Saturdays
- \* Forest Shelter: 10 days



### Medical Equipment Lending Program

The Community Center offers medical equipment items to community members through a free lending program. During the month of June, a client borrowed a wheelchair, a walker, crutches, a cane and a portable commode for assistance with mobility during various stages of her recovery from surgery on her leg. She was so grateful for the free lending program, she baked some very creative cupcakes to thank staff for their help. The program is available to anyone in need, and based on equipment availability.



# Parks and Recreation

## Parks Maintenance Update

- Converted half basketball court to two USAPA regulation pickleball courts
- Completed and opened bathrooms at Sofia and Town Center Parks
- Completed tall grass mowing operations per Oregon fire laws
- Completed early summer fertilizer applications at all parks
- Installed new bench and table boards on the 16 picnic tables at the River Shelter
- Successful event preparation for Arts Festival, Korean War Memorial Ceremony and Relay for Life



## Memorial Park Pickleball Courts Grand Opening



On Saturday, July 5th the first two pickleball courts in Wilsonville were dedicated in Memorial Park. Fifty four enthusiastic pickleball players were on hand for the event, and Parks and Recreation Director, Stan Sherer, performed the ceremonial ribbon cutting. The group was excited to have a dedicated court to play on in town. The group has been meeting regularly for drop in play since a clinic was held at the tennis courts on June 3rd in cooperation with representatives from the U.S. Pickleball Association. Interested parties are invited to drop in play on Tuesdays and Thursdays from 12:30 to 2:30 p.m. at the tennis/pickleball courts and all are welcome to attend.

## Upcoming Events and Programs

- \* July 11 - Movie in the Park - *Frozen* - Movie starts at dusk at River Shelter
- \* July 24 - Rotary Concert - *Johnny Limbo and the Lugnuts* - 6:30 pm at Town Center Park
- \* July 25 - Movie in the Park - *Gravity* - Movie starts at dusk at River Shelter
- \* July 31 - Rotary Concert - *Aaron Meyer* - 6:30 pm at Town Center Park
- \* August 2 - Fun in the Park - 10:00 am at Town Center Park
- \* August 7 - Rotary Concert - *Big Night Out* - 6:30 pm at Town Center Park
- \* August 8 - Movie in the Park - *The LEGO Movie* - Movie starts at dusk at River Shelter
- \* August 14 - Rotary Concert - *Tony Starlight* - 6:30 pm at Town Center Park
- \* August 22 - Movie in the Park - *Planes* - Movie starts at dusk at River Shelter

## Director's Report

Over the last two weeks SMART has been awarded two different grants from the Oregon Department of Transportation (ODOT).

The first is a *Congestion Mitigation and Air Quality Improvement (CMAQ)* grant that will allow us to improve our compressed natural gas (CNG) fueling equipment. It will make it possible for us to fuel more vehicles, do it more quickly and monitor the amounts that each vehicle is using.

SMART currently only has two small buses that run on CNG. We received Federal grant approval to purchase two more CNG buses later this year, and we expect to get at least two more in another year or two. The two CNG buses that we have had for nearly three years have proven to be very economical to operate and our passengers appear to prefer them too.

The second grant will provide the funding to allow SMART to update the City's Transit Master Plan. The current Master Plan was prepared in advance of TriMet's commuter rail service (WES) coming to Wilsonville and that Plan mostly focused on the various system changes that SMART needed to make to provide connections with WES trains. Given that WES has now been operational for more than five years, all of the WES-related changes that were going to be made have been made. It is now time for SMART to look forward to the next five years, and beyond. This will be especially important because Wilsonville continues to be one of the fastest growing cities in Oregon.

This fall will mark the completion of the Transit Integration Project, studying ways to improve the efficiency of SMART's operations between Wilsonville and Portland. The timing should work out so we can begin implementing the Integration Project along with an updated Master Plan over the next year or so.

**Stephan Lashbrook**

## NEW Farmers Market Shuttle

You can now ride a free shuttle to the Wilsonville Farmers Market beginning on July 10, 2014.

This shuttle is designed to meet up with the cross-town Route 4 to make an easy and quick transfer for folks coming from all over Wilsonville. This service is made possible as part of an existing Dial-a-Ride shift in response to increased requests to and from the Thursday Market.

