

**AGENDA**

**WILSONVILLE CITY COUNCIL MEETING  
JUNE 6, 2016  
7:00 P.M.**

**CITY HALL  
29799 SW TOWN CENTER LOOP  
WILSONVILLE, OREGON**

Mayor Tim Knapp

Council President Scott Starr  
Councilor Susie Stevens

Councilor Julie Fitzgerald  
Councilor Charlotte Lehan

**CITY COUNCIL MISSION STATEMENT**

To protect and enhance Wilsonville’s livability by providing quality service to ensure a safe, attractive, economically vital community while preserving our natural environment and heritage.

**Work and Executive Sessions are held in the Willamette River Room, City Hall, 2<sup>nd</sup> Floor**

- 5:00 P.M. EXECUTIVE SESSION [15 min.]**
  - A. Pursuant to ORS 192.660 (2)(e) Real Property Transactions
  - ORS 192.660(2)(f) Exempt Public Records
  - ORS 192.660(2)(h) Litigation
  
- 5:15 P.M. REVIEW OF AGENDA [5 min.]**
  
- 5:20 P.M. COUNCILORS’ CONCERNS [5 min.]**
  
- 5:25 P.M. PRE-COUNCIL WORK SESSION**
  - A. Declaration of Emergency Exercise (Kerber) [10 min.]
  - B. Water Treatment Plant Master Plan Update (Kerber/Mende) [15 min.] Page 4
  - C. Wilsonville Road Kinsman Road Truck Turning Radius Results [10 min.]  
(Kraushaar/Weigel)
  - D. Ozone Coalition Letter (Mende) [5 min.] Page 5
  - E. Basalt Creek Update (Kraushaar) [25 min.] Page 6
  
- 6:50 P.M. ADJOURN**

**CITY COUNCIL MEETING**

The following is a summary of the legislative and other matters to come before the Wilsonville City Council a regular session to be held, Monday, June 6, 2016 at City Hall. Legislative matters must have been filed in the office of the City Recorder by 10 a.m. on May 31, 2016. Remonstrances and other documents pertaining to any matters listed in said summary filed at or prior to the time of the meeting may be considered therewith except where a time limit for filing has been fixed.

**7:00 P.M. CALL TO ORDER**

- A. Roll Call
- B. Pledge of Allegiance
- C. Motion to approve the following order of the agenda and to remove items from the consent agenda.

**7:05 P.M. MAYOR'S BUSINESS**

- A. Tourism Committee Appointments
  - 1. Position #1 term ends 6/30/19 – Kathleen Mason, Aurora Colony Historical Society
  - 2. Position #2 term ends 6/30/19 – Elaine Swyt, Mediaphysics; Wilsonville Parks & Recreation Advisory Board

- B. Upcoming Meetings Page 15

**7:15 P.M. COMMUNICATIONS**

- A. Washington County Visitors Association Report, Carolyn McCormick, CEO
- B. Walk Friendly Communities Bronze Level Designation (staff – Neamtzu) Page 16

**7:30 P.M. CITIZEN INPUT & COMMUNITY ANNOUNCEMENTS**

This is an opportunity for visitors to address the City Council on items *not* on the agenda. It is also the time to address items that are on the agenda but not scheduled for a public hearing. Staff and the City Council will make every effort to respond to questions raised during citizens input before tonight's meeting ends or as quickly as possible thereafter. Please limit your comments to three minutes.

**7:35 P.M. COUNCILOR COMMENTS, LIAISON REPORTS & MEETING ANNOUNCEMENTS**

- A. Council President Starr – (Park & Recreation Advisory Board Liaison)
- B. Councilor Fitzgerald – (Development Review Panels A & B Liaison)
- C. Councilor Stevens – (Library Board and Wilsonville Seniors Liaison)
- D. Councilor Lehan– (Planning Commission and CCI Liaison)

**7:45 P.M. CONSENT AGENDA**

- A. **Resolution No. 2583** Page 24  
A Resolution Of The City Of Wilsonville Authorizing The City Manager To Execute An Addendum To The Frog Pond Area Plan Contract With Angelo Planning Group (staff – Neamtzu)

**7:50 P.M. PUBLIC HEARINGS**

- A. **Ordinance No.792** – 1<sup>st</sup> reading Page 43  
An Ordinance Of The City Of Wilsonville **Annexing** Approximately 10.2 Acres Of Territory Including A Segment Of SW Garden Acres Road Right-Of-Way And An Adjacent Parcel Of Land East Of SW Garden Acres Road North Of SW Ridder Road And The Current Republic Services Development Into The City Limits Of The City Of Wilsonville, Oregon. The Territory Is More Particularly Described As Tax Lot 600 Of Section 2C, T3S, R1W, Washington County, Oregon And Adjacent Right-Of-Way, Willamette Resource Incorporated, Petitioner.
- B. **Ordinance No. 793** – 1<sup>st</sup> Reading Page 60  
An Ordinance Of The City Of Wilsonville Approving A **Comprehensive Plan Map Amendment** From The Washington County Future Development – 20 (FD-20) District To The City Of Wilsonville Industrial Designation On Approximately 10.2 Acres Comprising Tax Lot 600 Of

Section 2C, T3S, R1W, Washington County, Oregon And Adjacent Right-Of-Way, Willamette Resource Incorporated, Applicant/Owner.

- C. **Ordinance No. 794** – 1<sup>st</sup> Reading Page 78  
An Ordinance Of The City Of Wilsonville Approving A **Zone Map Amendment** From The Washington County Future Development-20 (FD-20) Zone To The Planned Development Industrial-Regionally Significant Industrial Area (PDI-RSIA) Zone On Approximately 10.2 Acres Comprising Tax Lot 600 Of Section 2C, T3S, R1W, Washington County, Oregon And Adjacent Right-Of-Way, Willamette Resource Incorporated, Applicant/Owner.

**8:30 P.M. CONTINUING BUSINESS**

- A. **Ordinance No. 790** – 2<sup>nd</sup> reading Page 96  
An Ordinance Of The City Of Wilsonville Approving A **Comprehensive Plan Map Amendment** From Residential 0-1 Dwelling Units Per Acre To Residential 4-5 Units Per Acre On Approximately 4.37 Acres Located At 28500 And 28530 SW Canyon Creek Road South – Comprising Tax Lots 900 And 1000 Of Section 13b, Township 3 South, Range 1 West, Clackamas County, Oregon, Beth Ann Boeckman And Karen And Marvin Lewallen – Owners, Scott Miller, SAMM-MILLER LLC – Applicant.
- B. **Ordinance No. 791** – 2<sup>nd</sup> Reading Page 119  
An Ordinance Of The City Of Wilsonville Approving A **Zone Map Amendment** From The Residential Agriculture-Holding (Ra-H) Zone To The Planned Development Residential-3 (Pdr-3) Zone On Approximately 4.37 Acres Located At 28500 And 28530 SW Canyon Creek Road South-Comprising Tax Lots 900 And 1000 Of Section 13b, Township 3 South, Range 1 West, Clackamas County, Oregon, Beth Ann Boeckman And Karen And Marvin Lewallen – Owners. Scott Miller, SAMM-MILLER LLC – Applicant.
- C. **Ordinance No. 789** – 2<sup>nd</sup> Reading Page 137  
An Ordinance Of The City Of Wilsonville Approving A Minor Amendment To Wilsonville’s 2013 Transportation Systems Plan (2016 **TSP Amendment**) (Staff – Mende)  
*The Planning Commission Record is a large document and is available for review in the office of the City Recorder.*

**9:00 P.M. CITY MANAGER’S BUSINESS**

**9:10 P.M. LEGAL BUSINESS**

**9:15 P.M. ADJOURN**

Time frames for agenda items are not time certain (i.e. Agenda items may be considered earlier than indicated. The Mayor will call for a majority vote of the Council before allotting more time than indicated for an agenda item.) Assistive Listening Devices (ALD) are available for persons with impaired hearing and can be scheduled for this meeting if required at least 48 hours prior to the meeting. The city will also endeavor to provide the following services, without cost, if requested at least 48 hours prior to the meeting:-Qualified sign language interpreters for persons with speech or hearing impairments. Qualified bilingual interpreters. To obtain services, please contact the City Recorder, (503)570-1506 or [king@ci.wilsonville.or.us](mailto:king@ci.wilsonville.or.us)



## **CITY COUNCIL WORKSESSION INFORMATION ITEM**

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### Water Treatment Plant Master Plan Update

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Meeting Date: June 06, 2016  
Report Date:  
Source of Item: Engineering / CD

Contact: Eric Mende  
Contact Telephone Number: 570-1538  
Contact E-Mail: mende@ci.wilsonville.or.us

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### **BACKGROUND**

Capital Projects Engineering Manager Eric Mende and Public Works Director Delora Kerber will provide a short powerpoint presentation on the current status of the Water Treatment Plant MASTER PLAN. Discussion issues will include: General Status; Blue Ribbon Panel results; alternatives for future Water Quality Goals and Operating Strategy; and an update on the Willamette Governance Group process.

### **POLICY ISSUES**

In order to finalize the Master Plan, Council input is required on Water Quality Level of Service Goals and Operating Strategy for future expansion of the existing Water Treatment Plant.

### **ATTACHMENTS**

A. None



June 6, 2016

Ms. Kari Salis  
Manager, Technical Services – Region 1  
Oregon Drinking Water Protection Program  
800 NW Oregon St., Suite 640  
Portland, OR 97232-2162

Subject: Oregon Water Utility Council's Ozone Coalition and Petition for a Rule Change

Dear Ms. Salis,

On behalf of the City of Wilsonville, I would like to express our support for the rule change petition being submitted by the Oregon Water Utilities Council - Ozone Coalition. The petition seeks to change a rule in the current drinking water regulations, eliminating the requirement for 0.5-log inactivation of *Giardia* and 1.0-log inactivation of viruses downstream of filtration if required disinfection can be achieved through the use of ozonation upstream of filtration.

The City's multi-barrier Willamette River Water Treatment Plant (WRWTP) is one of the few plants in Oregon which currently uses ozone, and our plant has used ozone continuously since the plant went on-line in 2002. Ozone is a powerful disinfectant, and we believe the requested rule change meets all EPA SDWA and LT2 requirements and will not increase any public health risks with respect to overall disinfection performance. The use of ozone also provides many other water quality and treatment benefits to our residents, including:

- Reduction of challenging tastes and odors, especially for algal by-products such as MIB and geosmin;
- Reduction of specific algal toxins such as microcystin, anatoxin-a and cylindrospermopsin, and many other regulated organic compounds;
- Reduction of a variety of contaminants of emerging concern (CECs) including pharmaceuticals and personal care products (PPCPs);
- Improved filtration performance in granular media filters.

For Wilsonville, the rule change will have positive long-term net benefits to our rate-payers by reducing both capital and long-term operational costs and improving overall water quality. With this change, the WRWTP can eliminate a planned capital improvement for an expanded post-filter chlorine contact basin (and/or expensive baffling system), and we will see higher water quality and decreased operational costs through the reduced use of chlorine and other chemicals.

In summary, we believe the rule change is both protective of public health and economically desirable, and represents an excellent opportunity for OHA to partner with the regulated community on a rule change that makes sense for the long term. We urge OHA to adopt the proposed rule change at your earliest convenience. Thank you in advance.

Tim Knapp, Mayor  
City of Wilsonville



**CITY COUNCIL MEETING  
WORK SESSION – STAFF REPORT**

<b>Meeting Date:</b> June 6, 2016		<b>Subject:</b> Basalt Creek Concept Plan Update <b>Staff Member:</b> Nancy Kraushaar, PE, Community Development Director and Miranda Bateschell, Long Range Planning Manager  <b>Department:</b> Community Development	
<b>Action Required</b>		<b>Advisory Board/Commission Recommendation:</b> N/A	
<input type="checkbox"/> Motion <input type="checkbox"/> Public Hearing Date: <input type="checkbox"/> Ordinance 1 <sup>st</sup> Reading Date: <input type="checkbox"/> Ordinance 2 <sup>nd</sup> Reading Date: <input type="checkbox"/> Resolution <input checked="" type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input type="checkbox"/> Council Direction <input type="checkbox"/> Consent Agenda		<input type="checkbox"/> Approval <input type="checkbox"/> Denial <input type="checkbox"/> None Forwarded <input checked="" type="checkbox"/> Not Applicable  <b>Comments:</b> The DRAFT Ten Considerations for Success are also being presented to the Tualatin City Council by their staff at their June 13, 2016 meeting.	
<b>Staff Recommendation:</b> Receive update on Basalt Creek Concept Plan including Ten Considerations for Success and provide input on desired employment types.			
<b>Recommended Language for Motion:</b> N/A			
<b>Project / Issue Relates To:</b> <i>[Identify which goal(s), master plans(s) your issue relates to.]</i>			
<input checked="" type="checkbox"/> Council Goals/Priorities Thoughtful Land Use; Economic Development		<input type="checkbox"/> Adopted Master Plan(s)	<input type="checkbox"/> Not Applicable

**ISSUE BEFORE COUNCIL:** Receive update, discuss progress, and provide input on the Basalt Creek Concept Plan.

**EXECUTIVE SUMMARY:** Since the December 16, 2015 Joint Wilsonville and Tualatin City Councils Work Session, staff from both cities have been working together to clarify the “Ten Considerations for Success” that were discussed at the work session and how to incorporate them into the concept plan. In addition, staff is refining assumptions for land use and employment types within the future Wilsonville boundary with a Basalt Creek Parkway jurisdictional

boundary. It is important for the City Council to be updated, discuss, and provide input on these activities.

**EXPECTED RESULTS:** With direction from Council, staff will finalize the Ten Considerations for Success and the land use and employment types in the Concept Plan.

**TIMELINE:** With these final refinements, the project team will draft the Concept Plan for Council review this fall.

**CURRENT YEAR BUDGET IMPACTS:**

None. None. The City of Tualatin received approximately \$350K from Metro's Construction Excise Tax (CET) grant program to perform concept planning. The City of Wilsonville has, and will continue to, invest staff time into the process.

**FINANCIAL REVIEW / COMMENTS:**

Reviewed by: \_\_n/a\_\_ Date: \_\_n/a\_\_

**LEGAL REVIEW / COMMENT:**

Reviewed by: \_\_n/a\_\_ Date: \_\_n/a\_\_

**COMMUNITY INVOLVEMENT PROCESS:**

The project includes participation from affected residents, businesses, and property owners. Citizens will be asked to share ideas about the preferred land use alternative at a Public Open House. Additionally, the website is updated to reflect the most recent work and staff sends out monthly updates to an interested parties list and property owners via email and U.S. postal mail.

**POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY** (businesses, neighborhoods, protected and other groups):

The Basalt Creek area is important for the long-term growth of Tualatin, Wilsonville, and the Metro region. Conducting a thorough and thoughtful planning process will identify and resolve each city's vision for the area and potential impacts on the community. The Basalt Creek area presents an opportunity to maximize assessed property value, integrate jobs and housing, develop efficient transportation and utility systems, create an attractive residential and business community, incorporate natural resource areas, and provide recreational opportunities as community amenities and assets.

**ALTERNATIVES:** n/a

**CITY MANAGER COMMENT:**

**ATTACHMENTS:**

1. DRAFT Ten Considerations for Success to be Incorporated into the Basalt Creek Concept Plan
2. Basalt Creek Concept Plan – Proposed Boundary (from December 16, 2015 Joint Council Work Session)

**TEN CONSIDERATIONS FOR SUCCESS TO BE INCORPORATED  
INTO THE BASALT CREEK CONCEPT PLAN**

**May 25, 2016 DRAFT with changes from May 2, 2016 meeting between Wilsonville,  
Tualatin and consultant team staffs**

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The Cities agree to cooperatively work on the next phases of the Basalt Creek Concept Plan, including public involvement and integrating the elements that the Cities have identified as considerations for success.

These considerations for success were discussed at the December 2015 Tualatin and Wilsonville Joint City Council Work Session as part of the boundary selection. These considerations, as generally described below, will be incorporated into the Concept Plan as it is completed.

The Cities understand these considerations are a starting point and will be subject to further review by both cities. The Cities will work in good faith to carry out the intent of these elements with the opportunity to modify and refine them as needed as the concept plan is completed.

1. **Sewer.** Each City will serve its own jurisdiction area independently, to the extent reasonably possible with the understanding that future agreements may address potential cooperative areas. (WILSONVILLE AND TUALATIN STAFF AGREE ON THIS LANGUAGE.)

**Deliverable needed to implement this Consideration: Narrative in the Concept Plan and a service map. A sentence or acknowledgement in the Concept Plan that if in the future shared services are deemed to be needed, the Cities will cooperatively review and discuss the need at that time.**

**Team Assigned to work on this Deliverable: Consultant, Steve, Jeff, and Andy Braun.**

2. **Storm water.** The Cities and Clean Water Services (CWS) acknowledge that they must follow requirements established in their respective Storm water MS4 permits. Much of the area is in a basin that drains toward Wilsonville. Each City will serve its own jurisdiction area independently. The Cities and Clean Water Services will adopt an Intergovernmental Agreement that addresses areas where cooperative storm water management is needed. (TUALATIN, WILSONVILLE AND CLEAN WATER SERVICES STAFF AGREE ON THIS LANGUAGE.)

**Deliverable needed to implement this Consideration: Narrative in the Concept Plan and service map. An IGA between Wilsonville, Tualatin and Clean Water Services before the first annexation in the Basalt Creek Area.**

**Team Assigned to work on this Deliverable: Consultant, Steve, Kerry Rappold, Jeff, and Andy Braun.**

- 3. Industrial Lands.** The Basalt Creek Concept Plan area is already mapped and identified as an “Industrial Area” in Metro’s Title 4 Code. This designation will need to be removed from residential land already identified in the northern portion of the Basalt Creek area upon adoption of the Concept Plan, to allow for the residential land use buffer between Tualatin’s current southern boundary and the Basalt Creek employment area. Recognizing the regional need for employment land, all remaining Title 4 “Industrial areas” should remain available for industrial development and land uses into the future. (WILSONVILLE AND TUALATIN STAFF AGREE ON THIS LANGUAGE.)

**Deliverable needed to implement this Consideration: Narrative in the Concept Plan, recognizing the current Title 4 mapping and the need to remove the Title 4 designation for those lands on the north part of Basalt Creek, adjacent to current Tualatin, that will serve as a residential buffer.**

**Team Assigned to work on this Deliverable: Miranda, Aquilla and Karen Fox to determine the sequence needed for these activities and then will provide direction to the consultant team.**

- 4. Transportation Funding.** The Cities acknowledge that significant improvements will be needed to the existing and future transportation network in the Basalt Creek Concept Plan area. In order to achieve the vision established by the Cities and Washington County in the 2013 Basalt Creek Transportation Refinement Plan (TRP), particularly given the impacts of regional traffic from the Basalt Creek Parkway, Tualatin and Wilsonville agree to begin working together now to prioritize those network improvements. The Cities acknowledge that success of the Basalt Creek Concept Plan area depends on being served by an adequate transportation system as identified in the TRP. The Cities will work together to identify a cooperative funding strategy that considers local, county, regional, state, and federal funding tools. (WILSONVILLE AND TUALATIN STAFF AGREE ON THIS LANGUAGE.)

**Deliverable needed to implement this Consideration: Narrative in the Concept Plan to acknowledge the need for a cooperative funding strategy between Wilsonville, Tualatin and Washington County. The Concept Plan narrative will acknowledge the general sequence of transportation improvements needed to implement the build-out of the Plan, as identified in the Plan and also the Transportation Refinement Plan. The cooperative funding strategy will follow the Concept Plan and will identify funding sources and a strategy for building out the transportation network. The Cooperative Funding Strategy needs to be completed prior to the first annexation in the Basalt Creek area.**

**Team Assigned to work on this Deliverable: Consultant, Alice, Nancy, Andrew Singelakis, Zoe Monahan and other staff, as assigned by Nancy, Alice and Andrew Singelakis . The team will also include a Metro staff member. Alice will talk to Elissa Gertler or Andy Cotugno about this.**

5. **Future Regional Transportation Projects in the Basalt Creek Area.** To maintain the integrity of the transportation network in this employment area, the Cities will also work cooperatively to evaluate future regional transportation projects and decisions, beyond those identified in the TRP, which could direct additional traffic to the Basalt Creek Concept Plan Area. These projects will be evaluated to ensure that system capacity and adequate regional funding is available for needed improvements to mitigate additional regional traffic. (WILSONVILLE AND TUALATIN STAFF AGREE ON THIS LANGUAGE. TUALATIN AND WILSONVILLE STAFF ARE STILL DETERMINING THE BEST WAY TO IMPLEMENT THIS SECTION. THIS SECTION SHOULD BE DISCUSSED WITH ANDREW SINGELAKIS FOR HIS AWARENESS ONCE BOTH CITY COUNCILS HAVE AGREED TO THE LANGUAGE AT TUALATIN AND WILSONVILLE'S RESPECTIVE WORK SESSIONS.)
6. **Trips.** To date, the Cities have developed a mixture of land uses that do not exceed the trip total outlined in the TRP. The Cities will retain and maintain land uses that are consistent with the Concept Plan. Any land proposed for a change in land use designation should be reviewed for impacts to the transportation systems. (TUALATIN AND WILSONVILLE STAFF ARE STILL WORKING ON LANGUAGE FOR THIS SECTION AND DETERMINING THE BEST WAY TO IMPLEMENT THIS SECTION. NANCY K. TOLD ALICE C. ON MAY 20, 2015 THAT WILSONVILLE WISHES TO RE-EVALUATE THEIR LAND USES AND JOB TYPES IN THE ENVISION MODEL TO ENSURE THAT THEY HAVE CAPTURED USES THAT FIT THE CITY'S DESIRED FUTURE INDUSTRIAL LAND USE AND DEVELOPMENT TRENDS. WILSONVILLE WANTS TO HAVE A WORK SESSION WITH LELAND CONSULTING GROUP TO DISCUSS THIS FURTHER. ALICE C. ASKED NANCY K. TO INCLUDE KAREN AND AQUILLA IN THIS MEETING. THIS MEETING WITH LELAND CONSULTING GROUP MAY OR MAY NOT OCCUR BEFORE WILSONVILLE AND TUALATIN'S WORK SESSIONS WITH THEIR CITY COUNCILS ON JUNE 6<sup>TH</sup> AND JUNE 13<sup>TH</sup>).
7. **Basalt Creek Parkway and I-5 Crossings.** The Cities acknowledge that the Basalt Creek Parkway and I-5 crossings identified in the TRP will become critical to successful industrial and employment growth in the Basalt Creek Planning Area. The Cities agree to jointly seek a timely regional investment in these crossings to achieve regional industrial objectives. (WILSONVILLE AND TUALATIN STAFF AGREE ON THIS LANGUAGE.)

**Deliverable needed to implement this Consideration: Narrative in the Concept Plan. The consultants can refer back to the TRP for guidance. This will also be addressed in the Cooperative Funding Strategy. The funding strategy will be completed prior to the first annexation in the Basalt Creek area.**

**Team Assigned to work on this Deliverable: Consultant to take the lead on the narrative. Nancy, Alice, Zoe and Andrew Singelakis and other assigned staff will incorporate this section into the Cooperative Funding Strategy, to be completed prior to the first annexation in the Basalt Creek area.**

8. **North-South Collector (Kinsman Road).** An extension of Kinsman Road, designated as a proposed collector north of Day Road and between Grahams Ferry Road and Basalt Creek Canyon should be evaluated for efficient use of developable land and limited transportation funds. The Cities will review whether eliminating that roadway and associated costs may provide better transportation results by investing in other TRP improvements. (WILSONVILLE AND TUALATIN STAFF AGREE ON THIS LANGUAGE.)

**Deliverable needed to implement this Consideration: Analysis from transportation consultant to measure the transportation network performance when assuming the removal of the Kinsman Road collector. Summary of results of transportation consultant analysis and summary of overall issue in the Concept Plan.**

**Team Assigned to work on this Deliverable: Consultant team with review of transportation consultant analysis report by the Basalt Creek PMT.**

9. **Basalt Creek Canyon.** The Cities recognize the Basalt Creek Canyon natural resource value and will work together to reach agreement on joint management practices for the canyon. The Cities also recognize the benefits of locating north to south trails near the Basalt Creek Canyon and bicycle connections that would connect the cities and other trail systems and be an asset for both residents and employees in the area. (WILSONVILLE AND TUALATIN STAFF AGREE ON THIS LANGUAGE)

**Deliverable needed to implement this Consideration: City Councils had originally talked about an IGA. Nancy and Alice are proposing that it would be better to develop and list Joint Management Practices for protection of the Canyon in the Concept Plan. Then the Concept Plan should also acknowledge all existing regulations that would implement these Joint Management Practices into the future. The Concept Plan should also acknowledge that one of the values of this canyon is to allow for public access to the area in appropriate locations in order to serve the bicycle, pedestrian and recreational needs of the area.**

**Team Assigned to work on this Deliverable: Consultant, Miranda, Kerry Rappold, Aquilla, Karen Fox, Jeff and Andy Braun**

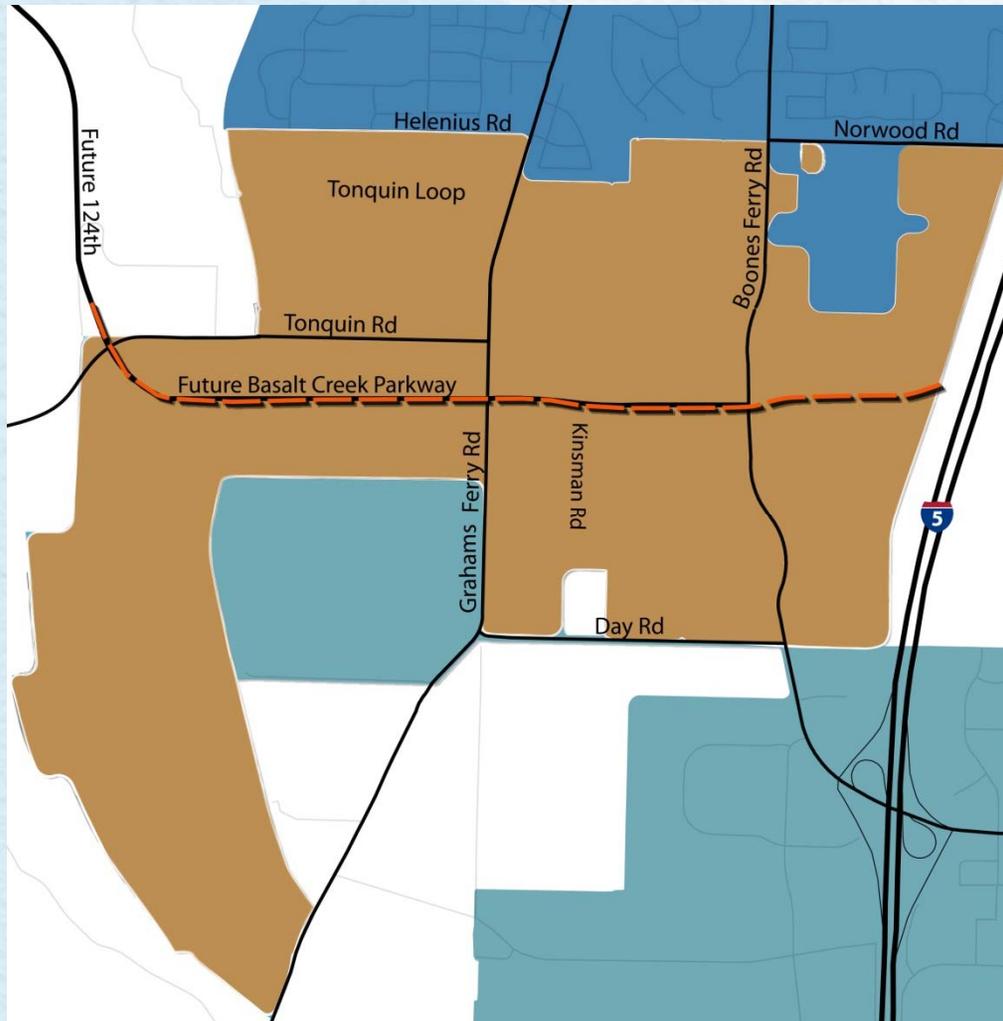
10. **Public Transportation.** Robust transit services are critical to the high-quality employment envisioned in Basalt Creek. The Cities support SMART service in the City of Wilsonville, including all land to be annexed into Wilsonville. The Cities agree to coordinate efforts on how the two transit providers' (SMART and TriMet) can best provide service throughout the area. (WILSONVILLE AND TUALATIN STAFF AGREE ON THIS LANGUAGE)

**Deliverable needed to implement this Consideration: Nancy and Stephan Lashbrook will think about the deliverable needed for this section and get back to Alice and team.**

**Team Assigned to work on this Deliverable: TBD**

DRAFT

# Proposed Boundary



May 23, 2016

Andy Duyck, Chair  
Roy Rogers, District Three Commissioner  
Board of Commissioners  
Washington County  
155 N First Ave, Suite 350  
Hillsboro, OR 97124-3072



**RE: Comments on the MSTIP 3e 150% Project List**

Dear Chair Duyck, Commissioner Rogers and members of the Board of Commissioners:

The City of Wilsonville strongly supports inclusion of the only two MSTIP 3e projects in South Washington County of the 34 listed in the 150% Project List. The two proposed projects—Tualatin-Sherwood Road improvements and Basalt Creek Parkway extension—are crucial for transportation mobility to serve the growing Tualatin-Sherwood-Wilsonville area.

Several thousand acres of urbanizing lands—including the Coffee Creek and SW Tualatin regionally significant industrial areas and Basalt Creek and Tonquin employment areas—require substantial public investment to underwrite critical infrastructure to advance development. Building and improving transportation resources are key to creating a workable urban landscape that accommodates residents and commuting workers and the movement for freight.

1. **The Tualatin-Sherwood Road improvement project**, from Teton Avenue to Langer Farms Parkway, widens over two miles of the current road to five lanes, with bike lanes, sidewalks and bus stops and shelters at an estimated cost of \$31.5 million. This project would complement MSTIP 3d funding for improvements on Tualatin-Sherwood/Roy Rogers Roads near Hwy 99W, provide transit amenities for the new Tri-Met bus line, and offer potential cost-savings in conjunction with the Willamette Water Supply Program pipeline colocation opportunity (west of 124th Ave).
2. **The Basalt Creek Parkway Extension project**, from Grahams Ferry Road to Boones Ferry Road, funds right-of-way acquisition and design development for a new half-mile-long, five-lane roadway, with bike lanes and sidewalks. A total of \$3 million MSTIP 3e funding would complement prior federal funding for environmental analysis and design, and could serve as a potential future local match for outside funding for construction.

Washington County is a key partner with local governments to leverage additional funding from regional, state and federal sources. We urge inclusion of these projects in the final 100% list.

Sincerely,

A handwritten signature in blue ink that reads "Tim Knapp".

Tim Knapp, Mayor  
Wilsonville City Council

cc: City Councils and City Managers of the Cities of Tualatin, Sherwood and Wilsonville;  
Metro Councilor Craig Dirksen

**CITY COUNCIL ROLLING SCHEDULE****Board and Commission Meetings 2016****Items known as of 05/31/16**

## JUNE

DATE	DAY	TIME	EVENT	LOCATION
6/1	Wednesday	6 P.M.	Budget Committee Meeting	Council Chambers
6/6/	Monday	7 p.m.	City Council Meeting	Council Chambers
6/8	Wednesday	6 p.m.	Planning Commission	Council Chambers
6/8	Wednesday	6 p.m.	Budget Committee Meeting if necessary	Willamette River I & II
6/9	Thursday	4:30 p.m.	Parks and Recreation Board	Parks and Recreation Administration Building
6/13	Monday	6:30 p.m.	DRB Panel A	Council Chambers
6/20	Monday	7 p.m.	City Council Meeting	Council Chambers
6/22	Wednesday	6:30 p.m.	Library Board	Library
6/27	Monday	6:30 p.m.	DRB Panel B	Council Chambers

## COMMUNITY EVENTS

**Wilsonville Farmers Market** – Thursdays starting May 19<sup>th</sup>  
4-6 P.M. Sofia Park In Villebois

**Town Center Park and Murase Plaza Water Features**

Water features will be activated for the season June 1<sup>st</sup>. They are open daily from 10 a.m. to 8 p.m.

**Proposed Recreation and Aquatic Center Open House****Wednesday, June 29**

Community members are invited to learn about the proposed recreation and aquatic center at a public Open House on Wednesday, June 29, 6:00-7:30 p.m., at the Parks and Recreation Admin. Building in Town Center Park, 29600 SW Park Place in Wilsonville. City leaders will share information about the proposed facility and available to answer questions from the public. At the open house community members are also encouraged to share their feedback about the proposed center.



**Wilsonville, OR**  
**BRONZE LEVEL 2016-2021**

[www.walkfriendly.org](http://www.walkfriendly.org)

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**Cosgrove, Bryan**

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**From:** Kraushaar, Nancy  
**Sent:** Friday, May 13, 2016 4:34 PM  
**To:** Cosgrove, Bryan  
**Cc:** King, Sandy; Neamtzu, Chris  
**Subject:** FW: Your Walk Friendly Re-designation  
**Attachments:** WFC Promotion Guide 2016.docx; WFC Talking Points Fall 2016.docx; wfc\_ad2\_wilsonvilleOR.jpg; wfc\_ad3\_wilsonvilleOR.jpg; wfc2012\_bronze.jpg; wfc2012\_bronze\_sm.jpg; bronze.png; bronze\_big.png; Walk Friendly Communities Announcement Release 10-16 LEGAL APPROVED DRAFT.docx

Bryan: It would be nice to include this in some way at a City Council meeting. Especially given the Council goals for neighborhoods connectivity for active transportation. Where can we fit it in? Thanks. -Nancy

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**From:** Neamtzu, Chris  
**Sent:** Friday, May 13, 2016 12:40 PM  
**To:** Kraushaar, Nancy; Cosgrove, Bryan  
**Cc:** Ward, Mike; Adams, Steve; Mende, Eric; Weigel, Zach; Ottenad, Mark; Gail, Jon; Massa Smith, Jen; Lashbrook, Stephan; Pauly, Daniel; Scola, Jennifer; Randall, Connie; Randall, Connie; Brown, Martin; Bateschell, Miranda; Bergeron, Tami  
**Subject:** FW: Your Walk Friendly Re-designation

Nancy,

I am pleased to announce that the city has been re-designated by Walk Friendly Communities as a BRONZE level award winner (same level as previous designation). This designation is for the next 5-years and was awarded based on a comprehensive resubmittal of detailed programs and projects that have been accomplished over the past five years since the original award that promote multi- modality community wide.

The entire community development team should be very proud of this recognition. We are one of 7 cities across the country with a current WF designation. This is no light weight recognition.

Great work to all!

**Chris Neamtzu, AICP**  
Planning Director  
City of Wilsonville | Community Development Department  
503-570-1574 | [neamtzu@ci.wilsonville.or.us](mailto:neamtzu@ci.wilsonville.or.us)

DISCLOSURE NOTICE: Messages to and from this Email address may be subject to the Oregon Public Records Law.

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**From:** Walk Friendly Info [<mailto:info@walkfriendly.org>]  
**Sent:** Friday, May 13, 2016 9:20 AM  
**To:** Neamtzu, Chris  
**Subject:** Your Walk Friendly Re-designation

Hi Chris

Thank you for your patience as we compiled our reviews and feedback for the new communities.

I'm pleased to let you know that we are re-designating Wilsonville as a Bronze level Walk Friendly Community for the next five years. Given the long wait, I'm sure you may be anxious to announce, but please do not release any information until Thursday, May 19.

To help you market this accomplishment, we are providing you with the following tools:

- Template press release: this can be personalized to your community and then distributed to local media outlets and through your city website
- Talking points: this will help you answer some of the questions you may receive from local media, and provide some background on the program and its development
- Promotion guide: some suggestions for things you can do to promote your successes in creating a safe and friendly community for pedestrians
- Logos and banner ads: to be used in publications, newsletters, etc. -- please let me know if a unique design opportunity comes up (sign, billboard, bus ad, ad in paper, etc..) and we'll be happy to design something for you at no cost

We will also prepare a report, outlining your strengths as well as areas that can be improved. **Please let us know if you have a format or audience that would be most useful for us to tailor our feedback towards.**

If you have questions or comments related to media materials and announcements, please contact James Gallagher (919-843-7007, [james@hsrc.unc.edu](mailto:james@hsrc.unc.edu)). For anything else, feel free to email us at [info@walkfriendly.org](mailto:info@walkfriendly.org) or call Carl Sundstrom at 919-843-4963.

Thanks!

Carl Sundstrom  
UNC Highway Safety Research Center



# Walk Friendly Communities

## *Walk Friendly Communities Promotion Guide*

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Congratulations! You are now a Walk Friendly Community and it is important to announce and celebrate this designation.

Here are some suggestions for things you can do to promote your successes in creating a safe and friendly community for pedestrians:

### **Get social!**

Post this news to your community's social media outlets, such as Twitter, Facebook and Instagram. Use the Walk Friendly Communities logo as well as images of some of the pedestrian facilities in your community in your posts.

Please be sure to use the following tags in your social media posts: @fedex and @walkfriendly on Twitter, @pedbike for Facebook, and #walkfriendly and #fedexcares on Twitter and Instagram. FedEx is a proud supporter of the Walk Friendly Communities program.

### **Alert the media!**

Share this news with your local media outlets, including newspapers, television stations, radio stations, bloggers and others. The attached News Release Template can be used to make this announcement or feel free to create your own.

### **Tell elected officials and community leaders!**

Make sure your elected officials and other community leaders, including those from your chamber of commerce and convention and visitors bureau, know about this huge achievement and encourage them to share the news through their official and personal social media accounts.

### **Hold an event!**

Organize a public event to draw attention to your achievement and to incorporate the community into your celebration. Events can be as simple as an announcement at a local trail or an organized walk around the block. The attached Media Advisory Template can be used to announce your event and to invite media coverage.

Be sure to invite include local dignitaries, including elected officials, leaders of the business community, and your local FedEx representative. (If you need help reaching your local FedEx representative, contact Dan Gelinne at [Gelinne@hsrc.unc.edu](mailto:Gelinne@hsrc.unc.edu).)

### **Keep us informed!**

Please let us know what steps you take to promote your Walk Friendly Designation.



**Using the Media Advisory Template:**

- Fill in the bracketed areas with your city or town's information.
- Logo: Please feel free to use the Walk Friendly Communities logo or print the release on your city or town's letterhead.
- Contact Information: It's important to choose a point person for the media advisory who will be available once the advisory is distributed. Use the contact phone number that has the greatest likelihood of reaching the contact person.
- Length: The ideal media advisory is no longer than one page in length.
- Distribution: Once the release has been finalized, distribute the release to local media contacts (television, radio and newspaper).
- Be sure to provide copies of the release to attendees at the walking celebration. In addition, you may also compile a press kit including fact sheets, your community's WFC profile sheet and backgrounders.
- To assure that the Walking Celebration serves as the announcement for this achievement, we suggest sending out an embargoed advisory 1-2 days prior to the event.

**EMBARGOED UNTIL XXX XX, 2016**

**[City/Town] celebrates Walk Friendly Community designation**

- WHAT:** Representatives from [city/town] will announce and celebrate the city's Walk Friendly Communities designation by taking a walk.
- WHEN:** [Day, Date and Time Range]
- WHERE:** [Location information, including a link to directions and/or parking information]
- ACTIVITIES:** [Briefly describe any relevant, newsworthy activities and/or events that may be taking place in conjunction with your WFC designation announcement. Potential examples include announcing a major pedestrian initiative, the opening of a new facility, or even just taking a walk around the block.]
- PHOTO OPPS:** [List the names and affiliations of confirmed attendees who may be a draw for the local media. Examples include the town's mayor, elected officials and/or local celebrities.]
- WHO:** The event is free and open to the public. Invited guests include city officials, business leaders and members of the media.



[City/Town] has been designated as a Walk Friendly Community (WFC) by the Pedestrian and Bicycle Information Center. Walk Friendly Communities is a national recognition program aimed to encourage communities to establish or recommit to higher level of walking.

The WFC program recognizes communities that are working to improve a wide range of conditions related to walking, including safety, mobility, access and comfort. The WFC designation, awarded from bronze to platinum, is given to applicant communities that have demonstrated a commitment to improving and sustaining walkability and pedestrian safety through comprehensive programs, plans and policies. The program is funded by the U.S. Department of Transportation Federal Highway Administration and FedEx.

For more information about the Walk Friendly Communities program, please visit [www.walkfriendly.org](http://www.walkfriendly.org).

[Additional program links]



**Using the News Release Template:**

- Fill in the bracketed areas with your city or town's information.
- Logo: Please feel free to use the Walk Friendly Communities logo or print the release on your city or town's letterhead.
- Contact Information: It's important to choose a point person for the news release who will be available once the release is distributed. Use the contact phone number that has the greatest likelihood of reaching the contact person.
- Length: The ideal news release is no longer than a page in length.
- Distribution: Once the release has been finalized, distribute the release to local media contacts (television, radio and newspaper). E-mail, fax, mail and hand-delivery are all ways to distribute the release.
- Distribute on the day of the Walking Celebration.
- If you chose not to hold a Walking Celebration, please use this release template to alert the media and other organizations within your community of this remarkable accomplishment.

**EMBARGOED UNTIL XXX XX, 2016**

**MEDIA CONTACT:** [Contact's name]  
[Contact's e-mail address]  
[Contact's phone number]

[Distribution date]

[Town or city] **Awarded** [Level of designation] **Walk Friendly Communities Designation**

[City, State] – The Pedestrian and Bicycle Information Center has designated [town or city] a [designation level] Walk Friendly Community for its walkability initiatives and programs. Sponsored by the U.S. Department of Transportation Federal Highway Administration and FedEx, Walk Friendly Communities is a national recognition program aimed at recognizing communities for their commitment to pedestrian safety.

[In this area, include a few sentences that provide background about your city or town and its pedestrian-related initiatives. This information can be easily found in your Walk Friendly Communities Profile sheet. Information to include would be population, community highlights, or programs and initiatives.]

[Include a quote from a local official or WFC program representative discussing the application process or the impact the designation will have on your community.]

[City/Town] is among six communities recognized in the latest round of Walk Friendly Communities announced. The Walk Friendly Community designation, awarded from bronze to platinum, is given to applicant communities that have demonstrated a commitment to improving and sustaining walkability and pedestrian safety through comprehensive programs, plans and policies. To date, 58 communities in the U.S. have been designated as Walk Friendly Communities.



For more information on Walk Friendly Communities, please visit <http://www.walkfriendly.org>.

###

**Additional Resources:**

Walk Friendly Communities Website

<http://www.walkfriendly.org>

Pedestrian and Bicycle Information Center Website

<http://www.pedbikeinfo.org>

Federal Highway Administration Website

[http://safety.fhwa.dot.gov/ped\\_bike/](http://safety.fhwa.dot.gov/ped_bike/)



**CITY COUNCIL MEETING  
STAFF REPORT**

<p><b>Meeting Date:</b> June 6, 2016</p>	<p><b>Subject: Resolution No. 2583</b> Frog Pond Master Plan Contract Addendum</p> <p><b>Staff Member:</b> Chris Neamtzu, Planning Director <b>Department:</b> Community Development</p>	
<p><b>Action Required</b></p>	<p><b>Advisory Board/Commission Recommendation</b></p>	
<p><input type="checkbox"/> Motion</p> <p><input type="checkbox"/> Public Hearing Date:</p> <p><input type="checkbox"/> Ordinance 1<sup>st</sup> Reading Date:</p> <p><input type="checkbox"/> Ordinance 2<sup>nd</sup> Reading Date:</p> <p><input checked="" type="checkbox"/> Resolution</p> <p><input type="checkbox"/> Information or Direction</p> <p><input type="checkbox"/> Information Only</p> <p><input type="checkbox"/> Council Direction</p> <p><input checked="" type="checkbox"/> Consent Agenda</p>	<p><input type="checkbox"/> Approval</p> <p><input type="checkbox"/> Denial</p> <p><input type="checkbox"/> None Forwarded</p> <p><input checked="" type="checkbox"/> Not Applicable</p>	<p><b>Comments:</b> N/A</p>
<p><b>Staff Recommendation:</b> Staff recommends Council adopt Resolution No. 2583.</p>		
<p><b>Recommended Language for Motion:</b> I move to approve Resolution No. 2583.</p>		
<p><b>Project / Issue Relates To:</b></p>		
<p><input checked="" type="checkbox"/> Council Goals/Priorities Completing a concept/master plan for the Frog Pond Area was a Council key performance area in FY 13'-15'.</p>	<p><input type="checkbox"/> Adopted Master Plan(s)</p>	<p><input type="checkbox"/> Not Applicable</p>

**ISSUE BEFORE COUNCIL:** City Code (Section 2.312 (2)) requires that contract addenda in an amount greater than 15% of the original contract amount come back to City Council for approval. Staff is seeking authorization of Addendum #4 to the contract with Angelo Planning Group in the amount of \$57,893 to complete the Frog Pond Master Plan.