If you would like help planning your trip please give us a call 503-682-7790.

[www.RideSmart.com](http://www.RideSmart.com)



*To Wilsonville Market*  
Thursdays in July and August 2014

Route 4	Route 4	Departs Shuttle	* Bus Arrives Route 4	Departs Shuttle	Departs Shuttle	Arrives Shuttle
Wilsonville Rd/Town Ctr	5th/Boones Ferry	Marquis Care	Transit Center	Transit Center	Wiedeman Sr. Ctr.	Farmers Market
3:36p	3:48p	3:50p	3:54p	4:00p	4:07p	4:11p
4:36p	4:48p	4:50p	4:54p	5:00p	5:07p	5:11p
5:36p	5:48p	5:50p	5:54p	6:00p	6:07p	6:11p
* Ride Route 4 to the Transit Center. Transfer to the Market Shuttle located at the "Villebois" bus shelter.						

*From Wilsonville Market*  
Thursdays in July and August 2014

Departs Shuttle	Arrives Shuttle	Arrives Shuttle	*Departs Route 4	Arrives Shuttle	Route 4	Route 4
Farmers Market	Wiedeman Sr. Ctr.	Transit Center	Transit Center	Marquis Care	5th/Boones Ferry	Wilsonville Rd/Town Center
5:20p	5:25p	5:31p	5:36p	5:41p	5:40p	6:06p
6:20p	6:25p	6:31p	6:36p	6:41p	6:40p	6:54p
7:20p	7:25p	7:31p	7:36p	7:41p	7:40p	7:54p
* Ride the Market Shuttle to the Transit Center. Transfer to Route 4 (eastbound or westbound).						

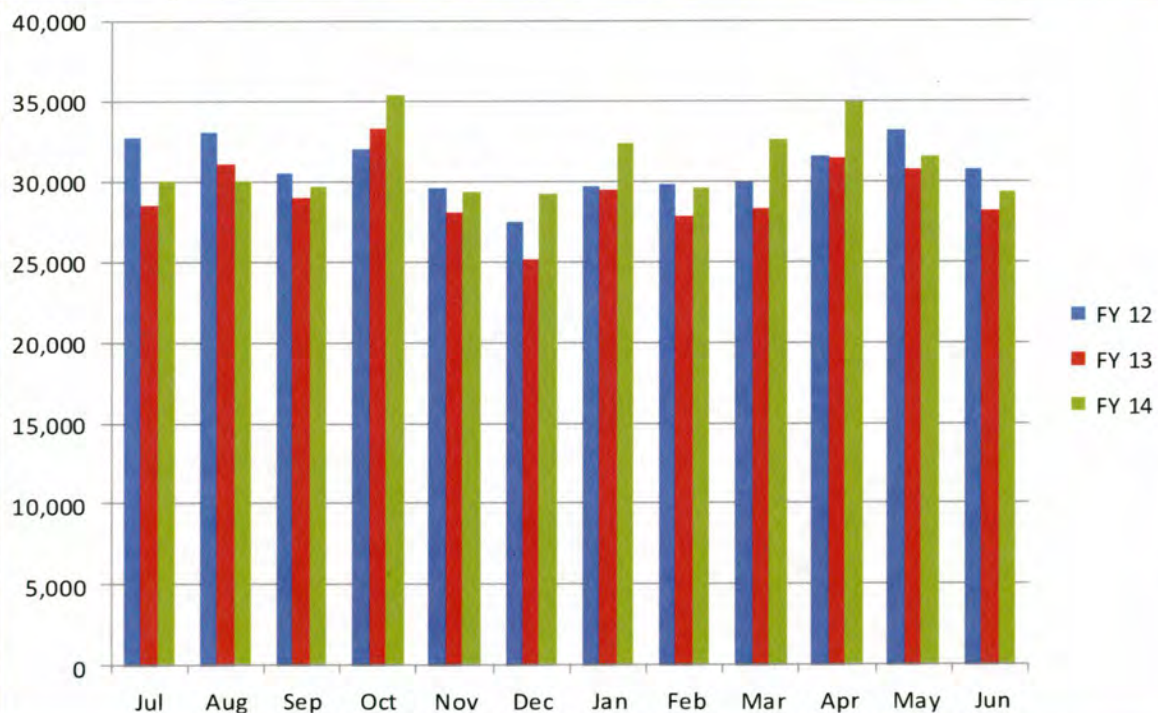
# Transit Operations Update

The end of FY14 realized a record breaking ridership year as we carried 374,408 riders. This surpassed the previous record ridership year of FY12 by 3,882 rides.

	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Total
FY 12	32,770	33,104	30,549	32,020	29,575	27,448	29,736	29,855	29,989	31,525	33,198	30,757	370,526
FY 13	28,511	31,067	29,066	33,326	28,027	25,199	29,525	27,812	28,356	31,492	30,776	28,217	351,374
FY 14	30,094	30,024	29,661	35,481	29,342	29,216	32,432	29,551	32,645	35,010	31,581	29,371	374,408

To get an understanding of the ridership of each route, the year end statistics are a good place to highlight how each route compares. The following statistics rank each route in the order of highest ridership:

	Total	General	Youth	E/D
Route 4 Crosstown	147,586	95,768	44,171	7,647
Route 2 Barbur Blvd.	83,807	68,584	9,370	5,853
Route 1X Salem	54,231	48,903	1,582	3,746
Route 6 Argyle Sq.	30,701	29,915	690	96
Route 5 95th Ave.	19,781	18,284	398	1,099
All Dial-a-Ride srvc.	17,708	317	740	16,651
Route 3 Canby	12,753	11,402	849	502
Route 7 Villebois	3,440	3,187	144	9
Route 8X Beaverton	2,372	2,287	16	69
Special Events	1,742	-	-	-
Route 9X Portland 1 trip	287	262	20	5





## New Grant Funding to Assist with Transit Master Plan Update

ODOT's Rail and Public Transit Division recently announced the selection of transit planning projects for the period of 2014 through 2017. Next steps for SMART will be to work with the Regional Transit Coordinator to develop a Scope of Work, a more detailed budget estimate, and securing staff and/or consultants for the project. SMART's project is expected kick off in Fall 2014 and be completed in 2016.

### 2014 RPTD Selected Planning Projects

Planning Grant Application					Project Award
Applicant Name	Project	Total Project Amt	Match Amt	Project Request	Award
Wilsonville, City of	Transit Master Plan Update	\$87,500	\$17,500	\$70,000	\$70,000

[www.oregon.egov.com/ODOT/PT](http://www.oregon.egov.com/ODOT/PT)

### Did You Know?

If you have a 25 minute commute, chances are you spend over 200 hours a year just commuting to and from work.

At an average speed of 200 words per minute, you could read more than 20 novels over the year in that time while riding the bus or train.

### Contact Us

**Stephan Lashbrook**  
Transit Director  
503-570-1576  
Lashbrook@ridesmart.com

**Steve Allen**  
Operations Manager  
503-570-1577  
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**Scott Simonton**  
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**Jen Massa Smith**  
Program Manager  
503-682-4523  
Massa@ridesmart.com

# Public Works

July 2014

## Annual French Prairie Dead-Wooding Program

*Roads*

This month the Roads crew partnered up with a company tree service to remove the dead-wood from the Oak trees on French Prairie Road. This is an annual event that helps promote a healthier tree. The removal of the dead-wood also makes it safer for pedestrians and vehicles traveling under the trees.