**EXECUTIVE SUMMARY:** Phase 1 of the Frog Pond Area Plan took longer and involved substantially more effort than originally scoped and budgeted. The additional time for Phase 1 was approximately five months. The additional work focused on the following: preparation of 4 additional land use plans and related information to provide options for concerns about lot sizes; analysis and modeling of the relationship between density, infrastructure costs, and housing

costs; additional work sessions and hearings, with preparation and presentations; update of the infrastructure funding plan; update of the transportation and infrastructure memoranda; and, a high level of coordination with the City and responsive information regarding community input. The additional work was funded by a combination of budget amendments and use of Phase 2 master plan grant resources. As a consequence, more budget resources are needed to complete the Phase 2 scope.

Overall, the original and updated Phase 2 scopes are similar; however, the updated scope is re-organized to improve the flow and clarity of the work products. The key differences are additional testing (through site studies) of the draft density calculations, expansion of infrastructure funding process and products, specification of design guidelines, more focused specification of the public realm design work (e.g. demonstration local street plan), and additional meetings. A contingency amount of \$15,000 has been included in order to streamline the ability to discuss and process additional tasks if they arise.

**EXPECTED RESULTS:** Approval of the funding request would allow the project to continue on schedule heading toward review and approval of a master plan at the end of the year.

**TIMELINE:** Funding of the request at this time allows the project to stay on schedule.

**CURRENT YEAR BUDGET IMPACTS:** None. The funds proposed to be used are part of the existing Planning Division budget.

**FINANCIAL REVIEW / COMMENTS:**

Reviewed by: SCole Date: 5/31/16

**LEGAL REVIEW / COMMENT:**

Reviewed by: \_\_\_\_\_ Date: \_\_\_\_\_

Resolution approved as to form.

**COMMUNITY INVOLVEMENT PROCESS:** The Frog Pond Master Plan involves significant public involvement as outlined in the project's comprehensive Public Involvement Plan (PIP).

**POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY** (businesses, neighborhoods, protected and other groups): Completion of the project will set the stage for private investment and development of needed single-family homes and neighborhoods in the community.

**ALTERNATIVES:** N/A

**CITY MANAGER COMMENT:**

**ATTACHMENTS:**

Resolution No. 2583

**RESOLUTION NO. 2583**

**A RESOLUTION OF THE CITY OF WILSONVILLE AUTHORIZING THE CITY MANAGER TO EXECUTE AN ADDENDUM TO THE FROG POND AREA PLAN CONTRACT WITH ANGELO PLANNING GROUP**

WHEREAS, the City received Community Planning and Development grant funds from Metro in the amount of \$341,000 to complete the Frog Pond Area Plan; and

WHEREAS, Wilsonville Code Section 2.312 (2) requires “*change orders or other amendments that increase the initial price of the contract by more than the above mentioned (15%) amount must be separately approved by the Contract Review Board...*”; and

WHEREAS, the proposed addendum amount of \$57,893 is more than 15% of the original contract requiring City Council approval; and

WHEREAS, the City Council adopted Phase 1 of the concept plan in November of 2015; and

WHEREAS, the Phase 2 Master Plan work is currently underway with an anticipated completion date of December, 2016; and

WHEREAS, Phase 1 involved substantial public input and took additional consultant services and 5 months longer to complete than initially scoped and as a result additional resources were used to complete the phase resulting in the need for additional resources be allocated to complete the project;

NOW, THEREFORE, THE CITY OF WILSONVILLE RESOLVES AS FOLLOWS:

1. The procurement process for the Project duly followed Oregon Public Contracting Rules.
2. City Council authorized the City Manager to execute the original contract in Resolution No. 2454.
3. City Council authorizes the City Manager to execute the Professional Services Agreement addendum to the original contract in the form attached hereto and incorporated herein as Exhibit A.
4. This Resolution is effective upon adoption.

ADOPTED by the Wilsonville City Council at a regular meeting thereof this 6th day of June, 2016, and filed with the Wilsonville City Recorder this date.

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Tim Knapp, Mayor

ATTEST:

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Sandra C. King, City Recorder

SUMMARY OF VOTES:

Mayor Knapp  
Council President Starr  
Councilor Fitzgerald  
Councilor Stevens  
Councilor Lehan

Exhibit A – Fourth Amendment to Professional Services Agreement, Angelo  
Planning Group – Frog Pond

**CITY OF WILSONVILLE  
FOURTH AMENDMENT TO PROFESSIONAL SERVICES AGREEMENT  
ANGELO PLANNING GROUP – FROG POND**

This Fourth Amendment to Professional Services Agreement (“Fourth Amendment”) is effective the \_\_\_\_ day of \_\_\_\_\_, 2016 (“Effective Date”) by and between the **City of Wilsonville**, a municipal corporation of the State of Oregon (“City”), and **Angelo Planning Group, Inc.**, an Oregon corporation (“Consultant”), upon the terms and conditions set forth below.

**RECITALS**

WHEREAS, the City entered into a Professional Services Agreement (“Agreement”) with Consultant on March 18, 2014, relating to the Frog Pond/Advance Road Master Planning Project (“Project”); and

WHEREAS, the City entered into a First Amendment to Professional Services Agreement (“First Amendment”) with Consultant on November 14, 2014; and

WHEREAS, the City entered into a Second Amendment to Professional Services Agreement (“Second Amendment”) with Consultant on January 7, 2015; and

WHEREAS, the City entered into a Third Amendment to Professional Services Agreement (“Third Amendment”) with Consultant on June 1, 2015; and

WHEREAS, the City requires additional services which Consultant is capable of providing, under terms and conditions hereinafter described; and

WHEREAS, Consultant represents that Consultant is qualified to perform the services described herein on the basis of specialized experience and technical expertise; and

WHEREAS, Consultant is prepared to provide such services as the City does hereinafter require;

NOW, THEREFORE, in consideration of these mutual promises and the terms and conditions set forth herein, the parties agree as follows:

**FOURTH AMENDMENT TO AGREEMENT**

**Section 1. Additional Services To Be Provided**

Consultant will perform the Additional Services not originally contemplated in the original Agreement Scope of Work or the amendments thereto for the Frog Pond/Advance Road Master Planning Project (“Project”), as more particularly described in **Exhibit A** attached hereto and incorporated by reference herein, pursuant to all original terms of the Agreement except as modified herein.

**Section 2. Time for Completion of Additional Services**

The Additional Services provided by Consultant pursuant to this Fourth Amendment shall be completed by no later than March 31, 2017.

**Section 3. Compensation**

The City agrees to pay Consultant on a time and materials basis, guaranteed not to exceed FIFTY-SEVEN THOUSAND EIGHT HUNDRED NINETY-THREE DOLLARS (\$57,893), to be added to the total Compensation Amount, for performance of the Additional Services (“Compensation Amount”). Consultant’s and sub-consultants’ rate schedules are attached hereto as **Exhibit B** and incorporated by reference herein.

**Section 4. All Other Terms**

All of the other terms and conditions of the Agreement shall remain in full force and effect, as therein written. Unless otherwise defined herein, the defined terms of the Agreement shall apply to this Fourth Amendment.

The Consultant and the City hereby agree to all provisions of this Fourth Amendment.

**CONSULTANT:**

**CITY:**

ANGELO PLANNING GROUP, INC.

CITY OF WILSONVILLE

By: \_\_\_\_\_

By: \_\_\_\_\_

(Print Name) \_\_\_\_\_

(Print Name) \_\_\_\_\_

As Its: \_\_\_\_\_

As Its: \_\_\_\_\_

Employer I.D. No. 93-1271507

**APPROVED AS TO FORM:**

**ATTESTED TO:**

\_\_\_\_\_  
Barbara A. Jacobson, City Attorney  
City of Wilsonville, Oregon

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder  
City of Wilsonville, Oregon

Date: March 20, 2016  
 To: Chris Neamtzu  
 From: Joe Dills  
 Re: Frog Pond Phase 2 Scope and Budget

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Chris – As requested, I have compiled this information in support of your processing of Amendment 4 for the Frog Pond contract.

### Project Status

- Phase 1, Frog Pond Area Plan, is complete
- Phase 2, Frog Pond Master Plan, is underway. You authorized proceeding with the draft scope of work and budget in our telephone communication prior to your recent vacation.

### Phase 2 Scope of Work and Budget

Please see attached, dated February 23, 2016. The scope and budget were sent by email on that date.

### Budget Summary

Phase	Grant	Current APG Contract	Actual Phase 1 Costs	Total Actual Phase 1 and Draft Phase 2
Phase 1 base	\$219,000	\$204,775	\$256,069	\$256,069
Phase 1 Amend.		\$6505 \$2480 \$25717		
Phase 2	\$122,000	\$93,156		\$134,457
<b>Total</b>	<b>\$341,000</b>	<b>\$332,633</b>		<b>\$390,526</b>
<b>Amend 4 Total</b>				<b>\$57,893</b>

#### Notes:

1. Grant amount is per letter from Martha Bennett to Chris Neamtzu, August 4, 2014
2. Current Contract amounts are for the APG contract, inclusive of Amendment 1 (\$6505), Amendment 2 (\$2480) and Amendment 3 (\$25,717).
3. Actual Phase 1 billing is per invoices through January, 2016 (Invoice 5815 dated February 8, 2016).

4. Draft Phase 2 budget is the budget dated February 23, 2016 and includes a \$15,000 Contingency line item.
5. Amendment 4 Total equals the difference between the current authorized budget of \$332,633 and the new total proposed budget of \$390,526, which equals \$57,893.

As you are aware, Phase 1 took longer and involved substantially more effort than originally scoped. The additional time for Phase 1 was approximately five months. The additional work focused on the following: preparation of 4 additional land use plans and related information to provide options for concerns about lot sizes; analysis and modelling of the relationship between density, infrastructure costs, and housing costs; additional work sessions and hearings, with preparation and presentations; update of the infrastructure funding plan; update of the transportation and infrastructure memoranda; and, a high level of coordination with the City and responsive information and advice regarding community input. The extra work was funded by a combination of budget amendments (thank you) and use of Phase 2 resources. As a consequence, more budget resources are needed to complete the Phase 2 scope.

#### **Phase 2 Scope Notes**

Overall, the original and updated Phase 2 scopes are pretty similar, however, the updated scope is re-organized to improve the flow and clarity of the work products. The key differences are additional testing (through site studies) of the draft density calculations, expansion of infrastructure funding process and products, specification of design guidelines rather than “character images”, more focused specification of the public realm design work (e.g. demonstration local street plan), and additional meetings. A Contingency amount of \$15,000 has been included in order to streamline the ability to discuss and process additional tasks.

#### **Amendment 4 Summary**

Bringing the above information together, Amendment 4 is defined as:

- Additional scope and budget in response to additional services and deliverables in Phase 1
- Updated Phase 2 scope and budget, as defined in the scope and budget attachments date February 23, 2016
- A budget increase of \$57,893

I hope this provides sufficient information to complete the processing of Amendment 4. Please let me know if I can be of further assistance. We greatly appreciate the collaboration with the City team on this project and look forward to a successful Phase 2.

# Frog Pond-Advance Road Concept and Master Plan

## City of Wilsonville

### Phase 2 Scope Update

February 23, 2016

## Phase 2: Master Plan for Frog Pond

### Introduction and Assumptions

At the conclusion of Phase 1, a review of the Phase 2 scope was prepared and discussed with the City. Based on that review, the following updated assumptions have been prepared as the basis for the Phase 2 Frog Pond Master Plan:

1. The basic approach to Phase 2 is to begin the process with initial work on the key implementation products (the Comprehensive Plan and zoning text), and then prepare drawings to support those regulations and the intended community character for Frog Pond West. The regulatory approach and drawings will support each other, and go through iterative refinement based on Planning Commission and public comment. The last step will be to wrap it all together into a report and adoption package.
2. The City's Significant Resource Overlay Zone (SROZ) will be retained as the natural resource boundary for the Master Plan, recognizing that field verification and adjustment will occur through the development review process as Frog Pond West is built.
3. Infrastructure plans from Phase 1 are adequate and appropriate for use in Phase 2. The Phase 2 scope does not include an infrastructure update task.
4. The Funding Plan will be updated through an iterative and collaborative process with the City team, resulting in a roadmap to funding infrastructure for Frog Pond West.
5. Phase 2 will include findings to support the Comprehensive Plan amendments in compliance with OAR 660-012-060. The actual TSP amendments will be prepared as part of the City's upcoming TSP "housekeeping" update and are not part of this scope.
6. The City is in the lead for coordinating with Metro on the Frog Pond Area Plan and Master Plan, including an work needed regarding compliance with UGB amendment conditions (e.g. Frog Pond school properties condition).
7. The Planning Commission will serve as the advisory committee for Phase 2, using their work sessions as the meeting time.
8. Two open houses will be conducted in concert with Planning Commission work sessions (immediately prior to the meetings). Information will be available on-line throughout the process (formal on-line open houses are not part of this scope).

**Task 9 – Frog Pond West Neighborhood Opportunities and Constraints**

The purpose of this task is to establish the key planning opportunities and constraints for the Frog Pond West Neighborhood. The Area Plan established 80% of what is needed. In this task, an opportunities and constraints diagram will be made at the scale of the Frog Pond West Neighborhood to capture ideas from the Area Plan, and identify new opportunities and constraints to be considered in the Master Plan.

Deliverables:

- a. Prepare an opportunities and constraints diagram at Frog Pond West Neighborhood scale.

**Task 10 – Comprehensive Plan, regulatory structure, and Title 11 compliance.**

The purpose of this task is to establish the policy and regulatory foundation for the Master Plan. Several memos are described below for use in two Planning Commission work sessions. The final content of each memo is flexible and will be defined as the agendas and topics for the two work sessions are planned.

Services and deliverables include:

- a. Review Title 11, the Phase 1 zoning strategy, and Comprehensive Plan (background review, no deliverable).
- b. Prepare Zoning Structure Part 1 Memo – The memo will include: a description of the recommended Comprehensive Plan designation; an outline and structure for zone districts and code, working from the Phase 1 zoning strategy as a base; and, an introduction to, and code issues description of, co-housing as a potential use in Frog Pond.
- c. Prepare a preliminary Comprehensive Plan map, text and policies.
- d. Prepare a preliminary zoning map.
- e. Calculate minimum and maximum densities for each zone and work with City to establish zoned densities that are generally consistent with Area Plan. A memo will be prepared to document the calculations. The City will provide guidance on how right-of-way deductions and other assumptions are currently calculated under the Wilsonville code. The City and consultant team will work together to review and prepare recommendations for how open space, right-of-way, and other “set-asides” will be handled.
- f. Using the working code standards, prepare two site studies, not to exceed 20 acres, to test some of the standards of the intended zones for the Master Plan
- g. Prepare a tree preservation strategy. Review how Wilsonville currently preserves trees in the development review process and options for how those regulations might be applied and/or updated in Frog Pond West.
- h. Prepare a Zoning Structure (Part 2 memo) describing the Task 10 analysis and recommendations not addressed in the Part 1 memo.

### Task 11 – Infrastructure Funding Strategy Evaluation

The purpose of this task is to refine the Frog Pond Funding Strategy specific to the West Neighborhood. The team, led by Andy Parks of Gel Oregon (Gel) and supported by Leland Consulting Group (LCG), will use the “Frog Plan Area Plan: Infrastructure Funding Strategy” (June 3, 2015), as a starting point. The team will refine the 2015 memorandum in the following ways:

- Produce a funding “road map” strategy, with decision tree options that the city can use over time to choose between different funding approaches. The road map strategy will be the funding strategy assuming Frog Pond West develops with many 5-20 acre projects over 10-20 years. The decision tree options will provide flexibility and guidance for if a different development pattern occurs, such as the consolidation of large parts of Frog Pond by a single developer.
- Focus on the West Reimbursement Area infrastructure projects identified in the Area Plan Funding Strategy: Boeckman Road (road upgrade and sewer), Stafford Road (road upgrade and sewer), and the two parks serving the West Neighborhood. This task will propose several options to fund each piece of infrastructure, including a preferred option.

In addition, the team will seek to address the following issues / questions:

- What are the advantages and disadvantages of each of the “reimbursement area” approaches discussed in the 2015 memorandum?
  - o Developer-initiated Reimbursement District(s)
  - o City initiated Reimbursement District(s)
  - o Local Improvement District(s)
- In order to evaluate advantages and disadvantages, Gel and LCG will develop multi-year cash flows showing capital outlays, debt service, and reimbursement payments, which were not completed for the reimbursement areas in the 2015 Area Plan Funding Strategy.
- What are the advantages and disadvantages of implementing multiple reimbursement districts, LIDs, or other to enable phased development, i.e. from south to north, starting with Boeckman Road?
- What would be the effect of making additional types of facilities (e.g, parks) SDC creditable?

Services and deliverables include:

- a. DKS will evaluate the need for the Boeckman Bridge and estimate: (1) the trigger point/year of need for the bridge; and, (2) how much of Frog Pond West’s trip generation contributes to the need for the bridge.
- b. DKS will participate in staff work session 1 to help brainstorm funding options for the bridge.
- c. Staff work session 1 (agenda and discussion questions will be provided prior). The City will provide questions it wishes to discuss in advance of the work session.
- d. Refine funding strategy to be specific to Frog Pond West
- e. Staff work session 2 - present refinements / address key staff questions
- f. Meet with property owners and/or developers to understand their development timelines and perspectives on infrastructure funding and construction

- g. Prepare a funding road map memorandum
- h. Staff work session 3
- i. Prepare draft and final Frog Pond West Funding Strategy
- j. Planning Commission presentation
- k. City Council presentation

**Task 12 (Phase 1 Outreach Meetings) and 13 (Phase 1 Financial Model and Additional Services)**

Tasks 12 and 13 were completed in Phase 1.

**Task 14 – Draft zoning text and design guidelines**

The purpose of this task is to prepare draft development code and design guidelines.

Services and deliverables include:

- a. Prepare preliminary and annotated code text (base zones, uses, standards) which implements the policies and density standards from Task 9. The code does not need to be fully codified at this point, but detailed enough facilitate discussion by the Planning Commission and final drafting in subsequent tasks.. Prepare a brief cover memo to the draft code.
- b. Prepare design guidelines/standards for Frog Pond West. The guidelines/standards will be prepared in two steps: (1) concepts and precedent pictures in a presentation that enables the Planning Commission to discuss them and provide input; and (2) code text and supporting graphics. Ten-fifteen topics will be addressed (e.g building orientation to streets, parking location, garage setbacks and width, use of alleys, tailored standards for large lots, architecture design elements, articulation, house plan variety, materials and color, landscaping). Code graphics will up to five graphics that consolidate several topics per graphic (graphic form to be determined in collaboration with the City). These design guidelines will be informed by what is in the existing code and in Villebois. The City will provide all applicable documents from Villebois. Prepare a brief cover memo for the design guidelines.

**Task 15 – Frog Pond West Public Realm Designs**

The purpose of this task is to define the Frog Pond West Neighborhood public realm: streets, parks and open space, and the Boeckman Trail.

Services and deliverables include:

- a. Prepare a street demonstration plan. A local street demonstration plan will be prepared for Frog Pond West. The purpose of the plan is to illustrate the level of connectivity expected for the neighborhood.
- b. Conduct a work session with City staff the consultants (APG, Walker Macy, DKS) to discuss the street demonstration and ideas for other products in this task: cross-sections for all functional classifications, park options, and the Boeckman Trail. APG will facilitate the work session. Subsequent sub-tasks will incorporate the ideas from the work session.
- c. Prepare precedent photos and one 3-dimensional cross-section for each Framework Street. Sections will illustrate street elements and dimensions, street character, and how the street interfaces with adjacent development and open space. The Framework Street sections will include:

- Boeckman Road
  - Stafford Road
  - Willow Creek Extension
  - Frog Pond Lane
  - East-West Framework Street located between Frog Pond Lane and Boeckman Road
  - The arching framework street at the west end of the neighborhood
- d. Prepare a park implementation strategy. This strategy will address funding (from Task 11), how park land will be acquired and developed, and how the park strategy relates to open space standards in development review.
  - e. Prepare a park location study. Using the street demonstration plan as a base, prepare a series of diagrams showing options for neighborhood park locations, and the principles applied. The purpose of the diagrams and principles is show precedents and guide future park siting decisions consistent with the principles.
  - f. Prepare a street tree plan. The plan will align recommended street tree types with the typology of streets
  - g. Prepare principles for monuments and signage. These principles will suggest basic locations, and use precedent photos to guide future more detailed work - .
  - h. Prepare a plan for Boeckman Trail. The plan will show a conceptual alignment and basic cross-section. A diagram will be prepared for how the trail connects to/under the Boeckman Bridge and crosses Stafford Road, if detailed drawings of the bridge design are available A diagram will also illustrate how this trail could interface with the western edge of future development in the study area. A diagram will illustrate potential crossings and connections to the west side of Boeckman Creek.

#### Task 16 – Master Plan Report and Adoption Products

The purpose of this task is to prepare the Master Plan and related adoption products. Hearings-ready code will be prepared by APG

Services and deliverables include:

- a. Master Plan Report. The Master Plan report will include selected drawings and descriptions from the Area Plan Report, and those created during Phase 2. APG will prepare an outline of the report organization and content for city review. The Master Plan will summarize the vision for the neighborhood, opportunities and constraints, policy and regulatory structure, zoning and design recommendations, and public realm recommendations.
- b. Final Comprehensive Plan text amendments. Prepare adoption-ready Comprehensive Plan text amendments.
- c. Zoning code amendments (hearings-ready). Update the preliminary code to be hearings-ready.
- d. Plan and zone maps. Prepare GIS maps of Comprehensive Plan map and zone map designations. GIS data for these designations will be provided in a format that can be easily integrated with the City's existing land use GIS.
- e. Transportation Planning Rule findings. Using and referencing the technical work from the Area Plan and the City's Transportation System Plan, prepare TPR compliance findings. DKS will prepare the technical memo, specific to Frog Pond West and the Comprehensive Plan amendments proposed as part of Phase 2. APG will prepare the findings.
- f. Statewide planning goal and Title 11 findings. Prepare findings for Metro Title 11 and the relevant statewide planning goals.

**Task 17 – Phase 2 meetings and hearings**

- a. Planning Commission work sessions. Attend four work sessions of the Planning Commission to review draft products from the Phase 2 tasks. Two informal open houses will be held prior to two of the work sessions.
- b. City Council briefings. Attend two briefings. APG will prepare a presentation, other materials for the Council will be prepared by the City.
- c. Public hearings. Participate in one public hearing of the Planning Commission and one public hearing of the City Council. Review testimony prior, coordinate with staff, and prepare presentations for each hearing.
- d. Team meetings. Five meetings at APG or Walker Macy’s offices to collaborate with City staff. These meetings are in addition to the meetings specifically listed above.
- e. “Listening post” outreach meetings. At the discretion of the City, participate in up to three informal “listening post” meetings with stakeholders. Examples include arts/culture groups, property owners, and developers.

**Phase 2 Preliminary Schedule - Milestones**

The following schedule is preliminary and subject to change in collaboration with the City.

Milestone	Tasks and Topics
<b>March 9 Planning Commission work session</b>	Phase 2 scope and schedule  Opportunities and constraints  Zoning structure, part 1  Co-housing  Design guidelines, part 1
<b>May 11 Planning Commission work session, preceded by informal open house</b>	Zoning structure, part 2  Comprehensive plan text and map  Design guidelines, part 2  Public realm designs, part 1
<b>June, date tbd, City Council briefing</b>	Summary of work to date  Work session on infrastructure funding
<b>July 13 Planning Commission work session, preceded by informal open house</b>	Draft zoning code amendments, with design guidelines

	<p>Draft comprehensive plan amendments</p> <p>Draft infrastructure funding plan</p>
<b>August 10 Planning Commission work session</b>	Draft Master Plan report
<b>August, date tbd, City Council briefing</b>	Working planning commission recommendations
<b>September – Planning Commission public hearing</b>	Adoption documents
<b>October – City Council public hearing</b>	Adoption documents

## ANGELO PLANNING GROUP 2016 RATE SCHEDULE

Frank Angelo, Principal	\$192
Cathy Corliss, Principal	\$149
Karen Siegel, Business Manager/Principal	\$124
Senior Project Manager	\$192
Project Manager	\$129 - \$171
Senior Planner	\$ 90
Project Planner	\$60 - \$90
Intern	\$60

**Expenses**

Direct costs from outside vendors are invoiced at cost.

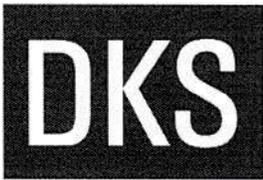
Other in-house costs which may be charged to a project include:

Photocopies \$0.10 per copy

Color Copies \$0.50 per copy

Mileage Federal Rate Long Distance At cost

Travel, Meals At cost



## Fee Schedule

*Effective January 1, 2016 through December 31, 2016*

<i>ENGINEERS and PLANNERS</i>				<i>TECHNICIANS and SUPPORT STAFF</i>	
Grade	Hourly Rate	Grade	Hourly Rate	Tech Level	Hourly Rate
Grade 1	\$ 55.00	Grade 26	180.00	Tech Level A	\$ 35.00
Grade 2	60.00	Grade 27	185.00	Tech Level B	40.00
Grade 3	65.00	Grade 28	190.00	Tech Level C	45.00
Grade 4	70.00	Grade 29	195.00	Tech Level D	50.00
Grade 5	75.00	Grade 30	200.00	Tech Level E	55.00
Grade 6	80.00	Grade 31	205.00	Tech Level F	60.00
Grade 7	85.00	Grade 32	210.00	Tech Level G	65.00
Grade 8	90.00	Grade 33	215.00	Tech Level H	70.00
Grade 9	95.00	Grade 34	220.00	Tech Level I	75.00
Grade 10	100.00	Grade 35	225.00	Tech Level J	80.00
Grade 11	105.00	Grade 36	230.00	Tech Level K	85.00
Grade 12	110.00	Grade 37	235.00	Tech Level L	90.00
Grade 13	115.00	Grade 38	240.00	Tech Level M	95.00
Grade 14	120.00	Grade 39	245.00	Tech Level N	100.00
Grade 15	125.00	Grade 40	250.00	Tech Level O	105.00
Grade 16	130.00	Grade 41	255.00	Tech Level P	110.00
Grade 17	135.00	Grade 42	260.00	Tech Level Q	115.00
Grade 18	140.00	Grade 43	265.00	Tech Level R	120.00
Grade 19	145.00	Grade 44	270.00	Tech Level S	125.00
Grade 20	150.00	Grade 45	275.00	Tech Level T	130.00
Grade 21	155.00	Grade 46	280.00	Tech Level U	135.00
Grade 22	160.00	Grade 47	285.00	Tech Level V	140.00
Grade 23	165.00	Grade 48	290.00	Tech Level W	145.00
Grade 24	170.00	Grade 49	295.00	Tech Level X	150.00
Grade 25	175.00	Grade 50	300.00	Tech Level Y	155.00
				Tech Level Z	160.00

- Project expenses will be billed at *cost plus ten percent* for service and handling. Project expenses include project-related costs such as reproduction through outside services, transportation, subsistence, delivery/postage, and vendor and subcontractor services.
- All invoices are due and payable within 30 days of date of invoice. Invoices outstanding over 30 days will be assessed a 1 1/4 percent service charge, compounded, for each 30 days outstanding beyond the initial payment period. Service charges are not included in any agreement for maximum charges.

April 25, 2016

Karen,

Leland Consulting Group's current rates are below. Note that some have increased slightly from last year.

In addition to the staff below, Andy Parks of GEL Oregon will be working with Leland as a member of the project team. Essentially he will be a sub to Leland. Joe and Chris N are aware of this. Andy's billing rate is \$175 / hour.

Please let me know if you need additional information. Thanks,

Brian

**Leland Consulting Group hourly rates**

<b>Staff Member</b>	<b>Billing Rate</b>
Chris Zahas, Managing Principal	\$180
Brian Vanneman, Principal	\$150
Wally Hobson, Senior Market Analyst	\$175
David Siegel, Senior Project Manager	\$165
Alisa Pyszka, Senior Associate	\$165
Ted Kamp, Senior Associate	\$150
Matthew Craigie, Associate	\$130
Kate Washington, Analyst	\$100
Jane St. Michael, Administrative	\$90

Brian Vanneman | Principal



**Billing Rates***April 21, 2016*

<b><i>Employee</i></b>	<b><i>Billing Rate</i></b>
Bono, Timothy	85.00
Brooks, Kate	95.00
Crampton, Matthew	90.00
Dickow, Paul	85.00
Durant, Kristina	95.00
Gillin, Calder	95.00
Head, Keriann	85.00
Heimes, Nanette	60.00
Hinshaw, Mark	145.00
Holzworth, Ian	95.00
Jenkins, Jake	70.00
Jones, Christopher	130.00
Kappen, Nathan	95.00
Kini, Saumya	70.00
Kuhn, Rollin	85.00
Kuns, Andrea	90.00
Machle, Alyssa	75.00
Macy, J. Douglas	185.00
Maples, Aaron	70.00
Marvin, Sarah	60.00
McCann, Chelsea	130.00
Melvin, Rebecca	75.00
Miller*, Leona	60.00
Norton, Megan	60.00
Olson, Dean	130.00
Payne, Jarvis	125.00
Pirie, Kenneth	115.00
Rose, Lara	130.00
Schneider, Ann	75.00
Short, Nia	70.00
Weiland, Steven	115.00
Zhu, Xiaoyang	60.00
Zilis, Michael	175.00



**CITY COUNCIL MEETING  
STAFF REPORT**

<b>Meeting Date:</b> June 6, 2016		<b>Subject: Ordinance Nos. 792, 793, and 794</b> Annexation, Comprehensive Plan Map Amendment, and Zone Map Amendment for Republic Services Expansion <b>Staff Member:</b> Daniel Pauly AICP, Associate Planner <b>Department:</b> Community Development, Planning Division	
<b>Action Required</b>		<b>Development Review Board Recommendation</b>	
<input checked="" type="checkbox"/> Motion <input checked="" type="checkbox"/> Public Hearing Date: June 6. <input checked="" type="checkbox"/> Ordinance 1 <sup>st</sup> Reading Date: June 6, 2016. <input checked="" type="checkbox"/> Ordinance 2 <sup>nd</sup> Reading Date: June 20, 2016 <input type="checkbox"/> Resolution <input type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input type="checkbox"/> Council Direction <input type="checkbox"/> Consent Agenda		<input checked="" type="checkbox"/> Approval <input type="checkbox"/> Denial <input type="checkbox"/> None Forwarded <input type="checkbox"/> Not Applicable	
		<b>Comment:</b> Following their review at the May 9, 2016 meeting, the Development Review Board, Panel A, recommends approval of Annexation, Comprehensive Plan Map Amendment and Zone Map Amendment for the subject property. The DRB also approved a Stage I Master Plan Revision, Stage II Final Plan, Site Design Review, Setback Waiver, and Type C Tree Removal Plan for development of a food waste to biogas to electricity facility on a portion of the property.	
<b>Staff Recommendation:</b> Staff recommends that the City Council adopt Ordinance Nos. 792, 793, and 794.			
<b>Recommended Language for Motion:</b> In three separate motions, I move to approve Ordinance Nos. 792, 793, and 794 on the 1 <sup>st</sup> reading.			
<b>PROJECT / ISSUE RELATES TO:</b> Annexation, Comprehensive Plan Map and Zone Map Amendments.			
<input type="checkbox"/> Council Goals/Priorities	<input checked="" type="checkbox"/> Adopted Master Plan(s) Coffee Creek Master Plan	<input type="checkbox"/> Not Applicable	

**ISSUE BEFORE COUNCIL:** Approve, modify, or deny Ordinance Nos.: 792, 793 and 794 on approximately 10.2 acres located east of Garden Acres Road north of Ridder Road and current Republic Services development.

**EXECUTIVE SUMMARY:** Republic Services has a growing operation in Wilsonville. The Wilsonville campus currently includes a Materials Recovery Facility (MRF) for processing and

sorting waste, collection operations offices, maintenance shop, and storage and support services. Republic Services and their predecessor companies have owned approximately 10 acres north of their campus for many years, which currently sits outside Wilsonville City limits. In support of their continued growth, Republic Services wishes to annex the property into the City, adopt City Comprehensive Plan and Zoning designations, and adopt a master plan for their entire campus. Concurrently, Republic Services has submitted land use applications for the development of a food waste to biogas facility by their partner, SORT Bioenergy.

**EXPECTED RESULTS:** Adoption of Ordinance Nos.: 792, 793 and 794.

**TIMELINE:** Annexation, Comprehensive Plan Map Amendment and Zone Map Amendment will be in effect 30 days after the ordinances are adopted.

**CURRENT YEAR BUDGET IMPACTS:** None.

**FINANCIAL REVIEW / COMMENTS:**

Reviewed by: \_\_\_\_\_, Date: \_\_\_\_\_, 2016

**LEGAL REVIEW / COMMENT:**

Reviewed by: \_\_\_\_\_, Date: \_\_\_\_\_, 2016

**COMMUNITY INVOLVEMENT PROCESS:** The required public hearing notices have been sent and public hearings are being held. City staff has made materials regarding the application readily available to the public.

**POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY:**

Development on new industrial lands consistent with the Coffee Creek Master Plan  
Potential additional community enhancement funds based on amount of waste brought to the site as the handling capacity of the Republic Services campus increases with expansion.

**ALTERNATIVES:** Not approve the ordinances preventing development of the property as part of the City of Wilsonville

**CITY MANAGER COMMENT:**

**EXHIBITS AND ATTACHMENTS:**

Exhibit A: Ordinance No. 792 Annexation

Attachment 1: Legal Description and Sketch of Annexation Area

Attachment 2: Petition for Annexation

Attachment 3 Annexation Findings May 12, 2016

Exhibit B: Ordinance No. 793 Comprehensive Plan Map Amendment

Attachment 1: Comprehensive Plan Map Order DB16-0005 including legal description and sketch depicting map amendment

Attachment 2: Comprehensive Plan Map Amendment findings

Exhibit C, Ordinance No. 794 Zone Map Amendment for the subject territory

Attachment 1: Zoning Order DB16-0006 including legal description and sketch depicting zone map amendment

Attachment 2: Zone Map Amendment Findings

Exhibit D, DRB Resolution No. 327 recommending approval of Annexation, Comprehensive Plan Map Amendment and Zone Map Amendment

Exhibit E, DRB Record including: Amended and Adopted Staff Report and DRB Recommendation, Exhibits (on CD)

**ORDINANCE NO. 792**

**AN ORDINANCE OF THE CITY OF WILSONVILLE ANNEXING APPROXIMATELY 10.2 ACRES OF TERRITORY INCLUDING A SEGMENT OF SW GARDEN ACRES ROAD RIGHT-OF-WAY AND AN ADJACENT PARCEL OF LAND EAST OF SW GARDEN ACRES ROAD NORTH OF SW RIDDER ROAD AND THE CURRENT REPUBLIC SERVICES DEVELOPMENT INTO THE CITY LIMITS OF THE CITY OF WILSONVILLE, OREGON. THE TERRITORY IS MORE PARTICULARLY DESCRIBED AS TAX LOT 600 OF SECTION 2C, T3S, R1W, WASHINGTON COUNTY, OREGON AND ADJACENT RIGHT-OF-WAY, WILLAMETTE RESOURCE INCORPORATED, PETITIONER.**

**RECITALS**

WHEREAS, consistent with ORS 222.111 (2) a proposal for annexation was initiated by petition by the owner of real property in the territory to be annexed, a copy of the petition is on file with the City Recorder; and

WHEREAS, written consent has been obtained from the only owner of the territory and no electors reside within the territory proposed to be annexed, a copy of which is on file with the City Recorder; and

WHEREAS, the land to be annexed is within Wilsonville's Urban Growth Boundary and a copy of the legal description and sketch is attached as Attachment 1, and both are incorporated by reference as if fully set forth herein; and

WHEREAS, the territory to be annexed is contiguous to the City and can be served by City services; and

WHEREAS, ORS 227.125 authorizes the annexation of territory based on consent of the only owner of the land and a majority of electors, if any, within the territory and enables the City Council to dispense with submitting the question of the proposed annexation to the electors of the City for their approval or rejection; and

WHEREAS, annexing adjacent public right-of-way will allow for improvements consistent with the City's Transportation Systems Plan and Public Works Standards; and

WHEREAS, Panel A of the Development Review Board considered the annexation, and after a duly advertised public hearing held on May 9, 2016 recommended City Council approve the annexation; and

WHEREAS, on June 6, 2016, the City Council held a public hearing as required by Metro Code 3.09.050 and received testimony; and

WHEREAS, reports were prepared and considered as required by law; and notice was duly given, the Council finds that the annexation is not contested by any party, neither before the DRB or at the City Council hearing, therefore, the City Council finds that it is not necessary to submit the matter to the voters and does hereby favor the annexation of the subject tract of land based on findings and conclusions attached hereto as Attachment 3.

NOW, THEREFORE, THE CITY OF WILSONVILLE ORDAIN AS FOLLOWS:

Section 1. The above recitals are fully incorporated herein. The territory legally described and depicted in Attachment 1 is declared annexed to the City of Wilsonville.

Section 2. The findings and conclusions and in Attachment 3 are adopted. The City Recorder shall immediately file a certified copy of this ordinance with Metro and other agencies required by Metro Code Chapter 3.09.050(g) and ORS 222.005. The annexation shall become effective upon filing of the annexation records with the Secretary of State as provided by ORS 222.180.

SUBMITTED to the Wilsonville City Council and read the first time at a meeting thereof on June 6, 2016 and scheduled for the second reading on June 20, 2016 commencing at the hour of 7:00 p.m. at the Wilsonville City Hall, 29799 Town Center Loop East, Wilsonville, OR.

ENACTED by the City Council on the \_\_\_\_ day of June 2016 by the following votes:

Yes: \_\_\_\_\_ No: \_\_\_\_\_

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder

DATED and signed by the Mayor this \_\_\_\_ day of June, 2016.

\_\_\_\_\_  
TIM KNAPP, MAYOR

SUMMARY OF VOTES:

Mayor Knapp  
Councilor Starr  
Councilor Fitzgerald  
Councilor Stevens  
Councilor Lehan

Attachments:

Attachment 1: Legal Description and Sketch of Annexation Area  
Attachment 2: Petition for Annexation  
Attachment 3 Annexation Findings May 12, 2016

EXHIBIT "A"

A TRACT OF LAND SITUATE IN THE SOUTHWEST ONE-QUARTER OF SECTION 2, TOWNSHIP 3 SOUTH, RANGE 1 WEST OF THE WILLAMETTE MERIDIAN, WASHINGTON COUNTY, OREGON, BEING LOT 12, OF THE PLAT OF "GARDEN ACRES", WASHINGTON COUNTY PLAT RECORDS AND A PORTION OF GARDEN ACRES ROAD, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

**BEGINNING** AT A POINT ON THE WESTERLY RIGHT-OF-WAY LINE OF GARDEN ACRES ROAD, SAID POINT BEARS SOUTH 89°41'03" WEST, A DISTANCE OF 20.00 FEET AND SOUTH 00°18'57" EAST, A DISTANCE OF 1652.60 FEET FROM THE QUARTER CORNER COMMON TO SECTIONS 2 AND 3, THENCE LEAVING SAID WESTERLY RIGHT-OF-WAY LINE ALONG A WESTERLY EXTENSION OF THE NORTH LINE OF SAID LOT 12 AND THE NORTH LINE OF SAID LOT 12 NORTH 89°28'40" EAST, A DISTANCE OF 1344.33 FEET TO A 5/8" IRON ROD MARKING THE NORTHEAST CORNER OF SAID LOT 12; THENCE LEAVING SAID NORTH LINE ALONG THE EAST LINE OF SAID LOT 12, SOUTH 00°19'54" EAST, A DISTANCE OF 330.43 FEET TO A 3 1/2" BRASS CAP STAMPED "BONNIVILLE POWER ADMINISTRATION", MARKING THE SOUTHEAST CORNER OF SAID LOT 12; THENCE LEAVING SAID EAST LINE ALONG THE SOUTH LINE OF SAID LOT 12 AND A WESTERLY EXTENSION THEREOF SOUTH 89°28'40' WEST, A DISTANCE OF 1344.42 FEET TO A POINT ON SAID WESTERLY RIGHT-OF-WAY LINE OF GARDEN ACRES ROAD; SAID POINT BEARS SOUTH 89°41'03" WEST, A DISTANCE OF 20.00 FEET AND NORTH 00°18'57" WEST, A DISTANCE OF 661.31 FEET FROM THE SECTION CORNER COMMON TO SECTIONS 3, 2, 10 AND 11; THENCE ALONG SAID WESTERLY RIGHT-OF-WAY LINE NORTH 00°18'57" WEST, A DISTANCE OF 330.43 FEET TO THE POINT OF BEGINNING.

CONTAINS 444,223 SQUARE FEET OR 10.198 ACRES, MORE OR LESS.

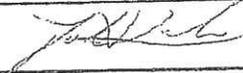
ANNEXATION CERTIFIED

BY VF

NOV 16 2015

WASHINGTON COUNTY A & T  
CARTOGRAPHY

REGISTERED  
PROFESSIONAL  
LAND SURVEYOR



OREGON  
JULY 15, 2003  
TOD V. KELSO  
50701

EXPIRATION DATE: 6/30/17

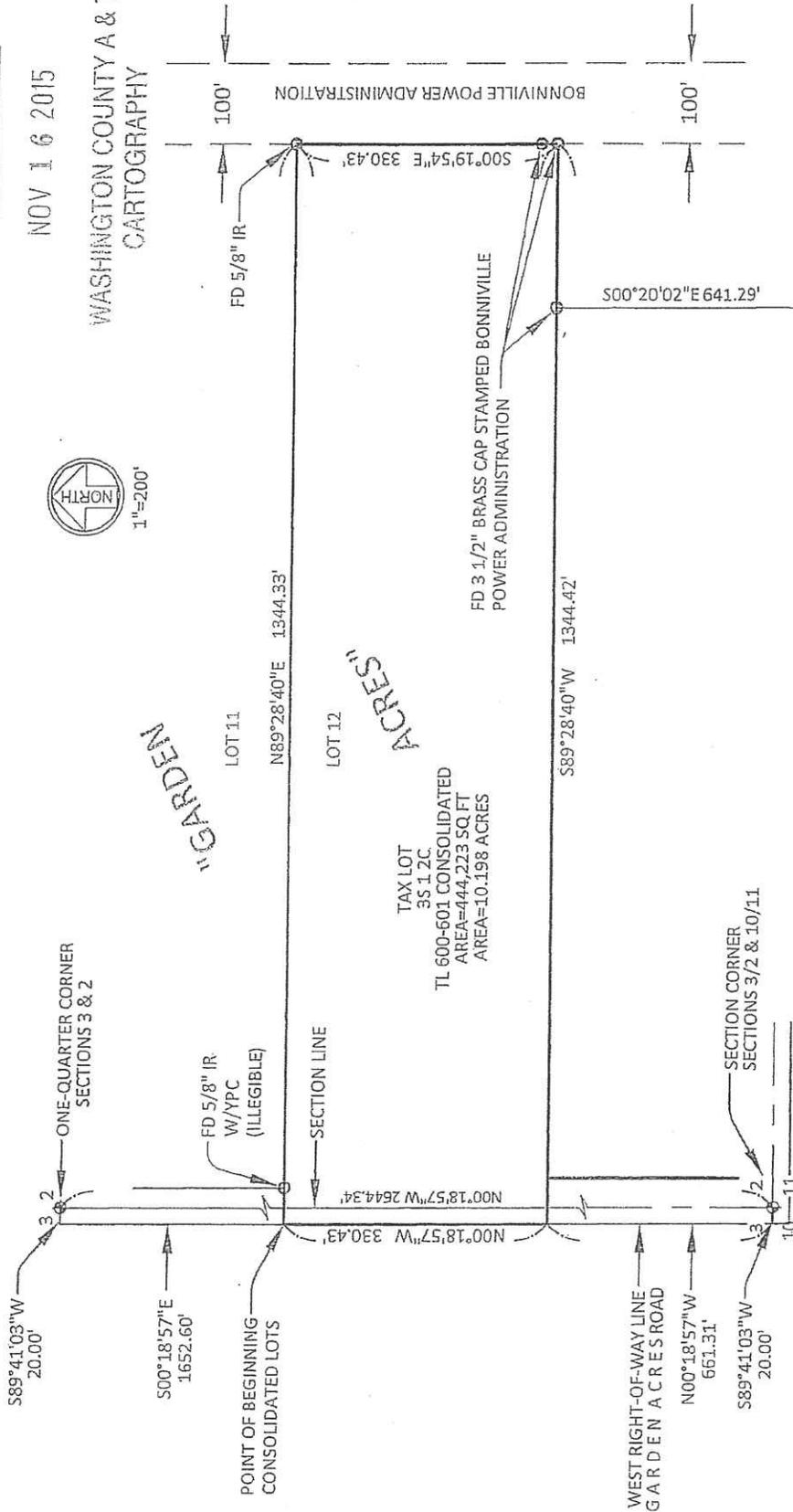
# LOT CONSOLIDATION EXHIBIT

ANNEXATION CERTIFIED

BY *TV*

NOV 16 2015

WASHINGTON COUNTY A & T  
CARTOGRAPHY



**Project**  
REP. SERV. ANNEXATION  
**No.**  
999-158B

**Designed by** TVK  
**Drawn by** TVK  
**Horiz. Scale:** 1"=200'  
**Vert. Scale:** 1"=200'  
**Type** EXHIBIT

**Date** 2015-11-03  
**Date** 2015-11-09

**2015**  
999 SW WASHINGTON COURSE, RD.  
SITE 170  
PORTLAND, OREGON 97233  
P-203-431-200  
11/16/15 (A)13  
PIONEER DESIGN GROUP, INC.  
www.pdg-pp.com

**Sheet**  
**2** of **2**



Ordinance No. 792  
 Staff Report  
 Wilsonville Planning Division

Republic Services Property Annexation

City Council  
 Quasi-Judicial Public Hearing

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<b>Hearing Date:</b>	June 6, 2016
<b>Date of Report:</b>	May 12, 2016
<b>Application No.:</b>	DB16-0004 Annexation

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**Request:** The City Council is being asked to review annexation of approximately 10.2 acres of property owned by Republic Services to expand their campus and adjacent right-of-way.