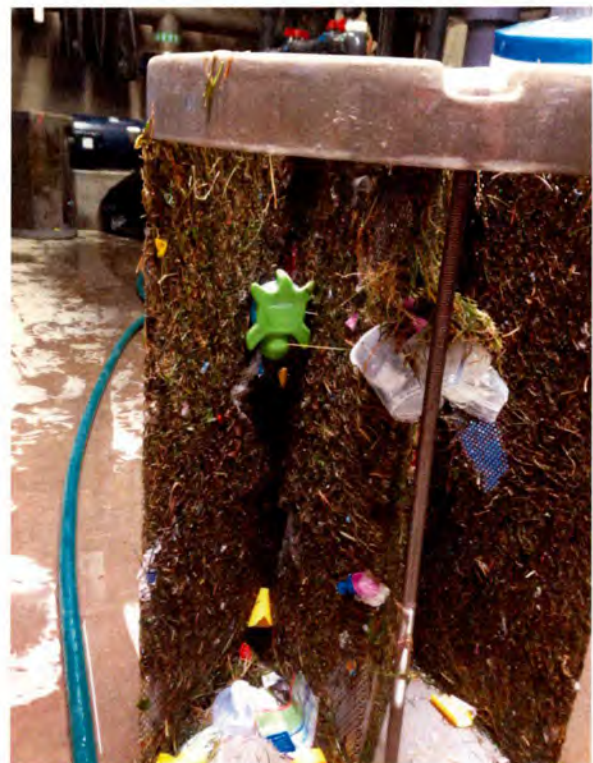


## Down the Drain at the Water Features Facilities

During the long, warm summer days many residents enjoy frolicking in the City's water features. On extra hot days, a couple of hundred people may use the features.

To ensure participants have a safe, clean and enjoyable time, staff is constantly monitoring, adjusting and cleaning the water and tanks.

Though there are screens on the drains to capture debris, as you can see below a variety of items such as toys, vegetation, cups do pass through the drains and are caught by the tank screens. The tank screens are cleared at least once a day.



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## IN THIS ISSUE

- 1 Charbonneau Tree Maintenance**
- 1 Water Features**
- 2 Hydrant Flushing**
- 2 Collection System Maintenance**
- 2 Reservoir Inspections**

### **Annual Hydrant Flushing Completed** *Utilities*

The Water Crew successfully completed the annual water main flushing task and found that the system's water mains, hydrants and valves were in good condition with only light accumulation of iron deposits in the mains.



### **Storm Water and Wastewater Collections System Maintenance** *Utilities*

The Vactor Crew was busy this month performing maintenance on above-ground storm water detention ponds, swales and outfalls while finishing annual wastewater collections system maintenance. Next month, the above-ground storm water work will continue but crews will shift from wastewater collections to begin annual cleaning of storm water catch basins.



### **Water Reservoir Inspections** *Water Distribution*

The Elligsen Reservoirs were inspected this month and found to be in good condition. The City contracts with commercial divers to inspect and sometimes, clean the interior of its reservoirs on a five-year cycle.



# **City of Wilsonville**

## **June 2014**



**Clackamas County Sheriff's Office**  
**2223 Kaen Rd**  
**Oregon City, OR 97045**

[www.co.clackamas.or.us/sheriff](http://www.co.clackamas.or.us/sheriff)

## Monthly Summary

During June 2014, the Clackamas County Sheriff's Office provided law enforcement service to the City of Wilsonville on a 24 hour a day basis. During this time period the Sheriff's Office answered 569 calls for service, which was an average of 19.0 calls per day.

The monthly average for calls for service during the past three years has been 485.5. The 569 calls in the City during the month of June reflect a 17.2% increase over the average during the last three years.

Below is a chart showing the number of calls for service in the City during the last 5 years.

<u>Year</u>	<u>Number of Calls</u>	<u>Monthly Average</u>	<u>Daily Average</u>
2009	6,273	522.8	17.2
2010	5,803	483.6	15.9
2011	5,539	461.6	15.2
2012	5,709	475.8	15.6
2013	6,230	519.2	17.1

An overall look at the shift activity reflects the following percentages of calls taken, traffic stops made and reports written for June.

	<u>Percentage of Calls Taken</u>	<u>Percentage of Traffic Stops</u>
<b>Graveyard:</b>	<b>19.9%</b>	<b>23.2%</b>
<b>Day Shift:</b>	<b>44.3%</b>	<b>38.8%</b>
<b>Swing Shift:</b>	<b>35.9%</b>	<b>38.0%</b>

During June 2014, 263 traffic stops were made in the City with the following breakdown for each shift.

	<u>Total</u>		<u>Graveyard</u>		<u>Days</u>		<u>Swing Shift</u>	
<b>Stops Made:</b>	<b>263</b>	<b>=</b>	<b>61</b>	<b>23.2%</b>	<b>102</b>	<b>38.8%</b>	<b>100</b>	<b>38.0%</b>
<b>Citations Issued:</b>	<b>161</b>	<b>=</b>	<b>17</b>	<b>10.6%</b>	<b>86</b>	<b>53.4%</b>	<b>58</b>	<b>36.0%</b>

Included in the above totals are 87 traffic stops (33.1%) and 71 citations (44.1%) issued by the Traffic Unit.