**Location:** Segment of Garden Acres Road and a parcel east of Garden Acres Road north of current Republic Services buildings The property is specifically known as Tax Lots 600 , Section 2C, Township 3 South, Range 1 West, Willamette Meridian, City of Wilsonville, Washington County, Oregon

**Owner/Petitioner:** Jason Jordan, Willamette Resources Inc.

**Applicants'**

**Representative:** Ben Altman, Pioneer Design Group

**Comprehensive Plan Designation (Current):** FD-20

**Comprehensive Plan Designation (Proposed):** Industrial

**Zone Map Classification (Current):** FD-20

**Zone Map Classification (Proposed):** PDI-RSIA

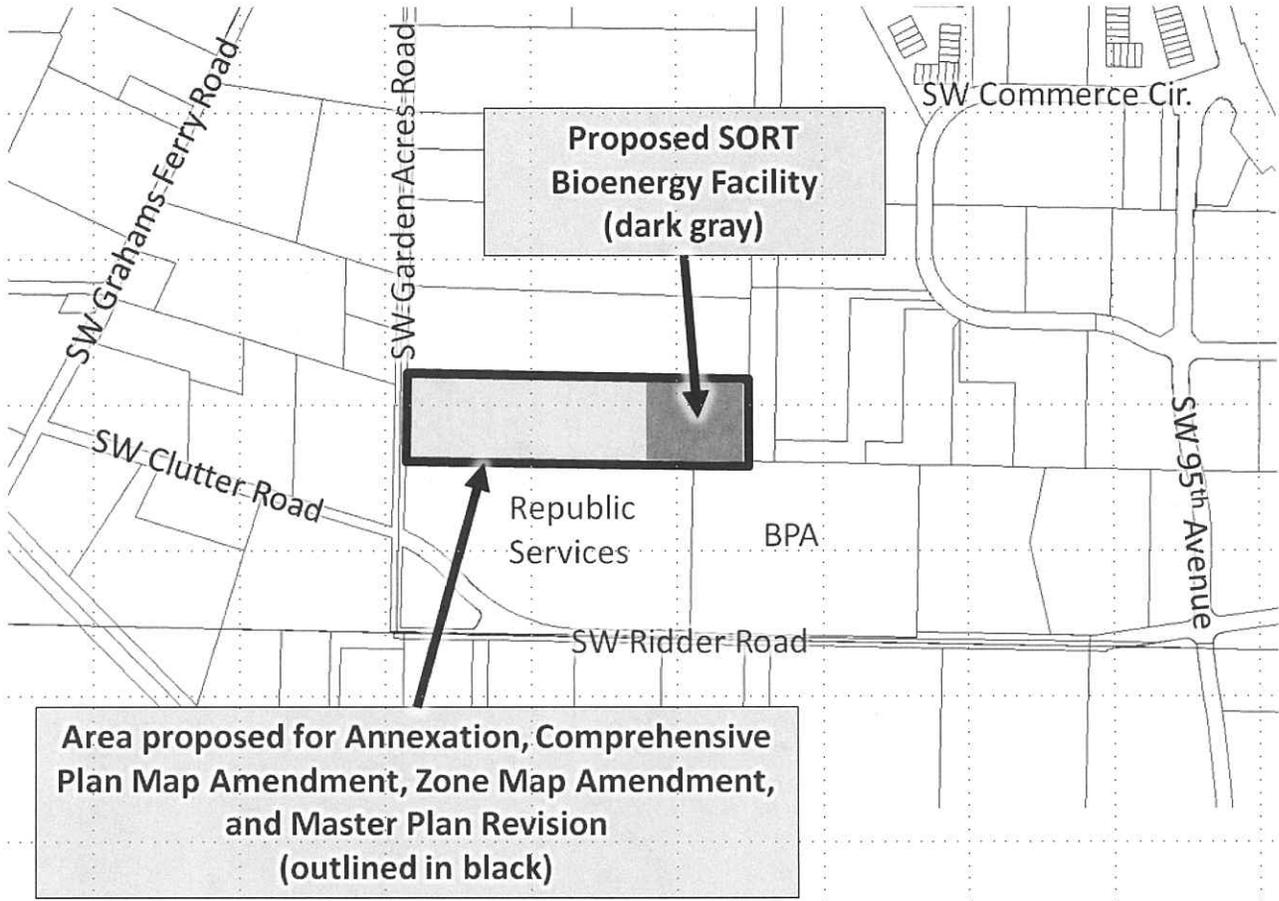
**Staff Reviewers:** Daniel Pauly AICP, Associate Planner

**Staff Recommendation:** Approve the requested Annexation.

**Applicable Review Criteria:**

<b>Development Code:</b>	
Section 4.700	Annexations and Urban Growth Boundary Amendments
<b>Other Planning Documents:</b>	
Wilsonville Comprehensive Plan	
Chapter 3.09 Metro Code	
ORS 222.111, 222.120, 222.125	
Oregon Statewide Planning Goals	

**Vicinity Map**



**Summary:**

Republic Services has a growing operation in Wilsonville. The Wilsonville campus currently includes a Materials Recovery Facility (MRF) for processing and sorting waste, collection operations offices, maintenance shop, and storage and support services. Republic Services and their predecessor companies have owned 10 acres north of their campus for many years, which currently sits outside Wilsonville City limits. In support of their continued growth, Republic Services wishes to annex the property into the City. Adjacent public right-of-way will also be annexed. The annexation follows all applicable regional and state rules and statutes. By bringing the land into the boundary of the City, the annexation sets the stage for the applicant to request other approvals from the City.

**Conclusion and Conditions of Approval:**

Staff and the DRB have reviewed the application and facts regarding the request and recommend the City Council approve the Annexation (DB16-0004).

**Procedural Statements and Background Information:**

1. The statutory 120-day time limit applies to this application. The application was received on February 5, 2016. On March 2, 2016 staff conducted a completeness review within the statutorily allowed 30-day review period and found the application to be incomplete. On March 21, 2016 the Applicant submitted new materials. On April 19, 2016 the application was deemed complete. The City must render a final decision for the request, including any appeals, by August 17, 2016.
2. Surrounding land uses are as follows:

Compass Direction	Zone:	Existing Use:
North:	FD-20	Rural
East:	PDI	Industrial Development
South:	PDI/PF	Republic Services/BPA Substation
West:	FD-20	Rural

3. Previous Planning Approvals:
  - Case File 91PC33 – Stage I Master Plan Willamette Resources
  - Case File 94DR18 – Site Design Review Willamette Resources
  - Case File 99DB03 – Site Design Review Keller Drop Box
  - Case File DB14-0032 Stage I Master Plan Revision
  - Case File DB14-0033 Stage II Final Plan for Maintenance Facility
  - Case File DB14-0034 Site Design Review for Maintenance Facility
  - Case File DB14-0035 Type C Tree Plan
4. The applicant has complied with Sections 4.008 through 4.011, 4.013-4.031, and 4.035 of the Wilsonville Code. Said sections pertain to review procedures and submittal requirements.

## Findings of Fact:

NOTE: Pursuant to Section 4.014 the burden of proving that the necessary findings of fact can be made for approval of any land use or development application rests with the applicant in the case.

### General Information

#### Application Procedures-In General Section 4.008

**Review Criteria:** This section lists general application procedures applicable to a number of types of land use applications and also lists unique features of Wilsonville's development review process.

**Finding:** These criteria are met.

**Details of Finding:** The application is being processed in accordance with the applicable general procedures of this Section.

#### Initiating Application Section 4.009

**Review Criterion:** "Except for a Specific Area Plan (SAP), applications involving specific sites may be filed only by the owner of the subject property, by a unit of government that is in the process of acquiring the property, or by an agent who has been authorized by the owner, in writing, to apply."

**Finding:** This criterion is satisfied.

**Details of Finding:** The application has been submitted on behalf of the property owner, Republic Services and is signed by an authorized representative.

#### Pre-Application Conference Subsection 4.010 (.02)

**Review Criteria:** This section lists the pre-application process

**Finding:** These criteria are satisfied.

**Details of Finding:** A Pre-application conferences were held on October 16, 2014 (PA14-0015) in accordance with this subsection.

#### Lien Payment before Approval Subsection 4.011 (.02) B.

**Review Criterion:** "City Council Resolution No. 796 precludes the approval of any development application without the prior payment of all applicable City liens for the subject property. Applicants shall be encouraged to contact the City Finance Department to verify that there are no outstanding liens. If the Planning Director is advised of outstanding liens while an application is under consideration, the Director shall advise the applicant that payments must be made current or the existence of liens will necessitate denial of the application."

**Finding:** This criterion is satisfied.

**Details of Finding:** No applicable liens exist for the subject property. The application can thus move forward.

#### General Submission Requirements

Subsection 4.035 (.04) A.

**Review Criteria:** "An application for a Site Development Permit shall consist of the materials specified as follows, plus any other materials required by this Code." Listed 1. through 6. j.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant has provided all of the applicable general submission requirements contained in this subsection.

#### Zoning-Generally

Section 4.110

**Review Criteria:** "The use of any building or premises or the construction of any development shall be in conformity with the regulations set forth in this Code for each Zoning District in which it is located, except as provided in Sections 4.189 through 4.192." "The General Regulations listed in Sections 4.150 through 4.199 shall apply to all zones unless the text indicates otherwise."

**Finding:** These criteria are satisfied.

**Details of Finding:** This proposed development is in conformity with the applicable zoning district and general development regulations listed in Sections 4.150 through 4.199 have been applied in accordance with this Section.

### DB16-0004 Annexation

#### Comprehensive Plan

##### Allowed Annexation

Implementation Measure 2.2.1.a.

A1. **Review Criteria:** "Allow annexation when it is consistent with future planned public services and when a need is clearly demonstrated for immediate urban growth."

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The land proposed for annexation is within the Coffee Creek Master Plan area, which is planned for industrial development within the City and for which public services are planned.

##### Annexation Review Standards

Implementation Measure 2.2.1.e.

A2. **Review Criteria:** "Changes in the City boundary will require adherence to the annexation procedures prescribed by State law and Metro standards. Amendments to the City limits shall be based on consideration of:" Listed 1 through 5.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** Necessary public facilities are available for the land proposed for annexation, is planned for industrial development as part of the City's Coffee Creek Master Plan. The land is within the Urban Growth Boundary and sufficient land for planned uses is not available on Republic Services land within the City. The annexation and development of the land is required to be consistent with Statewide Planning Goals and applicable Metro plans.

## **Development Code**

### **Authority to Review Annexation**

Subsections 4.030 (.01) A, 11, 4.031 (.01) K, and 4.033 (.01) F.

- A3. **Review Criteria:** These subsections prescribe the authority of the Planning Director to determine whether an annexation request is legislative or quasi-judicial, the DRB does the initial review of quasi-judicial annexation, and the City Council takes final local action of quasi-judicial annexation.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The subject annexation request has been determined to be quasi-judicial and is being reviewed by the DRB and City Council consistent with these subsections.

### **Annexation**

Section 4.700

- A4. **Review Criteria:** This section defines the criteria and process for annexation review within the City.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant has met all submittal requirements and procedures described in this including submission of a petition, legal descriptions describing the land to be annexed, an analysis of the relationship with the Comprehensive Plan, state statutes, Statewide Planning Goals, and Metro plans.

## **Metro Code**

### **Local Government Boundary Changes**

Chapter 3.09

- A5. **Review Criteria:** This chapter establishes hearing, notice, and decision requirements as well as review criteria for local government boundary changes in the Metro region.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** A public hearing will be held within 45 days of completeness. Notice has been mailed and posted on the property 20 days prior to the hearing and includes the required information. The decision will be mailed to Metro and other required parties. A petition has been submitted including property owner information, jurisdictional information, and a legal description of the property.

## Oregon Revised Statutes

### Authority and Procedure for Annexation ORS 222.111

A6. **Review Criteria:** ORS 222.111 establishes the authority and procedures for annexation by City's within the state of Oregon.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The owner of the property has initiated the annexation. An election is not required pursuant to ORS 222.120.

### Procedure Without Election by City Electors ORS 222.120

A7. **Review Criteria:** ORS 222.120 establishes the authority and procedures for annexation by City's within the state of Oregon without an election.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** A public hearing has been set between. Required notices have been or will be sent. The annexation is being adopted by Ordinance by the City Council. No electors reside on the property. All property owners have consented to annexation by signing the petition thus the ordinance can declare the property annexed with no votes by electors within the territory to be annexed.

### Annexation by Consent of All Owners of Land and Majority of Electors ORS 222.125

A8. **Review Criteria:** "The legislative body of a city need not call or hold an election in the city or in any contiguous territory proposed to be annexed or hold the hearing otherwise required under ORS 222.120 (Procedure without election by city electors) when all of the owners of land in that territory and not less than 50 percent of the electors, if any, residing in the territory consent in writing to the annexation of the land in the territory and file a statement of their consent with the legislative body. Upon receiving written consent to annexation by owners and electors under this section, the legislative body of the city, by resolution or ordinance, may set the final boundaries of the area to be annexed by a legal description and proclaim the annexation."

**Finding:** These criteria are satisfied.

**Explanation of Finding:** All property owners of territory proposed to be annexed have provided their consent in writing. No electors reside within the territory proposed to be annexed. However, a public hearing process is being followed as prescribed in the City's Development Code concurrent with a Comprehensive Plan Map and Zone Map Amendment request and other quasi-judicial land use applications.

## Oregon Statewide Planning Goals

Goals 1, 2, 5, 6, 8, 9, 10, 11, 12, 13

**A9. Review Criteria:** The goals include: citizen involvement, land use planning, natural resources and open spaces, air water and land resource quality, recreational needs, economic development, housing, public facilities and services, transportation, and energy conservation.

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The area requested to be annexed will be developed consistent with the City's Comprehensive Plan and the Coffee Creek Master Plan, both which have been found to meet the statewide planning goals.

**ORDINANCE NO. 793**

**AN ORDINANCE OF THE CITY OF WILSONVILLE APPROVING A COMPREHENSIVE PLAN MAP AMENDMENT FROM THE WASHINGTON COUNTY FUTURE DEVELOPMENT – 20 (FD-20) DISTRICT TO THE CITY OF WILSONVILLE INDUSTRIAL DESIGNATION ON APPROXIMATELY 10.2 ACRES COMPRISING TAX LOT 600 OF SECTION 2C, T3S, R1W, WASHINGTON COUNTY, OREGON AND ADJACENT RIGHT-OF-WAY, WILLAMETTE RESOURCE INCORPORATED, APPLICANT/OWNER.**

**RECITALS**

WHEREAS, Willamette Resources Incorporated (“Applicant”) have made a development application requesting, among other things, a Comprehensive Plan Map Amendment of the Property; and

WHEREAS, the development application form has been signed by Jason Jordan, General Manager, on behalf of Willamette Resources Incorporated, owner of real property legally described and shown in Attachment 1, attached hereto and incorporated by reference herein (“Property”); and

WHEREAS, the City desires to also place a Comprehensive Plan designation on adjacent public right-of-way annexed into the City; and

WHEREAS, the City of Wilsonville Planning Staff analyzed the Comprehensive Plan Map Amendment request and prepared a staff report for the Development Review Board, finding that the application met the requirements for a Comprehensive Plan Map Amendment and recommending approval of the Comprehensive Plan Map Amendment, which staff report was presented to the Development Review Board on May 9, 2016;

WHEREAS, the Development Review Board Panel A held a public hearing on the application for a Comprehensive Plan Map Amendment on May 9, 2016, and after taking public testimony and giving full consideration to the matter, adopted Resolution No. 327 which recommends that the City Council approve a request for a Comprehensive Plan Map Amendment (Case File DB16-0005) contingent on the concurrent Annexation; and

WHEREAS, on June 6, 2016, the Wilsonville City Council held a public hearing regarding the above described matter, wherein the City Council considered the full public record made before the Development Review Board, including the Development Review Board and City Council staff reports; took public testimony; and, upon deliberation, concluded that the

proposed Comprehensive Plan Map Amendment meets the applicable approval criteria under the City of Wilsonville Development Code;

NOW, THEREFORE, THE CITY OF WILSONVILLE ORDAINS AS FOLLOWS:

Section 1. Findings. The City Council adopts, as findings and conclusions, the forgoing Recitals and Comprehensive Plan Map Amendment Findings in Attachment 2, as if fully set forth herein.

Section 2. Order. The official City of Wilsonville Comprehensive Plan Map is hereby amended by Comprehensive Plan Map Order DB16-0008, attached hereto as Attachments 1, from Washington County FD-20 to Industrial.

SUBMITTED to the Wilsonville City Council and read the first time at a meeting thereof on the \_\_\_\_ day of June 2016, and scheduled for the second and reading on June 20, 2016, commencing at the hour of 7 p.m. at the Wilsonville City Hall, 29799 SW Town Center Loop East, Wilsonville, OR.

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder

ENACTED by the City Council on the 20<sup>th</sup> day of June, 2016, by the following

votes:

Yes:\_\_\_\_ No:\_\_\_\_

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder

DATED and signed by the Mayor this \_\_\_\_ day of June, 2016.

\_\_\_\_\_  
TIM KNAPP, MAYOR

SUMMARY OF VOTES:

- Mayor Knapp
- Councilor Starr
- Councilor Lehan
- Councilor Stevens
- Councilor Fitzgerald

Exhibits and Attachments:

Attachment 1 – Comprehensive Plan Map Order DB16-0005 including legal description and sketch depicting map amendment.

Attachment 2 – Comprehensive Plan Map Amendment Findings, May 12, 2016.

**ORDINANCE NO. 793– ATTACHMENT 1**

**BEFORE THE CITY COUNCIL OF THE  
CITY OF WILSONVILLE, OREGON**

In the Matter of the Application of )  
Willamette Resources Incorporated )  
for an Amendment ) **COMPREHENSIVE PLAN MAP**  
of the City of Wilsonville ) **ORDER DB16-0005**  
Comprehensive Plan Map )

The above-entitled matter is before the Council to consider the application of DB16-0005, for a Comprehensive Plan Map Amendment and an Order, amending the official Comprehensive Plan Map of the City of Wilsonville.

The Council finds that the subject property (“Property”), legally described and shown in the attached legal description and sketch, has heretofore appeared on the Washington County Comprehensive Plan Map with a designation of FD-20.

The Property is being annexed into the City of Wilsonville by Ordinance No. 792.

The Council having heard and considered all matters relevant to the application for a Comprehensive Plan Map Amendment, including the Development Review Board record and recommendation, finds that the application should be approved.

THEREFORE IT IS HEREBY ORDERED that The Property, consisting of approximately 10.2 including tax lot 600 of Section 2C, T3S, R1W, Washington County, Oregon and adjacent right-of-way known as SW Garden Acres Road, as more particularly shown and described in the attached legal description and sketch, is hereby designated as Industrial, subject to conditions detailed in this Order’s adopting Ordinance. The foregoing designation is hereby declared an amendment to the Wilsonville Comprehensive Plan Map and shall appear as such from and after entry of this Order.

Dated: June 20, 2016.

\_\_\_\_\_  
TIM KNAPP, MAYOR

APPROVED AS TO FORM:

---

Barbara A. Jacobson, City Attorney

ATTEST:

---

Sandra C. King, MMC, City Recorder

Attachment: Legal Description and Sketch Depicting Properties Subject to the Comprehensive Plan Map Amendment

EXHIBIT "A"

A TRACT OF LAND SITUATE IN THE SOUTHWEST ONE-QUARTER OF SECTION 2, TOWNSHIP 3 SOUTH, RANGE 1 WEST OF THE WILLAMETTE MERIDIAN, WASHINGTON COUNTY, OREGON, BEING LOT 12, OF THE PLAT OF "GARDEN ACRES", WASHINGTON COUNTY PLAT RECORDS AND A PORTION OF GARDEN ACRES ROAD, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

**BEGINNING** AT A POINT ON THE WESTERLY RIGHT-OF-WAY LINE OF GARDEN ACRES ROAD, SAID POINT BEARS SOUTH 89°41'03" WEST, A DISTANCE OF 20.00 FEET AND SOUTH 00°18'57" EAST, A DISTANCE OF 1652.60 FEET FROM THE QUARTER CORNER COMMON TO SECTIONS 2 AND 3, THENCE LEAVING SAID WESTERLY RIGHT-OF-WAY LINE ALONG A WESTERLY EXTENSION OF THE NORTH LINE OF SAID LOT 12 AND THE NORTH LINE OF SAID LOT 12 NORTH 89°28'40" EAST, A DISTANCE OF 1344.33 FEET TO A 5/8" IRON ROD MARKING THE NORTHEAST CORNER OF SAID LOT 12; THENCE LEAVING SAID NORTH LINE ALONG THE EAST LINE OF SAID LOT 12, SOUTH 00°19'54" EAST, A DISTANCE OF 330.43 FEET TO A 3 1/2" BRASS CAP STAMPED "BONNIVILLE POWER ADMINISTRATION", MARKING THE SOUTHEAST CORNER OF SAID LOT 12; THENCE LEAVING SAID EAST LINE ALONG THE SOUTH LINE OF SAID LOT 12 AND A WESTERLY EXTENSION THEREOF SOUTH 89°28'40' WEST, A DISTANCE OF 1344.42 FEET TO A POINT ON SAID WESTERLY RIGHT-OF-WAY LINE OF GARDEN ACRES ROAD; SAID POINT BEARS SOUTH 89°41'03" WEST, A DISTANCE OF 20.00 FEET AND NORTH 00°18'57" WEST, A DISTANCE OF 661.31 FEET FROM THE SECTION CORNER COMMON TO SECTIONS 3, 2, 10 AND 11; THENCE ALONG SAID WESTERLY RIGHT-OF-WAY LINE NORTH 00°18'57" WEST, A DISTANCE OF 330.43 FEET TO THE POINT OF BEGINNING.

CONTAINS 444,223 SQUARE FEET OR 10.198 ACRES, MORE OR LESS.

ANNEXATION CERTIFIED

BY VF

NOV 16 2015

WASHINGTON COUNTY A & T  
CARTOGRAPHY

REGISTERED  
PROFESSIONAL  
LAND SURVEYOR



OREGON  
JULY 15, 2003  
TOD V. KELSO  
50701

EXPIRATION DATE: 6/30/17

# LOT CONSOLIDATION EXHIBIT

ANNEXATION CERTIFIED

BY *VF*

NOV 16 2015

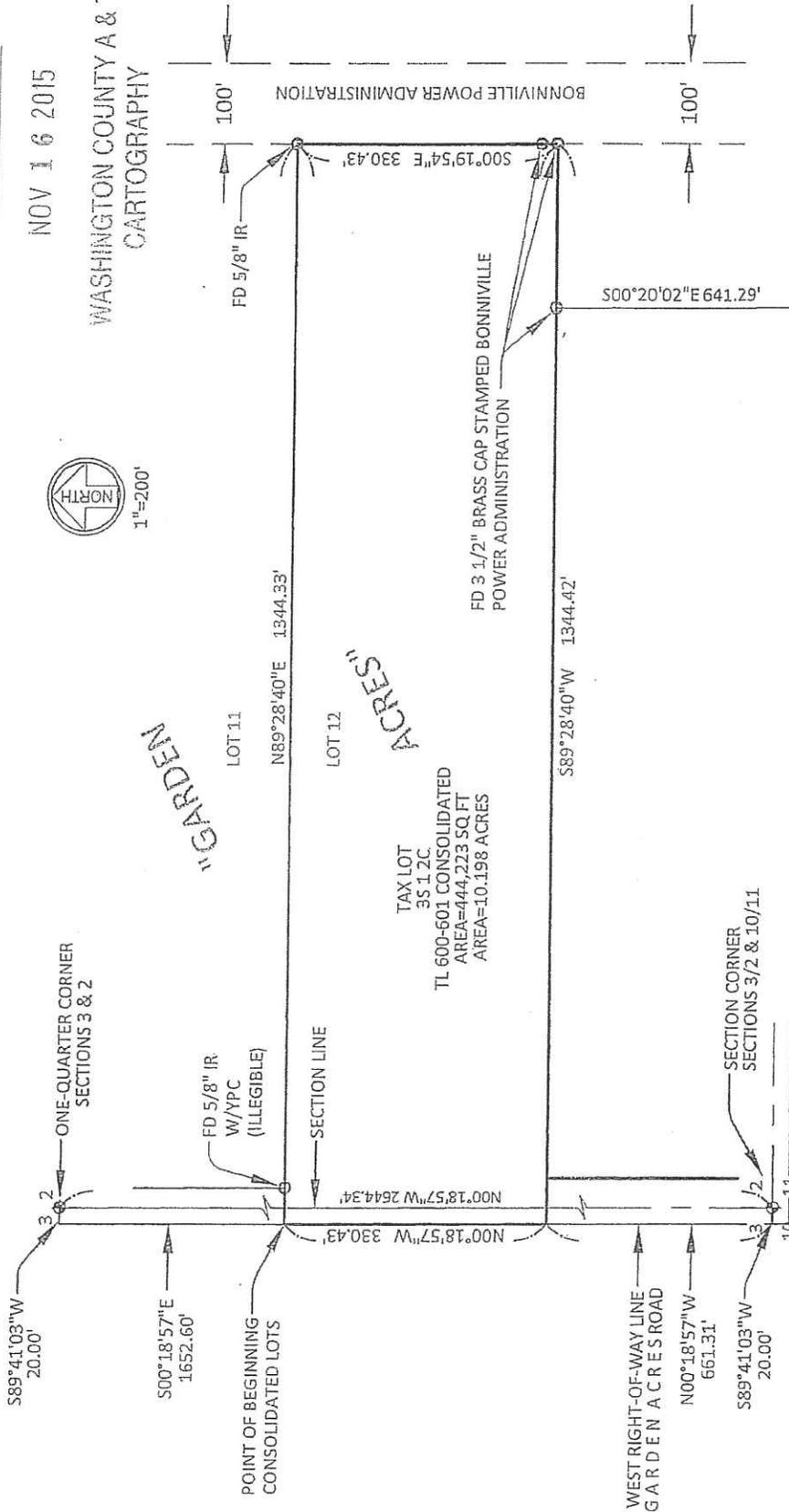
WASHINGTON COUNTY A & T  
CARTOGRAPHY



"GARDEN"  
LOT 11  
LOT 12

ACRES"

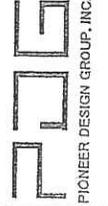
TAX LOT  
35.1.2C  
TL 600-601 CONSOLIDATED  
AREA=444,223 SQ. FT  
AREA=10.198 ACRES



Project  
REP. SERV. ANNEXATION

No. 999-158B

Designed by TVK Date 2015-11-03  
Drawn by TVK Date 2015-11-09  
Horiz. Scale: 1"=200' Vert. Scale:  
999-158B PH2 LLA EXHIBIT Type EXHIBIT



9020 SW WASHINGTON SQUARE RD.  
SUITE 170  
PORTLAND, OREGON 97223  
P 503.443.8200  
F 503.415.6143  
www.pds-llc.com

Sheet 2 of 2

Ordinance No. 793  
 Staff Report  
 Wilsonville Planning Division

Republic Services Expansion  
 Comprehensive Plan Map Amendment

City Council  
 Quasi-Judicial Public Hearing

<b>Hearing Date:</b>	June 6, 2016
<b>Date of Report:</b>	May 12, 2016
<b>Application Nos.:</b>	DB16-0005 Comprehensive Plan Map Amendment

**Request:** The City Council is being asked to review a Comprehensive Plan Map Amendment from Washington County FD-20 to City of Wilsonville Industrial on approximately 10.2 acres of property owned by Republic Services to expand their campus and adjacent right-of-way.

**Location:** Segment of Garden Acres Road and a parcel east of Garden Acres Road north of current Republic Services buildings The property is specifically known as Tax Lots 600, Section 2C, Township 3 South, Range 1 West, Willamette Meridian, City of Wilsonville, Washington County, Oregon

**Owner/Petitioner:** Jason Jordan, Willamette Resources Inc.

**Applicants'**

**Representative:** Ben Altman, Pioneer Design Group

**Comprehensive Plan Designation (Current):** FD-20

**Comprehensive Plan Designation (Proposed):** Industrial

**Staff Reviewers:** Daniel Pauly AICP, Associate Planner

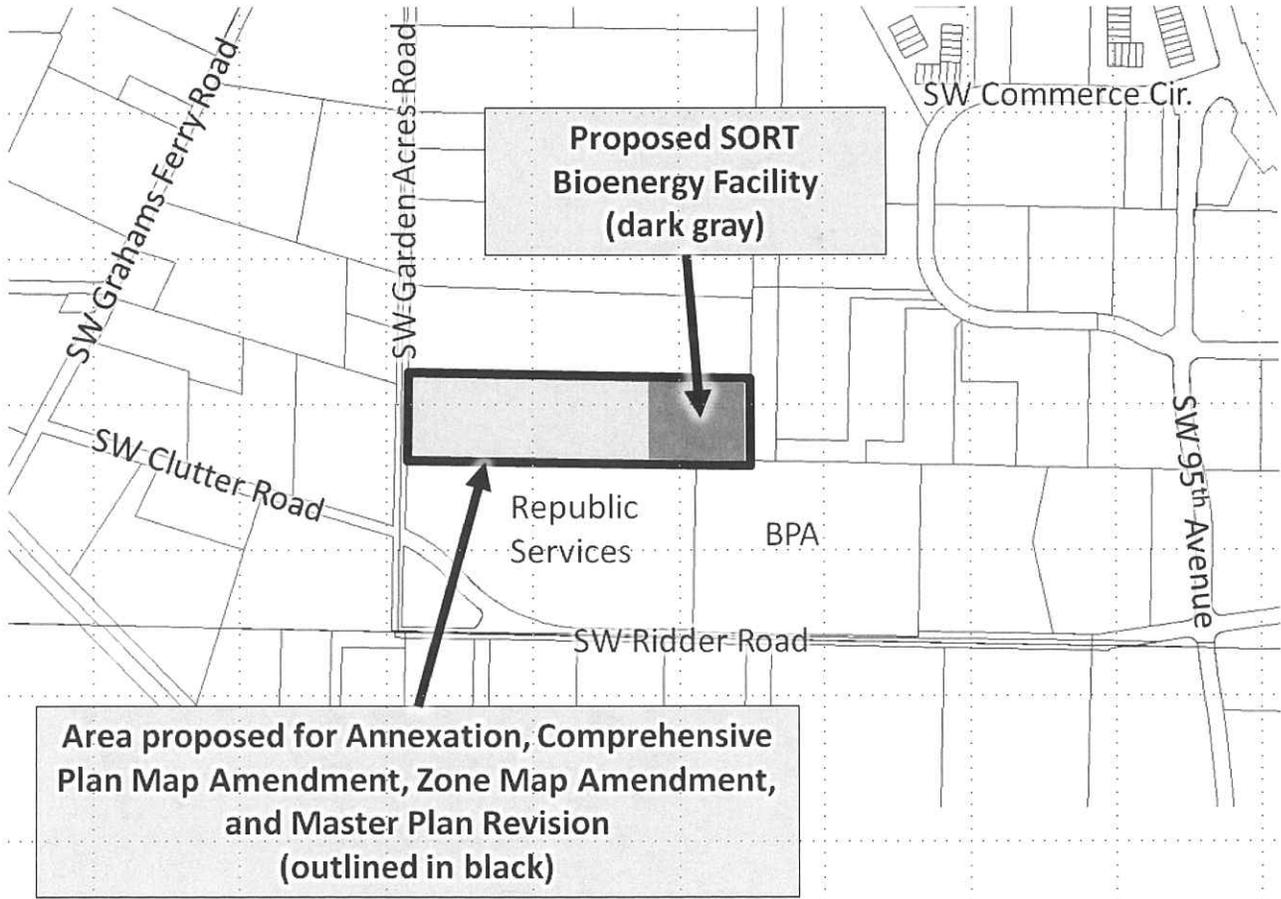
**Staff Recommendation:** Approve the requested Comprehensive Plan Map Amendment.

**Applicable Review Criteria:**

<b>Development Code:</b>	
Section 4.008	Application Procedures-In General
Section 4.009	Who May Initiate Application
Section 4.010	How to Apply
Section 4.011	How Applications are Processed
Section 4.014	Burden of Proof
Section 4.031	Authority of the Development Review Board
Subsection 4.035 (.04)	Site Development Permit Application
Subsection 4.035 (.05)	Complete Submittal Requirement

Section 4.198	Comprehensive Plan Changes
<b>Other Documents:</b>	
Comprehensive Plan	
Oregon Statewide Planning Goals	

**Vicinity Map**



**Background/Summary:**

Republic Services has a growing operation in Wilsonville. The Wilsonville campus currently includes a Materials Recovery Facility (MRF) for processing and sorting waste, collection operations offices, maintenance shop, and storage and support services. Republic Services and their predecessor companies have owned 10 acres north of their campus for many years, which currently sits outside Wilsonville City limits. In support of their continued growth and requested annexation, Republic Services requests the City adopt an appropriate Comprehensive Plan designation.

The subject property is within the Coffee Creek Master Plan area. The Coffee Creek Master Plan calls for industrial development. Consistent with the Coffee Creek Master Plan Republic Services proposes the subject property be designated as "Industrial" on the City's Comprehensive Plan Map.

**Conclusion and Conditions of Approval:**

Staff and the DRB have reviewed the application and facts regarding the request and recommend the City Council approve the Comprehensive Plan Map Amendment (DB16-0005).

**Procedural Statements and Background Information:**

1. The statutory 120-day time limit applies to this application. The application was received on February 5, 2016. On March 2, 2016 staff conducted a completeness review within the statutorily allowed 30-day review period and found the application to be incomplete. On March 21, 2016 the Applicant submitted new materials. On April 19, 2016 the application was deemed complete. The City must render a final decision for the request, including any appeals, by August 17, 2016.
2. Surrounding land uses are as follows:

Compass Direction	Zone:	Existing Use:
North:	FD-20	Rural
East:	PDI	Industrial Development
South:	PDI/PF	Republic Services/BPA Substation
West:	FD-20	Rural

3. Previous Planning Approvals:
  - Case File 91PC33 – Stage I Master Plan Willamette Resources
  - Case File 94DR18 – Site Design Review Willamette Resources
  - Case File 99DB03 – Site Design Review Keller Drop Box
  - Case File DB14-0032 Stage I Master Plan Revision
  - Case File DB14-0033 Stage II Final Plan for Maintenance Facility
  - Case File DB14-0034 Site Design Review for Maintenance Facility
  - Case File DB14-0035 Type C Tree Plan

4. The applicant has complied with Sections 4.008 through 4.011, 4.013-4.031, and 4.035 of the Wilsonville Code. Said sections pertain to review procedures and submittal requirements.

### **Findings of Fact:**

NOTE: Pursuant to Section 4.014 the burden of proving that the necessary findings of fact can be made for approval of any land use or development application rests with the applicant in the case.

## **General Information**

### Application Procedures-In General

Section 4.008

**Review Criteria:** This section lists general application procedures applicable to a number of types of land use applications and also lists unique features of Wilsonville's development review process.

**Finding:** These criteria are met.

**Details of Finding:** The application is being processed in accordance with the applicable general procedures of this Section.

### Initiating Application

Section 4.009

**Review Criterion:** "Except for a Specific Area Plan (SAP), applications involving specific sites may be filed only by the owner of the subject property, by a unit of government that is in the process of acquiring the property, or by an agent who has been authorized by the owner, in writing, to apply."

**Finding:** This criterion is satisfied.

**Details of Finding:** The application has been submitted on behalf of the property owner, Republic Services and is signed by an authorized representative.

### Pre-Application Conference

Subsection 4.010 (.02)

**Review Criteria:** This section lists the pre-application process

**Finding:** These criteria are satisfied.

**Details of Finding:** A Pre-application conferences were held on October 16, 2014 (PA14-0015) in accordance with this subsection.

### Lien Payment before Approval

Subsection 4.011 (.02) B.

**Review Criterion:** "City Council Resolution No. 796 precludes the approval of any development application without the prior payment of all applicable City liens for the subject property. Applicants shall be encouraged to contact the City Finance Department to verify that there are

no outstanding liens. If the Planning Director is advised of outstanding liens while an application is under consideration, the Director shall advise the applicant that payments must be made current or the existence of liens will necessitate denial of the application.”

**Finding:** This criterion is satisfied.

**Details of Finding:** No applicable liens exist for the subject property. The application can thus move forward.

#### General Submission Requirements

Subsection 4.035 (.04) A.

**Review Criteria:** “An application for a Site Development Permit shall consist of the materials specified as follows, plus any other materials required by this Code.” Listed 1. through 6. j.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant has provided all of the applicable general submission requirements contained in this subsection.

#### Zoning-Generally

Section 4.110

**Review Criteria:** “The use of any building or premises or the construction of any development shall be in conformity with the regulations set forth in this Code for each Zoning District in which it is located, except as provided in Sections 4.189 through 4.192.” “The General Regulations listed in Sections 4.150 through 4.199 shall apply to all zones unless the text indicates otherwise.”

**Finding:** These criteria are satisfied.

**Details of Finding:** This proposed development is in conformity with the applicable zoning district and general development regulations listed in Sections 4.150 through 4.199 have been applied in accordance with this Section.

### **Request: DB16-0005 Comprehensive Plan Map Amendment**

#### **Zoning and Land Development Ordinance**

#### **Comprehensive Plan Amendment Process**

Procedures and Criteria in Comprehensive Plan  
Subsection 4.198 (.01)

**B1. Review Criteria:** “Proposals to amend the Comprehensive Plan, or to adopt new elements or sub-elements of the Plan, shall be subject to the procedures and criteria contained in the Comprehensive Plan.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The proposed map amendment is being reviewed against applicable Comprehensive Plan criteria, and procedures described in the Comprehensive Plan are being followed.

Review Bodies

Subsection 4.198 (.02)

**B2. Review Criteria:** “Following the adoption and signature of the Resolution by the Development Review Board or Planning Commission, together with minutes of public hearings on the proposed Amendment, the matter shall be shall be scheduled for public hearing before the City Council.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The DRB and City Council are considering the matter as described.

Applicant Agreeing to Conditions of Approval

Subsection 4.198 (.05)

**B3. Review Criteria:** “In cases where a property owner or other applicant has requested an amendment to the Comprehensive Plan map and the City Council has approved the change subject to conditions, the owner or applicant shall sign a statement accepting, and agreeing to complete the conditions of approval before the Comprehensive Plan map shall be changed.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The owner will be required to sign a statement accepting any conditions.

**Comprehensive Plan Amendment Required Findings**

Meets Identified Public Need

Subsection 4.198 (.01) A.

**B4. Review Criteria:** “Each such amendment shall include findings in support of the following: That the proposed amendment meets a public need that has been identified;”

**Finding:** These criteria are satisfied.

**Details of Finding:** The Coffee Creek Master Plan has identified the subject territory as industrial land for needed employment land for the City and the region. Changing the Comprehensive Plan designation from Washington County’s current designation of FD-20 to the City designation of “Industrial” reflects the need established in the Coffee Creek Master Plan.

Meets Identified Public Need As Well As Reasonable Alternative

Subsection 4.198 (.01) B.

**B5. Review Criteria:** “Each such amendment shall include findings in support of the following: That the proposed amendment meets the identified public need at least as well as any other amendment or change that could reasonably be made;”

**Finding:** These criteria are satisfied.

**Details of Finding:** As part of the Coffee Creek Master Plan, the subject land is identified for industrial development, thus no other City Comprehensive Plan Map designation is feasible upon annexation.

Supports Statewide Planning Goals  
Subsection 4.198 (.01) C.

**B6. Review Criteria:** “Each such amendment shall include findings in support of the following: That the proposed amendment supports applicable Statewide Planning Goals, or a Goal exception has been found to be appropriate;”

**Finding:** These criteria are satisfied.

**Details of Finding:** The City of Wilsonville’s Comprehensive Plan and the Coffee Creek Master Plan have been found to be consistent with Statewide Planning Goals. By being consistent with the Comprehensive Plan and Coffee Creek Master Plan, the proposed Comprehensive Plan Map Amendment is also consistent with the Statewide Planning Goals.

No Conflict with Other Portions of Plan  
Subsection 4.198 (.02) D.

**B7. Review Criteria:** “Each such amendment shall include findings in support of the following: That the proposed change will not result in conflicts with any portion of the Comprehensive Plan that is not being amended.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant is requesting an amendment of the Comprehensive Plan Map for the subject properties. The applicant does not propose to modify or amend any other portion of the Comprehensive Plan or Plan Map. The “Industrial” designation reflects the Coffee Creek Master Plan, an adopted sub-element of the Comprehensive Plan.

**Comprehensive Plan and Plan Components**

For a majority of Comprehensive Plan Goals, Policies, and Implementation Measures compliance is ensured by the development code standards applied to any development on the proposed property. The following findings are limited to procedural language and Comprehensive Plan language without a clear relationship to implementing development code standards.

**Initiating, Applying for, and Considering Plan Amendments**

Who May Initiate Plan Amendments  
Introduction Page 7 “Plan Amendments” 1.

**B8. Review Criteria:** “An Amendment to the adopted Plan may be initiated by: a. The City Council, b. The Planning Commission (for legislative amendments) or Development Review Board (for quasi-judicial amendments); or c. Application of property owner(s) or contract purchaser(s) affected or their authorized agents, as specified in #2 below.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The property owner of the subject property initiated the proposed

amendment.

#### How to Make Application

Introduction Page 7 "Plan Amendments" 2.

**B9. Review Criteria:** "An application for an amendment to the Plan maps or text shall be made on forms provided by the City. The application, except when initiated by the City Council, DRB, or Planning Commission, as noted in #1, above, shall be accompanied by a Plan Amendment Fee.

**Finding:** These criteria are satisfied.

**Details of Finding:** The proposed amendment has been initiated by the property owner of the subject property who has submitted a signed application form provided by the City and paid the required application fee.

#### Consideration of Plan Amendments

Introduction Page 7 "Plan Amendments" 3.

**B10. Review Criteria:** This language specifies how the City should consider a plan amendment including: requiring the City Council consider a plan amendment only after receiving findings and recommendation from the Planning Commission or Development Review Board; having sufficient time before the first evidentiary hearing for public notice and staff report preparation, considering compliance with Statewide Planning Goals and applicable Metro Plans.

**Finding:** These criteria are satisfied.

**Details of Finding:** The City Council will consider the plan amendment only after receiving a recommendation from the Development Review Board.

### **Standards for Approval of Plan Amendments**

#### Conformance with Other Portions of the Plan

Introduction Page 7 "Plan Amendments" 4. a.

**B11. Review Criterion:** "The proposed amendment is in conformance with those portions of the Plan that are not being considered for amendment."

**Finding:** This criterion is satisfied.

**Details of Finding:** The designation of newly annexed land consistent with the Coffee Creek Master Plan, an adopted sub-element of the Comprehensive Plan, does not create any conflicts with the portions of the plan not being considered for amendment.

#### Public Interest

Introduction Page 7 "Plan Amendments" 4. b.

**B12. Review Criterion:** "The granting of the amendment is in the public interest."

**Finding:** This criterion is satisfied.

**Details of Finding:** A public interest is being met by bringing industrial land into the City consistent with the Coffee Creek Master Plan. See also Finding B4.

Public Interest Best Served by Timing of Amendment  
Introduction Page 7 "Plan Amendments" 4. c.

**B13. Review Criterion:** "The public interest is best served by granting the amendment at this time."

**Finding:** This criterion is satisfied.

**Details of Finding:** The timing of the amendment is appropriate. See Finding B5.

Factors to Address in Amendment  
Introduction Page 7 "Plan Amendments" 4. d.

**B14. Review Criterion:** "The following factors have been adequately addressed in the proposed amendment:

- the suitability of the various areas for particular land uses and improvements;
- the land uses and improvements in the area;
- trends in land improvement;
- density of development;
- property values;
- the needs of economic enterprises in the future development of the area;
- transportation access;
- natural resources; and
- the public need for healthful, safe and aesthetic surroundings and conditions.

**Finding:** This criterion is satisfied.

**Details of Finding:** All of the listed items were considered in the Coffee Creek Master Plan, an industrial use is appropriate for the subject land consistent with the master plan.

Conflict with Metro Requirements  
Introduction Page 7 "Plan Amendments" 4. e.

**B15. Review Criterion:** "Proposed changes or amendments to the Comprehensive Plan do not result in conflicts with applicable Metro requirements."

**Finding:** This criterion is satisfied.

**Details of Finding:** No conflicts with Metro requirements have been identified.

Public Notice Requirements  
Introduction Page 8 "Plan Amendments" 5.

**B16. Review Criterion:** This language describes the noticing requirements implemented by the City's noticing requirements for quasi-judicial review.

**Finding:** This criterion is satisfied.

**Details of Finding:** Public hearing notices have or will be sent as required.

## Land Use and Development

### Balance of Different Land Uses

Goal 4.1 and Policy 4.1.1

**B17. Review Criterion:** “The City of Wilsonville shall make land use and planning decisions to achieve Goal 4.1: To have an attractive, functional, economically vital community with a balance of different types of land uses.”

**Finding:** This criterion is satisfied.

**Details of Finding:** The designation of newly annexed land as “Industrial” consistent with the Coffee Creek Master Plan, an adopted sub-element of the Comprehensive Plan, maintains the mix of land uses planned and for the City.

### Favoring Capital Intensive Industries

Implementation Measure 4.1.3.c.

**B18. Review Criterion:** “Favor capital intensive, rather than labor intensive, industries within the City.

**Finding:** This criterion is satisfied.

**Details of Finding:** While it is unclear the mechanism to favor such industries, in accompanying applications the subject land is planned for capital intensive waste processing and support facilities.

## Areas of Special Concern

### Area H

**B19. Review Criterion:** “Note: the previous Area 8 has been replaced with Area H, dealing with the Day Road area, northwest of the current City limits, including the new State prison. This area is bordered by Clay and Day Roads on the north and railroad tracks on the west.

A master plan for this neighborhood will be needed to address property-owner concerns and mitigate the effects of the 110-acre prison development. The City is providing urban services to the prison prior to annexation, and expects to provide services to the entire area when it has been master planned and annexed.”

**Finding:** This criterion is satisfied.

**Details of Finding:** Area H has been further implemented by the Coffee Creek Master Plan of which the subject property is a part.

## Coffee Creek Master Plan

### Land Use

Table 1 Master Plan Summary, Page 2

**B20. Review Criteria:** This table identifies the land use for the Coffee Creek Master Plan as “Regionally Significant Industrial Area, allows light industrial with strict limits on non-industrial uses.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The proposed Comprehensive Plan Map designation of “Industrial” allows for development of land uses consistent with the Coffee Creek Master Plan.

### **Metro Urban Growth Managements Functional Plan**

#### **Industrial and Other Employment Areas**

##### **Title 4**

**B21. Review Criteria:** This title establishes the regional requirements for industrial and other employment areas including protection of regionally significant industrial areas for industrial development especially on large lots, and limits to retail uses in all industrial and employment areas.

**Finding:** These criteria are satisfied.

**Details of Finding:** Conformance with Title 4 is ensured by the development code standards applied to any development on the proposed property. The proposed Comprehensive Plan designation of “Industrial” and concurrent rezone to Planned Development Industrial Regionally Significant Industrial Area allowed the appropriate implementing development code standards to apply.

**ORDINANCE NO. 794**

**AN ORDINANCE OF THE CITY OF WILSONVILLE APPROVING A ZONE MAP AMENDMENT FROM THE WASHINGTON COUNTY FUTURE DEVELOPMENT-20 (FD-20) ZONE TO THE PLANNED DEVELOPMENT INDUSTRIAL-REGIONALLY SIGNIFICANT INDUSTRIAL AREA (PDI-RSIA) ZONE ON APPROXIMATELY 10.2 ACRES COMPRISING TAX LOT 600 OF SECTION 2C, T3S, R1W, WASHINGTON COUNTY, OREGON AND ADJACENT RIGHT-OF-WAY, WILLAMETTE RESOURCE INCORPORATED, APPLICANT/OWNER.**

**RECITALS**

WHEREAS, Willamette Resources Incorporated (“Applicant”) have made a development application requesting, among other things, a Zone Map Amendment of the Property; and

WHEREAS, the development application form has been signed by Jason Jordan, General Manager, on behalf of Willamette Resources Incorporated, owner of real property legally described and shown in Attachment 1, attached hereto and incorporated by reference herein (“Property”); and

WHEREAS, the City desires to also place a Zone Map designation on adjacent public right-of-way annexed into the City; and

WHEREAS, concurrently with the Zone Map Amendment the Applicant is requesting a change of the Comprehensive Plan Map designation to “Industrial”; and

WHEREAS, the City of Wilsonville desires to have the properties zoned consistent with the Comprehensive Plan Map designation of “Industrial” and the Metro Title 4 Map Designation of Regionally Significant Industrial Area, upon approval the requested Comprehensive Plan Map designation; and

WHEREAS, the City of Wilsonville Planning Staff analyzed the Zone Map Amendment request and prepared a staff report for the Development Review Board, finding that the application met the requirements for a Zone Map Amendment and recommending approval of the Zone Map Amendment, which staff report was presented to the Development Review Board on May 9, 2016;

WHEREAS, the Development Review Board Panel A held a public hearing on the application for a Zone Map Amendment on May 9, 2016, and after taking public testimony and giving full consideration to the matter, adopted Resolution No. 327 which recommends that the

City Council approve a request for a Zone Map Amendment (Case File DB16-0006) contingent on the concurrent Comprehensive Plan Map Amendment and Annexation; and

WHEREAS, on June 6, 2016, the Wilsonville City Council held a public hearing regarding the above described matter, wherein the City Council considered the full public record made before the Development Review Board, including the Development Review Board and City Council staff reports; took public testimony; and, upon deliberation, concluded that the proposed Zone Map Amendment meets the applicable approval criteria under the City of Wilsonville Development Code;

NOW, THEREFORE, THE CITY OF WILSONVILLE ORDAINS AS FOLLOWS:

Section 1. Findings. The City Council adopts, as findings and conclusions, the forgoing Recitals and Zone Map Amendment Findings in Attachment 2, as if fully set forth herein.

Section 2. Order. The official City of Wilsonville Zone Map is hereby amended by Zoning Order DB16-0006, attached hereto as Attachments 1, from the Washington County Future Development 20 (FD-20) Zone to Planned Development Industrial-Regionally Significant Industrial Area (PDI-RSIA) Zone.

SUBMITTED to the Wilsonville City Council and read the first time at a meeting thereof on the 6<sup>th</sup> day of June 2016, and scheduled for the second and final reading on June 20, 2016, commencing at the hour of 7 p.m. at the Wilsonville City Hall, 29799 SW Town Center Loop East, Wilsonville, OR.

ENACTED by the City Council on the 20<sup>th</sup> day of June, 2016, by the following

votes: Yes:\_\_\_ No:\_\_\_

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder

DATED and signed by the Mayor this \_\_\_ day of June, 2016.

\_\_\_\_\_  
TIM KNAPP, MAYOR

SUMMARY OF VOTES:

Mayor Knapp  
Councilor Starr  
Councilor Lehan  
Councilor Stevens  
Councilor Fitzgerald

Exhibits and Attachments:

Attachment 1 – Zoning Order DB16-0006 including legal description and sketch depicting map amendment.

Attachment 2 – Zone Map Amendment Findings, May 12, 2016.

**ORDINANCE NO. 794– ATTACHMENT 1**

**BEFORE THE CITY COUNCIL OF THE  
CITY OF WILSONVILLE, OREGON**

In the Matter of the Application of	)	
Willamette Resources Incorporated	)	
for a Rezoning of Land and Amendment	)	<b>ZONING ORDER DB16-0006</b>
of the City of Wilsonville	)	
Zoning Map Incorporated in Section 4.102	)	
of the Wilsonville Code.	)	

The above-entitled matter is before the Council to consider the application of DB16-0006, for a Zone Map Amendment and an Order, amending the official Zoning Map as incorporated in Section 4.102 of the Wilsonville Code.

The Council finds that the subject property (“Property”), legally described and shown in the attached legal description and sketch, has heretofore appeared on the Washington County Zoning Map as Future Development-20 (FD-20).

The Property is being annexed into the City of Wilsonville by Ordinance No. 792.

The Council having heard and considered all matters relevant to the application for a Zone Map Amendment, including the Development Review Board record and recommendation, finds that the application should be approved.

THEREFORE IT IS HEREBY ORDERED that The Property, consisting of approximately 10.2 including tax lot 600 of Section 2C, T3S, R1W, Washington County, Oregon and adjacent right-of-way known as SW Garden Acres Road, comprising tax lot 600 of Section 2C, T3S, R1W, Washington County, Oregon, as more particularly shown and described in the attached legal description and sketch, is hereby rezoned to Planned Development Industrial-Regionally Significant Industrial Area (PDI-RSIA), subject to conditions detailed in this Order’s adopting Ordinance. The foregoing rezoning is hereby declared an amendment to the Wilsonville Zoning Map (Section 4.102 WC) and shall appear as such from and after entry of this Order.

Dated: June 20, 2016.

\_\_\_\_\_  
TIM KNAPP, MAYOR

APPROVED AS TO FORM:

---

Barbara A. Jacobson, City Attorney

ATTEST:

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Sandra C. King, MMC, City Recorder

Attachment: Legal Description and Sketch Depicting Land/Territory to be Rezoned

EXHIBIT "A"

A TRACT OF LAND SITUATE IN THE SOUTHWEST ONE-QUARTER OF SECTION 2, TOWNSHIP 3 SOUTH, RANGE 1 WEST OF THE WILLAMETTE MERIDIAN, WASHINGTON COUNTY, OREGON, BEING LOT 12, OF THE PLAT OF "GARDEN ACRES", WASHINGTON COUNTY PLAT RECORDS AND A PORTION OF GARDEN ACRES ROAD, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

**BEGINNING** AT A POINT ON THE WESTERLY RIGHT-OF-WAY LINE OF GARDEN ACRES ROAD, SAID POINT BEARS SOUTH 89°41'03" WEST, A DISTANCE OF 20.00 FEET AND SOUTH 00°18'57" EAST, A DISTANCE OF 1652.60 FEET FROM THE QUARTER CORNER COMMON TO SECTIONS 2 AND 3, THENCE LEAVING SAID WESTERLY RIGHT-OF-WAY LINE ALONG A WESTERLY EXTENSION OF THE NORTH LINE OF SAID LOT 12 AND THE NORTH LINE OF SAID LOT 12 NORTH 89°28'40" EAST, A DISTANCE OF 1344.33 FEET TO A 5/8" IRON ROD MARKING THE NORTHEAST CORNER OF SAID LOT 12; THENCE LEAVING SAID NORTH LINE ALONG THE EAST LINE OF SAID LOT 12, SOUTH 00°19'54" EAST, A DISTANCE OF 330.43 FEET TO A 3 1/2" BRASS CAP STAMPED "BONNVILLE POWER ADMINISTRATION", MARKING THE SOUTHEAST CORNER OF SAID LOT 12; THENCE LEAVING SAID EAST LINE ALONG THE SOUTH LINE OF SAID LOT 12 AND A WESTERLY EXTENSION THEREOF SOUTH 89°28'40' WEST, A DISTANCE OF 1344.42 FEET TO A POINT ON SAID WESTERLY RIGHT-OF-WAY LINE OF GARDEN ACRES ROAD; SAID POINT BEARS SOUTH 89°41'03" WEST, A DISTANCE OF 20.00 FEET AND NORTH 00°18'57" WEST, A DISTANCE OF 661.31 FEET FROM THE SECTION CORNER COMMON TO SECTIONS 3, 2, 10 AND 11; THENCE ALONG SAID WESTERLY RIGHT-OF-WAY LINE NORTH 00°18'57" WEST, A DISTANCE OF 330.43 FEET TO THE POINT OF BEGINNING.

CONTAINS 444,223 SQUARE FEET OR 10.198 ACRES, MORE OR LESS.

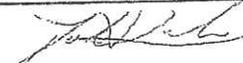
ANNEXATION CERTIFIED

BY VF

NOV 16 2015

WASHINGTON COUNTY A & T  
CARTOGRAPHY

REGISTERED  
PROFESSIONAL  
LAND SURVEYOR



OREGON  
JULY 15, 2003  
TOD V. KELSO  
50701

EXPIRATION DATE: 6/30/17

# LOT CONSOLIDATION EXHIBIT

ANNEXATION CERTIFIED

BY *VF*

NOV 16 2015

WASHINGTON COUNTY A & T  
CARTOGRAPHY



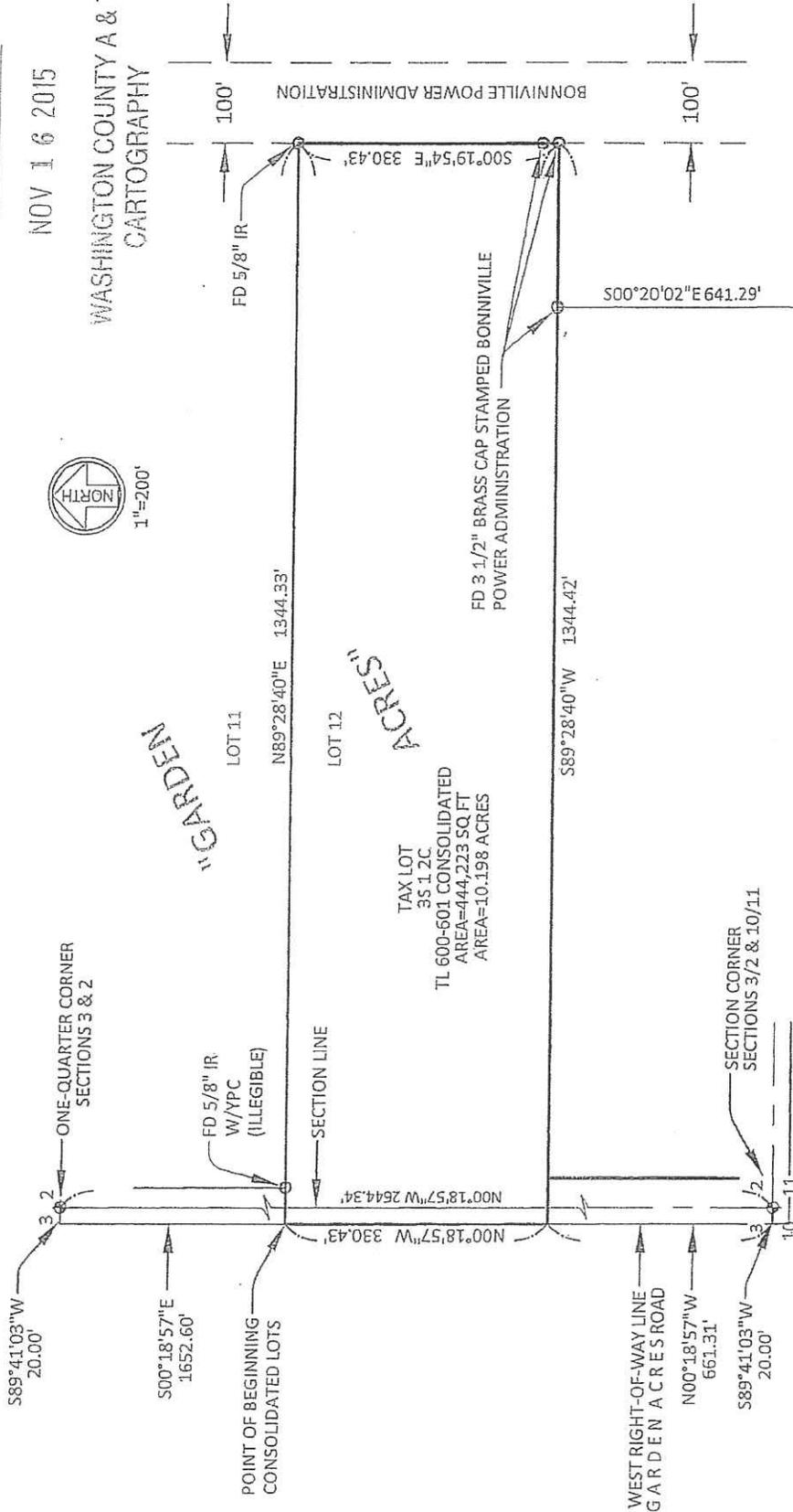
GARDEN

LOT 11

LOT 12

ACRES

TAX LOT  
35.1.2C  
TL 600-601 CONSOLIDATED  
AREA=444,223 SQ FT  
AREA=10.198 ACRES



**Project**  
REP. SERV. ANNEXATION  
**No.** 999-158B

**Designed by** TVK  
**Drawn by** TVK  
**Horiz. Scale:** 1"=200'  
**Vert. Scale:** EXHIBIT  
**Type** EXHIBIT

**2006**  
9825 SW WASHINGTON SQUARE RD.  
SUITE 170  
PORTLAND, OREGON 97223  
P. 203.443.8200  
F. 1.441.16.4143  
PIONEER DESIGN GROUP, INC. www.pdg.com

**Sheet**  
2 of 2

Ordinance No. 794  
 Staff Report  
 Wilsonville Planning Division

Republic Services Expansion  
 Comprehensive Plan Map Amendment

City Council  
 Quasi-Judicial Public Hearing

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<b>Hearing Date:</b>	June 20, 2016
<b>Date of Report:</b>	May 12, 2016
<b>Application No.:</b>	DB16-0006 Zone Map Amendment

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**Request:** The City Council is being asked to review a Zone Map Amendment from Washington County FD-20 to City of Wilsonville Planned Development Industrial-Regionally Significant Industrial Area (PDI-RSIA) on approximately 10.2 acres of property owned by Republic Services to expand their campus and adjacent right-of-way.

**Location:** Segment of Garden Acres Road and a parcel east of Garden Acres Road north of current Republic Services buildings. The property is specifically known as Tax Lots 600, Section 2C, Township 3 South, Range 1 West, Willamette Meridian, City of Wilsonville, Washington County, Oregon and adjacent right-of-way.

**Owner/Applicant:** Jason Jordan, Willamette Resources Inc.

**Applicants' Representative:** Ben Altman, Pioneer Design Group

**Comprehensive Plan Designation (Current):** FD-20  
**Comprehensive Plan Designation (Proposed):** Industrial

**Zone Map Classification (Current):** FD-20  
**Zone Map Classification (Proposed):** PDI-RSIA

**Staff Reviewers:** Daniel Pauly AICP, Associate Planner

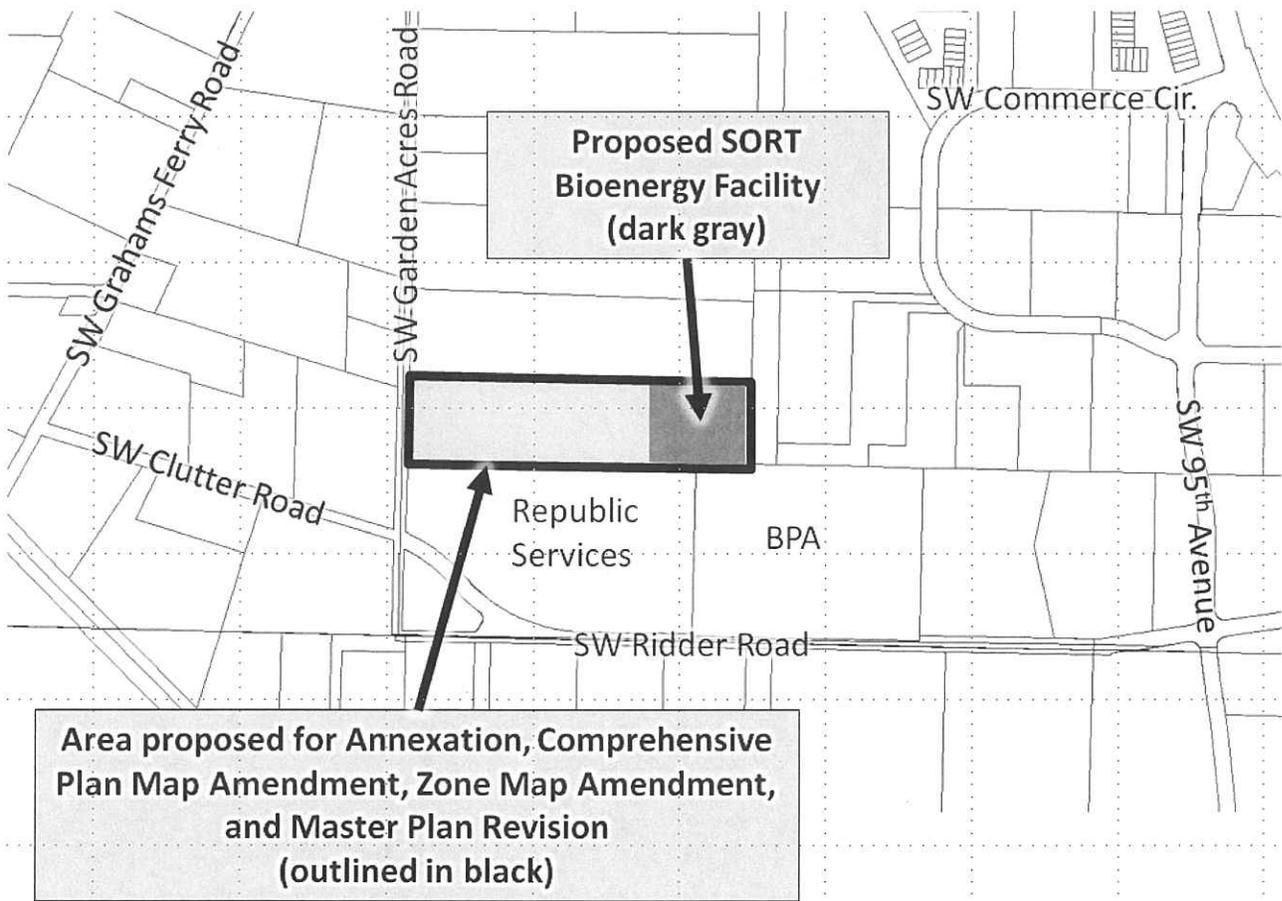
**Staff Recommendation:** Approve the requested Zone Map Amendment.

**Applicable Review Criteria:**

<b>Development Code:</b>	
Section 4.008	Application Procedures-In General
Section 4.009	Who May Initiate Application
Section 4.010	How to Apply
Section 4.011	How Applications are Processed

Section 4.014	Burden of Proof
Section 4.031	Authority of the Development Review Board
Subsection 4.035 (.04)	Site Development Permit Application
Subsection 4.035 (.05)	Complete Submittal Requirement
Section 4.110	Zones
Section 4.117	Standards Applying to Industrial Development in Any Zone
Section 4.118	Standards Applying to Planned Development Zones
Section 4.135.5	PDI-RSIA Zone
Section 4.197	Zone Changes and Amendments to the Development Code
<b>Other Documents:</b>	
Comprehensive Plan	
Oregon Statewide Planning Goals	

**Vicinity Map**



**Background/Summary:**

Republic Services has a growing operation in Wilsonville. The Wilsonville campus currently includes a Materials Recovery Facility (MRF) for processing and sorting waste, collection operations offices, maintenance shop, and storage and support services. Republic Services and their predecessor companies have owned 10 acres north of their campus for many years, which currently sits outside Wilsonville City limits. In support of their continued growth and requested annexation, Republic Services request the City adopt an appropriate Zone Map designation.

Republic Services requests a zoning designation consistent with the proposed Comprehensive Plan Map designation of "Industrial". In addition to the Comprehensive Plan Map designation of "Industrial", Metro's Title 4, Industrial and Other Employment Areas Map shows the property as a "Regionally Significant Industrial Area." Consistent with this designation Republic Services proposes the property be designation as Planned Development Industrial-Regionally Significant Industrial Area (PDI-RSIA).

**Conclusion and Conditions of Approval:**

Staff and the DRB have reviewed the application and facts regarding the request and recommend the City Council approve the Zone Map Amendment (DB16-0006).

**Procedural Statements and Background Information:**

1. The statutory 120-day time limit applies to this application. The application was received on February 5, 2016. On March 2, 2016 staff conducted a completeness review within the statutorily allowed 30-day review period and found the application to be incomplete. On March 21, 2016 the Applicant submitted new materials. On April 19, 2016 the application was deemed complete. The City must render a final decision for the request, including any appeals, by August 17, 2016.
2. Surrounding land uses are as follows:

Compass Direction	Zone:	Existing Use:
North:	FD-20	Rural
East:	PDI	Industrial Development
South:	PDI/PF	Republic Services/BPA Substation
West:	FD-20	Rural

3. Previous Planning Approvals:
  - Case File 91PC33 – Stage I Master Plan Willamette Resources
  - Case File 94DR18 – Site Design Review Willamette Resources
  - Case File 99DB03 – Site Design Review Keller Drop Box
  - Case File DB14-0032 Stage I Master Plan Revision
  - Case File DB14-0033 Stage II Final Plan for Maintenance Facility

Case File DB14-0034 Site Design Review for Maintenance Facility  
Case File DB14-0035 Type C Tree Plan

4. The applicant has complied with Sections 4.008 through 4.011, 4.013-4.031, and 4.035 of the Wilsonville Code. Said sections pertain to review procedures and submittal requirements.

### **Findings of Fact:**

NOTE: Pursuant to Section 4.014 the burden of proving that the necessary findings of fact can be made for approval of any land use or development application rests with the applicant in the case.

## **General Information**

### **Application Procedures-In General** Section 4.008

**Review Criteria:** This section lists general application procedures applicable to a number of types of land use applications and also lists unique features of Wilsonville's development review process.

**Finding:** These criteria are met.

**Details of Finding:** The application is being processed in accordance with the applicable general procedures of this Section.

### **Initiating Application** Section 4.009

**Review Criterion:** "Except for a Specific Area Plan (SAP), applications involving specific sites may be filed only by the owner of the subject property, by a unit of government that is in the process of acquiring the property, or by an agent who has been authorized by the owner, in writing, to apply."

**Finding:** This criterion is satisfied.

**Details of Finding:** The application has been submitted on behalf of the property owner, Republic Services and is signed by an authorized representative.

### **Pre-Application Conference** Subsection 4.010 (.02)

**Review Criteria:** This section lists the pre-application process

**Finding:** These criteria are satisfied.

**Details of Finding:** A Pre-application conferences were held on October 16, 2014 (PA14-0015) in accordance with this subsection.

### **Lien Payment before Approval** Subsection 4.011 (.02) B.

**Review Criterion:** "City Council Resolution No. 796 precludes the approval of any development application without the prior payment of all applicable City liens for the subject property. Applicants shall be encouraged to contact the City Finance Department to verify that there are no outstanding liens. If the Planning Director is advised of outstanding liens while an application is under consideration, the Director shall advise the applicant that payments must be made current or the existence of liens will necessitate denial of the application."

**Finding:** This criterion is satisfied.

**Details of Finding:** No applicable liens exist for the subject property. The application can thus move forward.

#### General Submission Requirements Subsection 4.035 (.04) A.

**Review Criteria:** "An application for a Site Development Permit shall consist of the materials specified as follows, plus any other materials required by this Code." Listed 1. through 6. j.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant has provided all of the applicable general submission requirements contained in this subsection.

#### Zoning-Generally Section 4.110

**Review Criteria:** "The use of any building or premises or the construction of any development shall be in conformity with the regulations set forth in this Code for each Zoning District in which it is located, except as provided in Sections 4.189 through 4.192." "The General Regulations listed in Sections 4.150 through 4.199 shall apply to all zones unless the text indicates otherwise."

**Finding:** These criteria are satisfied.

**Details of Finding:** This proposed development is in conformity with the applicable zoning district and general development regulations listed in Sections 4.150 through 4.199 have been applied in accordance with this Section.

### **Request: DB16-0006 Zone Map Amendment**

#### **Development Code**

#### Zoning Consistent with Comprehensive Plan Section 4.029

C1. **Review Criterion:** "If a development, other than a short-term temporary use, is proposed on a parcel or lot which is not zoned in accordance with the Comprehensive Plan, the applicant must receive approval of a zone change prior to, or concurrently with the approval of an application for a Planned Development."

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The applicant is applying for a zone change concurrently with a Stage I Master Plan revision for the entirety of the subject property and Stage II Final Plan for a planned development on the eastern portion of the property.

Base Zones

Subsection 4.110 (.01)

C2. **Review Criterion:** This subsection identifies the base zones established for the City, including the Village Zone.

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The requested zoning designation of Planned Development Industrial-Regionally Significant Industrial Area "PDI-RSIA" is among the base zones identified in this subsection.

**Standards for Planned Development Industrial-Regionally Significant Industrial Area Zone**

Purpose of PDI-RSIA

Subsection 4.135.5 (.01)

C3. **Review Criteria:** The purpose of the PDI-RSIA Zone is to provide opportunities for regionally significant industrial operations along with a limited and appropriate range of related and compatible uses; to provide the flexibility to accommodate the changing nature of industrial employment centers, to protect industrially zoned lands for industrial uses, primarily in those areas near significant transportation facilities for the movement of freight and to facilitate the redevelopment of under-utilized industrial sites.

**Finding:** These criteria are satisfied.

**Details of Finding:** The zoning will allow only industrial uses consistent with the purpose stated in this subsection.

Uses Typically Permitted

Subsection 4.135.5 (.03)

C4. **Review Criteria:** Uses that are typically permitted:

- A. Wholesale houses, storage units, and warehouses.
- B. Laboratories, storage buildings, warehouses, and cold storage plants.
- C. Assembly of electrical equipment, including the manufacture of small parts.
- D. The light manufacturing, simple compounding or processing packaging, assembling and/or treatment of products, cosmetics, drugs, and food products, unless such use is inconsistent with air pollution, excess noise, or water pollution standards.
- E. Office Complexes-Technology (as defined in Section 4.001).
- F. Experimental, film or testing laboratories.
- G. Storage and distribution of grain, livestock feed, provided dust and smell is effectively controlled.

- H. Motor vehicle service facilities complementary or incidental to permitted uses.
- I. Any use allowed in a PDC Zone or any other light industrial uses provided that any such use is compatible with industrial use and is planned and developed in a manner consistent with the purposes and objectives of Sections 4.130 to 4.140 and is subject to the following criteria:
  - 1. Service Commercial (defined as professional services that cater to daily customers such as financial, insurance, real estate, legal, medical or dental offices) shall not exceed 3000 square feet of floor space in a single building or 20,000 square feet of combined floor area within a multiple building development.
  - 2. Office Use (as defined in Section 4.001) shall not exceed 20% of total floor area within a project site.
  - 3. Retail uses not to exceed 3000 square feet of indoor and outdoor sales, service, or inventory storage area for a single building or 20,000 square feet of indoor and outdoor sales, service or inventory storage area for multiple buildings.
  - 4. Combined uses under I.1 and 3. above shall not exceed a total of 3000 square feet of floor area in a single building or 20,000 square feet of combined floor area within a multi-building development.
- J. Residential uses shall not exceed 10% of total floor area.
- K. Accessory uses, buildings and structures customarily incidental to any of the aforesaid principal permitted uses.
- L. Temporary buildings or structures for uses incidental to construction work, which buildings or structures shall be removed upon completion or abandonment of the construction work.
- M. Expansion of a building, structure or use approved prior to October 25, 2004 of up to 20% additional floor area and/or 10% additional land area.
- N. Other similar uses which in the judgment of the Planning Director are consistent with the purpose of the PDI-RSIA Zone.

**Finding:** These criteria are satisfied.

**Details of Finding:** The proposed zoning will allow only uses consistent with the list established in this subsection.

#### Zone Change Procedures

Subsection 4.197 (.02) A.

- C5. **Review Criteria:** "That the application before the Commission or Board was submitted in accordance with the procedures set forth in Section 4.008, Section 4.125(.18)(B)(2), or, in the case of a Planned Development, Section 4.140;"

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The request for a zone map amendment has been submitted as set forth in the applicable code sections.

Conformance with Comprehensive Plan Map, etc.  
Subsection 4.197 (.02) B.

- C6. **Review Criteria:** "That the proposed amendment is consistent with the Comprehensive Plan map designation and substantially complies with the applicable goals, policies and objectives, set forth in the Comprehensive Plan text;"  
**Finding:** These criteria are satisfied.  
**Explanation of Finding:** The proposed zone map amendment is consistent with the proposed Comprehensive Map designation of "Industrial".

Public Facility Concurrency  
Subsection 4.197 (.02) D.

- C7. **Review Criteria:** "That the existing primary public facilities, i.e., roads and sidewalks, water, sewer and storm sewer are available and are of adequate size to serve the proposed development; or, that adequate facilities can be provided in conjunction with project development. The Planning Commission and Development Review Board shall utilize any and all means to insure that all primary facilities are available and are adequately sized."  
**Finding:** These criteria are satisfied.  
**Explanation of Finding:** As part of Stage II Final Plan reviews, concurrency standards are or will be applied to projects in the area being rezoned. Based on existing nearby utilities and utility master plans, the transportation master plan, and the Coffee Creek Master Plan, necessary facilities are or can be made available for development of the subject property consistent with the proposed zoning.

Impact on SROZ Areas  
Subsection 4.197 (.02) E.

- C8. **Review Criteria:** "That the proposed development does not have a significant adverse effect upon Significant Resource Overlay Zone areas, an identified natural hazard, or an identified geologic hazard. When Significant Resource Overlay Zone areas or natural hazard, and/ or geologic hazard are located on or about the proposed development, the Planning Commission or Development Review Board shall use appropriate measures to mitigate and significantly reduce conflicts between the development and identified hazard or Significant Resource Overlay Zone;"  
**Finding:** These criteria are satisfied.  
**Explanation of Finding:** No SROZ is within the area to be rezoned.

Development within 2 Years  
Subsection 4.197 (.02) F.

- C9. **Review Criterion:** "That the applicant is committed to a development schedule demonstrating that the development of the property is reasonably expected to commence within two (2) years of the initial approval of the zone change."

**Finding:** This criterion is satisfied.

**Explanation of Finding:** Concurrently submitted land use approvals for SORT Bioenergy expire after 2 years, so requesting the land use approvals assumes development would commence within two (2) years. However, in the scenario where the applicant or their successors do not commence development within two (2) years allowing related land use approvals to expire, the zone change shall remain in effect.

Development Standards and Conditions of Approval  
Subsection 4.197 (.02) G.

C10. **Review Criteria:** "That the proposed development and use(s) can be developed in compliance with the applicable development standards or appropriate conditions are attached to insure that the project development substantially conforms to the applicable development standards."

**Finding:** These criteria are satisfied.

**Explanation of Finding:** As can be found in the findings for the accompanying requests, the applicable development standards will be met either as proposed or as a condition of approval.

DEVELOPMENT REVIEW BOARD  
RESOLUTION NO. 327

A RESOLUTION ADOPTING FINDINGS RECOMMENDING APPROVAL TO CITY COUNCIL OF ANNEXATION, A COMPREHENSIVE PLAN MAP AMENDMENT FROM WASHINGTON COUNTY – FUTURE DEVELOPMENT (FD-20) DESIGNATION TO CITY – INDUSTRIAL (I) DESIGNATION, AND A ZONE MAP AMENDMENT FROM WASHINGTON COUNTY – FUTURE DEVELOPMENT (FD-20) ZONE TO CITY PLANNED DEVELOPMENT INDUSTRIAL – REGIONAL SIGNIFICANT INDUSTRIAL AREA (PDI-RSIA) ZONE, AND ADOPTING FINDINGS AND CONDITIONS APPROVING A STAGE I MASTER PLAN REVISION FOR AN EXPANDED REPUBLIC SERVICES CAMPUS, A STAGE II FINAL PLAN FOR SORT BIOENERGY, SITE DESIGN REVIEW FOR SORT BIOENERGY, A SETBACK WAIVER FOR SORT BIOENERGY AND TYPE C TREE PLAN FOR SORT BIOENERGY TO ALLOW FOR FUTURE EXPANSION OF REPUBLIC SERVICES OPERATIONS AND ALLOW FOR THE DEVELOPMENT OF FACILITIES TO CONVERT COMMERCIAL FOOD WASTE INTO BIOGAS AND USE THE BIOGAS TO GENERATE ELECTRICITY AMONG OTHER USES. THE SUBJECT SITE IS LOCATED ON TAX LOT 600 OF SECTION 2C, TOWNSHIP 3 SOUTH, RANGE 1 EAST, WILLAMETTE MERIDIAN, CITY OF WILSONVILLE, WASHINGTON COUNTY, OREGON. THE STAGE I MASTER PLAN REVISION ALSO AFFECTS TAX LOT 1400 OF THE SAME SECTION. BEN ALTMAN, PIONEER DESIGN GROUP, LLC – REPRESENTATIVE FOR JASON JORDAN, REPUBLIC SERVICES AND PAUL WOODS, SORT BIOENERGY - APPLICANTS.

WHEREAS, an application, together with planning exhibits for the above-captioned development, has been submitted in accordance with the procedures set forth in Section 4.008 of the Wilsonville Code, and

WHEREAS, the Planning Staff has prepared staff report on the above-captioned subject dated May 2, 2016, and

WHEREAS, said planning exhibits and staff report were duly considered by the Development Review Board Panel A at a scheduled meeting conducted on May 9, 2016, at which time exhibits, together with findings and public testimony were entered into the public record, and

WHEREAS, the Development Review Board considered the subject and the recommendations contained in the staff report, and

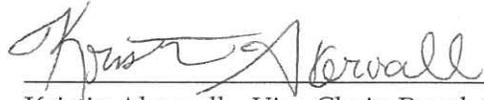
WHEREAS, interested parties, if any, have had an opportunity to be heard on the subject.

NOW, THEREFORE, BE IT RESOLVED that the Development Review Board of the City of Wilsonville does hereby adopt the staff report dated May 2, 2016, attached hereto as Exhibit A1, with findings and recommendations contained therein, and recommends approval of Annexation (DB16-0004), a Comprehensive Plan Map Amendment (DB16-0005) and a Zone Map Amendment (DB16-0006) to City Council and authorizes the Planning Director to issue permits consistent with said recommendations, contingent on City Council approval of the Annexation (DB16-0004),

Comprehensive Plan Map Amendment (DB16-0005) and Zone Map Amendment Requests (DB16-0006) and certain Transportation Plan Amendments (LP16-0001) for:

- DB16-0007 Stage I Preliminary Plan Revision for SORT Bioenergy
- DB16-0008 Stage II Final Plan for SORT Bioenergy
- DB16-0009 Site Design Review for SORT Bioenergy
- DB16-0010 Setback Waiver for SORT Bioenergy
- DB16-0011 Type C Tree Removal Plan for SORT Bioenergy

ADOPTED by the Development Review Board of the City of Wilsonville at a regular meeting thereof this 9<sup>th</sup> day of May, 2016 and filed with the Planning Administrative Assistant on May 10, 2016. This resolution is final on the 15<sup>th</sup> calendar day after the postmarked date of the written notice of decision per *WC Sec 4.022(.09)* unless appealed per *WC Sec 4.022(.02)* or called up for review by the council in accordance with *WC Sec 4.022(.03)*.



Kristin Akervall – Vice-Chair, Panel A  
Wilsonville Development Review Board

Attest:



Shelley White, Planning Administrative Assistant

**ORDINANCE NO. 790**

**AN ORDINANCE OF THE CITY OF WILSONVILLE APROVING A COMPREHENSIVE PLAN MAP AMENDMENT FROM RESIDENTIAL 0-1 DWELLING UNITS PER ACRE TO RESIDENTIAL 4-5 UNITS PER ACRE ON APPROXIMATELY 4.37 ACRES LOCATED AT 28500 AND 28530 SW CANYON CREEK ROAD SOUTH – COMPRISING TAX LOTS 900 AND 1000 OF SECTION 13B, TOWNSHIP 3 SOUTH, RANGE 1 WEST, CLACKAMAS COUNTY, OREGON, BETH ANN BOECKMAN AND KAREN AND MARVIN LEWALLEN – OWNERS, SCOTT MILLER, SAMM-MILLER LLC – APPLICANT.**

RECITALS

WHEREAS, Beth Ann Boeckman and Daren and Marvin Lewallen (“Owners”) and Scott Miller of SAMM-Miller LLC (“Applicant”) have made a development application requesting, among other things, a Comprehensive Plan Map Amendment of the Property; and

WHEREAS, the development application form has been signed by the Owners of the real property legally described and shown in Attachment 1, attached hereto and incorporated by reference herein (“Property”); and

WHEREAS, THE City of Wilsonville Planning Staff analyzed the Comprehensive Plan Map Amendment request and prepared a staff report for the Development Review board, finding that the application met the requirements for a Comprehensive Plan Map Amendment and recommending approval of the Comprehensive Plan Map Amendment, which staff report was presented to the Development Review Board on March 28 and April 25, 2016; and

Whereas, the Development Review Board Panel B held a public hearing on the application for a Comprehensive Plan Map Amendment on March 28 and April 25, 2016, and after taking public testimony and giving full consideration t the matter, adopted Resolution No. 324 which recommends that the City Council approve a request for a Comprehensive Plan Map Amendment (Case File DB 15-0108); and

WHEREAS, on May 16, 2016, the Wilsonville City Council held a public hearing regarding the above described matter, wherein the City Council considered the full public record made before the Development Review Board, including the Development Review Board and City Council staff reports; took public testimony;, and upon deliberation, concluded that the proposed Comprehensive Plan Map Amendment meets the applicable approval criteria under the City of Wilsonville Development Code; and

NOW, THEREFORE, THE CITY OF WILSONVILLE ORDAINS AS FOLLOWS:

Section 1. Findings. The City Council adopts, as findings and conclusions, the foregoing Recitals and Comprehensive Plan Map Amendment Findings in Attachment 2, as if fully set forth herein.

Section 2. Order. The official City of Wilsonville Comprehensive Plan Map is hereby amended by Comprehensive Plan Map Order DB15-0108, attached hereto as Attachments 1 from Residential 0-1 dwelling units per acre to Residential 4-5 dwelling units per acre.

SUBMITTED to the Wilsonville City Council and read the first time at a meeting thereof on the 16<sup>th</sup> day of May 2016, and scheduled for the second reading on June 6, 2016, commencing at the hour of 7 p.m. at the Wilsonville City Hall, 29799 SW Town Center Loop East, Wilsonville, Oregon.

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder

ENACTED by the City Council on the 6<sup>th</sup> day of June 2016 by the following votes:

Yes: \_\_\_\_\_ No: \_\_\_\_\_

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder

DATED and signed by the Mayor this \_\_\_\_ day of June, 2016.