### Calls for Service

Number of Calls Per Shift	June 2014		Monthly Average 2013	
	<b>569</b>		<b>519.2</b>	
Graveyard (2100-0700)	113	19.9%	103.3	19.9%
Day Shift (0700-1700)	252	44.3%	228.4	44.0%
Swing Shift (1100-0300)	204	35.9%	187.4	36.1%
Average Number of Calls Per Day	19.0		17.1	

### Other Officer Activity

Type of Activity	June 2014	2013 Monthly Average
Bike Patrol	3	N/A
Follow-Up Contact	93	73.4
Foot Patrol	9	4.2
Premise Check	187	97.0
Subject Stop	67	31.6
Suspect Contact	4	4.3
Suspicious Vehicle Stop	75	38.5
Warrant Service	11	15.1
<b>Total:</b>	<b>449</b>	<b>264.0</b>

The chart on the following page shows the types of calls for service received during the month. These calls do not reflect actual criminal activity. In some cases the call was dispatched as a particular type of incident, but it was later determined to be of a different nature. For actual criminal activity during the month see the "Reports Written" chart.

### Types of Calls

Type of Calls	June 2014	2013 Monthly Average
Abandoned Vehicle	2	0.8
Accidents (All)	33	25.2
Alarms	49	49.8
Animal Complaint	9	8.8
Assault	3	4.1
Assist Outside Agency	10	11.6
Assist Public	29	36.7
Burglary	8	6.2
Criminal Mischief	33	14.3
Death Investigation	2	2.2
Disturbance	25	26.7
Extra Patrol Request	14	19.5
Fire Services	13	7.1
Fraud	17	11.5
Hazard	7	12.6
Juvenile Problem	17	14.4
Kidnap		.0
Mental	3	5.6
Minor In Possession		0.8
Missing Person	2	2.0
Noise Complaints	18	8.6
Open Door / Window	2	2.5
Promiscuous Shooting		1.3
Property Found / Lost / Recovered	11	13.9
Provide Information	23	27.2
Prowler	2	0.8
Recovered Stolen Vehicle	2	2.3
Robbery	1	0.9
Runaway Juvenile	2	5.3
Sexual Crime (All)	3	3.1
Shooting		0.2
Stolen Vehicle / UUMV	2	4.9
Suicide Attempt / Threat	8	6.7
Suspicious Circumstances	11	12.4
Suspicious Person	34	25.3
Suspicious Vehicle	17	11.3
Theft / Shoplift	35	37.9
Threat / Harassment / Menacing	17	14.8
Traffic Complaint	33	26.9
Unknown / Incomplete Call	14	13.3
Unwanted / Trespassing	11	10.9
Vice Complaints (Drugs)	8	6.3
Violation of Restraining Order	4	1.5
Welfare Check	29	13.5
Other Not Listed Above	6	7.7
Total:	<b>569</b>	<b>519.2</b>

### Reports Written

Type of Report	June 2014	2013 Monthly Average
Accident	Not available at this time.	15.4
Theft		30.1
Criminal Mischief		11.0
Burglary		4.5
Stolen Vehicle		3.8
Identity Theft		1.9
Assault		2.1
Drug Crimes		4.8
Miscellaneous Reports		127.9
<b>Report Totals:</b>		<b>201.5</b>

Shift Totals	June 2014	2013 Monthly Average
Graveyard Shift:		38.7 19.2%
Day Shift:		103.4 51.3%
Swing Shift:		59.4 29.5%



City of Wilsonville  
 Work Session and City Council Meeting Calendar

**ITEMS ARE TENTATIVELY SCHEDULED AND MAY BE MOVED TO ANOTHER MEETING.**

<b>JULY 16</b>	Joint Tualatin/Wilsonville City Council meeting Basalt Creek Planning 6 p.m. Police Training Room, 8650 Tualatin Rd, Tualatin.
<b>JULY 21 STAFF REPORT DUE</b>	Executive Session – Work Session <ul style="list-style-type: none"> <li>• Form Based Code (Neamtzu)</li> <li>• ODOT Right-of-way Sound wall @ Jory Trial (Neamtzu)</li> <li>• Charbonneau Consolidated Improvement Plan (Kraushaar)</li> <li>• Frog Pond Concept Plan Update (Mangle)</li> </ul>
<b>JULY 8<sup>TH</sup></b>	
Fitzgerald OOT	
	Mayor’s Business <ul style="list-style-type: none"> <li>• Proclamation Honoring 10 Years of Wilsonville Rotary Summer Concerts (Ottenad)</li> </ul>
	Communications
	Consent Agenda
	Public Hearing
	Continuing Business
	New Business