\_\_\_\_\_  
TIM KNAPP, MAYOR

SUMMARY OF VOTES:

- Mayor Knapp
- Councilor Starr
- Councilor Fitzgerald
- Councilor Stevens
- Councilor Lehan

Exhibits and Attachments:

- Attachment 1 – Comprehensive Plan Map Order DB15-0108 including legal description and sketch depicting map amendment
- Attachment 2 – Comprehensive Plan Map Amendment Findings, April 26, 2016

**ORDINANCE NO. 790 – ATTACHMENT 1**

**BEFORE THE CITY COUNCIL OF THE  
CITY OF WILSONVILLE, OREGON**

In the Matter of the Application of	)	
SAMM-MILLER LLC for an	)	<b>Comprehensive Plan Map</b>
Amendment of the City of Wilsonville	)	<b>Order DB15-0108</b>
Comprehensive Plan Map	)	

The above entitled matter is before the Council to consider the application of DB15-0108, for a Comprehensive Plan Map Amendment and an Order, amending the official Comprehensive Plan Map of the City of Wilsonville.

The Council finds that the subject property (“Property”), legally described and shown in the attached legal description and sketch, has heretofore appeared on the City of Wilsonville Comprehensive Plan Map with a designation of Residential 0-1 dwelling units per acre.

The Council having heard and considered all matters relevant to the application for a Comprehensive Plan Map Amendment, including the Development Review Board record and recommendation, finds that the application should be approved.

THEREFORE, IT IS HEREBY OREDERD, that the Property, consisting of approximately 4.37 acres located at 28500 and 28530 SW Canyon Creek Road South, comprising tax lots 900 and 1000 of Section 13B, T3S, R1W, Clackamas County, Oregon, as more particularly shown and described in the attached legal description and sketch, is hereby designated as Residential 4-5 dwelling units per acre, subject to conditions detailed in this Order’s adopting Ordinance. The foregoing designation is hereby declared an amendment to the Wilsonville Comprehensive Plan Map and shall appear as such from and after entry of this Order.

DATED: June 6, 2016

\_\_\_\_\_  
TIM KNAPP, MAYOR

APPROVED AS TO FORM:

\_\_\_\_\_  
Barbara A. Jacobson, City Attorney

ATTEST:

---

Sandra C. King, MMC, City Recorder

Attachment:

Legal Description and Sketch Depicting Properties subject to the Comprehensive Plan Map Amendment.

# EMERIO *Design*

Civil Engineering  
Land Survey  
Land Use Planning  
Construction Management

ZONE CHANGE  
CANYON CREEK ROAD SOUTH

JOB NO. 500-015  
APRIL 27, 2016

EXHIBIT A (PAGE 1 OF 2)

LOTS 5 AND 6, "BRIDLE TRAIL RANCHETTS", AND OTHER LAND LOCATED IN THE N.W. 1/4 OF SECTION 13, TOWNSHIP 3 SOUTH, RANGE 1 WEST OF THE WILLAMETTE MERIDIAN, CITY OF WILSONVILLE, CLACKAMAS COUNTY, OREGON, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF LOT 5, "BRIDLE TRAIL RANCHETTS", BEING ON THE EAST RIGHT OF WAY LINE OF CANYON CREEK ROAD SOUTH (50.0 FEET WIDE); THENCE ALONG THE NORTH LINE OF SAID LOT 5 AND ITS EASTERLY EXTENSION, NORTH 89°29'30" EAST, 652.49 FEET TO THE WEST LINE OF TRACT J, "WILSONVILLE MEADOWS NO. 5"; THENCE ALONG SAID WEST LINE AND THE WEST LINE OF TRACT M, "WILSONVILLE MEADOWS NO. 6", SOUTH 00°40'00" WEST, 292.04 FEET TO THE EASTERLY EXTENSION OF THE SOUTH LINE OF LOT 6, "BRIDLE TRAIL RANCHETTS"; THENCE ALONG SAID EASTERLY EXTENSION AND SOUTH LINE, SOUTH 89°29'30" WEST, 649.98 FEET TO THE SOUTHWEST CORNER OF SAID LOT 6 ON THE EAST RIGHT OF WAY LINE OF CANYON CREEK ROAD SOUTH; THENCE ALONG SAID EAST RIGHT OF WAY LINE, NORTH 00°10'30" EAST, 292.00 FEET TO THE POINT OF BEGINNING.

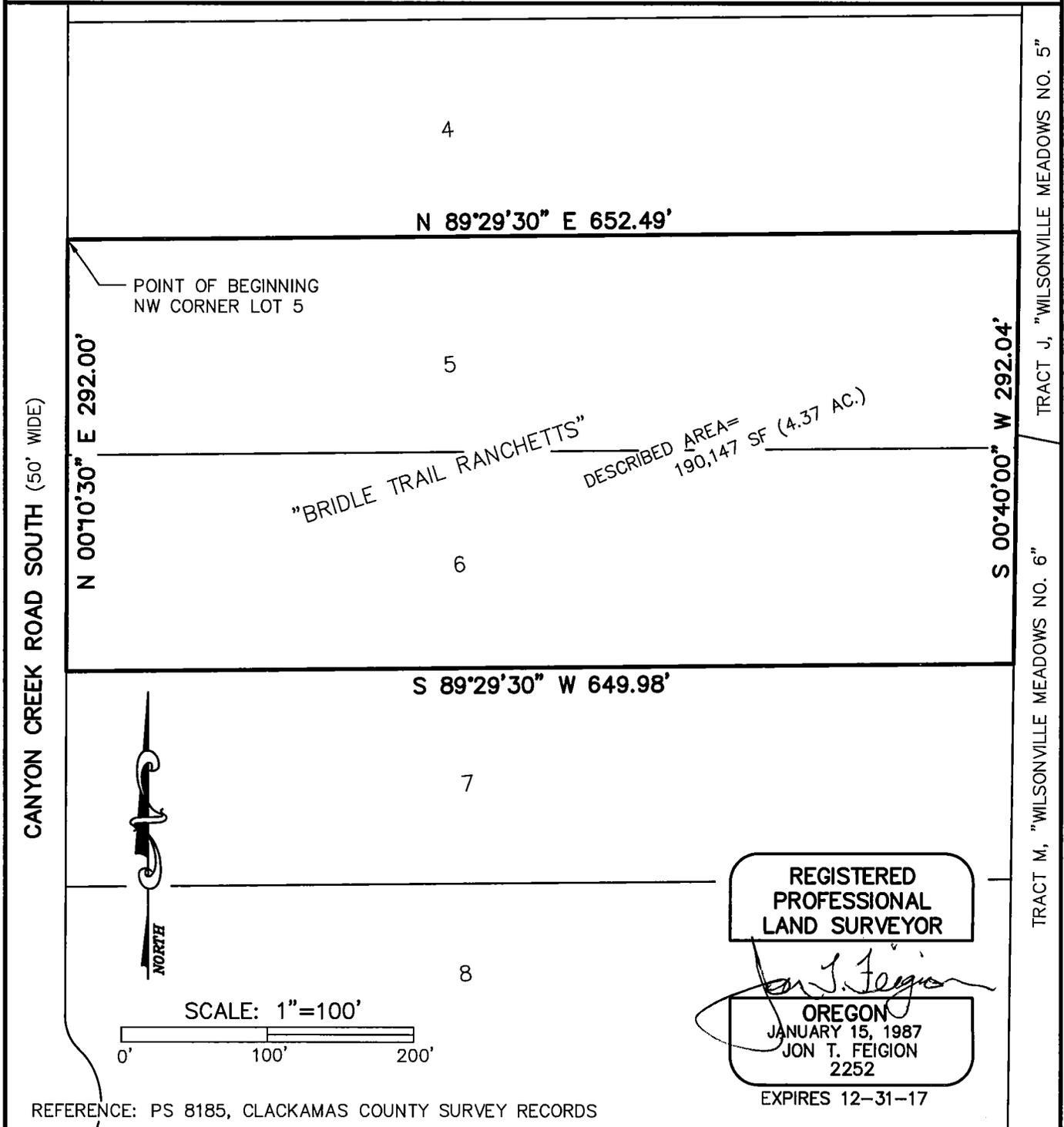
CONTAINING 190,147 SQUARE FEET (4.37 ACRES), MORE OR LESS.



REFERENCE: PS 8185, CLACKAMAS COUNTY SURVEY RECORDS

**EXHIBIT A (PAGE 2 OF 2)**

LOTS 5 AND 6, "BRIDLE TRAIL RANCHETTS" AND  
OTHER LAND LOCATED IN THE  
N.W. 1/4 OF SEC. 13, T.3S., R.1W., W.M.  
CITY OF WILSONVILLE, CLACKAMAS COUNTY, OREGON



REFERENCE: PS 8185, CLACKAMAS COUNTY SURVEY RECORDS

DRAWN BY: JTF	
JOB NO.: 500-015	ZONE CHANGE
SCALE: 1" = 100'	
DATE: 4-27-16	

**EMERIO**  
*Design*

8285 SW NIMBUS AVE, SUITE 180  
BEAVERTON, OREGON 97008  
TEL: (503) 746-8812  
FAX: (503) 639-9592  
www.emeriodesign.com

Ordinance No. 790  
 Staff Report  
 Wilsonville Planning Division

14-Lot Single-Family Subdivision at 28500 and 28530 SW Canyon Creek Rd. South  
 Comprehensive Plan Map Amendment

City Council  
 Quasi-Judicial Public Hearing

**Hearing Date:** May 16, 2016

**Date of Report:** April 26, 2016

**Application No.:** DB15-0108 Comprehensive Plan Map Amendment

**Request:** The City is being asked to review a Quasi-judicial Comprehensive Plan Map Amendment for a 14-lot single-family subdivision.

**Location:** Approximately 4.37 acres at 28500 and 28530 SW Canyon Creek Road South. East side of SW Canyon Creek Road South at and just south of SW Daybreak Street. The property is specifically known as Tax Lots 900 and 1000, Section 13B, Township 3 South, Range 1 West, Willamette Meridian, City of Wilsonville, Clackamas County, Oregon

**Owners:** Beth Ann Boeckman (28500 SW Canyon Creek Rd. S.)  
 Karen and Marvin Lewallen (28530 SW Canyon Creek Rd. S.)

**Applicant:** Scott Miller, Samm-Miller LLC

**Applicant’s Representative:** AnneMarie Skinner, Emerio Design

**Comprehensive Plan Designation (Current):** Residential 0-1 dwelling units per acre  
**Comprehensive Plan Designation (Proposed):** Residential 4-5 dwelling units per acre

**Staff Reviewer:** Daniel Pauly AICP, Associate Planner

**Staff Recommendation:** Approve the requested Comprehensive Plan Map Amendment.

**Applicable Review Criteria:**

<u>Development Code:</u>	
Section 4.008	Application Procedures-In General
Section 4.009	Who May Initiate Application
Section 4.010	How to Apply
Section 4.011	How Applications are Processed
Section 4.014	Burden of Proof
Section 4.031	Authority of the Development Review Board
Subsection 4.035 (.04)	Site Development Permit Application

Subsection 4.035 (.05)	Complete Submittal Requirement
Section 4.198	Comprehensive Plan Changes
<b>Other Documents:</b>	
Comprehensive Plan	
Oregon Statewide Planning Goals	

**Vicinity Map**

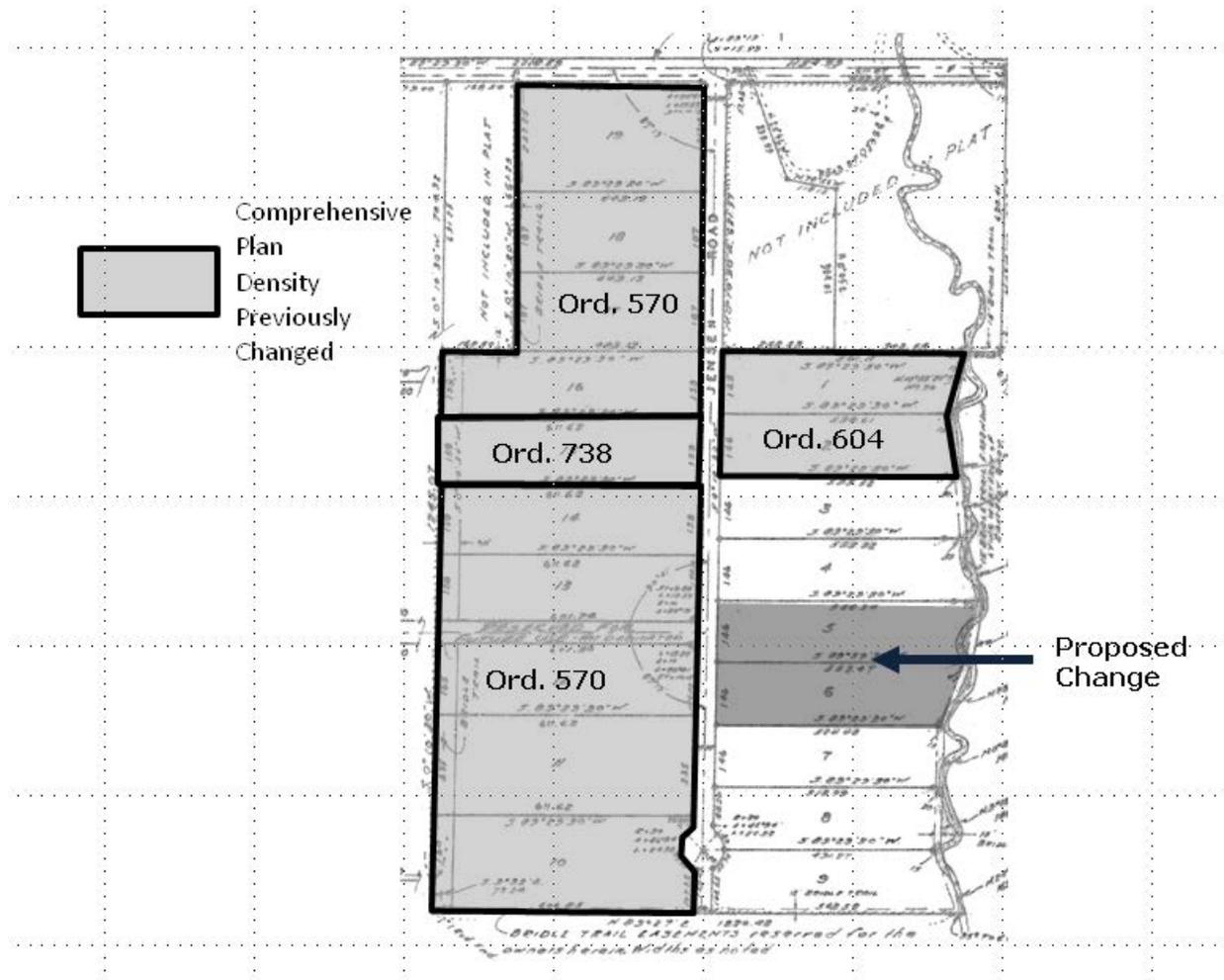


**Background/Summary:**

Comprehensive Plan Map Amendment (DB15-0108)

For areas of the City designated as residential on the Comprehensive Plan Map, planned densities are also indicated in dwelling units per acre. The applicant requests a change of the planned residential density of the subject properties from 0-1 dwelling units per acre to 4-5 dwelling units per acre.

The subject properties are part of the 1964 Bridle Trail Ranchettes subdivision where each lot was approximately 2 acres. When the current Comprehensive Plan Map was adopted the density for this area reflected the existing subdivision. Beginning in the mid 2000's, many of the Bridle Trail Ranchette lots were approved for Comprehensive Plan Map amendments to increase the density from 0-1 to 4-5 dwelling units an acre. Currently 12 of the original 19 Bridle Trail Ranchette lots have been approved by the City for increased density.



The first and largest approved change in this area from 0-1 to 4-5 dwelling units was in 2004 with the adoption of Ordinance No. 570 for Renaissance at Canyon Creek. The supporting staff

report discussed the need of additional single-family homes to provide housing for people working in Wilsonville as well as others desiring to live here. In addition, the findings point out the limited amount of vacant residential land within the City, and that the subject area is surrounded by residential designations for higher density.

In early 2006, Ordinance No. 604 similarly changed the comprehensive plan designation for approximately 4 acres on the east side of Canyon Creek Road South from 0-1 to 4-5 dwelling units an acre for the development of the 13-lot Cross Creek Subdivision. The same findings regarding the need of additional housing units, the limited amount of vacant land within the City, and the density of surrounding areas were made.

More recently, Ordinance No. 738 approved the same density change in 2014 for a property whose owners had elected not to participate in the 2004 project and now desired to redevelop.

The owners of the subject properties and their development partner now desire for a similar change of density for the subject property for similar reasons as the other lots redeveloped in Bridle Trail Ranchettes.

**Conclusion:**

Staff and the DRB have reviewed the application and facts regarding the request and recommend the City Council approve the Comprehensive Plan Map Amendment (DB15-0108).

**Findings of Fact:**

1. The statutory 120-day time limit applies to this application. The application was received on December 23, 2015. On January 21, 2016 staff conducted a completeness review within the statutorily allowed 30-day review period and found the application to be incomplete. On February 1, 2016, the Applicant submitted new materials. On February 17, 2016 the application was deemed complete. The City must render a final decision for the request, including any appeals, by June 16, 2016.
2. Surrounding land uses are as follows:

Compass Direction	Zone:	Existing Use:
North:	RA-H	Single-family Residential
East:	PDR-4	Single-family Residential
South:	RA-H	Single-family Residential
West:	PDR-3	Single-family Residential

3. Previous Planning Approvals:  
Current subdivision (Bridle Trail Ranchettes) approved prior to City incorporation.

4. The applicant has complied with Sections 4.013-4.031 of the Wilsonville Code, said sections pertaining to review procedures and submittal requirements. The required public notices have been sent and all proper notification procedures have been satisfied.

### Conclusionary Findings:

NOTE: Pursuant to Section 4.014 the burden of proving the necessary findings of fact can be made for approval of any land use or development application rests with the applicant in the case.

## General Information

### Application Procedures-In General Section 4.008

**Review Criteria:** This section lists general application procedures applicable to a number of types of land use applications and also lists unique features of Wilsonville’s development review process.

**Finding:** These criteria are met.

**Details of Finding:** Processing of the application follows the applicable general procedures of this Section.

### Initiating Application Section 4.009

**Review Criterion:** “Except for a Specific Area Plan (SAP), applications involving specific sites may be filed only by the owner of the subject property, by a unit of government that is in the process of acquiring the property, or by an agent who has been authorized by the owner, in writing, to apply.”

**Finding:** This criterion is satisfied.

**Details of Finding:** Applications have been signed by property owners of both properties involved.

### Pre-Application Conference Subsection 4.010 (.02)

**Review Criteria:** This section lists the pre-application process

**Finding:** These criteria are satisfied.

**Details of Finding:** A Pre-application conferences was held on April 23, 2015 (PA15-0008) in accordance with this subsection.

### Lien Payment before Approval Subsection 4.011 (.02) B.

**Review Criterion:** “City Council Resolution No. 796 precludes the approval of any development application without the prior payment of all applicable City liens for the subject property. Applicants shall be encouraged to contact the City Finance Department to verify that there are

no outstanding liens. If the Planning Director is advised of outstanding liens while an application is under consideration, the Director shall advise the applicant that payments must be made current or the existence of liens will necessitate denial of the application.”

**Finding:** This criterion is satisfied.

**Details of Finding:** No applicable liens exist for the subject property. The application can thus move forward.

#### General Submission Requirements

Subsection 4.035 (.04) A.

**Review Criteria:** “An application for a Site Development Permit shall consist of the materials specified as follows, plus any other materials required by this Code.” Listed 1. through 6. j.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant has provided all of the applicable general submission requirements contained in this subsection.

#### Zoning-Generally

Section 4.110

**Review Criteria:** “The use of any building or premises or the construction of any development shall be in conformity with the regulations set forth in this Code for each Zoning District in which it is located, except as provided in Sections 4.189 through 4.192.” “The General Regulations listed in Sections 4.150 through 4.199 shall apply to all zones unless the text indicates otherwise.”

**Finding:** These criteria are satisfied.

**Details of Finding:** This proposed development is in conformity with the applicable zoning district and general development regulations listed in Sections 4.150 through 4.199 have been applied in accordance with this Section.

### DB15-0108 Comprehensive Plan Amendment

#### Zoning and Land Development Ordinance

#### Comprehensive Plan Amendment Process

##### Procedures and Criteria in Comprehensive Plan

Subsection 4.198 (.01)

A1. **Review Criteria:** “Proposals to amend the Comprehensive Plan, or to adopt new elements or sub-elements of the Plan, shall be subject to the procedures and criteria contained in the Comprehensive Plan.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The lot of the subject development site is of sufficient size to be developed in a manner consistent with the purposes and objectives of Section 4.140.

Review Bodies

Subsection 4.198 (.02)

**A2. Review Criteria:** “Following the adoption and signature of the Resolution by the Development Review Board or Planning Commission, together with minutes of public hearings on the proposed Amendment, the matter shall be shall be scheduled for public hearing before the City Council.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The DRB and City Council are considering the request as described.

Applicant Agreeing to Conditions of Approval

Subsection 4.198 (.05)

**A3. Review Criteria:** “In cases where a property owner or other applicant has requested an amendment to the Comprehensive Plan map and the City Council has approved the change subject to conditions, the owner or applicant shall sign a statement accepting, and agreeing to complete the conditions of approval before the Comprehensive Plan map shall be changed.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The owner will be required to sign a statement accepting conditions.

**Comprehensive Plan Amendment Required Findings**

Meets Identified Public Need

Subsection 4.198 (.01) A.

**A4. Review Criteria:** “Each such amendment shall include findings in support of the following: That the proposed amendment meets a public need that has been identified;”

**Finding:** These criteria are satisfied.

**Details of Finding:** The “Residential Development” portion of the Comprehensive Plan (Policy 4.1.4) identifies the need for additional housing within the City to serve housing and economic needs of residents and employees working within the City.

On the basis of the Housing Data used for the 2015 City of Wilsonville Housing Report, of the City’s 10,283 housing units, 55% are multi-family (apartments and condos), 45% are single-family.

Policy 4.1.4 and its implementation measures seek to “provide opportunities for a wide range of housing types, sizes, and densities at prices and rent levels to accommodate people who are employed in Wilsonville.” The proposal provides additional single-family homes supporting an ongoing desire for single-family homes at various price levels as part of Wilsonville’s strong diversity of housing unit types.

Meets Identified Public Need As Well As Reasonable Alternative  
Subsection 4.198 (.01) B.

- A5. **Review Criteria:** “Each such amendment shall include findings in support of the following: That the proposed amendment meets the identified public need at least as well as any other amendment or change that could reasonably be made;”

**Finding:** These criteria are satisfied.

**Details of Finding:** The proposed subdivision has similarities in site density and housing product to other subdivisions nearby such as Renaissance at Canyon Creek and Cross Creek, and provides a consistent density and development type as the area becomes more dense and urban over time. The consistency with nearby development, while accommodating the required usable open space, makes the proposed continued residential use at the proposed density meet the need for a variety of single-family homes better than other density or design options for the site.

Supports Statewide Planning Goals  
Subsection 4.198 (.01) C.

- A6. **Review Criteria:** “Each such amendment shall include findings in support of the following: That the proposed amendment supports applicable Statewide Planning Goals, or a Goal exception has been found to be appropriate;”

**Finding:** These criteria are satisfied.

**Details of Finding:** With the implementation of the proposed conditions of approval, the project supports the applicable Statewide Planning Goals.

No Conflict with Other Portions of Plan  
Subsection 4.198 (.02) D.

- A7. **Review Criteria:** “Each such amendment shall include findings in support of the following: That the proposed change will not result in conflicts with any portion of the Comprehensive Plan that is not being amended.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant is requesting an amendment of the Comprehensive Plan Map for the subject properties. The applicant does not propose to modify or amend any other portion of the Comprehensive Plan or Plan Map.

## **Comprehensive Plan and Plan Components**

### **Initiating, Applying for, and Considering Plan Amendments**

#### Who May Initiate Plan Amendments Introduction Page 7 “Plan Amendments” 1.

- A8. **Review Criteria:** “An Amendment to the adopted Plan may be initiated by: a. The City Council, b. The Planning Commission (for legislative amendments) or Development Review Board (for quasi-judicial amendments); or c. Application of property owner(s) or contract purchaser(s) affected or their authorized agents, as specified in #2 below.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The proposed amendment has been initiated by the property owners of the subject lots.

#### How to Make Application

Introduction Page 7 "Plan Amendments" 2.

**A9. Review Criteria:** "An application for an amendment to the Plan maps or text shall be made on forms provided by the City. The application, except when initiated by the City Council, DRB, or Planning Commission, as noted in #1, above, shall be accompanied by a Plan Amendment Fee.

**Finding:** These criteria are satisfied.

**Details of Finding:** The proposed amendment has been initiated by the property owners of the subject lots who have submitted signed application forms provided by the City and paid the required application fee.

#### Consideration of Plan Amendments

Introduction Page 7 "Plan Amendments" 3.

**A10. Review Criteria:** This language specifies how the City should consider a plan amendment including: requiring the City Council consider a plan amendment only after receiving findings and recommendation from the Planning Commission or Development Review Board; having sufficient time before the first evidentiary hearing for public notice and staff report preparation, considering compliance with Statewide Planning Goals and applicable Metro Plans.

**Finding:** These criteria are satisfied.

**Details of Finding:** The City Council will consider the plan amendment only after receiving a recommendation from the Development Review Board.

### Standards for Approval of Plan Amendments

#### Conformance with Other Portions of the Plan

Introduction Page 7 "Plan Amendments" 4. a.

**A11. Review Criterion:** "The proposed amendment is in conformance with those portions of the Plan that are not being considered for amendment."

**Finding:** This criterion is satisfied.

**Details of Finding:** The change of residential density for the subject properties does not lead to nonconformance with other portions of the Comprehensive Plan.

#### Public Interest

Introduction Page 7 "Plan Amendments" 4. b.

**A12. Review Criterion:** "The granting of the amendment is in the public interest."

**Finding:** This criterion is satisfied.

**Details of Finding:** The request is in the public interest by providing needed housing. See

also Finding A4.

Public Interest Best Served by Timing of Amendment  
Introduction Page 7 "Plan Amendments" 4. c.

**A13. Review Criterion:** "The public interest is best served by granting the amendment at this time."

**Finding:** This criterion is satisfied.

**Details of Finding:** The timing of the amendment is appropriate. See Finding A5.

Factors to Address in Amendment  
Introduction Page 7 "Plan Amendments" 4. d.

**A14. Review Criterion:** "The following factors have been adequately addressed in the proposed amendment:

- the suitability of the various areas for particular land uses and improvements;
- the land uses and improvements in the area;
- trends in land improvement;
- density of development;
- property values;
- the needs of economic enterprises in the future development of the area;
- transportation access;
- natural resources; and
- the public need for healthful, safe and aesthetic surroundings and conditions.

**Finding:** This criterion is satisfied.

**Details of Finding:** The area is suitable for the proposed development as it is in a residential area with similar development and has the necessary public services, including streets, available. It is similar to and follows the trends in recent nearby developments such as Renaissance at Canyon Creek and Cross Creek. The density is consistent with these other recent nearby developments. No evidence has been presented that the development would negatively impact property values. Preservation of Natural Resource areas is part of the development. Healthful, safe and aesthetic surroundings are ensured by application of design standards.

Conflict with Metro Requirements  
Introduction Page 7 "Plan Amendments" 4. e.

**A15. Review Criterion:** "Proposed changes or amendments to the Comprehensive Plan do not result in conflicts with applicable Metro requirements."

**Finding:** This criterion is satisfied.

**Details of Finding:** No conflicts with Metro requirements have been identified. Particularly, Wilsonville's housing mix continues to exceed Metro's requirements.

Public Notice Requirements  
Introduction Page 8 "Plan Amendments" 5.

**A16. Review Criterion:** This language describes the noticing requirements implemented by the City's noticing requirements for quasi-judicial review.

**Finding:** This criterion is satisfied.

**Details of Finding:** Public hearing notices have or will be sent as required.

## Urban Growth Management

### Urbanization for Adequate Housing

Implementation Measure 2.1.1.b.

**A17. Review Criteria:** "Allow urbanization to occur to provide adequate housing to accommodate workers who are employed within the City."

**Finding:** These criteria are satisfied.

**Details of Finding:** The proposal provides for additional housing density to accommodate those employed with the City. See also Finding A4.

### Revenue Sources for Urbanization

Implementation Measure 2.1.1.d.

**A18. Review Criteria:** "Establish and maintain revenue sources to support the City's policies for urbanization and maintain needed public services and facilities."

**Finding:** These criteria are satisfied.

**Details of Finding:** Existing requirements for improvements and systems development charges apply to the development proposed concurrently with the Comprehensive Plan Map amendment.

### New Development and Concurrency

Implementation Measure 2.1.1.e.

**A19. Review Criteria:** "Allow new development to proceed concurrently with the availability of adequate public services and facilities as specified in Public Facilities and Services Section (Section C) of the Comprehensive Plan."

**Finding:** These criteria are satisfied.

**Details of Finding:** The City's concurrency requirements in the Development Code apply to the concurrently proposed development.

### Encourage Master Planning

Implementation Measure 2.1.1.f.2.

**A20. Review Criteria:** "To maximize design quality and conformity to the Comprehensive Plan, the City shall encourage master planning of large land areas. However, as an added growth management tool, the Development Review Board may, as a condition of approval, set an annual phasing schedule coordinated with scheduled Capital Improvements, particularly streets and related transportation facilities."

**Finding:** These criteria are satisfied.

**Details of Finding:** The subject properties are large enough, being greater than the 2 acre

threshold for planned development established in Section 4.140, to be designed consistent with the City's planned development regulations to support design quality and conformity with the Comprehensive Plan.

## Public Facilities and Services

### Urban Development Only Where Facilities and Services Can Be Provided

Implementation Measure 3.1.2.a.

**A21. Review Criterion:** "Urban development will be allowed only in areas where necessary facilities and services can be provided."

**Finding:** This criterion is satisfied.

**Details of Finding:** Application of the concurrency standards of the City's development code ensure the development proposed concurrently with this amendment request will have all necessary facilities and services provided. See Stage II Final Plan in Request D.

### Paying for Facilities and Services

Implementation Measures 3.1.3.a., 3.1.4.f., 3.1.5.c., 4.1.4.h.

**A22. Review Criteria:** "Developers will continue to be required to pay for demands placed on public facilities/services that are directly related to their developments. The City may establish and collect systems development charges (SDCs) for any or all public facilities/services, as allowed by law. An individual exception to this standard may be justified, or SDC credits given, when a proposed development is found to result in public benefits that warrant public investment to support the development." "The cost of all line extensions and individual services shall be the responsibility of the developer and/or property owners(s) seeking service. When a major line is to be extended, the City may authorize and administer formation of a Local Improvement District (LID). All line extensions shall conform to the City Sanitary Sewer Collection System Master Plan, urbanization policies, and Public Works Standards." "Extensions shall be made at the cost of the developer or landowner of the property being served." "Require new housing developments to pay an equitable share of the cost of required capital improvements for public services."

**Finding:** These criteria are satisfied.

**Details of Finding:** The City has all necessary codes and processes in place to ensure the development pays for public facilities/services directly related to the development.

### Growth and Sewer Capacity

Implementation Measure 3.1.4.b

**A23. Review Criterion:** "The City shall continue to manage growth consistent with the capacity of sanitary sewer facilities."

**Finding:** This criterion is satisfied.

**Details of Finding:** The City will not allow development without adequate sanitary sewer capacity. As reviewed in the Stage II Final Plan, adequate sanitary sewer capacity exists by connecting to the existing sewer in Canyon Creek Road South.

## Land Use and Development

### Variety of Housing Types

Implementation Measures 4.1.4.b, 4.1.4.j., and 4.1.4.o.

**A24. Review Criterion:** “Plan for and permit a variety of housing types consistent with the objectives and policies set forth under this section of the Comprehensive Plan, while maintaining a reasonable balance between the economics of building and the cost of supplying public services. It is the City’s desire to provide a variety of housing types needed to meet a wide range of personal preferences and income levels. The City also recognizes the fact that adequate public facilities and services must be available in order to build and maintain a decent, safe, and healthful living environment.” “The City shall have a diverse range of housing types available within its City limits.” “The City will encourage the development of housing of various types and densities. Guided by the urbanization, public facilities, and economic elements, the City will, however, manage residential growth to ensure adequate provision of public facilities and that proposed housing satisfies local need and desires, i.e., type, price and rent levels.”

**Finding:** This criterion is satisfied.

**Details of Finding:** Wilsonville has a rich diversity of housing types. Infill in other areas of the Bridle Trail Ranchettes involved single-family residential development of a similar density as proposed (including Renaissance at Canyon Creek and Cross Creek subdivisions). The proposal supports the area’s continued role as a single-family area amongst Wilsonville’s housing mix.

### Encouraging Variety

Implementation Measure 4.1.4.c

**A25. Review Criterion:** “encouraging variety through the use of planned developments and clusters.”

**Finding:** This criterion is satisfied.

**Details of Finding:** Being relatively small for a planned development, not a lot of variety would be expected within the development. However, a variety of lot sizes and widths are provided allowing diversity of housing products.

### Housing Balance

Implementation Measure 4.1.4.d

**A26. Review Criteria:** “Encourage the construction and development of diverse housing types, but maintain a general balance according to housing type and geographic distribution, both presently and in the future. Such housing types may include, but shall not be limited to: Apartments, single-family detached, single-family common wall, manufactured homes, mobile homes, modular homes, and condominiums in various structural forms.”

**Finding:** These criteria are satisfied.

**Details of Finding:** On the basis of the Housing Data for the 2015 City of Wilsonville Housing Report of the City’s 10,283 housing units, 55% are multi-family and 45% are

single-family.

The proposal adds single-family to the housing mix having a minor impact on making single-family housing more balanced with multi-family. In addition, the development is proposed in a single-family area of the community where multi-family is not planned thus supporting the planned geographic distribution.

#### Housing Needs of Existing Residents

Implementation Measure 4.1.4.f.

**A27. Review Criteria:** “Accommodate the housing needs of the existing residents of the City of Wilsonville.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The proposed housing will fit into the rich diversity of Wilsonville’s housing to allow existing residents to move up or move down, thus opening their units to others.

#### Housing Development and the Social and Economic Needs of the Community

Implementation Measure 4.1.4.g.

**A28. Review Criteria:** “Coordinate housing development with the social and economic needs of the community.”

**Finding:** These criteria are satisfied.

**Details of Finding:** Wilsonville has a rich diversity of housing types, to which these additional single-family homes would contribute. The diversity of housing types supports the variety of needs of members of the community.

#### Jobs Housing Balance

Implementation Measures 4.1.4.l. and 4.1.4.p.

**A29. Review Criteria:** “The City shall work to improve the balance of jobs and housing within its jurisdictional boundaries.” “In an effort to balance residential growth with the City’s employment base, the City shall encourage the development of housing to meet the needs of the employees working in the City.”

**Finding:** These criteria are satisfied.

**Details of Finding:** It is anticipated the planned homes could be occupied by people working in Wilsonville. The location is close to employment centers including Town Center and the industrial area north of Boeckman between Canyon Creek and Parkway.

#### Residential Districts and Density

Implementation Measures 4.1.4.u. and 4.1.4.z.

**A30. Review Criteria:** “To provide variety and flexibility in site design and densities, residential lands shown on the Land Use Map of the Comprehensive Plan have been divided into districts, with different density ranges for each district. In all residential developments, other than those that are so small that it is not mathematically feasible to achieve the

prescribed minimum density, the 80% minimum shall apply. The following density ranges have been prescribed for each district:

- Density: 0-1 units/acre
- 2-3 units/acre
- 4-5 units/acre
- 6-7 units/acre
- 10-12 units/acre
- 18-20 units/acre"

"The City shall continue to apply a minimum density standard to all zones allowing residential use, such that all development, including subdivisions, will result in the eventual build-out of 80 percent or more of the maximum number of dwelling units per net acre permitted by the zoning designation for a given development. The minimum density requirement does not apply inside areas designated by the City as open spaces or significant resource sites. The maximum-zoned density does not include the density bonus for zones that allow them."

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant requests the density to change from 0-1 dwelling units per acre to 4-5 dwelling units per acre in an area transitioning from rural residential to denser urban residential. Similar changes have occurred on other nearby properties including the areas currently occupied by Renaissance at Canyon Creek and Cross Creek subdivisions.

#### 2-3 or 4-5 Dwelling Unit Per Acre Residential District

"Residential Planning Districts" page D-19

**A31. Review Criteria:** "The purpose of this district is to provide for low density residential areas. The 2-3 du/acre density would generally fall under the PDR-2 zoning district category as outlined in the Development Code. The 4-5 du/acre density would generally fall under the PDR-2 and PDR-3 (or other categories that could work out to this level of density) zoning district category as outlined in the Development Code.

The following areas should be designated and developed at this density:

1. Areas with access to a minor arterial, collector, or local streets. However, direct vehicular access from individual lots onto a minor arterial will be restricted.
2. Undeveloped areas adjacent to existing lower density developments, or near the fringe of the Urban Growth Boundary.
3. Areas where sensitivity to the natural environment or natural hazards warrant a reduced density."

**Finding:** These criteria are satisfied.

**Details of Finding:** The 4-5 dwelling units designation is appropriate as adequate access to streets is available creating traffic volumes within the limits set by the City, it is adjacent to a variety of residential densities, including low density, and it is an appropriate density to allow development while preserving the natural slope and riparian areas of the properties.

## Metro Urban Growth Functional Plan

### Maintaining or Increasing Housing Capacity

Title 1 3.07.110

**A32. Review Criteria:** “Requiring each city and county to maintain or increase its housing capacity . . .”

**Finding:** These criteria are satisfied.

**Details of Finding:** The proposal will increase the City’s housing capacity within the current City limits.

## Statewide Planning Goals

### Citizen Involvement

Goal 1

**A33. Review Criteria:** “To develop a citizen involvement program that insures the opportunity for citizens to be involved in all phases of the planning process.”

**Finding:** These criteria are satisfied.

**Details of Finding:** A thorough citizen involvement process, as defined in Wilsonville’s Development Code and Comprehensive Plan, ensures citizen involvement in the decision.

### Land Use Planning

Goal 2

**A34. Review Criteria:** “To establish a land use planning process and policy framework as a basis for all decision and actions related to use of land and to assure an adequate factual base for such decisions and actions.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The Comprehensive Plan Amendment is required to meet policies based on the statewide framework and is required to provide adequate facts to make a decision based on the applicable review criteria.

### Agriculture Lands

Goal 3

**A35. Review Criteria:** “To preserve and maintain agricultural lands.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The areas proposed for new housing development are not currently in commercial agriculture use. Increasing development within the City limits has the potential to slightly lessen the demand for housing on land currently in use for commercial agriculture.

Natural Resources, Scenic and Historic Areas, and Open Spaces  
Goal 5

**A36. Review Criteria:** “To protect natural resources and conserve scenic and historic and open spaces.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The City’s SROZ overlay standards are ensuring significant natural resources on the eastern portion of the subject properties are protected.

Air, Water and Land Resources Quality  
Goal 6

**A37. Review Criteria:** “To maintain and improve the quality of the air, water and land resources of the state.”

**Finding:** These criteria are satisfied.

**Details of Finding:** The requirements to preserve the natural area as well as storm water requirements help maintain water quality. No significant negative impacts to air and land resources can reasonably be anticipated.

**ORDINANCE NO. 791**

**AN ORDINANCE OF THE CITY OF WILSONVILLE APPROVING A ZONE MAP AMENDMENT FROM THE RESIDENTIAL AGRICULTURE-HOLDING (RA-H) ZONE TO THE PLANNED DEVELOPMENT RESIDENTIAL-3 (PDR-3) ZONE ON APPROXIMATELY 4.37 ACRES LOCATED AT 28500 AND 28530 SW CANYON CREEK ROAD SOUTH- COMPRISING TAX LOTS 900 AND 1000 OF SECTION 13B, TOWNSHIP 3 SOUTH, RANGE 1 WEST, CLACKAMAS COUNTY, OREGON, BETH ANN BOECKMAN AND KAREN AND MARVIN LEWALLEN – OWNERS. SCOTT MILLER, SAMM-MILLER LLC – APPLICANT.**

**RECITALS**

WHEREAS, Beth Ann Boeckman and Karen and Marvin Lewallen (“Owners”) and Scott Miller of SAMM-MILLER LLC (“Applicant”) have made a development application requesting, among other things, a Zone Map Amendment of the Property; and

WHEREAS, the development application form has been signed by the Owners of the real property legally described and shown in Attachment 1, attached hereto and incorporated by reference herein (“Property”); and

WHEREAS, concurrently with the Zone Map Amendment the Applicant is requesting a change of the Comprehensive Plan Map designation to “Residential 4-5 dwelling units per acre”; and

WHEREAS, the City of Wilsonville desires to have the properties zoned consistent with the Comprehensive Plan Map designation of “Residential 4-5 dwelling units per acre”, upon approval the requested Comprehensive Plan Map designation; and

WHEREAS, the City of Wilsonville Planning Staff analyzed the Zone Map Amendment request and prepared a staff report for the Development Review Board, finding that the application met the requirements for a Zone Map Amendment and recommending approval of the Zone Map Amendment, which staff report was presented to the Development Review Board on March 28 and April 25, 2016;

WHEREAS, the Development Review Board Panel B held a public hearing on the application for a Zone Map Amendment on March 28 and April 25, 2016, and after taking public testimony and giving full consideration to the matter, adopted Resolution No. 324 which recommends that the City Council approve a request for a Zone Map Amendment (Case File DB15-0109) contingent on the concurrent Comprehensive Plan Map Amendment; and

WHEREAS, on May 16, 2016, the Wilsonville City Council held a public hearing regarding the above described matter, wherein the City Council considered the full public record made before the Development Review Board, including the Development Review Board and City Council staff reports; took public testimony; and, upon deliberation, concluded that the proposed Zone Map Amendment meets the applicable approval criteria under the City of Wilsonville Development Code;

NOW, THEREFORE, THE CITY OF WILSONVILLE ORDAINS AS FOLLOWS:

Section 1. Findings. The City Council adopts, as findings and conclusions, the forgoing Recitals and Zone Map Amendment Findings in Attachment 2, as if fully set forth herein.

Section 2. Order. The official City of Wilsonville Zone Map is hereby amended by Zoning Order DB15-109, attached hereto as Attachments 1, from the Residential Agriculture-Holding (RA-H) Zone to Planned Development Residential-3 (PDR-3) Zone.

SUBMITTED to the Wilsonville City Council and read the first time at a meeting thereof on the 16<sup>th</sup> day of May 2016, and scheduled for the second and final reading on June 6, 2016, commencing at the hour of 7 p.m. at the Wilsonville City Hall, 29799 SW Town Center Loop East, Wilsonville, OR.

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder

ENACTED by the City Council on the 6<sup>th</sup> day of June, 2016, by the following

votes: Yes:\_\_\_ No:\_\_\_

\_\_\_\_\_  
Sandra C. King, MMC, City Recorder

DATED and signed by the Mayor this \_\_\_\_ day of June, 2016.

\_\_\_\_\_  
TIM KNAPP, MAYOR

**SUMMARY OF VOTES:**

Mayor Knapp  
Councilor Starr  
Councilor Lehan  
Councilor Stevens  
Councilor Fitzgerald

**Exhibits and Attachments:**

- Attachment 1 – Zoning Order DB15-0109 including legal description and sketch depicting map amendment.
- Attachment 2 – Zone Map Amendment Findings, April 26, 2016.

**ORDINANCE NO. 791- ATTACHMENT 1**

**BEFORE THE CITY COUNCIL OF THE  
CITY OF WILSONVILLE, OREGON**

In the Matter of the Application of	)	
SAMM-MILLER, LLC	)	
for a Rezoning of Land and Amendment	)	<b>ZONING ORDER DB15-0109</b>
of the City of Wilsonville	)	
Zoning Map Incorporated in Section 4.102	)	
of the Wilsonville Code.	)	

The above-entitled matter is before the Council to consider the application of DB15-0109, for a Zone Map Amendment and an Order, amending the official Zoning Map as incorporated in Section 4.102 of the Wilsonville Code.

The Council finds that the subject property (“Property”), legally described and shown in the attached legal description and sketch, has heretofore appeared on the City of Wilsonville zoning map as Residential Agriculture-Holding (RA-H).