<p><b>AUGUST 4, 2014</b></p> <p><b>STAFF REPORTS DUE</b></p> <p><b>JULY 22<sup>ND</sup></b></p>          <p><b>STEVENS OOT MID AUGUST</b></p>	<p>Executive Session – Work Session</p> <ul style="list-style-type: none"> <li>• Stormwater Utility Fee Update (Kraushaar) <i>placeholder</i></li> <li>• Charbonneau Consolidated Improvement Plan (Kraushaar)</li> <li>• Public Works Standards Update (Weigle/Adams) <i>may be delayed placeholder</i></li> <li>• URA Strategic Plan (Retherford)</li> </ul> <p>Communications</p> <ul style="list-style-type: none"> <li>• Sheriff Roberts present the Sheriff’s Office accreditation certificate to the City of Wilsonville.</li> </ul> <p>Consent Agenda</p> <ul style="list-style-type: none"> <li>• TIF Zone Legal Description and Map Amendment (Retherford)</li> </ul> <p>Public Hearing</p> <ul style="list-style-type: none"> <li>• Charbonneau consolidated infrastructure plan CIP (Kraushaar)</li> <li>• </li> </ul> <p>Continuing Business</p> <p>New Business</p> <ul style="list-style-type: none"> <li>• Resolution Storm water utility rate (Kraushaar)</li> </ul> <p>City Manager’s Business</p> <ul style="list-style-type: none"> <li>• Commuter Survey Results Update</li> </ul> <p><b>URA MEETING</b> TIF Zone Legal Description and Map Amendment (Retherford)</p>
<p><b>AUGUST 18</b></p>	<p><b>No Council meeting August 18 – Summer Recess</b></p>

<p>SEPTEMBER 4</p> <p><b>THIS IS A THURSDAY</b></p> <p>STAFF REPORTS DUE AUGUST 19TH</p>	<p>Executive Session – Work Session</p> <ul style="list-style-type: none"> <li>Public Works Standards Update (Weigle/Adams)</li> </ul> <p>Communications</p> <p>Consent Agenda</p> <p>Public Hearing</p> <ul style="list-style-type: none"> <li>Ord. 1<sup>st</sup> Reading Public Works Standards Update (Weigle/Adams)</li> </ul> <p>Continuing Business</p> <ul style="list-style-type: none"> <li>Resolution No. 2471 Urban Renewal Strategic Plan (Retherford)</li> </ul> <p>New Business</p>
<p>SEPTEMBER 15</p> <p>STAFF REPORTS DUE SEPTEMBER 2<sup>ND</sup></p>	<p>Executive Session – Work Session</p> <ul style="list-style-type: none"> <li>Transit Integration Project Update – Jen Massa Smith</li> <li></li> </ul> <p>Communications</p> <p>Consent Agenda</p> <p>Public Hearing</p> <p>Continuing Business</p> <ul style="list-style-type: none"> <li>Ord. 2nd Reading Public Works Standards Update (Weigle/Adams)</li> </ul> <p>New Business</p>

Schedule joint work session with West Linn-Wilsonville School Board 5:30 – 7 p.m. late September early October

<p style="text-align: center;"><b>OCTOBER 6</b></p> <p style="text-align: center;"><b>STAFF REPORTS DUE SEPTEMBER 16</b></p>	<p>Executive Session – Work Session</p> <ul style="list-style-type: none"> <li>• Risk Management Elected Official Training (Troha)</li> </ul> <p>Communications</p> <ul style="list-style-type: none"> <li>• Community Survey Results (Cosgrove)</li> </ul> <p>Consent Agenda</p> <p>Public Hearing</p> <p>Continuing Business</p> <p>New Business</p>	<p>Schedule joint work session with West Linn-Wilsonville School Board 5:30 – 7 p.m. late September early October</p>
<p style="text-align: center;">JANUARY 2015</p>	<p style="text-align: center;">Citizen’s Academy</p>	