The Council having heard and considered all matters relevant to the application for a Zone Map Amendment, including the Development Review Board record and recommendation, finds that the application should be approved.

THEREFORE IT IS HEREBY ORDERED that The Property, consisting of approximately 4.37 acres at 28500 and 28530 SW Canyon Creek Road South. Comprising tax lots 900 and 1000 of Section 13B, T3S, R1W, Clackamas County, Oregon, as more particularly shown and described in the attached legal description and sketch, is hereby rezoned to Planned Development Residential-3 (PDR-3), subject to conditions detailed in this Order’s adopting Ordinance. The foregoing rezoning is hereby declared an amendment to the Wilsonville Zoning Map (Section 4.102 WC) and shall appear as such from and after entry of this Order.

Dated: June 6, 2016.

\_\_\_\_\_  
TIM KNAPP, MAYOR

APPROVED AS TO FORM:

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Barbara A. Jacobson, City Attorney

ATTEST:

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Sandra C. King, MMC, City Recorder

Attachment: Legal Description and Sketch Depicting Land/Territory to be Rezoned

# EMERIO *Design*

Civil Engineering  
Land Survey  
Land Use Planning  
Construction Management

ZONE CHANGE  
CANYON CREEK ROAD SOUTH

JOB NO. 500-015  
APRIL 27, 2016

EXHIBIT A (PAGE 1 OF 2)

LOTS 5 AND 6, "BRIDLE TRAIL RANCHETTS", AND OTHER LAND LOCATED IN THE N.W. 1/4 OF SECTION 13, TOWNSHIP 3 SOUTH, RANGE 1 WEST OF THE WILLAMETTE MERIDIAN, CITY OF WILSONVILLE, CLACKAMAS COUNTY, OREGON, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF LOT 5, "BRIDLE TRAIL RANCHETTS", BEING ON THE EAST RIGHT OF WAY LINE OF CANYON CREEK ROAD SOUTH (50.0 FEET WIDE); THENCE ALONG THE NORTH LINE OF SAID LOT 5 AND ITS EASTERLY EXTENSION, NORTH 89°29'30" EAST, 652.49 FEET TO THE WEST LINE OF TRACT J, "WILSONVILLE MEADOWS NO. 5"; THENCE ALONG SAID WEST LINE AND THE WEST LINE OF TRACT M, "WILSONVILLE MEADOWS NO. 6", SOUTH 00°40'00" WEST, 292.04 FEET TO THE EASTERLY EXTENSION OF THE SOUTH LINE OF LOT 6, "BRIDLE TRAIL RANCHETTS"; THENCE ALONG SAID EASTERLY EXTENSION AND SOUTH LINE, SOUTH 89°29'30" WEST, 649.98 FEET TO THE SOUTHWEST CORNER OF SAID LOT 6 ON THE EAST RIGHT OF WAY LINE OF CANYON CREEK ROAD SOUTH; THENCE ALONG SAID EAST RIGHT OF WAY LINE, NORTH 00°10'30" EAST, 292.00 FEET TO THE POINT OF BEGINNING.

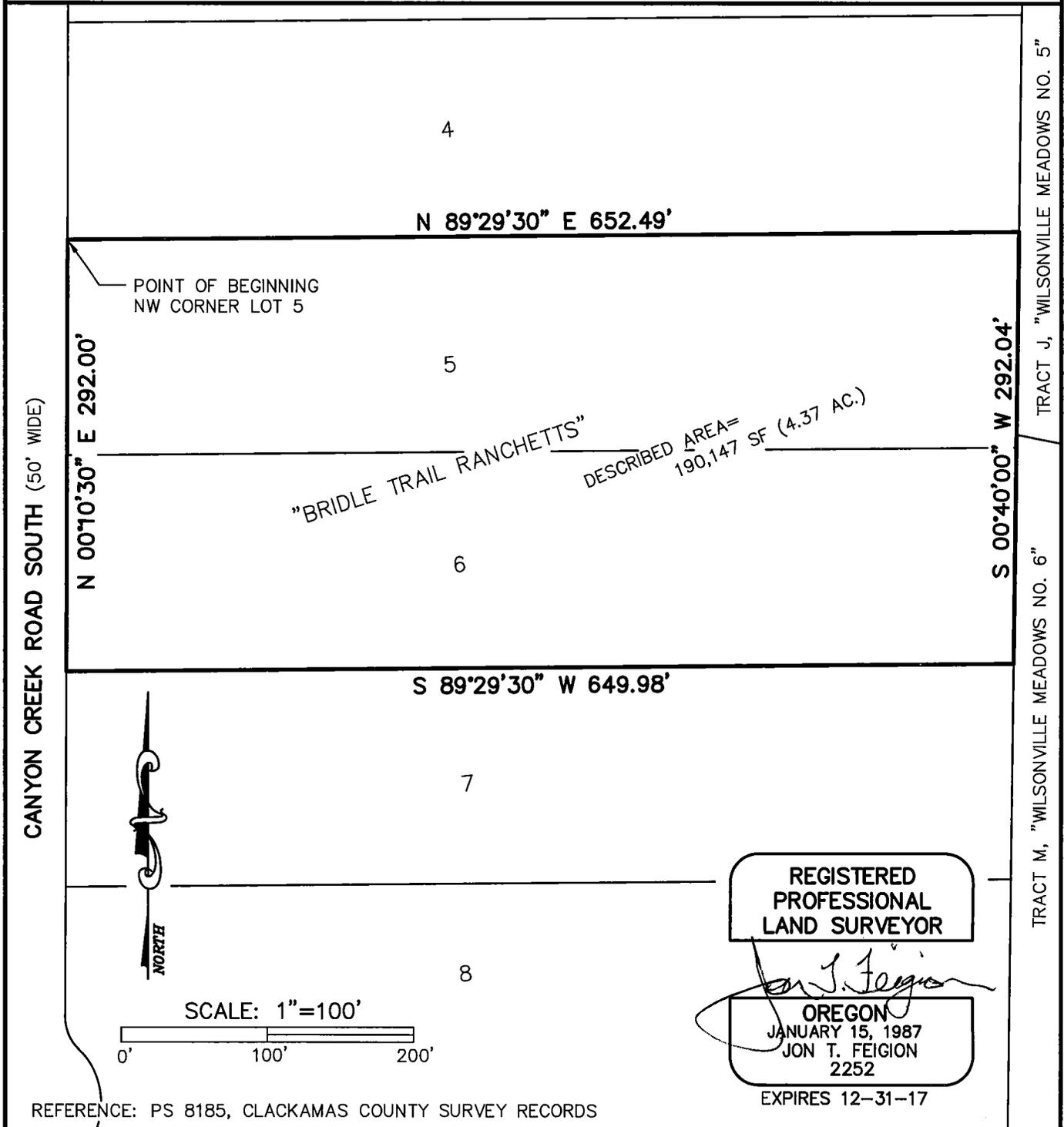
CONTAINING 190,147 SQUARE FEET (4.37 ACRES), MORE OR LESS.



REFERENCE: PS 8185, CLACKAMAS COUNTY SURVEY RECORDS

**EXHIBIT A (PAGE 2 OF 2)**

LOTS 5 AND 6, "BRIDLE TRAIL RANCHETTS" AND  
OTHER LAND LOCATED IN THE  
N.W. 1/4 OF SEC. 13, T.3S., R.1W., W.M.  
CITY OF WILSONVILLE, CLACKAMAS COUNTY, OREGON



DRAWN BY: JTF	
JOB NO.: 500-015	ZONE CHANGE
SCALE: 1" = 100'	
DATE: 4-27-16	

**EMERIO**  
*Design*

8285 SW NIMBUS AVE, SUITE 180  
BEAVERTON, OREGON 97008  
TEL: (503) 746-8812  
FAX: (503) 639-9592  
www.emeriodesign.com

Ordinance No. 791  
Staff Report  
Wilsonville Planning Division

14-Lot Single-Family Subdivision at 28500 and 28530 SW Canyon Creek Rd. South  
Zone Map Amendment

City Council  
Quasi-Judicial Public Hearing

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<b>Hearing Date:</b>	May 16, 2016
<b>Date of Report:</b>	April 26, 2016
<b>Application No.:</b>	DB15-0109 Zone Map Amendment

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**Request:** The City is being asked to review a Quasi-judicial Zone Map Amendment for a 14-lot single-family subdivision.

**Location:** Approximately 4.37 acres at 28500 and 28530 SW Canyon Creek Road South. East side of SW Canyon Creek Road South at and just south of SW Daybreak Street. The property is specifically known as Tax Lots 900 and 1000, Section 13B, Township 3 South, Range 1 West, Willamette Meridian, City of Wilsonville, Clackamas County, Oregon

**Owners:** Beth Ann Boeckman (28500 SW Canyon Creek Rd. S.)  
Karen and Marvin Lewallen (28530 SW Canyon Creek Rd. S.)

**Applicant:** Scott Miller, Samm-Miller LLC

**Applicant's Representative:** AnneMarie Skinner, Emerio Design

**Comprehensive Plan Designation (Current):** Residential 0-1 dwelling units per acre

**Comprehensive Plan Designation (Proposed with Ord. 790):** Residential 4-5 dwelling units per acre

**Zone Map Designation (Current):** Residential Agriculture-Holding (RA-H)

**Zone Map Designation (Proposed):** Planned Development Residential-3 (PDR-3)

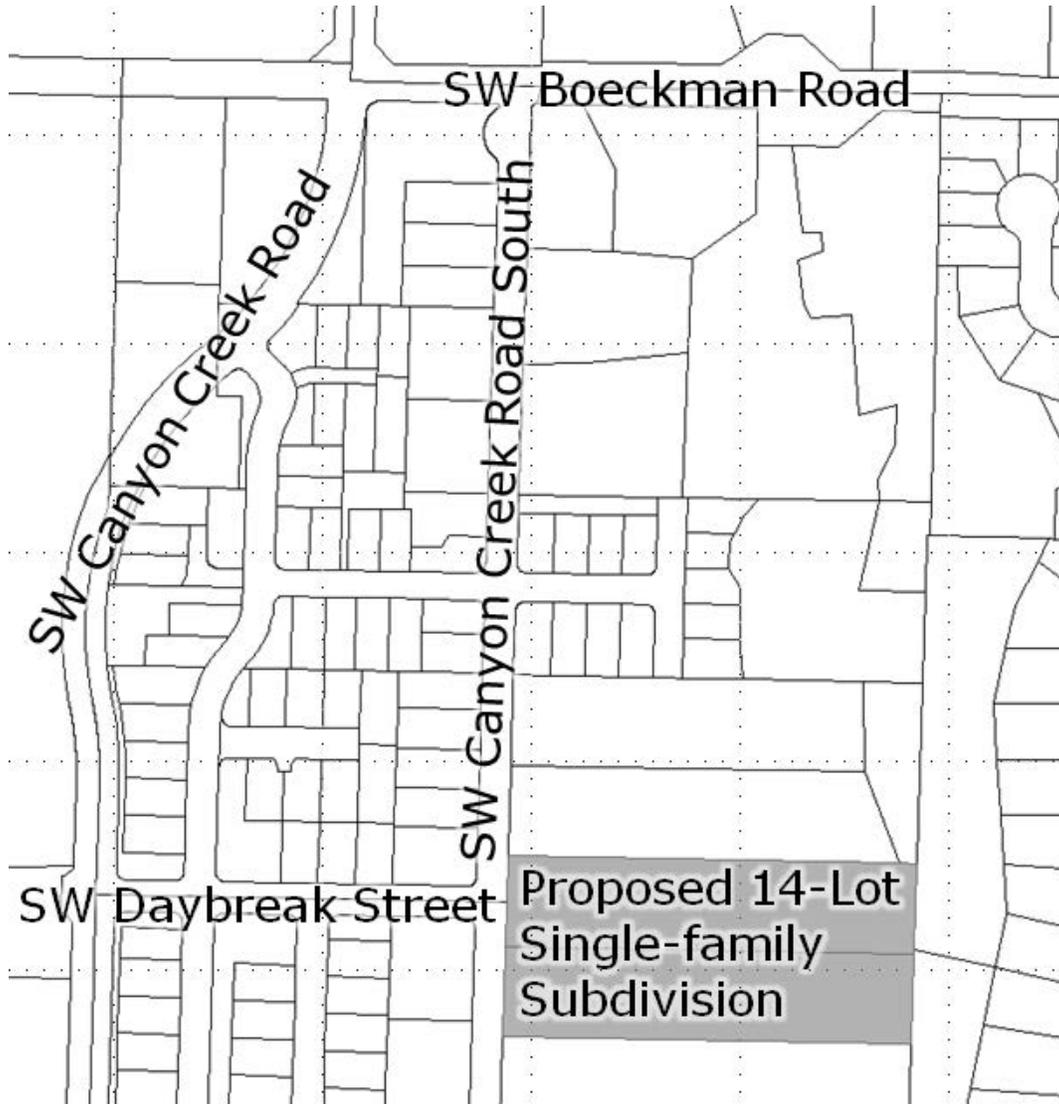
**Staff Reviewer:** Daniel Pauly AICP, Associate Planner

**Staff Recommendation:** Approve the requested Zone Map Amendment contingent on approval of the concurrent Comprehensive Plan Map Amendment (Ordinance No. 790).

**Applicable Review Criteria:**

<b><u>Development Code:</u></b>	
Section 4.008	Application Procedures-In General
Section 4.009	Who May Initiate Application
Section 4.010	How to Apply
Section 4.011	How Applications are Processed
Section 4.014	Burden of Proof
Section 4.031	Authority of the Development Review Board
Subsection 4.035 (.04)	Site Development Permit Application
Subsection 4.035 (.05)	Complete Submittal Requirement
Section 4.110	Zones
Section 4.113	Standards Applying to Residential Development in Any Zone
Section 4.118	Standards Applying to Planned Development Zones
Section 4.124	Standards Applying to All Planned Development Residential Zones
Section 4.124.3	PDR-3 Zone
Sections 4.139.00 through 4.139.11	Significant Resource Overlay Zone (SROZ)
Section 4.197	Zone Changes and Amendments to the Development Code
<b><u>Other Documents:</u></b>	
Comprehensive Plan	
Oregon Statewide Planning Goals	

**Vicinity Map**



**Background/Summary:**

Zone Map Amendment (DB15-0109)

Contingent on approval of the Comprehensive Plan Map Amendment for an increased density of 4-5 dwelling units per acre, the subject properties would receive a corresponding PDR zoning of PDR-3. This is the same zoning as other portions of Bridle Trail Ranchettes where an increased density to 4-5 dwelling units per acre has been approved.

**Conclusion:**

Staff and the DRB have reviewed the application and facts regarding the request and recommend the City Council approve the Zone Map Amendment (DB15-0109) contingent on approval of the concurrent request for a Comprehensive Plan Map Amendment.

**Findings of Fact:**

1. The statutory 120-day time limit applies to this application. The application was received on December 23, 2015. On January 21, 2016 staff conducted a completeness review within the statutorily allowed 30-day review period and found the application to be incomplete. On February 1, 2016, the Applicant submitted new materials. On February 17, 2016 the application was deemed complete. The City must render a final decision for the request, including any appeals, by June 16, 2016.

2. Surrounding land uses are as follows:

Compass Direction	Zone:	Existing Use:
North:	RA-H	Single-family Residential
East:	PDR-4	Single-family Residential
South:	RA-H	Single-family Residential
West:	PDR-3	Single-family Residential

3. Previous Planning Approvals:

Current subdivision (Bridle Trail Ranchettes) approved prior to City incorporation.

4. The applicant has complied with Sections 4.013-4.031 of the Wilsonville Code, said sections pertaining to review procedures and submittal requirements. The required public notices have been sent and all proper notification procedures have been satisfied.

## Conclusionary Findings:

NOTE: Pursuant to Section 4.014 the burden of proving the necessary findings of fact can be made for approval of any land use or development application rests with the applicant in the case.

### General Information

#### Application Procedures-In General Section 4.008

**Review Criteria:** This section lists general application procedures applicable to a number of types of land use applications and also lists unique features of Wilsonville’s development review process.

**Finding:** These criteria are met.

**Details of Finding:** Processing of the application follows the applicable general procedures of this Section.

#### Initiating Application Section 4.009

**Review Criterion:** “Except for a Specific Area Plan (SAP), applications involving specific sites may be filed only by the owner of the subject property, by a unit of government that is in the process of acquiring the property, or by an agent who has been authorized by the owner, in writing, to apply.”

**Finding:** This criterion is satisfied.

**Details of Finding:** Applications have been signed by property owners of both properties involved.

#### Pre-Application Conference Subsection 4.010 (.02)

**Review Criteria:** This section lists the pre-application process

**Finding:** These criteria are satisfied.

**Details of Finding:** A Pre-application conferences was held on April 23, 2015 (PA15-0008) in accordance with this subsection.

#### Lien Payment before Approval Subsection 4.011 (.02) B.

**Review Criterion:** “City Council Resolution No. 796 precludes the approval of any development application without the prior payment of all applicable City liens for the subject property. Applicants shall be encouraged to contact the City Finance Department to verify that there are no outstanding liens. If the Planning Director is advised of outstanding liens while an application is under consideration, the Director shall advise the applicant that payments must be made current or the existence of liens will necessitate denial of the application.”

**Finding:** This criterion is satisfied.

**Details of Finding:** No applicable liens exist for the subject property. The application can thus move forward.

General Submission Requirements  
Subsection 4.035 (.04) A.

**Review Criteria:** “An application for a Site Development Permit shall consist of the materials specified as follows, plus any other materials required by this Code.” Listed 1. through 6. j.

**Finding:** These criteria are satisfied.

**Details of Finding:** The applicant has provided all of the applicable general submission requirements contained in this subsection.

Zoning-Generally  
Section 4.110

**Review Criteria:** “The use of any building or premises or the construction of any development shall be in conformity with the regulations set forth in this Code for each Zoning District in which it is located, except as provided in Sections 4.189 through 4.192.” “The General Regulations listed in Sections 4.150 through 4.199 shall apply to all zones unless the text indicates otherwise.”

**Finding:** These criteria are satisfied.

**Details of Finding:** This proposed development is in conformity with the applicable zoning district and general development regulations listed in Sections 4.150 through 4.199 have been applied in accordance with this Section.

**DB15-0109 Zone Map Amendment**

**Comprehensive Plan**

Diversity of Housing Types  
Implementation Measure 4.1.4.b.,d.

**B1. Review Criteria:** “Plan for and permit a variety of housing types consistent with the objectives and policies set forth under this section of the Comprehensive Plan, while maintaining a reasonable balance between the economics of building and the cost of supplying public services. It is the City's desire to provide a variety of housing types needed to meet a wide range of personal preferences and income levels. The City also recognizes the fact that adequate public facilities and services must be available in order to build and maintain a decent, safe, and healthful living environment.” “Encourage the construction and development of diverse housing types, but maintain a general balance according to housing type and geographic distribution, both presently and in the future. Such housing types may include, but shall not be limited to: Apartments, single-family detached, single-family common wall, manufactured homes, mobile homes, modular homes, and condominiums in various structural forms.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** On the basis of the housing data used in the 2015 City of Wilsonville Housing Report of the City's 10,283 housing units, 55% are multi-family and 45% are single-family. Currently hundreds of new single-family home lots have been approved, mainly in Villebois, to be developed over the next few years. Only a few smaller multi-family developments are approved or under construction. In addition, the Frog Pond west planning area is planned exclusively for single-family homes as it begins to develop in the coming years. The proposal will provide additional single-family options outside of Villebois within the existing City limits supporting a trend of increasing the number of single-family homes in relation to multi-family homes.

## Development Code

Zoning Consistent with Comprehensive Plan  
Section 4.029

- B2. Review Criterion:** "If a development, other than a short-term temporary use, is proposed on a parcel or lot which is not zoned in accordance with the Comprehensive Plan, the applicant must receive approval of a zone change prior to, or concurrently with the approval of an application for a Planned Development."

**Finding:** This criterion is met or will be satisfied.

**Explanation of Finding:** The applicant is applying for a comprehensive plan map amendment and a zone change concurrently with a Stage I Master Plan, Stage II Final Plan, and other related development approvals. The proposed zoning is consistent with the proposed comprehensive plan residential density of 4-5 dwelling units per acre. The approval of the zone map amendment is contingent on City approval of the related comprehensive plan map amendment.

Base Zones  
Subsection 4.110 (.01)

- B3. Review Criterion:** This subsection identifies the base zones established for the City, including the Village Zone.

**Finding:** This criterion is satisfied.

**Explanation of Finding:** The requested zoning designation of Planned Development Residential-3 "PDR-3" is among the base zones identified.

## Standards for All Planned Development Residential Zones

Typically Permitted Uses  
Subsection 4.124 (.01)

- B4. Review Criteria:** This subsection list the allowed uses in the PDR Zones.

**Finding:** These criteria are satisfied.

**Details of Finding:** The list of typically permitted uses includes single-family dwelling units, open space, and parks, covering all proposed uses on the subject properties.

Appropriate PDR Zone  
Subsection 4.124 (.05)

**B5. Review Criteria:**

Comprehensive Plan Density	Zoning District
0-1 u/acre	PDR-1
2-3 u/acre	PDR-2
4-5 u/acre	PDR-3
6-7 u/acre	PDR-4
10-12 u/acre	PDR-5
16-20 u/acre	PDR-6
20 + u/acre	PDR-7

**Finding:** These criteria are satisfied.

**Details of Finding:** PDR-3 is the appropriate PDR designation based on the Comprehensive Plan density designation, as proposed, of 4-5 dwelling units per acre.

Zone Change Procedures  
Subsection 4.197 (.02) A.

**B6. Review Criteria:** “That the application before the Commission or Board was submitted in accordance with the procedures set forth in Section 4.008, Section 4.125(.18)(B)(2), or, in the case of a Planned Development, Section 4.140;”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant submitted the request for a zone map amendment as set forth in the applicable code sections.

Conformance with Comprehensive Plan Map, etc.  
Subsection 4.197 (.02) B.

**B7. Review Criteria:** “That the proposed amendment is consistent with the Comprehensive Plan map designation and substantially complies with the applicable goals, policies and objectives, set forth in the Comprehensive Plan text;”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposed zone map amendment is consistent with the proposed (see Request A) Comprehensive Map designation of Residential 4-5 dwelling units per acre. As shown in Request A and Finding B1 the request complies with applicable Comprehensive Plan text.

Residential Designated Lands  
Subsection 4.197 (.02) C.

**B8. Review Criteria:** “In the event that the subject property, or any portion thereof, is designated as “Residential” on the City’s Comprehensive Plan Map; specific findings shall be made addressing substantial compliance with Implementation Measure 4.1.4.b, d, e, q, and x of Wilsonville’s Comprehensive Plan text;”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** Findings B1 under this request and A24-A30 under Request A provide the required specific findings.

Public Facility Concurrency  
Subsection 4.197 (.02) D.

**B9. Review Criteria:** “That the existing primary public facilities, i.e., roads and sidewalks, water, sewer and storm sewer are available and are of adequate size to serve the proposed development; or, that adequate facilities can be provided in conjunction with project development. The Planning Commission and Development Review Board shall utilize any and all means to insure that all primary facilities are available and are adequately sized.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The applicant’s Exhibits B1 and B2 (compliance report and the plan sheets) demonstrate the existing primary public facilities are available or can be provided in conjunction with the project.

Impact on SROZ Areas  
Subsection 4.197 (.02) E.

**B10. Review Criteria:** “That the proposed development does not have a significant adverse effect upon Significant Resource Overlay Zone areas, an identified natural hazard, or an identified geologic hazard. When Significant Resource Overlay Zone areas or natural hazard, and/ or geologic hazard are located on or about the proposed development, the Planning Commission or Development Review Board shall use appropriate measures to mitigate and significantly reduce conflicts between the development and identified hazard or Significant Resource Overlay Zone;”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** The proposed design of the development preserves and protects the SROZ area on the properties.

Development within 2 Years  
Subsection 4.197 (.02) F.

**B11. Review Criterion:** “That the applicant is committed to a development schedule demonstrating that the development of the property is reasonably expected to commence within two (2) years of the initial approval of the zone change.”

**Finding:** This criterion is satisfied.

**Explanation of Finding:** Related land use approvals will expire after 2 years, so requesting the land use approvals assumes development would commence within two (2) years. However, in the scenario where the applicant or their successors do not commence development within two (2) years allowing related land use approvals to expire, the zone change shall remain in effect.

Development Standards and Conditions of Approval  
Subsection 4.197 (.02) G.

**B12. Review Criteria:** “That the proposed development and use(s) can be developed in compliance with the applicable development standards or appropriate conditions are attached to insure that the project development substantially conforms to the applicable development standards.”

**Finding:** These criteria are satisfied.

**Explanation of Finding:** As can be found in the findings for the accompanying requests, the applicable development standards will be met either as proposed or as a condition of approval.

**DEVELOPMENT REVIEW BOARD  
RESOLUTION NO. 324**

**A RESOLUTION ADOPTING FINDINGS RECOMMENDING APPROVAL TO CITY COUNCIL OF A COMPREHENSIVE PLAN MAP AMENDMENT FROM RESIDENTIAL 0-1 DWELLING UNITS PER ACRE TO RESIDENTIAL 4-5 DWELLING UNITS PER ACRE, A ZONE MAP AMENDMENT FROM RESIDENTIAL AGRICULTURE-HOLDING (RA-H) TO PLANNED DEVELOPMENT RESIDENTIAL 3 (PDR-3) AND ADOPTING FINDINGS AND CONDITIONS APPROVING A STAGE I MASTER PLAN, STAGE II FINAL PLAN, SITE DESIGN REVIEW, TYPE C TREE PLAN, WAIVER AND TENTATIVE SUBDIVISION PLAT FOR A 14-LOT SINGLE-FAMILY SUBDIVISION LOCATED AT 28500 AND 28530 SW CANYON CREEK ROAD SOUTH. THE SUBJECT SITE IS LOCATED ON TAX LOTS 900 AND 1000 OF SECTION 13B, TOWNSHIP 3 SOUTH, RANGE 1 WEST, WILLAMETTE MERIDIAN, CITY OF WILSONVILLE, CLACKAMAS COUNTY, OREGON. BETH ANN BOECKMAN AND KAREN AND MARVIN LEWALLEN – OWNERS. SCOTT MILLER, SAMM-MILLER LLC – APPLICANT.**

WHEREAS, an application, together with planning exhibits for the above-captioned development, has been submitted in accordance with the procedures set forth in Section 4.008 of the Wilsonville Code, and

WHEREAS, the Planning Staff has prepared staff report on the above-captioned subject dated April 18, 2016, and

WHEREAS, said planning exhibits and staff report were duly considered by the Development Review Board Panel B at a scheduled meeting conducted on April 25, 2016, at which time exhibits, together with findings and public testimony were entered into the public record, and

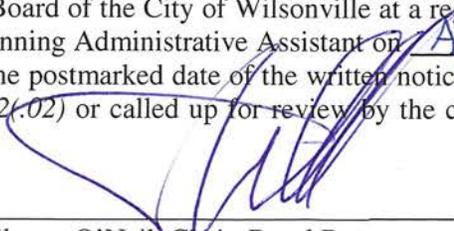
WHEREAS, the Development Review Board considered the subject and the recommendations contained in the staff report, and

WHEREAS, interested parties, if any, have had an opportunity to be heard on the subject.

NOW, THEREFORE, BE IT RESOLVED that the Development Review Board of the City of Wilsonville does hereby adopt the staff report dated April 18, 2016, attached hereto as Exhibit A1, with findings and recommendations contained therein, and authorizes the Planning Director to issue permits consistent with said recommendations, subject to City Council approval of the Comprehensive Plan Map Amendment and Zone Map Amendment Requests (DB15-0108 and DB15-0109) for:

DB15-0110 through DB15-0115, Stage I Preliminary Plan, Stage II Final Plan, Site Design Review, Type C Tree Plan, Waiver, and Tentative Subdivision Plat for a 14-lot residential subdivision, and associated parks and open space and other improvements.

ADOPTED by the Development Review Board of the City of Wilsonville at a regular meeting thereof this 25<sup>th</sup> day of April, 2016 and filed with the Planning Administrative Assistant on April 26, 2016. This resolution is final on the 15th calendar day after the postmarked date of the written notice of decision per WC Sec 4.022(.09) unless appealed per WC Sec 4.022(.02) or called up for review by the council in accordance with WC Sec 4.022(.03).

  
\_\_\_\_\_  
Shawn O'Neil, Chair, Panel B  
Wilsonville Development Review Board

Attest:  
  
\_\_\_\_\_  
Shelley White, Planning Administrative Assistant



**CITY COUNCIL  
STAFF REPORT**

<p><b>Meeting Date:</b>  June 06, 2016</p>	<p><b>Subject: Ordinance No. 789:</b> Proposed 2016 Minor Amendments to the 2013 Transportation System Plan (TSP).  <b>Staff Member:</b> Eric Mende, Capital Projects Manager <b>Department:</b> Community Development</p>	
<p><b>Action Required</b></p> <p><input checked="" type="checkbox"/> Motion</p> <p><input checked="" type="checkbox"/> Public Hearing Date: <input checked="" type="checkbox"/> Ordinance (2<sup>nd</sup> Reading) <input type="checkbox"/> Information or Direction <input type="checkbox"/> Information Only <input type="checkbox"/> Council Direction <input type="checkbox"/> Consent Agenda</p>	<p><b>Advisory Board/Commission Recommendations</b></p> <p><b>Comments:</b> On April 13, 2016 the Planning Commission conducted a public hearing on the proposed TSP modifications forwarding a unanimous recommendation of approval onto the City Council. Additional changes were made as a result of the May 02, 2016 Council Hearing and May 16, 2016 Council Work Session.</p>	
<p><b>Staff Recommendation:</b> Staff recommends Council adopt Ordinance No. 789.</p>		
<p><b>Recommended Language for Motion:</b> I move to approve Ordinance No. 789.</p>		
<p><b>PROJECT / ISSUE RELATES TO:</b></p>		
<p><input checked="" type="checkbox"/> Council Goals/Priorities Ensure efficient, cost effective and sustainable development and infrastructure. Multi-modal transportation</p>	<p><input checked="" type="checkbox"/> Adopted Master Plan(s) 2013 Transportation Systems Plan</p>	<p><input type="checkbox"/> Not Applicable</p>

**ISSUE BEFORE THE CITY COUNCIL:**

The issue before the City Council is a 2nd Reading of an Ordinance approving and adopting minor amendments (2016 TSP Amendment) to the 2013 Transportation Systems Plan (TSP), as a sub-element of the City’s Comprehensive Plan.

**EXECUTIVE SUMMARY:**

During the Planning Commission Hearing on April 13, 2016 and the City Council public hearing on May 2, 2016, public testimony was received, with no objections to the proposed amendments voiced by the public. Since those meetings, five projects have been added to the 2016 TSP Amendment as summarized and listed below:

- At the May 2 hearing, Council discussed the scope of projects affecting three specific locations in the City, directed staff to prepare additional information, and continued the public hearing to May 16, 2016. The May 2 information request was addressed in a Staff Memorandum dated May 04, 2016 (**Attachment 2**) that was included in the May 16 Council Work Session packet, resulting in *three additional projects*.
- At the May 16 Work Session additional Council discussion led to a request to include a *fourth additional project*. The public hearing and 2nd reading of Ordinance No. 789 was again continued to June 6, 2016.
- Final staff review of the proposed 2016 TSP Amendments indicated a *fifth additional project* was needed for consistency with the proposed 2016-17 budget that is being considered by the Budget Committee.

All additional projects are now included in a revised Summary Memo from DKS Associates, dated May 25, 2016 (**Attachment 1**) and in the revised 2016 TSP Amendment (**Reference Document #3**). A summary of previously discussed issues, staff recommendations, and the five projects added after the Planning Commission Hearing are as follows:

- Alignment issues and other impacts on Wilsonville's the long range plan to create a continuous west side freight route if Kinsman Rd is deleted in favor of Garden Acres Road as the primary north-south collector through the Coffee Creek area. *Staff recommends deletion of the Kinsman Road project (RE-07) and substitution of the Garden Acres alignment (UU-08) be approved as originally presented.*
- Conversion of Printer Parkway from a private street to a public street: Was the street built to our Code? Will the City be responsible for improvements to this street if it is now included in the TSP? *Staff recommends the Printer Parkway project (UU-09) be included in the Amendment as originally presented; the road likely will not become public until future development on adjacent properties occurs at which time improvements would need to be addressed.*
- Potential TSP additions to address congestion at Boones Ferry Road / Wilsonville Road / Interstate 5: Are previous traffic projections accurate compared to existing volumes? Should spot improvements on Boones Ferry Road be included in the TSP Amendment? Should I-5 improvements (e.g., auxiliary lane/widened on ramp) be included in the TSP Amendment? *Staff recommends three new projects be added: 1) Project SI-05: Boones Ferry Road Turning Movement Improvements, as a Higher Priority Project; 2) Project RW-P2: I-5 On Ramp Widening, as an Additional Planned Project; and 3) Project RW-P3: I-5 Southbound Auxiliary Lane, as an Additional Planned Project.*
- Removal of the landscaped center median on Wilsonville Road east of Boones Ferry Road, and other intersection modifications to alleviate congestion at this intersection. *Staff recommends this project be added as High Priority Project RW – 03.*
- Addition of Truck Turning Spot Improvements at SW Kinsman Road and Wilsonville Road. *An engineering study for this intersection has recently been completed and staff recommends adding this Project to the TSP Amendment as High Priority Project SI-06.*

**EXPECTED RESULTS:**

Adoption of the Amendment will result in continued compliance with Statewide Planning Goal 12, the Transportation Planning Rule and Metro’s Regional Transportation Functional Plan, providing a sound, integrated planning document that will continue to guide the next 20 years of transportation projects and policies.

**TIMELINE:**

On April 13, 2016, the Planning Commission conducted a public hearing and unanimously approved Resolution LP 16-0001 recommending approval of the minor amendments. On May 2, 2016 City Council held a public hearing and solicited testimony. After public testimony, the Council moved to continue the public hearing and hold the record open until the May 16, 2016 regular meeting. At the May 16, 2016 City Council meeting, the Council moved to continue the hearing again to June 6, 2016. The 2nd reading and adoption of Ordinance 789 is now scheduled for this meeting (June 6, 2016). The Amendments would become effective 30 days following second reading and adoption of the ordinance.

**CURRENT YEAR BUDGET IMPACTS:**

Costs were for staff time for Community Development personnel and Consulting Services provided by DKS Associates for technical evaluation and document preparation and are currently budgeted (FY 15/16).

**FINANCIAL REVIEW / COMMENTS:**

Reviewed by: SCole Date: 5/31/16

**LEGAL REVIEW / COMMENT:**

Reviewed by: \_\_\_\_\_ Date: \_\_\_\_\_

**COMMUNITY INVOLVEMENT PROCESS:**

Adoption of the 2013 TSP included a substantial public engagement process over the course of two years with numerous opportunities for input and community dialog. The Planning Commission and City Council spent considerable time and energy shaping the Plan and the corresponding Comprehensive Plan text amendments.

For this minor Amendment, Staff created a broad property owner notification that targeted all owners in the general vicinity of the proposed changes reflected in the Amendment. Key target areas included the Coffee Creek area and Commerce Circle businesses, and the Canyon Creek, Meadows, and Landover residential areas. Businesses and residences outside city limits, but potentially impacted by the Amendment, were notified, and potentially affected governmental entities such as Metro, Washington County, and Tualatin Valley Fire and Rescue were also provided notice, as required by law. The City received very few inquiries as a result of the notification, and only one set of written comments. As referenced in the Conclusionary Findings (Attachment 1 to Exhibit A) Statewide Planning Goal #1 – Citizen Involvement, is met.

**POTENTIAL IMPACTS or BENEFIT TO THE COMMUNITY:**

The TSP and this proposed Amendment identify projects and policies that set the framework for the next 20 years of transportation improvements in all modes. These projects are intended to support community livability and economic development by providing a wide variety of transportation choices that connect the community both internally as well as externally.

**ALTERNATIVES:**

The Council can approve the Amendment as presented, as presented but with changes, or may choose not to approve. City Council can also direct Staff to further modify the policies, projects, or programs recommended in the draft Amendment, and bring it back for further hearing.

**CITY MANAGER COMMENT:**

**STAFF REPORT ATTACHMENTS**

1. May 25, 2016 DKS Memorandum, Wilsonville Transportation System Plan (TSP) Amendment Summary (Revised)
2. May 04, 2016 Staff Memo by Eric Mende

Reference documents for this staff report and Ordinance 789 are not included in packet due to their length but are available for review in the Office of the City Recorder:

1. Planning Commission Hearing Record – includes Conclusionary Findings and Planning Commission Resolution LP 16-0001
2. May 02, 2016 City Council Public Record
3. May 16, 2016 City Council Public Record



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## MEMORANDUM

**DATE:** May 25, 2016

**TO:** Project Management Team

**FROM:** Scott Mansur, P.E., PTOE  
Jordin Ketelsen, EIT

**SUBJECT:** **Wilsonville Transportation System Plan (TSP) Amendment Summary (Revised)** P15125-003

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This memorandum discusses necessary amendments to the City of Wilsonville's Transportation System Plan (TSP) since the prior TSP was adopted in 2013. Based on public hearings at the Planning Commission (April 13<sup>th</sup>, 2016), and City Council (May 2<sup>nd</sup>, 2016), and other input from Council and City Staff, these changes include the following project modifications:

- Delete the minor arterial segment for Kinsman Road between Ridder Road and Day Road, including associated truck route and bicycle route designations and delete associated Capital Improvement Project RE-07.
- Add the proposed east to west Java Road collector, including bicycle route designations and update Capital Improvements Project SI-01 accordingly.
- Add Garden Acres Road as a three-lane collector designation, including truck route and bicycle route designations. Prepare a cost estimate and project description for inclusion as a Higher Priority Project.
- Add the designated collector roadways and update the proposed trail locations from the recently adopted Frog Pond Area Plan.<sup>1</sup>
- Provide updated information for project UU-01 (Boeckman Road Dip Improvements) based on the recent OBEC bridge study.<sup>2</sup>
- Add the Printer Parkway collector (UU-08), including the proposed bicycle facilities. Prepare cost estimate and project description for inclusion as a Higher Priority Project.
- Add the collector roadways and site improvements associated with the proposed Advance Middle School site.

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<sup>1</sup> *Frog Pond Area Plan*, Angelo Planning Group, DKS Associates, November 2015.

<sup>2</sup> Boeckman Dip Reconstruction Option A Preliminary Cost Estimate, OBEC.



- Update the City's urban growth boundary (UGB) to include the area surrounding the proposed Meridian Creek Middle School and City Park site.
- Extend Capital Improvement Project BW-P2 to include sidewalk infill on Boones Ferry Road from Commerce Circle Loop to Day Road.
- Add a project to allow for set aside funds to allow purchase of strategically located properties that can facilitate bicycle and pedestrian connections as these properties become available.
- Add projects to address congestion on SW Boones Ferry Road and SW Wilsonville Road near the I-5/Wilsonville Road Interchange.
- Add project to improve truck movements at SW Kinsman Road and SW Wilsonville Road.

The following sections provide more detail for the specific proposed modifications to the TSP.

## PROPOSED AMENDMENTS FOR TSP COMPLIANCE

The discussion of recommended revisions is generally organized by reference to the applicable chapter(s) of the TSP. In all chapters, revisions to existing TSP language are presented with deletions shown in ~~strikethrough~~ and additions or new code shown as underlined. The revised TSP figures, referenced in the sections below, are attached at the end of this memorandum. The revisions identified in this memorandum will also be addressed in a final amended TSP document once the revisions are approved by the Planning Commission and City Council.

### Executive Summary

The following changes are recommended to the Executive Summary of the City of Wilsonville's TSP.

#### ***Higher Priority Projects Figure*** (Page iv)

See the recommended changes to this figure in Chapter 5 (page 4 of this memorandum).

#### ***Higher Priority Projects Table*** (Page v)

Remove the following projects from this table:

- ~~Project RE-07 Kinsman Road Extension (North)~~
- ~~Project BW-11 Frog Pond Trails~~
- ~~Project RT-02 Frog Pond Trail~~
- ~~SI-01 Clutter Road Intersection Improvements with Realignment or Grade Lowering~~

Add or update the following projects to this table:

- UU-08 Garden Acres Road Urban Upgrade
- UU-09 Printer Parkway Urban Upgrade
- UU-10 Advance Road Urban Upgrade
- RE-11 Meridian Creek Middle School Collector Roadways



- RE-12A Frog Pond West Neighborhood Collector Roads
- RE-12B Frog Pond South Neighborhood Collector Road
- RE-13 Java Road Connection and Signal
- RT-07 Revised Frog Pond Regional Trail
- BW-15 Property Acquisitions for Bike/Ped Connectivity
- SI-05 Curb Extension Removal on Boones Ferry Road
- SI-06 Truck Turning Movements at SW Kinsman Road

## Chapter 3: The Standards

The following changes are recommended to Chapter 3 of the City of Wilsonville's TSP.

### **Figure 3-2: Functional Class Designations** (Page 3-5)

Summary of changes:

- Update the UGB to include the area surrounding the proposed Meridian Creek Middle School and include 63<sup>rd</sup> Avenue and Hazel Street as collector roadways.
- Show Advance Road as a collector road to 60<sup>th</sup> Avenue.
- Remove the Kinsman Road extension between Day Road and Ridder Road.
- Modify the functional classification of Garden Acres Road from a local street to a collector.
- Add the future collector roadways proposed in the adopted Frog Pond Area Plan.
- Modify the functional classification of Printer Parkway from a private local street to a collector.
- Modify the functional classification of 60<sup>th</sup> Avenue adjacent to the proposed Advance Middle School site to a collector.
- Add the future Java Road collector.

### **Figure 3-4: Freight Routes** (Page 3-9)

Summary of changes:

- Update the UGB to include the area surrounding the proposed Meridian Creek Middle School.
- Remove the Kinsman Road extension.
- Classify Garden Acres Road as a truck route.

### **Figure 3-5: Bicycle Routes** (Page 3-11)

Summary of changes:

- Update the UGB to include the area surrounding the proposed Meridian Creek Middle School and show bike lanes on 63<sup>rd</sup> Avenue and Hazel Street.
- Update to show bike lanes on Advance Road to 60<sup>th</sup> Avenue.
- Remove the Kinsman Road extension and update the alignment of the proposed future shared use paths in the area.
- Add the planned bike lanes on the future Java Road collector.
- Show the planned future bike lanes on Garden Acres Road.
- Show the planned future bike facilities on Printer Parkway.
- Update the bicycle facilities and shared used paths in the Frog Pond area as designated in the Frog Pond Area Plan.



## Chapter 4: The Needs

The following changes are recommended to Chapter 4 of the City of Wilsonville's TSP.

### **Figure 4-1: Roadway Cross-Section Deficiencies** (Page 4-5)

Summary of changes:

- Update the UGB to include the area surrounding the proposed Meridian Creek Middle School.
- Highlight Garden Acres Road as experiencing existing collector cross-section deficiencies.
- Highlight Printer Parkway as experiencing existing collector cross-section deficiencies.
- Highlight Advance Road between Stafford Road and 60<sup>th</sup> Avenue as experiencing collector cross-section deficiencies.
- Highlight 60<sup>th</sup> Avenue adjacent to the proposed Meridian Creek Middle School site as experiencing collector cross-section deficiencies.

### **Figure 4-2: Future 2035 Capacity Deficiencies** (Page 4-7)

Summary of changes:

- Update the UGB to include the area surrounding the proposed Meridian Creek Middle School.
- Remove the Kinsman Road extension.

## Chapter 5: The Projects

The following changes are recommended to Chapter 5 of the City of Wilsonville's TSP.