**Council Goals Quarterly Updates:**

**July-September**, update to be given first council meeting of **October**  
**October-December**, update to be given first Council meeting of **January**

**UNSCHEDULED ITEMS**

- CNG Bus Purchase SMART – (Lashbrook)
- Substantial Amendment for West Side URA District next 6-9 months (Retherford)
- Facilities Master Plan – (Kerber)
- Fiber Franchise (Miller/Kohlhoff)
- Amend Chapter 9, uniform mechanical, structural & fire code revisions (Brown)
- Chapter 4 & 8 Trash Enclosures (Kraushaar)

City of Wilsonville  
July 21, 2014 City Council Meeting

SPEAKER CARD

NAME: Doris Wehler

ADDRESS: 6855 SW Boeckman Rd, W

TELEPHONE: 682-0426 E-MAIL —

AGENDA ITEM YOU WANT TO ADDRESS: \_\_\_\_\_

CGP

Please limit your comments to 3 minutes. Thank you.

**City of Wilsonville  
City Council Meeting  
July 21, 2014 Sign In Sheet**

<b>Name</b>	<b>Mailing Address</b>
Ken Wise	
Doris Wehler	
Mary Stewart	

## City of Wilsonville

### July 21, 2014 City Council Meeting Action Minutes

COUNCILORS	STAFF	STAFF	STAFF
Mayor Knapp	Bryan Cosgrove	Delora Kerber	Zack Weigle
Councilor Goddard	Barbara Jacobson	Jon Gail	
Councilor Fitzgerald	Jeanna Troha	Steve Adams	
Councilor Fitzgerald – excused	Sandra King	Angela Handran	
Councilor Stevens – attended work session excused from regular meeting	Chris Neamtzu	Chris Neamtzu	
	Nancy Kraushaar	Stan Sherer	
	Katie Mangle	Barbara Jacobson	

AGENDA	ACTIONS
<b>WORK SESSION</b>	
1. Review of Agenda	1. No questions/comments
2. Councilor Concerns – During peak traffic periods vehicles turning left from Boones Ferry to go East on Wilsonville Road are blocking the intersection. Additional turn lanes needed, and enforcement of traffic code.	2. Engineering staff will begin with a traffic analysis, a request for additional law enforcement at the intersection will be made
3. Old Town Welcome Sign – include business information	3. Staff will bring back information at the August 4 <sup>th</sup> meeting.
4. A-frame warning signs at Murase water feature unattractive	4. Signs are there for safety; staff will explore options for improving the quality of the signs.
5. Coffee Creek Industrial Area Form Based Code & Pattern Book	5. Consultants and Staff introduced the Form Based Code and Pattern Book document Councilors were asked to email additional questions to staff.
6. I-5 ROW Landscaping at the Terrene at the Grove.	6. Staff presented a proposal to replace birch trees along the sound wall fronting I-5 at the Terrene with a variety of evergreen trees. Council directed staff to proceed with the project.
7. Charbonneau Consolidated Improvement Plan	7. Council continued their discussion of the projects to be included in the Plan. They suggested minor changes to the draft resolution and requested a time line be brought forward with the resolution. The June 30 <sup>th</sup> informational meeting at Charbonneau will be posted in the event a quorum of the Council attends the meeting.
8. Frog Pond Area Plan	8. Staff shared the draft vision statement and guiding principles for the development of Frog Pond. Councilors asked for the top attributes

9. Appeal and Call-up Procedures	selected by the task force.  9. Councilors were comfortable with the revisions proposed by Legal staff.
<b>REGULAR MEETING</b>	
<u>Mayor's Business</u> Proclamation declaring August as "Wilsonville Rotary/End Polio Now Month"	The Mayor read the proclamation into the record.
<u>Consent Agenda</u> • Minutes of June 2, 2014 and July 7, 2014 Council meetings	Consent Agenda adopted 3-0
<u>City Manager's Business</u>	There was no report.
<u>Legal Business</u>	There was no report.
Adjourn	8:36 p.m.

**RECORDED BY: SCK**