### **Figure 5-2: Higher Priority Projects** (Page 5-5)

Remove the following projects:

- ~~RE-07 Kinsman Road Extension~~
- ~~BW-11 Frog Pond Trails~~
- ~~RT-02 Frog Pond Trail~~

Add the following projects:

- UU-08 Garden Acres Road Urban Upgrade
- UU-09 Printer Parkway Urban Upgrade
- UU-10 Advance Road Urban Upgrade
- RE-11 Meridian Creek Middle School Collector Roads
- RE-12A Frog Pond West Neighborhood Collector Roads
- RE-12B Frog Pond South Neighborhood Collector Road
- RE-13 Java Road Connection and Signal
- RT-07 Revised Frog Pond Regional Trail
- SI-05 Curb Extension Removal on Boones Ferry Road
- RW-03 Widen Wilsonville Road East of Boones Ferry Road
- SI-06 Truck Turning Movements at SW Kinsman Road



**Table 5-2: Higher Priority Projects (Northwest Quadrant)** (Page 5-6)

Remove the following projects:

- ~~RE-07 Kinsman Road Extension~~
- ~~SI-01 Clutter Road Intersection Improvements with Realignment or Grade Lowering~~

Add the following projects and their associated costs and descriptions:

- UU-08 Garden Acres Road Urban Upgrade (\$14,260,000)

Upgrade Garden Acres Road to a three-lane collector with bicycle lanes and upgrade the Garden Acres Road/Day Road intersection to either a signal or a roundabout. Realign Ridder Road to Garden Acres Road. Close the existing Clutter Road connection to Grahams Ferry Road after completion of Project RE-13. Close the existing Coffee Creek Correctional Facility driveway to Grahams Ferry Road and relocate the driveway to Cahalin Road.

- RE-13 Java Road Connection and Signal (\$1,500,000)

Construct Java Road with collector designation between Grahams Ferry Road and Garden Acres Road with a signal at the Java Road/Grahams Ferry Road intersection and disconnect Clutter Street from Grahams Ferry Road.

Update the description and cost of the following project:

- RW-02 Day Road Widening (\$6,600,000-\$5,900,000)

Widen Day Road from Boones Ferry Road to Grahams Ferry Road to include additional travel lanes in both directions along with bike lanes and sidewalks; project includes improvements at the Day Road/Boones Ferry Road and Day Road/Grahams Ferry Road intersections

**Figure 5-3: Higher Priority Projects (Northwest Quadrant)** (Page 5-7)

Update this figure based on the changes made in the northwest quadrant of Figure 5-2 outlined above.

**Table 5-3: Higher Priority Projects (Northeast Quadrant)** (Page 5-8)

Update the costs of the following projects:

- UU-01 Boeckman Road Dip Improvements (-\$5,850,000 \$12,220,000)
- UU-06 Stafford Road Urban Upgrade (\$3,900,000 \$4,200,000)
- RT-01A Boeckman Creek Trail (North) (\$800,000 \$850,000)

Remove the following projects:

- ~~BW-11 Frog Pond Trails~~
- ~~RT-02 Frog Pond Trail~~



Add the following projects and their associated costs and descriptions:

- UU-09 Printer Parkway Urban Upgrade (\$3,600,000)  
*Upgrade Printer Parkway to a three-lane collector with bicycle lanes and multiuse path*
- UU-10 Advance Road Urban Upgrade (\$3,175,000)  
*Upgrade Advance Road to collector standards starting at Stafford Road to the proposed 63<sup>rd</sup> Avenue (entrance to proposed Meridian Creek Middle School)*
- RE-11 Meridian Creek Middle School Site Improvements (\$1,600,000)  
*Construct the collector roadways and site improvements associated with the proposed Meridian Creek Middle School site*
- RE-12A Frog Pond West Neighborhood Collector Roads (\$9,510,000)  
*Construct the collector roadways within the west neighborhood as identified in the Frog Pond Area Plan*
- RE-12B Frog Pond South Neighborhood Collector Roads (\$2,650,000)  
*Construct the collector roadways within the south neighborhood as identified in the Frog Pond Area Plan*
- RT-07 Revised Frog Pond Regional Trail (\$700,000)  
*Construct the regional trail identified in the Frog Pond Area Plan*

**Figure 5-4: Higher Priority Projects (Northeast Quadrant)** (Page 5-9)

Update this figure based on the changes made in the northeast quadrant of Figure 5-2 outlined above.

**Table 5-4: Higher Priority Projects (Southwest Quadrant)** (Page 5-10)

Add the following project and associated costs and descriptions:

- SI-05 Curb Extension Removal on Boones Ferry Road (\$200,000)  
*Remove curb extension and add an additional northbound through lane on SW Boones Ferry Road starting at the southern SW Boones Ferry Road/Fred Meyer access and ending at the SW Boones Ferry Road/SW Wilsonville Road intersection where the curbside through lane will terminate into the existing right turn lane.*
- RW-03 Widen Wilsonville Road East of Boones Ferry Road (\$500,000)  
*Widen eastbound SW Wilsonville Road east of SW Boones Ferry Road by removing the center median. This project involves lane configuration analysis to best address congestion.*



- SI-06 Truck Turning Improvements SW Kinsman Road (\$750,000)

Rebuild the northwest corner of the Wilsonville Road/Kinsman Road intersection to accommodate truck turning movements and improve pedestrian safety. Requires right-of-way acquisition, widening, pedestrian ramp replacement, and traffic signal pole relocation.

**Figure 5-5: Higher Priority Projects (Southwest Quadrant)** (Page 5-11)

Update this figure based on the changes made in the northeast quadrant of Figure 5-2 outlined above.

**Table 5-6: Higher Priority Projects (Citywide)** (Page 5-14)

Add the following project and associated costs and descriptions:

- BW-15 Property Acquisitions for Bike/Ped Connectivity (\$1,000,000)

Provide set-aside funds to allow purchase of strategically located properties that can facilitate bicycle and pedestrian connections as these properties become available.

**Figure 5-7: Additional Planned Projects** (Page 5-17)

Summary of changes:

- Update project BW-P2 Commerce Circle Loop Sidewalk Infill to include sidewalk infill on Boones Ferry Road from Commerce Circle to Day Road.
- Delete project UU-P1 Advance Road Urban Upgrade.
- Add project RW-P2 Additional Queuing Lane on Southbound I-5 Ramp
- Add project RW-P3 I-5 Auxiliary Lane Across Boone Bridge

**Table 5-9: Additional Planned Projects (Northwest Quadrant)** (Page 5-18)

Update the following project and the associated cost and description:

- BW-P2 Commerce Circle Loop and Boones Ferry Road Sidewalk Infill (~~\$100,000~~-\$150,000)

Fill in gaps in the sidewalks network on Commerce Circle Loop and Boones Ferry Road.

**Figure 5-8: Additional Planned Projects (Northwest Quadrant)** (Page 5-19)

Update this figure based on the changes made in the northwest quadrant of Figure 5-7 outlined above.

**Table 5-10: Additional Planned Projects (Northeast Quadrant)** (Page 5-20)

Remove Project UU-P1 Advance Road Urban Upgrade.

**Figure 5-9: Additional Planned Projects (Northeast Quadrant)** (Page 5-21)

Update this figure based on the changes made in the northeast quadrant of Figure 5-7 outlined above.

**Table 5-11: Additional Planned Projects (Southwest Quadrant)** (Page 5-22)

Add the following projects and their associated costs and descriptions:



- RW-P2 Additional Queuing Lane on Southbound I-5 Ramp (\$1,280,000)

Construct a third queuing lane on the southbound I-5 ramp at the I-5/Wilsonville Road Interchange.

I-5 is an ODOT facility and therefore high priority has not been identified.

- RW-P3 I-5 Auxiliary Lane Across Boone Bridge (N/A)

Construct a northbound auxiliary lane on I-5 beginning at the Charbonneau northbound entrance ramp and terminating just north of the Wilsonville Road Interchange.

I-5 is an ODOT facility and therefore high priority has not been identified.

**Figure 5-10: Additional Planned Projects (Southwest Quadrant)** (Page 5-23)

Update this figure based on the changes made in the northeast quadrant of Figure 5-7 outlined above.

**Table 5-12: Additional Planned Projects (Southeast Quadrant)** (Page 5-24)

Add the following projects and their associated costs and descriptions:

- LT-P7 School Connection Trail (\$460,000)

Construct the School Connection Trail identified in the Frog Pond Area Plan.

Medium priority due to existing connections; will become important when school and park are constructed.

- LT-P8 60<sup>th</sup> Avenue Trail (\$240,000)

Construct the 60<sup>th</sup> Avenue Trail identified in the Frog Pond Area Plan.

Medium priority due to existing connections; will become important when school and park are constructed.

**Figure 5-11: Additional Planned Projects (Southeast Quadrant)** (Page 5-25)

Update this figure based on the changes made in the southeast quadrant of Figure 5-2 outlined above.



# MEMORANDUM

Date: May 04, 2016

To: Mayor and City Council

From: Eric Mende PE, Capital Projects Engineering Manager

**Re: TSP Amendments – May 02, 2016 Council Comments**

Dear Mayor and Councilors,

This Memorandum addresses questions and concerns raised during the May 02, 2016 Public Hearing for Ordinance 789 - 2016 TSP Amendments.

Key concerns raised by Mayor Knapp and Councilor Starr at the Hearing included:

1. Mis-alignment issues with a future Kinsman alignment south of Ridder Road, and other impacts on Wilsonville's long range plan for Kinsman as a continuous west side freight route if Kinsman Road From Ridder to Day is deleted in favor of Garden Acres Road.
2. Conversion of Printer Parkway from a private street to a public street: Was the street built to the City's Public Works Standards? Will the City be responsible for improvements to this street if it is now included in the TSP?
3. Potential TSP additions to address congestion at Boones Ferry Road / Wilsonville Road / Interstate 5: Are previous traffic projections accurate compared to existing volumes? Will spot improvements on Boones Ferry Road help alleviate congestion? Should I-5 improvements (e.g., Auxiliary lane/widened on ramp) be included in the TSP Amendment?

Each of these concerns are addressed below.

## Garden Acres Road / Kinsman Road / West Side Freight Route

Ben Altman, representing Republic Services, provided accurate and detailed testimony concerning the coordination effort and decision making resulting in the proposed deletion of the Kinsman Road alignment, and the substitution of Garden Acres Road as the primary north-south Collector and Freight Route through the Coffee Creek Industrial Area. The 2007 Coffee Creek Master Plan, 2011 Coffee Creek Infrastructure Analysis, and 2013 TSP all assumed that the Kinsman alignment would take half of the Right of Way from Republic Services, and half of the Right of Way from the adjoining property, as is general City policy. Subsequent discussions with the adjoining property owner, Bonneville Power Administration (BPA), indicated very strong opposition to any dedication of Right of Way, primarily because the area in question is underlain by a large grounding grid for the electrical substation. To maintain the general alignment plan combined with the inability to place a road partially on the substation property would shift the entire 70+ foot Right of Way onto Republic Services property.

Discussions with Republic Services and a review of their current operations indicated this shift would force closure and relocation of Republic's primary access driveway, impacting the weigh scales and circulation pattern for the entire existing facility as well as the access point for the proposed SORT bioenergy facility located due north of the existing operations. Staff concurred with this opinion, and initiated an analysis of multiple alternative alignments of the street network within the Coffee Creek area, assuming the Kinsman alignment was deleted. The traffic models showed the Garden Acres alignment did not appreciably change the overall traffic distribution, and peak (build out) traffic volumes were well within the capacity of the proposed Collector street section. Preliminary layouts were then prepared to ensure that tractor-trailer truck turning movements at Ridder Road could be accommodated (see Figure "A"). Cost estimates for these new layouts were also prepared, and are incorporated into the TSP Amendment project estimates.

While the revised Coffee Creek street plan adequately accommodates freight movement to and from Sherwood, Tualatin and the I-5 Elligsen interchange, direct connection to south Wilsonville via a continuous west side truck route remains difficult. The issue with BPA north of Ridder Road also exists south of Ridder Road. Along the nominal Kinsman alignment, BPA owns all of the property from Ridder Road south to Freeman Drive, as well as a fee-simple power line corridor extending south to the railroad tracks. (Figure "B") Staff intends to revisit the currently shown future alignment of Kinsman Road between Boeckman Road and Ridder Road during the next major update to the TSP, however, the planning efforts needed to re-evaluate options for a continuous west side freight route were felt to be beyond the scope of this current Amendment.

*Staff recommends deletion of the Kinsman Road project (RE-07) and substitution of the Garden Acres alignment (UU-08) be approve as originally presented, with consideration of the remainder of the west side truck route postponed to the next major update to the TSP.*

#### Printer Parkway

Printer Parkway was originally constructed by Xerox Corporation as a private street in the mid-1970's. It does not meet current City standards for width or section. Acceptance of Printer Parkway as a public street (and inclusion in the project list for this TSP Amendment) is the result of negotiations with the current property owner (SKB Companies) as part of a proposed multi-faceted, multi-phased development for which a broad range of development related Conditions of Approval will be written, and likely a formal Development Agreement. Presently we have an understanding with the developer for the following improvements to Printer Parkway prior to acceptance by the City (see Figure "C"):

- 1) The western portion of Printer Parkway will be brought up to full City Standards, with the developer responsible for design and construction of a three-lane collector including bike lanes, landscaping, street lights, storm facilities and sidewalks. Per standard development policy, the City would be responsible for reimbursement of the bike lanes and any increased structural section of the roadway from a local street to a collector street.

- 2) The central and eastern portion of Printer Parkway will remain as a two-lane street, without curbs or parking, but will include streetlighting and will meet the general criteria for a collector section, however, a 12' wide multi-use public pathway may be constructed in lieu of bike lanes and

two 5-ft sidewalks. This design will provide a meandering multi-use path which will minimize impacts to the natural environment on the site. The structural section will be upgraded based on a geotechnical evaluation of the current structural section. Depending on the condition of this private roadway this could be as simple as an asphalt overlay or could be more extensive (the extent of repairs/upgrades depends on how the street was built). The geotechnical investigation, design and construction of this portion of the roadway is solely at the developers cost.

Although only about a third of the vacant land is proposed for current development (and the developer desires to limit the street improvements to only the developed frontage), our Conditions will require the entire length of Printer Parkway be improved before being accepted as a City (public) street. The advantage to the City is that a public street connection between Parkway Drive and Canyon Creek is established at minimal cost to the City. This connection is currently “envisioned” in the TSP via the (unconstructed) Weideman Road collector, which will remain in the TSP as a “future” road (additional planned but unfunded project), but could potentially be eliminated in a future TSP major update. The advantage to the developer is that they will not have to construct (or partially construct) Weideman Road as part of their development, as would be the case if the Printer Parkway option did not exist. The Weideman Road alignment contains many similar constraints to Kinsman, particularly BPA ownership.

*Staff recommends the Printer Parkway project (UU-09) be included in the Amendment as originally presented.*

#### Boones Ferry Road / Wilsonville Road / I-5 congestion

The cause(s) of traffic congestion at the southbound I-5 Ramp, Wilsonville Road, and Boones Ferry Road are well understood by City Staff. However, remedies are generally on the Interstate itself, which is outside the control of the City and cannot be fixed by a City initiated improvement project. This does not necessarily prevent the City from including I-5 related projects in our TSP to highlight the need for improvements, and inform the regional transportation planning process.

The congestion experienced on Wilsonville Road and Boones Ferry Road are primarily a result of a lack of capacity on southbound I-5 across the Boone Bridge. Traffic backs up on I-5, which backs up the on-ramps, which backs up onto Wilsonville Road and Boones Ferry Road. A review of previous traffic studies shows the analyses/predictions of traffic volumes have been generally conservative – i.e., higher than actual traffic counts. A 2002 *Freeway Access Study* predicted a 2020 southbound peak hour on-ramp volume of 1590. The 2008 Fred Meyer traffic study showed existing volume at 1211, and a predicted Stage II (build out) volume of 1389. Recent counts (Dec. 2014) showed 1191 vehicles per hour – essentially unchanged from 2008. The southbound ramp meter, set by ODOT at 1260 vehicles per hour, is higher than the actual counts, indicating that the ramp meter is not the primary cause of the backups, since the congestion across the Boone Bridge appears to be limiting actual volumes to less than the ramp meter setting.

What has changed is the traffic volume on Wilsonville Road and Boones Ferry Road feeding into the interchange, and because the capacity of the Boone Bridge remains the same, the additional vehicles wanting to go south on I-5 are stuck on City streets. Given the inability of Wilsonville's TSP to guide improvements to I-5, the current TSP approach to resolving congestion in this area is to remove vehicles from the area that are not destined to get on I-5. The two primary projects that do this are the Brown Road Extension (Project RE-04B) – also known as the Old Town Escape and recently re-titled as Boones Ferry Road to Brown Road (BFR2BR) Connector Corridor Plan (CIP 4196), and the Kinsman Road Extension (Project RE-08)(CIP 4004). These projects remain in the Amendment.

The ultimate solution to traffic congestion in and around the I-5 / Wilsonville Road interchange requires increased capacity on I-5 across the Boone Bridge. Specifically, a southbound auxiliary lane from Wilsonville Road to either the Charbonneau exit or further to the Canby/Hubbard exit will improve capacity across the Boone Bridge, allowing traffic stuck on Wilsonville Road to enter the interstate more efficiently. A southbound auxiliary lane was identified in the 2002 Freeway Access Study, although ODOT has said many times that the bridge width is insufficient to implement that project. No cost estimate has been developed for this project, however, staff will continue to participate with ODOT to evaluate capacity constraints for I-5 through Wilsonville and across and south of the Boone Bridge. A widened on-ramp (i.e., 3 lane stacking queue) does not increase capacity on I-5, but would at least get a few more vehicles off of Wilsonville Road and Boones Ferry Road. (See Figure "D") A 2014 ODOT cost estimate puts the project cost at \$1.28M. Neither of these projects can be implemented directly by Wilsonville, but including them in our TSP will allow these projects to be uploaded to the Regional Transportation Plan (RTP) at some point in the future.

Spot improvement on Boones Ferry Road – specifically removal of the curb bump out at the Fred Meyers/Albertson entrance drive and re-striping of the lanes, will result in a small capacity increase on Boones Ferry Road, and allow the intersection to function more efficiently during periods of congestion. Indirectly, these spot improvements may also impact the overall congestion at the southbound on-ramp. Importantly, this project can be implemented by Wilsonville without ODOT involvement. The cost estimate for this project is \$150k to \$200k.

*Staff recommends three new projects be added to the Amendment (See revised Figures 5-2, 5-5, 5-7, and 5-10): 1) Project SI-05: Boones Ferry Road Turning Movement Improvements, as a Higher Priority Project; 2) Project RW-P2: I-5 On-Ramp Widening, as an Additional Planned Project; and 3) Project RW-P3: I-5 Southbound Auxiliary Lane, as an Additional Planned Project.*

Respectfully Submitted,

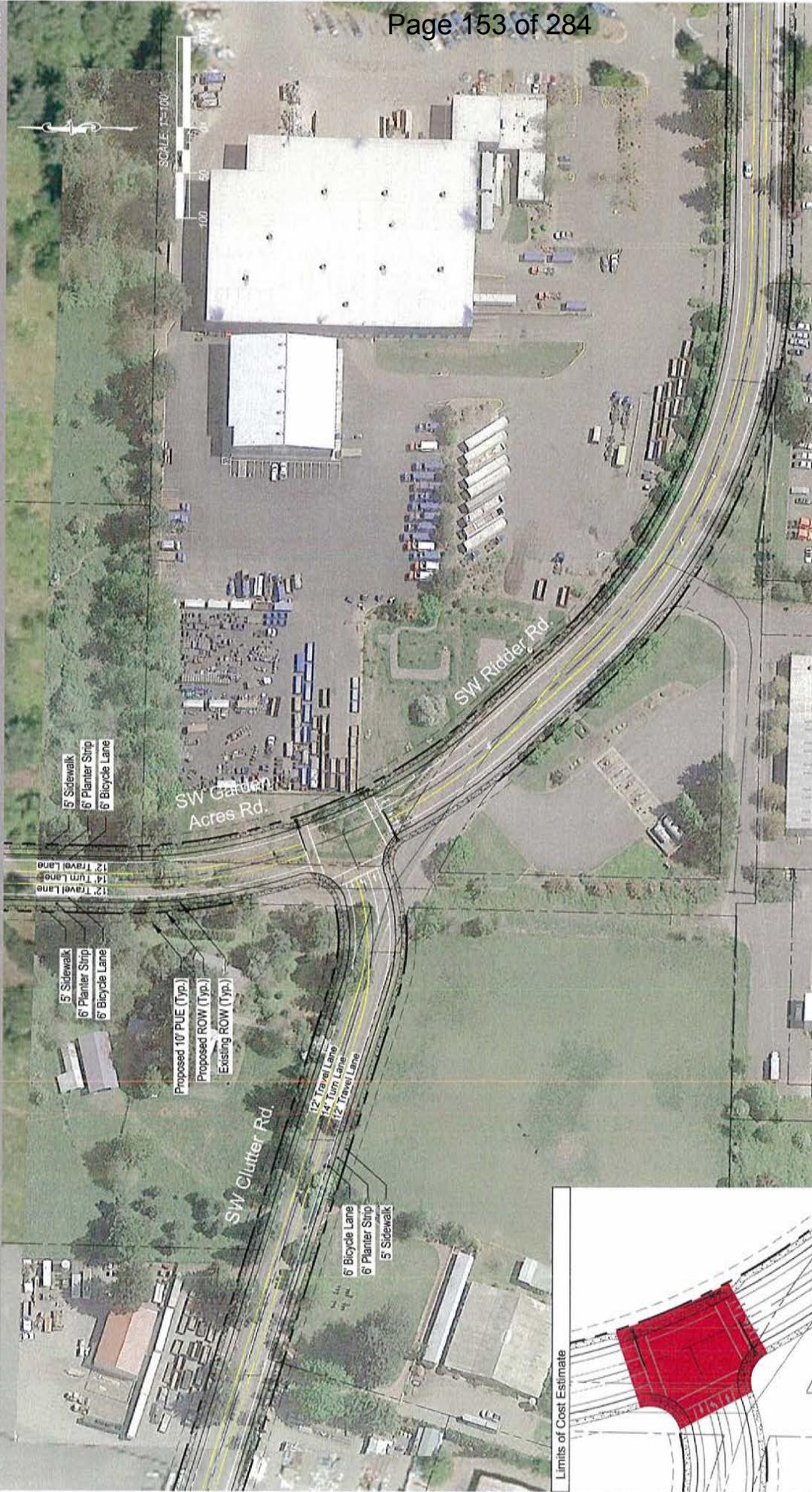


Eric Mende

# FIGURE "A"

## Coffee Creek Area Concepts

Preliminary Design Subject to Change  
Date: December 15, 2015



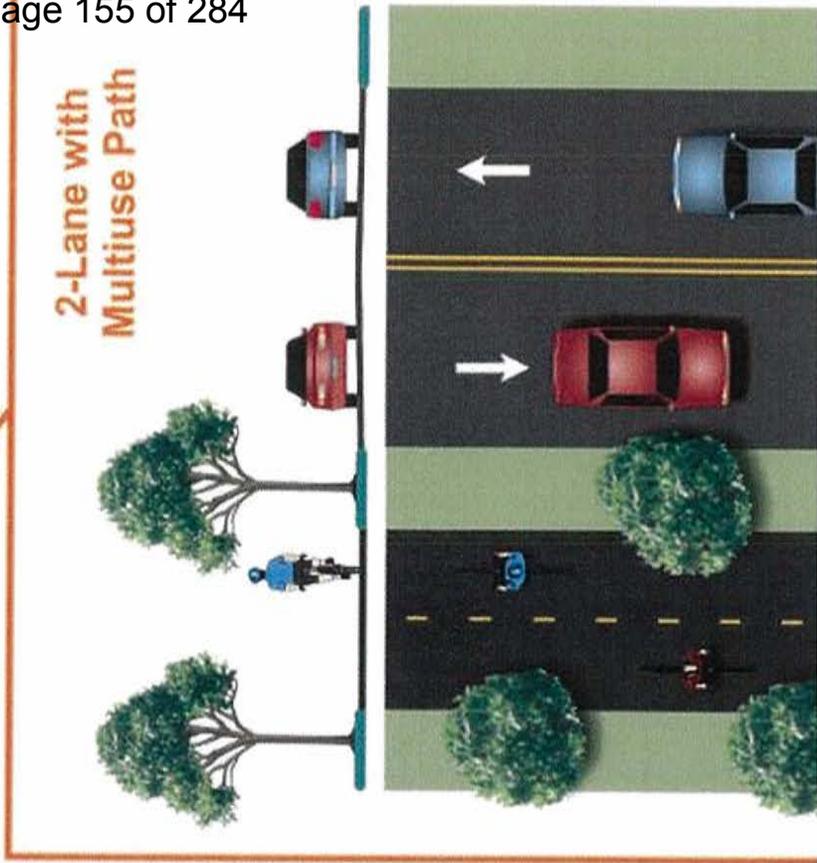
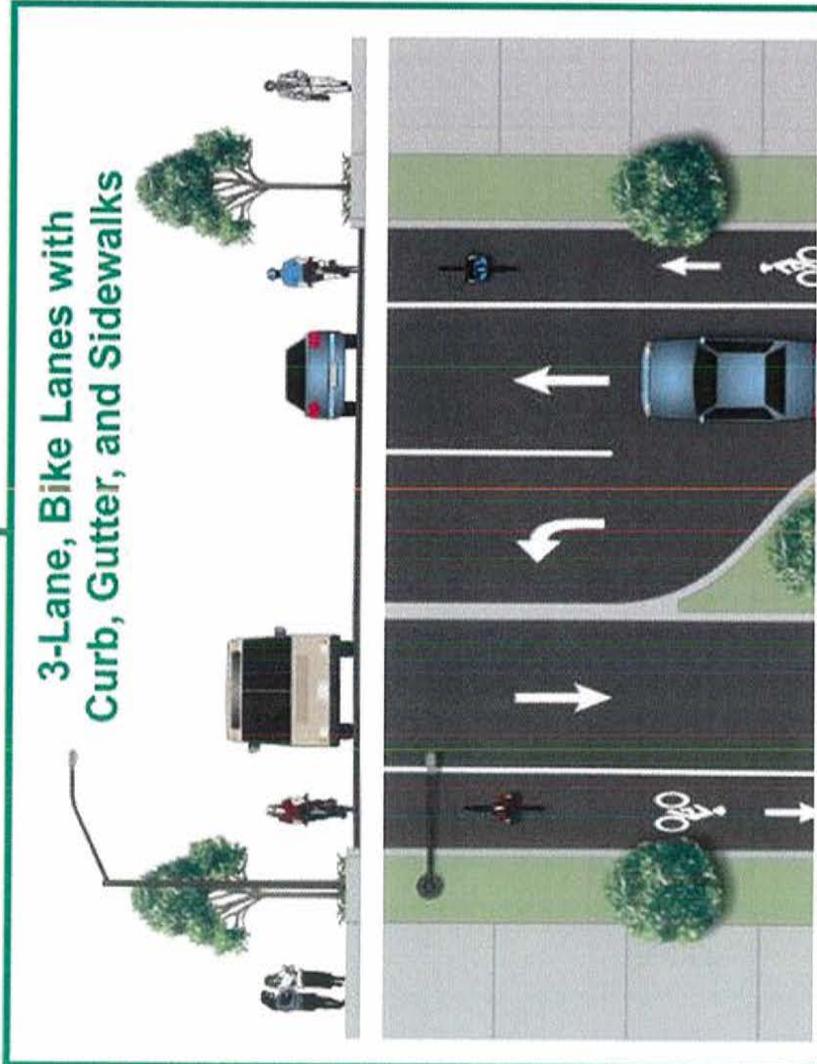
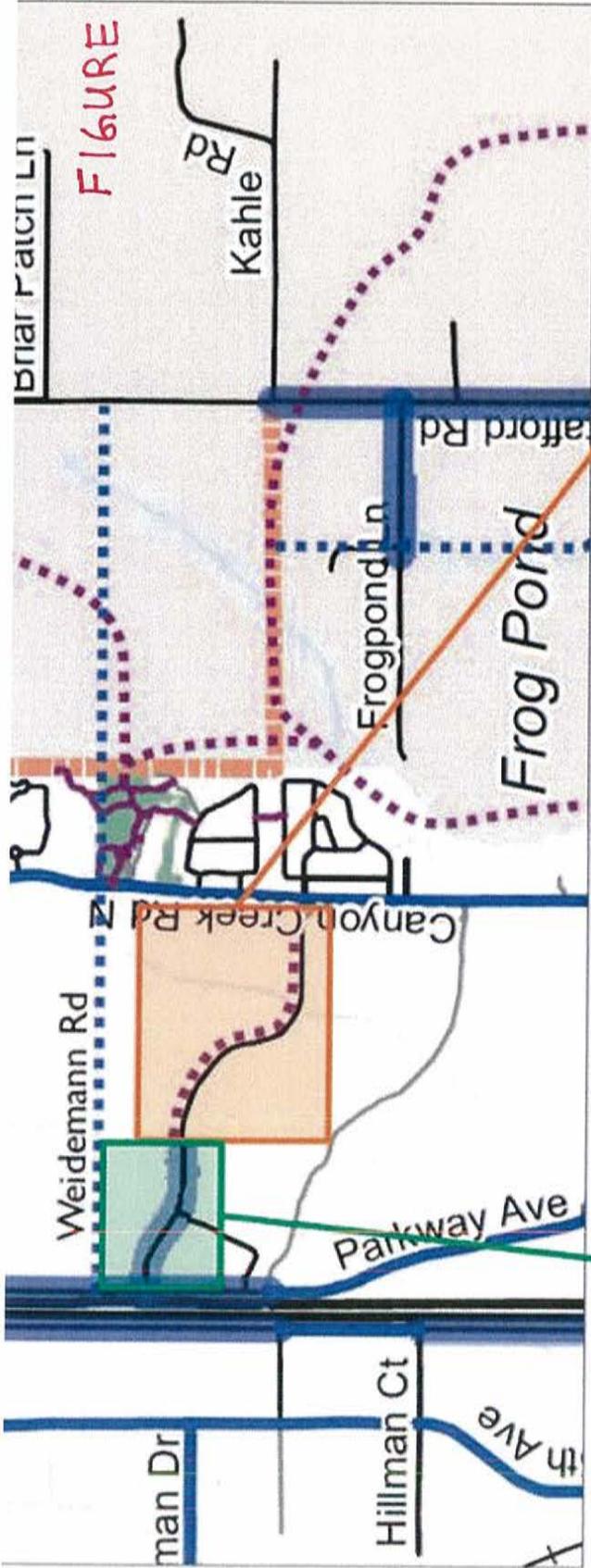
SW Ridder Rd to SW Garden Acres Rd Concept  
Wilsonville, Oregon

**K** KITTELSON & ASSOCIATES, INC.  
PHYSICIAN (NO) 225-2323  
CONTRACT: Road & Bridge of City, Baraboo



Figure "B"

FIGURE "C"



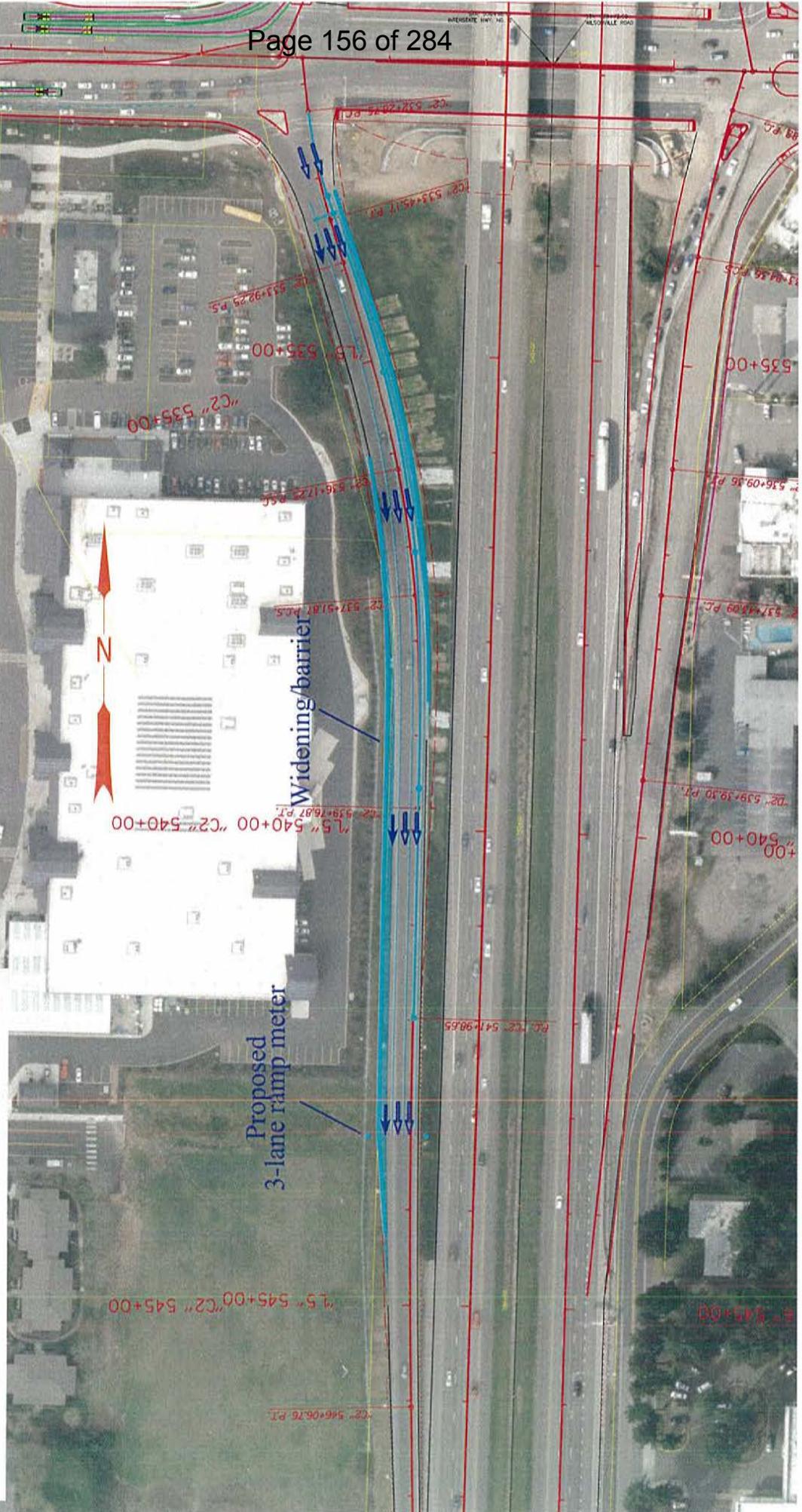
# Wilsonville Intch. SB Entrance Ramp

## Proposed 3-Lane Ramp Meter

### FIGURE "D"

October 2014

SCALE 1" = 100'



Proposed 3-lane ramp meter

Widening barrier

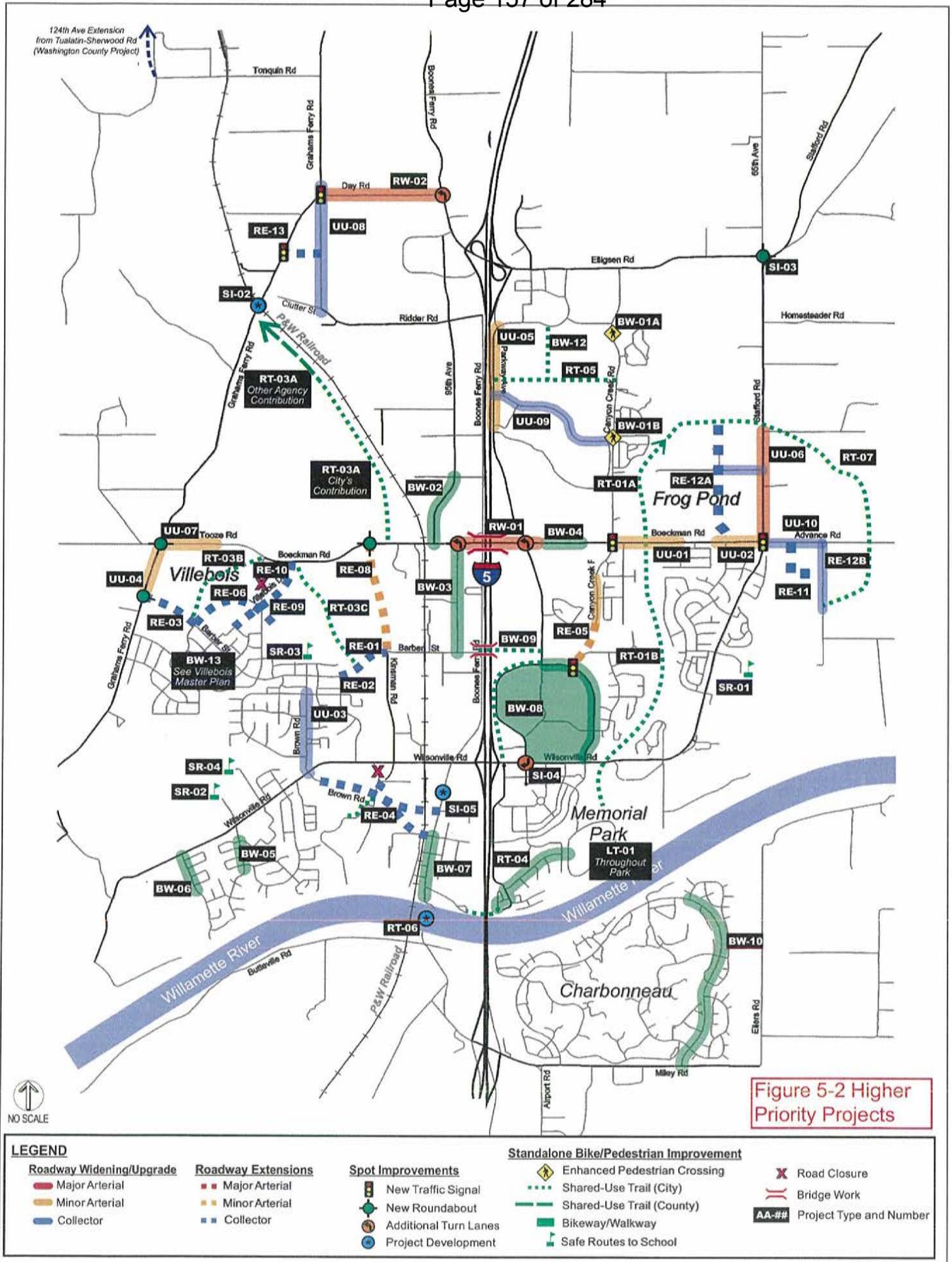
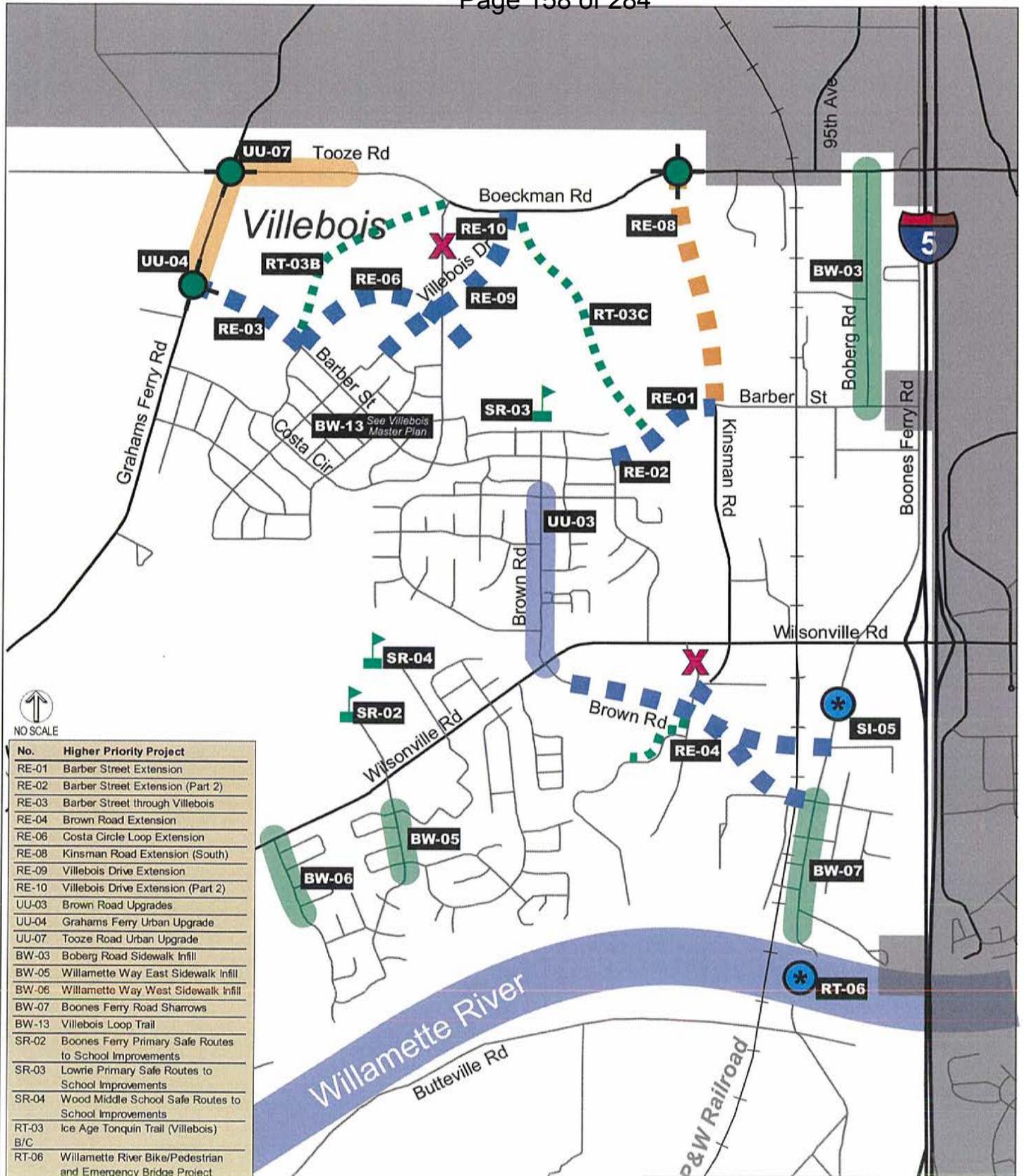


Figure 5-2 Higher Priority Projects



NO SCALE

No.	Higher Priority Project
RE-01	Barber Street Extension
RE-02	Barber Street Extension (Part 2)
RE-03	Barber Street through Villebois
RE-04	Brown Road Extension
RE-06	Costa Circle Loop Extension
RE-08	Kinsman Road Extension (South)
RE-09	Villebois Drive Extension
RE-10	Villebois Drive Extension (Part 2)
UU-03	Brown Road Upgrades
UU-04	Grahams Ferry Urban Upgrade
UU-07	Tooze Road Urban Upgrade
BW-03	Boberg Road Sidewalk Infill
BW-05	Willamette Way East Sidewalk Infill
BW-06	Willamette Way West Sidewalk Infill
BW-07	Boones Ferry Road Sharrows
BW-13	Villebois Loop Trail
SR-02	Boones Ferry Primary Safe Routes to School Improvements
SR-03	Lowrie Primary Safe Routes to School Improvements
SR-04	Wood Middle School Safe Routes to School Improvements
RT-03	Ice Age Tonquin Trail (Villebois)
B/C	
RT-06	Willamette River Bike/Pedestrian and Emergency Bridge Project Development

(See Project Table for Additional Details)

Figure 5-5  
Higher Priority Projects (Southwest Quadrants)

**LEGEND**

<p><b>Roadway Widening/Upgrade</b></p> <ul style="list-style-type: none"> <li><span style="color: red;">—</span> Major Arterial</li> <li><span style="color: orange;">—</span> Minor Arterial</li> <li><span style="color: blue;">—</span> Collector</li> </ul>	<p><b>Roadway Extensions</b></p> <ul style="list-style-type: none"> <li><span style="color: red;">- - -</span> Major Arterial</li> <li><span style="color: orange;">- - -</span> Minor Arterial</li> <li><span style="color: blue;">- - -</span> Collector</li> </ul>	<p><b>Spot Improvements</b></p> <ul style="list-style-type: none"> <li> New Traffic Signal</li> <li> New Roundabout</li> <li> Additional Turn Lanes</li> <li> Project Development</li> </ul>	<p><b>Standalone Bike/Pedestrian Improvement</b></p> <ul style="list-style-type: none"> <li> Enhanced Pedestrian Crossing</li> <li><span style="color: green;">- - -</span> Shared-Use Trail (City)</li> <li><span style="color: green;">—</span> Shared-Use Trail (County)</li> <li> Bikeway/Walkway</li> <li> Safe Routes to School</li> </ul>	<ul style="list-style-type: none"> <li><span style="color: red;">X</span> Road Closure</li> <li> Bridge Work</li> <li><b>AA-##</b> Project Type and Number</li> </ul>
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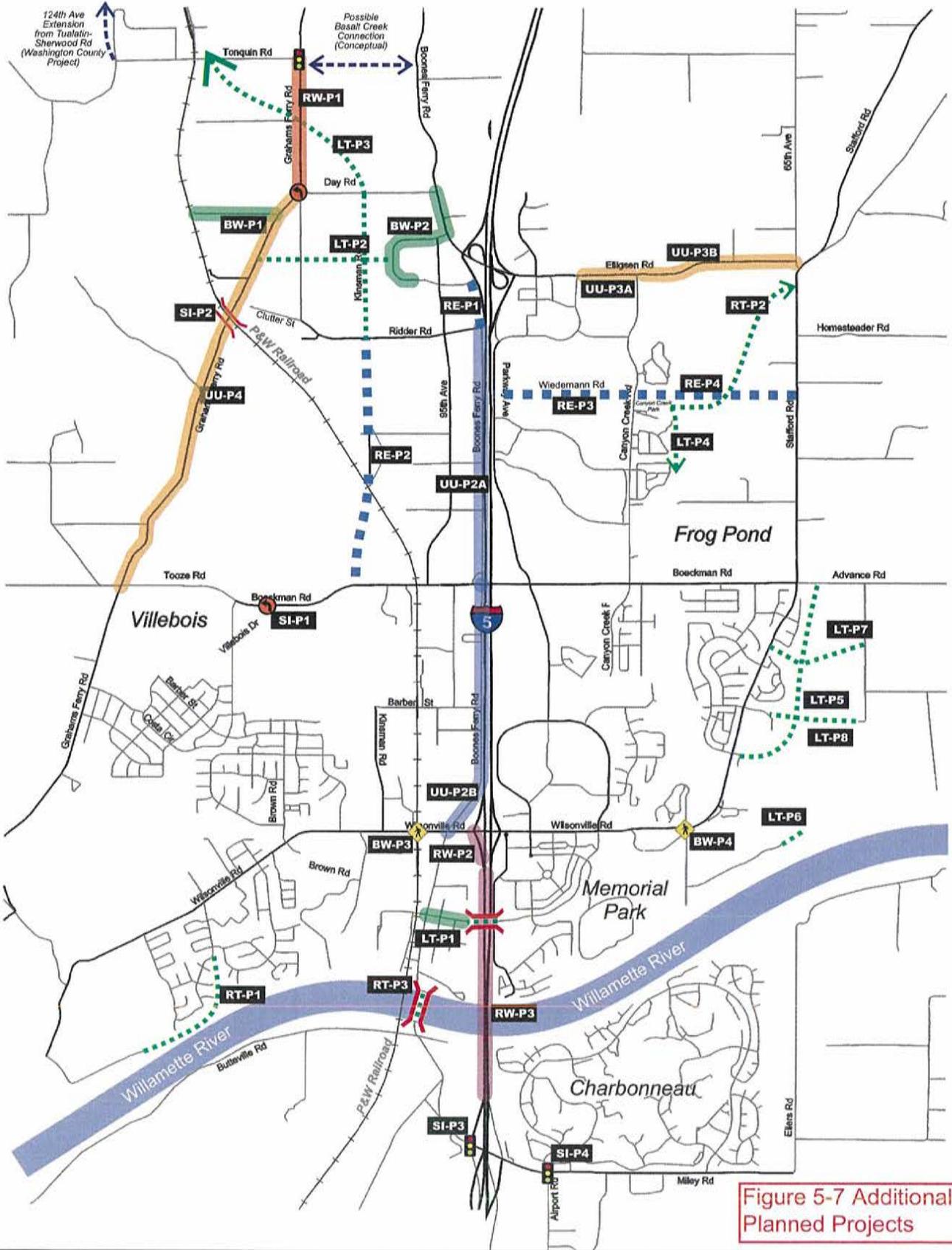
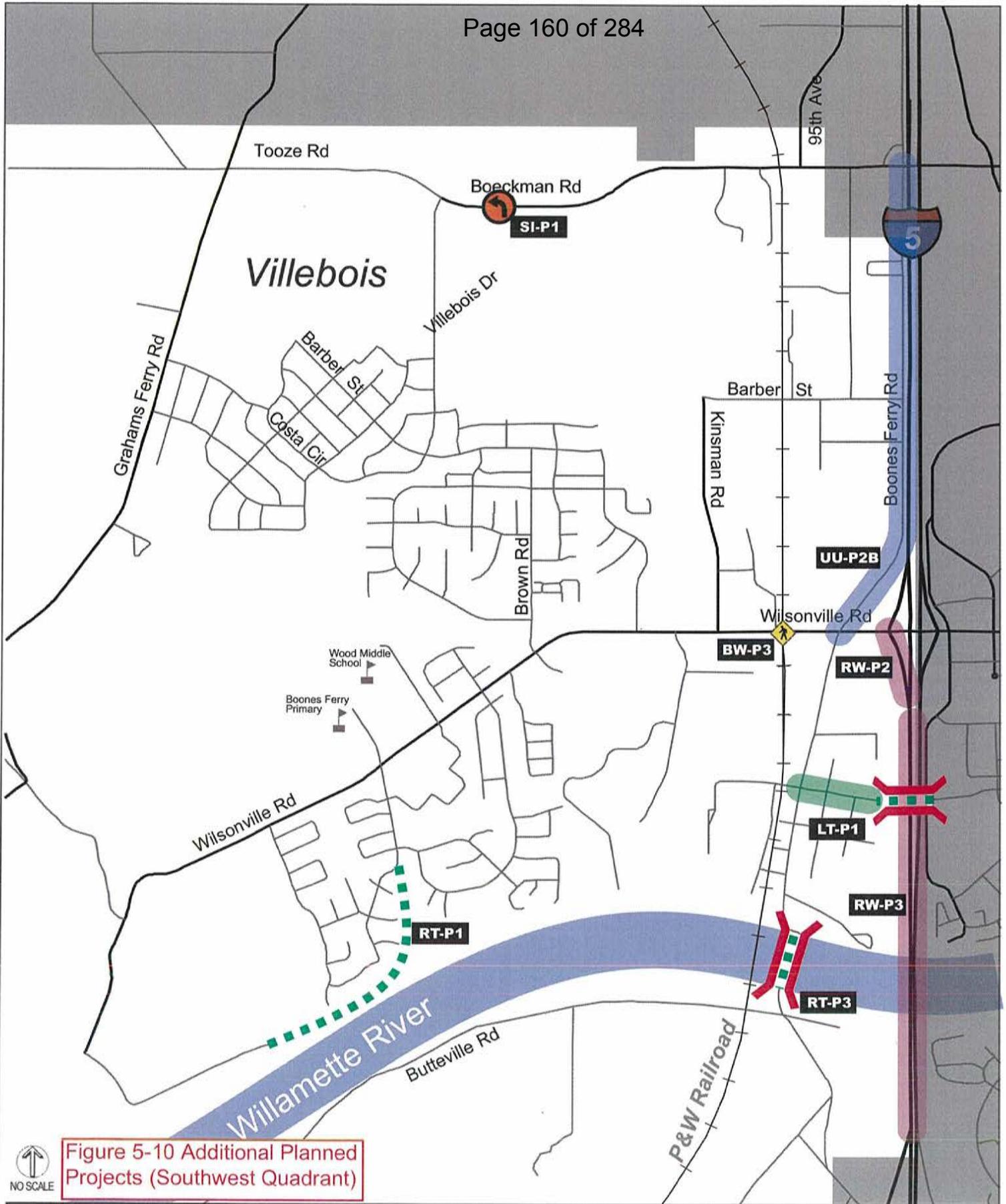


Figure 5-7 Additional Planned Projects

**LEGEND**

Roadway Widening/Upgrade	Roadway Extensions	Spot Improvements	Standalone Bike/Pedestrian Improvement	Other
Major Arterial	Major Arterial	New Traffic Signal	Enhanced Pedestrian Crossing	Road Closure
Minor Arterial	Minor Arterial	New Roundabout	Shared-Use Trail (City)	Bridge Work
Collector	Collector	Additional Turn Lanes	Shared-Use Trail (County)	AA-## Project Type and Number
ODOT Facility		Project Development	Bikeway/Walkway	
			Safe Routes to School	



**Figure 5-10 Additional Planned Projects (Southwest Quadrant)**



**LEGEND**

**Roadway Widening/Upgrade**

- Major Arterial
- Minor Arterial
- Collector
- ODOT Facility

**Roadway Extensions**

- - - Major Arterial
- - - Minor Arterial
- - - Collector

**Spot Improvements**

- New Traffic Signal
- New Roundabout
- Additional Turn Lanes
- Project Development

**Standalone Bike/Pedestrian Improvement**

- Enhanced Pedestrian Crossing
- - - Shared-Use Trail (City)
- Shared-Use Trail (County)
- Bikeway/Walkway
- Safe Routes to School

- Road Closure
- Bridge Work
- AA-##** Project Type and Number

**ORDINANCE NO. 789**

**AN ORDINANCE OF THE CITY OF WILSONVILLE ADOPTING A MINOR AMENDMENT TO THE CITY OF WILSONVILLE'S 2013 TRANSPORTATION SYSTEMS PLAN (2016 TRANSPORTATION SYSTEMS PLAN AMENDMENT).**

WHEREAS, the City of Wilsonville desires to use best professional practices to ensure land development contributes to creating a safe and attractive transportation network that supports Wilsonville's economy and quality of life; and

WHEREAS, the City of Wilsonville desires to plan for transportation system improvements that can be incorporated into its capital improvement program; and

WHEREAS, the City of Wilsonville adopted the 2013 Transportation System Plan (TSP) on June 17, 2013; and

WHEREAS, the Wilsonville Planning Commission held a work session on March 9, 2016 and a public hearing on April 13, 2016 to discuss and take public testimony on the proposed amendments; and

WHEREAS, the Wilsonville City Council held public hearings on May 2 and 16, 2016 and June 6, 2016 to discuss and take public testimony on the proposed amendments; and

WHEREAS, the City provided public hearing notices to 1,002 property owners within the City limits, a list of interested parties and agencies, and posted the notice in three locations throughout the City and on the City website; and

WHEREAS, the Wilsonville Planning Commission approved Resolution LP16-0001 recommending adoption of the proposed amendments at the public hearing on April 13, 2016; and

WHEREAS, the City Council having conducted public hearings on the proposed amendments on May 2 and 16, 2016 and June 06, 2016, and duly considering the entire record, herein finds that the proposed minor amendments to the TSP are in the best interest of the community by providing for system improvements and development that contribute to the creation of a safe and multi-modal transportation network;

NOW, THEREFORE, THE CITY OF WILSONVILLE ORDAINS AS FOLLOWS:

1. The City Council hereby adopts the 2016 Amendment to the City of Wilsonville 2013 Transportation System Plan attached hereto as Exhibit A incorporated herein.
2. This ordinance will become effective 30 days after adoption.

SUBMITTED to the Wilsonville City Council and read for the first time at a regular meeting thereof on May 2, 2016, and scheduled for a second reading at a regular meeting of the Council on May 16, 2016, which was then continued to June 6, 2016, commencing at the hour of 7:00 P.M. at the Wilsonville City Hall.

---

Sandra C. King, MMC, City Recorder

ENACTED by the City Council on the XX day of June, 2016 by the following votes:

Yes: -X-

No: -X-

---

Sandra C. King, MMC, City Recorder

DATED and signed by the Mayor this \_\_\_\_ day of \_\_\_\_, 2016.

---

TIM KNAPP, Mayor

SUMMARY OF VOTES:

Mayor Knapp -

Council President Starr -

Councilor Lehan -

Councilor Fitzgerald -

Councilor Stevens -

EXHIBIT A: 2016 Transportation System Plan Amendment

Wilsonville TSP Cover Placeholder  
(Cover Prepared in CorelDraw)

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# Acknowledgements

This project was partially funded by a grant from the Transportation Growth Management (TGM) Program, a joint program of the Oregon Department of Transportation and the Oregon Department of Land Conservation and Development. This TGM grant is financed, in part, by federal Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU), local government, and State of Oregon funds. The contents of this document do not necessarily reflect views or policies of the State of Oregon.

This report was prepared through the collective effort of the following people:



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\*\* Former City Councilor involved in the process prior to adoption

# How to Use This Plan

The Wilsonville TSP consists of two parts:

- **Main body**  
(This report)
- **Technical Appendix**  
(Separate document containing resources used to develop this plan)

Various sections answer the following questions:

- **Table of Contents**  
(What does the TSP include and where can I find it?)
- **Glossary of Terms**  
(What do the words and acronyms mean?)
- **Executive Summary**  
(What are the TSP's key findings?)
- **Chapter 2: The Vision**  
(What are the City's vision, goals, and policies?)
- **Chapter 3: The Standards**  
(What standards will guide improvements?)
- **Chapter 5: The Projects**  
(Which projects does the City expect to be able to fund in the 20-year planning horizon?)
- **Chapter 6: Programs**  
(What system management efforts is the City engaged in?)

## RELATIONSHIP TO OTHER CITY PLANS

The Wilsonville Transportation System Plan (TSP) replaces the 2003 TSP in its entirety. In addition, it updates and builds upon the 2006 Bicycle and Pedestrian Master Plan and the 2008 Transit Master Plan. Where these documents may be in conflict, the new TSP takes precedence. However, there are many helpful details provided in the prior plans, which should be used for added clarity and direction.



## TSP CONTENT AND LAYOUT

The sections of these documents are listed in the Table of Contents. Following the Table of Contents, a **glossary of terms** is included to help the reader better understand the terminology used in the report. Then, the **executive summary** provides an overview of the TSP and the key findings of each chapter.

The TSP chapters tell a story of how the City's planning efforts are helping the community achieve its desired transportation system. They explain the planning **context** (Chapter 1), the City's overall **vision** and related goals and policies (Chapter 2), and the **standards** that support progress towards that vision (Chapter 3). The chapters then identify the existing and future transportation **needs** (Chapter 4), the **projects** to resolve infrastructure needs (Chapter 5), and the **programs** that support ongoing management of the transportation system (Chapter 6). Finally, the last chapter lists **performance** measures to help the City determine if its planning efforts are leading to the desired outcomes (Chapter 7).

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**A**

**Access Management** is the use of various techniques to improve traffic flow and safety by reducing conflict points at intersections and driveways while providing reasonable access to individual properties.

**Additional Planned Project List** includes those projects that would contribute to the City's desired transportation system through 2035 but that were not included as "Higher Priority" projects due to estimated funding limitations. This list represents a coordinated transportation network and adequate facilities to serve the community through 2035.

**Alternative Fuels** are transportation energy sources other than gasoline, including batteries (i.e., electric vehicles) and compressed natural gas.

**Americans with Disabilities Act (ADA)** is Federal legislation that seeks to remove and prevent barriers experienced by individuals with disabilities. With regards to transportation, it affects infrastructure design (especially curb ramps and sidewalks) as well as transit serve requirements.

**Arterials** are roadways where a higher priority is placed on moving traffic rather than accessing individual parcels. The City has two arterial functional classifications: Major Arterial and Minor Arterial.

**B**

**Buffered Bike Lanes** are on-street bike facilities that include a striped buffer between the bike lane and motor vehicle travel lane. When on-street parking is provided, the parking is located curbside, with the bike lane remaining adjacent to the motor vehicle travel lane.

**Bicycle Routes** are the designated on- and off-street bicycle facilities that connect neighborhoods, schools, parks, community centers, business districts, and natural resource areas. They are intended to create a

network that supports bicycle travel by residents of varying physical capabilities, ages, and skill levels.

**Bicycle Friendly Community (BFC)** is a campaign administered by the League of American Bicyclists and awards cities one of four designations (from lowest to highest: bronze, silver, gold, and platinum) to recognize its efforts to improve its bicycle facilities.

**C**

**Capital Improvement Program (CIP)** is the City's short-range 5-year plan that identifies upcoming capital projects and equipment purchases, provides a planning schedule, and identifies financing options. It provides an important link between the projects identified in the City's master plans and its annual budget.

**Collectors** are roadways intended to serve as a transition between mobility and access. They are the primary roadways that "collect" traffic from neighborhoods and deliver it to the arterial network.

**Comprehensive Plan** is the City's generalized, coordinated land use map and policy statement, which interrelates all functional and natural systems and activities relating to the use of lands, including sewer and water systems, transportation systems, recreational facilities, natural resources, and air and water quality management programs.

**Connectivity** refers to the ease of movement between the city's neighborhoods, schools, parks, and retail/industrial areas.

**Cycle Tracks** are a relatively new on-street bicycle facility type where additional separation is provided between motor vehicle travel lanes and the bicycle facility. When on-street parking is provided, the parking is located adjacent to the travel lane and the cycle track is moved adjacent to the curb. Cycle tracks can be one-way (similar to a buffered bike lane but

with a physical separation) or two-way (where both directions are served on the same side of the street).

## E

**Enhanced Pedestrian Crossings** are striped crosswalks that include additional crossing treatments, such as traffic signs, center median islands, flashing beacons, and/or other safety enhancements.

**Environmental Justice** is the fair treatment and meaningful involvement of all people regardless of race, color, national origin, or income with respect to the development, implementation, and enforcement of environmental laws, regulations, and policies. (Source: U.S. EPA, *Environmental Justice, Compliance and Enforcement, Website, 2007*).

## F

**Freight Routes** are roads designated by the City to connect the city's industrial and commercial sites with I-5 and other regional facilities. They are a useful tool for improving the coordination between freight and other travel modes.

**Functional Classifications** are designations assigned to public roadways to provide a hierarchy for managing them practically and cost effectively. For example, they provide a framework for identifying which street elements to include in a street's design. Wilsonville's classifications include, Major Arterial, Minor Arterial, Collector, and Local Street.

## H

**Higher Priority Project List** includes the City's recommended projects reasonably expected to be funded through 2035. These are the highest priority solutions to meet the City's most important needs. These projects will inform the City's yearly budget and 5-year Capital Improvement Plan (CIP).

## I

**Ice Age Tonquin Trail** is a partially-completed regional trail located in the southwestern portion of

the Portland metropolitan area that would span approximately 22 miles and travel through the communities of Wilsonville, Sherwood, and Tualatin. This trail would provide an active transportation link between the Willamette and Tualatin Rivers, while enhancing local pedestrian and bicycle connectivity connecting to neighborhoods, businesses, schools, and parks.

**Intelligent Transportation System (ITS)** strategies involve the deployment and management of advanced technologies that collect and distribute information to both users and operator staff so they can most effectively use and manage the transportation system.

**Interchange Area Management Plans (IAMP)** are transportation and land use plans prepared jointly by the Oregon Department of Transportation and local jurisdictions to balance and manage transportation and land use decisions in freeway interchange areas to protect their function while also supporting the local street network.

**Implementation Measures** are City actions identified to put broader policies into action.

## L

**Level of Service (LOS)** is a "report card" rating (A through F) based on the average delay experienced by vehicles at the intersection. LOS A, B, and C indicate conditions where traffic moves without significant delays. LOS D and E are progressively worse, and LOS F represents conditions where average vehicle delay has become excessive and demand has exceeded capacity, which is typically evident in long queues and delays.

**Low Impact Development (LID)** is an approach to development and infrastructure improvements that works with nature to manage stormwater as close to the source as possible (i.e., adjacent to the roadway).

**Local Streets** are roadways where a higher priority is placed on local access rather than mobility. They are usually lower volume, lower speed streets with a narrow cross-section and numerous driveways.

## M

**Metro** is the elected regional government for the Portland metropolitan area and provides region-wide planning, policy making, and coordination to manage growth, infrastructure, and development issues that cross jurisdictional boundaries.

**Multimodal** refers to the integration of multiple travel modes, which include walking, bicycling, riding transit, or driving.

## P

**Parking Management Plans** inventory bicycle and motor vehicle parking supply in high demand locations (for example, park-and-ride lots, transit stations, and commercial areas). They do not require parking limitations but instead ensure that deliberate decisions are being made regarding parking provision and management.

**Performance Measures** are quantitative tools (based on data) or qualitative tools (based on judgment) used to evaluate how effectively the transportation system is operating and/or progressing towards identified performance targets.

**Planning Horizon** is the future year (in this case, 2035) that is the basis of the Transportation System Plan's future needs assessment.

**Policies** are the principles or rules the City has developed to serve as its blueprint for making decisions regarding its transportation investments, including how the system is designed, constructed, operated, and maintained. The City's transportation policies guide actions relative to its development code, capital project investment, and other investments.

## R

**Regional Transportation Functional Plan (RTFP)** codifies the requirements that local plans must comply with to be consistent with the Regional Transportation Plan.

**Regional Transportation Plan (RTP)** is the long-range blueprint to guide transportation planning and investment in the region.

**Roadway Extensions** are new transportation facilities that begin at the termini of existing roads and connect neighborhoods to one another and to other important destinations.

## S

**Safe Routes to School (SRTS)** is a collaborative program between schools and local agencies that combines ongoing educational and outreach efforts with pedestrian and bicycle infrastructure improvements along routes used by school children.

**Shared-Use Paths** are a type of trail designed to be part of the transportation system that provide off-road routes for a variety of users, which principally include bicyclists and pedestrians.

**South Metro Area Regional Transit (SMART)** is a City department that operates several fixed bus routes serving Wilsonville and making connections to regional transit providers. SMART also manages various programs, including Dial-a-Ride (door-to-door service for elderly and disabled residents) and SMART Options (programs that support, educate, and encourage the use of active transportation modes and rideshare).

**Spot Improvements** are isolated intersection and safety improvements throughout the city.

**System Deficiencies** are performance, design, or operational constraints that limit travel by a given mode. Examples may include unsafe designs, bicycle and pedestrian connections that contain obstacles,

*inadequate intersection or roadway capacity, insufficient bus frequency, and congestion.*

**System Development Charges (SDCs)** are a one-time fee charged to new developments based on land use and size. These funds are legally required to be used for capacity-related improvements.

**System Gaps** are missing connections or barriers in the urban transportation system that functionally prohibit travel for a given mode. While a gap generally means a connection does not exist, it could also be the result of a physical barrier (such as I-5, the Willamette River, other natural feature, or existing development) or a social barrier (including lack of information, language, education, and/or limited resources).

## T

**Technical Advisory Committee (TAC)** consisted of agency staff from the City of Wilsonville and other local, regional, and state agencies that provided feedback on the Transportation System Plan deliverables throughout the update process.

**Transportation Demand Management (TDM)** refers to the implementation of strategies that support other travel choices (including other travel modes and travel during off-peak periods) in order to reduce traffic congestion.

**Transportation System Management and Operations (TSMO)** refers to strategies that improve the safety and efficiency of the transportation system in order to optimize the use of existing infrastructure.

**Transportation System Plan (TSP)** is the City's long-term transportation plan that guides the construction and operation of its transportation system. It is an element of its Comprehensive Plan and includes policies, projects, and programs that could be implemented through the City's Capital Improvement Plan, development requirements, or grant funding.

## U

**Urban Growth Boundaries (UGB)** are regional boundaries that restrict where urban growth can occur in order to reduce urban sprawl and protect nearby natural resources.

**Urban Renewal Districts (URD)** are “blighted” areas where private development has stagnated or is not feasible and public funds are needed (and are raised through tax increment financing) to stimulate economic development, usually through the construction of supporting infrastructure.

**Urban Upgrades** are projects that widen existing roadways to meet the City's cross-section standards and often improve multimodal connectivity by adding bike lanes, sidewalks, and turn lanes that accommodate access to adjacent neighborhoods.

## V

**Volume to Capacity Ratio (V/C)** is a decimal representation (typically between 0.00 and 1.00) of the proportion of capacity being used at a turn movement, approach leg, or intersection. A lower ratio indicates smooth operations and minimal delays. As the ratio approaches 1.00, congestion increases and performance is reduced. A ratio greater than 1.00 represents future conditions where demand is estimated to exceed capacity.

## W

**Walk Friendly Communities** is a national recognition program developed to encourage cities across the U.S. to establish or recommit to supporting safer walking environments. It awards cities one of five designations (from lowest to highest: honorable mention, bronze, silver, gold, and platinum).

**Westside Express Service (WES)** is a commuter rail line serving Beaverton, Tigard, Tualatin, and Wilsonville that runs during the weekday morning and afternoon rush hours and provides service to Wilsonville's SMART Central transit center.

# Executive Summary



## INTRODUCTION

The Wilsonville Transportation System Plan (TSP) is the City's long-term transportation plan and is an element of its Comprehensive Plan. It includes policies, projects, and programs that could be implemented through the City's Capital Improvement Plan, development requirements, or grant funding. The TSP's transportation planning story is outlined in the box at right, and the key findings of each TSP chapter are highlighted below.

## THE CONTEXT (SEE CHAPTER 1)

The 2013 TSP process built upon two decades of community planning to create a complete community transportation plan that integrates all travel modes. This update is needed to account for changing economic and social circumstances and to ensure consistency with state and regional planning policies. It also ensures the City will be prepared to support land use growth within the urban growth boundary through the 2035 planning horizon.

Most of the policies and projects come from prior adopted plans, including the Comprehensive Plan, 2003 TSP, 2006 Bicycle and Pedestrian Master Plan, and 2008 Transit Master Plan. While the TSP replaces the 2003 TSP in its entirety, it updates and builds upon the 2006 Bicycle and Pedestrian Master Plan and 2008 Transit Master Plan. Where these documents may be in conflict, the new TSP takes precedence.

The City's future financial outlook was also evaluated to identify the City's forecasted resources and financial limitations. The City draws upon multiple funding sources to manage, operate, and improve its transportation system. For capital improvement projects, the City relies heavily on developer contributions and fees (including system development charges) and urban

## A TRANSPORTATION PLANNING STORY

The TSP chapters tell a story of how the City's planning efforts are helping the community achieve its desired transportation system:

- **Chapter 1: The Context** provides the background of the City's transportation planning efforts.
- **Chapter 2: The Vision** shares the City's visions of its desired transportation system.
- **Chapter 3: The Standards** outlines the standards the City is implementing to ensure ongoing progress towards its vision.
- **Chapter 4: The Needs** identifies the existing and anticipated needs of the transportation system through the 2035 planning horizon.
- **Chapter 5: The Projects** explains the transportation improvement projects that will allow the City to meet its infrastructure needs.
- **Chapter 6: The Programs** describes the ongoing transportation programs that help the City manage its transportation system.
- **Chapter 7: The Performance** lists the performance measures to be considered in subsequent TSP updates to determine if its planning efforts are leading to the desired outcomes.

renewal funds, which are primarily associated with new growth areas. With ongoing planning and investment in its transportation system, the City can continue to serve its residents, businesses, and the region.

## THE VISION (SEE CHAPTER 2)

As Wilsonville grows, it is essential for the community to work collaboratively toward its shared vision, which is summarized in the call-out box at right.

Transportation goals and policies form the bases for how the local transportation system will be developed and maintained through the TSP's 2035 horizon year. Wilsonville's seven transportation goals are identified in the table below. The City's vision and goals support a multimodal approach to transportation, which means that the system accommodates users of all travel modes.

### **WILSONVILLE'S TRANSPORTATION VISION**

*Wilsonville's coordinated multimodal transportation system is strategically designed and collaboratively built. Our system provides mode and route choices, delivering safe and convenient local accessibility to assure that Wilsonville retains its high levels of quality of life and economic health. Neighborhoods, employment centers, schools, shopping, and parks are connected by a network of streets and pathways that give residents options to easily get around town.*

*Our local accessibility is further enhanced through arterial connectivity with our neighboring communities, thereby providing excellent intercity and interstate mobility serving our residential and business needs. The system is designed, built and maintained to be cost effective and to maximize the*

## Wilsonville's Transportation Goals

Goals	Description
<b>1 Safe</b>	Follow current safety practices for design, operations, and maintenance of transportation facilities.
<b>2 Connected and Accessible</b>	Provide all users with access to integrated facilities and services that connect Wilsonville's neighborhoods, parks, schools, employment centers, and retail areas to each other and to the surrounding region.
<b>3 Functional and Reliable</b>	Provide, manage, and maintain sufficient transportation infrastructure and services throughout Wilsonville to ensure functional and reliable multimodal and freight operations as development occurs.
<b>4 Cost Effective</b>	Utilize diverse and stable funding sources to implement transportation solutions that provide the greatest benefit to Wilsonville residents and businesses, while mitigating impacts to the city's social, economic, and environmental resources.
<b>5 Compatible</b>	Develop and manage a transportation system that is consistent with the City's Comprehensive Plan and coordinates with other local, regional, and state jurisdictions.
<b>6 Robust</b>	Encourage and support the availability of a variety of transportation choices for moving people and goods.
<b>7 Promotes Livability</b>	Design and construct transportation facilities in a manner that enhances the livability of Wilsonville and health of its residents.

## THE STANDARDS (SEE CHAPTER 3)

Wilsonville’s transportation standards ensure the City develops and operates consistent with its goals and vision. Wilsonville’s six types of transportation standards are listed in the call-out box at right.

How well a street serves its users ultimately depends upon which elements are included, their dimensions, and how they relate to each other (all of which are informed by the City’s standards). For example, streets designed consistent with adjacent land uses can contribute to the identity and character of a neighborhood and increase property values. They can also affect traffic speeds, reduce environmental impacts, and allow for safe multimodal use.

## THE NEEDS (SEE CHAPTER 4)

Wilsonville’s transportation standards and policies serve as a benchmark for determining what needs exist throughout the city. The city’s needs are categorized as gaps (missing connections or barriers in the transportation network) or deficiencies (shortcomings of the existing system). The TSP identifies the gaps and deficiencies that currently exist or are anticipated to arise through the 2035 horizon year as additional local and regional development occurs.

## THE PROJECTS (SEE CHAPTER 5)

Many of the city’s existing and future transportation needs can be addressed through capital improvement projects. The projects needed through 2035 were principally based on prior City plans.

Constructing all identified transportation projects would cost approximately \$218.2 million, which exceeds the \$123.4 million forecasted to be available through 2035. Therefore, the transportation projects were separated into two lists:

- The “Higher Priority” project list includes the recommended projects reasonably expected to be funded through 2035. These are the highest priority projects and will inform the City’s yearly

## WILSONVILLE’S TRANSPORTATION STANDARDS

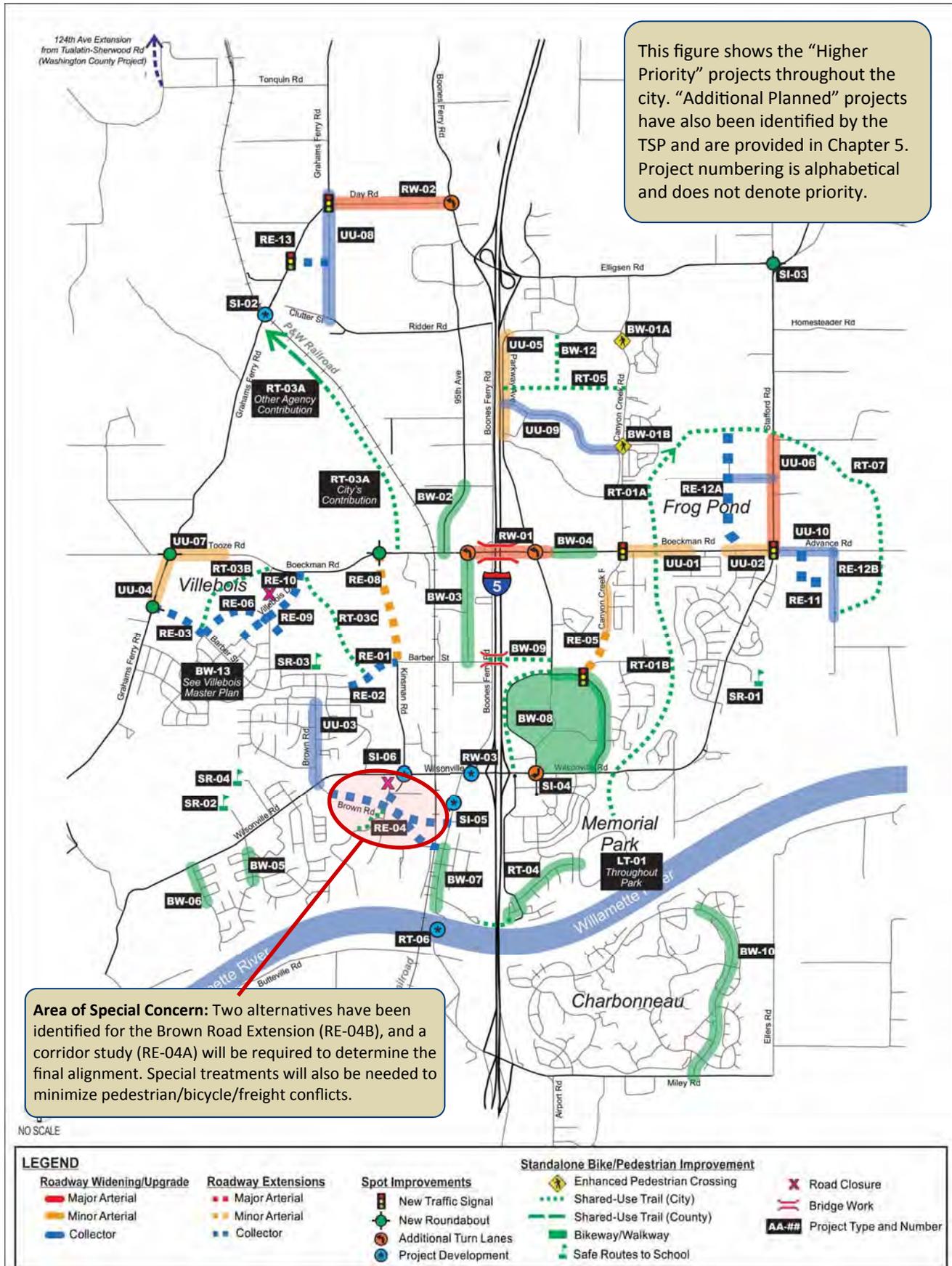
Wilsonville’s six types of transportation standards support its management of an effective multimodal transportation system:

- **Functional Classifications** provide a hierarchy for determining how streets should function and which street design elements to include.
- **Connectivity and Facility Spacing Standards** ensure that direct routes and travel options are available for all transportation users.
- **Freight Routes** connect the city’s industrial and commercial sites with I-5 and other regional facilities and improve coordination between freight and other travel modes.
- **Bicycle Routes** connect neighborhoods, schools, parks, community centers, business districts, and natural resource areas to support bicycle travel by residents of varying physical capabilities, ages, and skill levels.
- **Cross-Section Standards** provide guidance for selecting and sizing various design elements to serve intended users’ needs.
- **Access Management** balances the transportation system’s need to provide safe, efficient, and timely travel with the need to allow access to individual properties.

budget and 5-year Capital Improvement Plan (CIP). These projects are identified in the following figure (page v) and table (page vi).

- The “Additional Planned” project list includes those projects that would contribute to the City’s desired transportation system through 2035 but that are not considered “Higher Priority” projects due to estimated funding limitations. These projects are identified in Chapter 5 and should be pursued as funding opportunities are available.

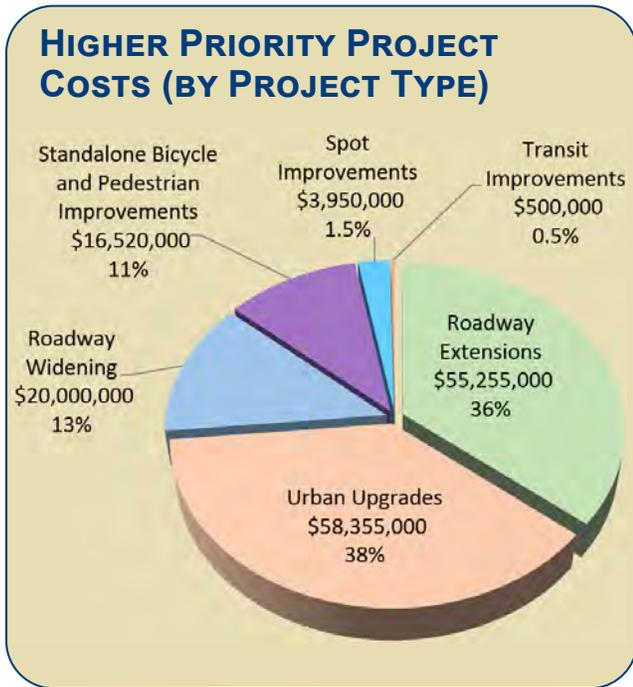
HIGHER PRIORITY PROJECTS



## HIGHER PRIORITY PROJECTS (LISTED ALPHABETICALLY BY IMPROVEMENT)

No.	Higher Priority Project	No.	Higher Priority Project
<b>Roadway Extensions (Multimodal Connectivity)</b>		<b>Bikeways and Walkways (Standalone Pedestrian and Bicycle Improvements) . . . Continued</b>	
RE-01	Barber Street Extension	BW-03	Boberg Road Sidewalk Infill
RE-02	Barber Street Extension (Part 2)	BW-04	Boeckman Road Bike Lanes and Sidewalk Infill
RE-03	Barber Street through Villebois	BW-05	Willamette Way East Sidewalk Infill
RE-04A	Corridor Study for Brown Road Extension	BW-06	Willamette Way West Sidewalk Infill
RE-04B	Brown Road Extension (with Bailey Street or 5th Street Connection)	BW-07	Boones Ferry Road Sharrows
RE-05	Canyon Creek Road Extension	BW-08	Town Center Loop Pedestrian, Bicycle, and Transit Improvements
RE-06	Costa Circle Loop Extension	BW-09	Town Center Loop Bike/Pedestrian Bridge
RE-08	Kinsman Road Extension (South)	BW-10	French Prairie Drive Pathway
RE-09	Villebois Drive Extension	BW-12	Parkway Center Trail Connector
RE-10	Villebois Drive Extension (Part 2)	BW-13	Villebois Loop Trail
RE-11	Meridian Creek Middle School Improvements	BW-14	Wayfinding Signage
RE-12A	Frog Pond West Neighborhood Collector Roads	BW-15	Property Acquisition for Bike/Ped Connectivity
RE-12B	Frog Pond South Neighborhood Collector Road	<b>Safe Routes to School (Standalone Pedestrian and Bicycle Improvements)</b>	
RE-13	Java Road Connection and Signal	SR-01	Boeckman Creek Primary Safe Routes to School Improvements
<b>Roadway Widening (Capacity)</b>		SR-02	Boones Ferry Primary Safe Routes to School Improvements
RW-01	Boeckman Road Bridge and Corridor Improvements	SR-03	Lowrie Primary Safe Routes to School Improvements
RW-02	Day Road Widening	SR-04	Wood Middle School Safe Routes to School Improvements
RW-03	Widen Wilsonville Road East of Boones Ferry Road	<b>Local Trails (Standalone Pedestrian and Bicycle Improvements)</b>	
<b>Urban Upgrades (Multimodal Connectivity and Safety)</b>		LT-01	Memorial Park Trail Improvements
UU-01	Boeckman Road Dip Improvements	<b>Regional Trails (Standalone Pedestrian and Bicycle Improvements Safety)</b>	
UU-02	Boeckman Road Urban Upgrade	RT-01A	Boeckman Creek Trail (North)
UU-03	Brown Road Upgrades	RT-01B	Boeckman Creek Trail (South)
UU-04	Grahams Ferry Urban Upgrade	RT-03A	Tonquin Trail (North)
UU-05	Parkway Avenue Urban Upgrade	RT-03B/C	Tonquin Trail (Villebois)
UU-06	Stafford Road Urban Upgrade	RT-04	Waterfront Trail Improvements
UU-07	Tooze Road Urban Upgrade	RT-05	Wiedeman Road Trail
UU-08	Garden Acres Road Urban Upgrade	RT-06	Willamette River Bike/Pedestrian/Emergency Bridge Project Dev.
UU-09	Printer Parkway Urban Upgrade	RT-07	Revised Frog Pond Trail
UU-10	Advance Road Urban Upgrade	<b>Transit Improvements</b>	
<b>Spot Improvements (Transportation System Management/Operations)</b>		TI-01	Pedestrian Access to Transit
SI-02	Grahams Ferry Railroad Undercrossing Project Development	TI-02	Transit Street Improvements
SI-03	Stafford Road/65th Avenue Intersection Improvements		
SI-04	Wilsonville Rd/Town Center Loop West Intersection Improvements		
SI-05	Curb Extension Removal on Boones Ferry Road		
SI-06	Truck Turning Movements at SW Kinsman Road		
<b>Bikeways and Walkways (Standalone Pedestrian and Bicycle Improvements)</b>			
BW-01 A/B	Canyon Creek Road Enhanced Pedestrian Crossings		
BW-02	95th Avenue Sidewalk Infill		

Wilsonville’s “Higher Priority” project list includes several project types. The pie chart below provides the cost breakdown by project type. The highest costs would be incurred for the three roadway improvement types, which include facility improvements for all travel modes.



**Estimated Funding Available through 2035 for Capital Improvements**

Funding Source	Estimated Capital Funding through 2035
Street System Development Charges (SDCs)	\$42 million
Developer Contributions	\$30 million
West Side Plan – Urban Renewal District (URD)	\$27 million
Year 2000 Plan – Urban Renewal District (URD)	\$5 million
Park System Development Charges (SDCs)	\$0.7 million
Local/Regional Partnerships	\$2.9 million
Grants	\$3.2 million
State and Federal Funding	\$12.6 million
<b>Total Funds</b>	<b>\$123.4 million</b>

To fund its capital improvements projects, the City relies heavily on developer contributions and fees (including system development charges) and urban renewal funds, which are primarily associated with new growth areas. The table to the lower left lists the estimated funding available for capital improvements through the 2035 planning horizon year.

**THE PROGRAMS (SEE CHAPTER 6)**

Wilsonville’s transportation programs (listed below) also play an important role in the City’s ongoing efforts to provide a coordinated, cost-effective, multimodal transportation system. Well-run programs help extend the service life of the City’s infrastructure improvements and increase the value of transportation investments. The City’s Community Development and SMART Transit departments are responsible for managing the majority of its transportation programs.

**TRANSPORTATION PROGRAMS**

Wilsonville has various transportation programs that support ongoing operations and services:

- Capital Improvement Program (CIP)
- Safety (Proposed)
- Safe Routes to School
- ADA Comprehensive Access (Proposed)
- SMART Transit
- SMART Options and Transportation Demand Management (TDM)
- Intelligent Transportation System (ITS)
- Bike Smart and Walk Smart

**THE PERFORMANCE (SEE CHAPTER 7)**

Wilsonville’s Transportation System Plan (TSP) provides policies, standards, projects, and programs that, when put into action, will improve the city’s transportation system. By tracking appropriate performance measures in future TSP updates, the City can evaluate their progress.

# The Context

## Chapter 1



Wilsonville has a rich history as an important transportation connection between the north and south areas of the Willamette Valley. With ongoing planning and investment in its transportation system, the City can continue to serve its residents, businesses, and the region.

Prior to the arrival of non-indigenous settlers, the Willamette River served as a water route for Kalapuyan people. As settlers moved into the area in the early 1800's, the need arose for a way to cross the river. In 1847, Alphonso Boone, grandson of Daniel Boone, established Boones Ferry (located near the present day Boones Ferry Park) and an early settlement began providing needed support to the ferry.

Over time, steamboats, the railroad, and then Interstate-5 came to town—and Wilsonville continued to grow. In 1969, Wilsonville became a city. Shortly afterwards, the City began preparing planning documents to guide its development. As economic and social circumstances change and new state and regional planning policies are adopted, the City continues to improve and refine its planning efforts. In doing so, it takes a strategic approach to growth management.

By understanding the context surrounding its growth, the community can continue to build upon its rich history. The following pages provide a timeline of important events associated with Wilsonville's transportation planning history, current planning framework, and future growth. The City's future financial outlook is also provided to better frame the City's forecasted resources and challenges.

*By understanding its . . .*

- *Unique history,*
- *Current planning framework,*
- *Future growth areas, and*
- *Financial outlook,*

*Wilsonville can continue to . . .*

- *Manage growth,*
- *Serve its residents and business, and*
- *Be an important transportation connection for the region.*



## TRANSPORTATION PLANNING HISTORY IN WILSONVILLE

### Early 1800's

Wilsonville area (traditional territory of the Kalapuyan people) was settled by people other than the indigenous Native Americans.



**Early 1900's**  
Steamboats were used as the primary mode of shipping.



### Pre-1960's

Before the construction of Interstate-5 and the Boone Bridge, personal automobiles had to be ferried across the Willamette River.

Early 1800's

1900

1910

1920

1930

1940

1950



**1847**

Alphonso Boone, grandson of Daniel Boone, established Boone's Ferry across the Willamette River.

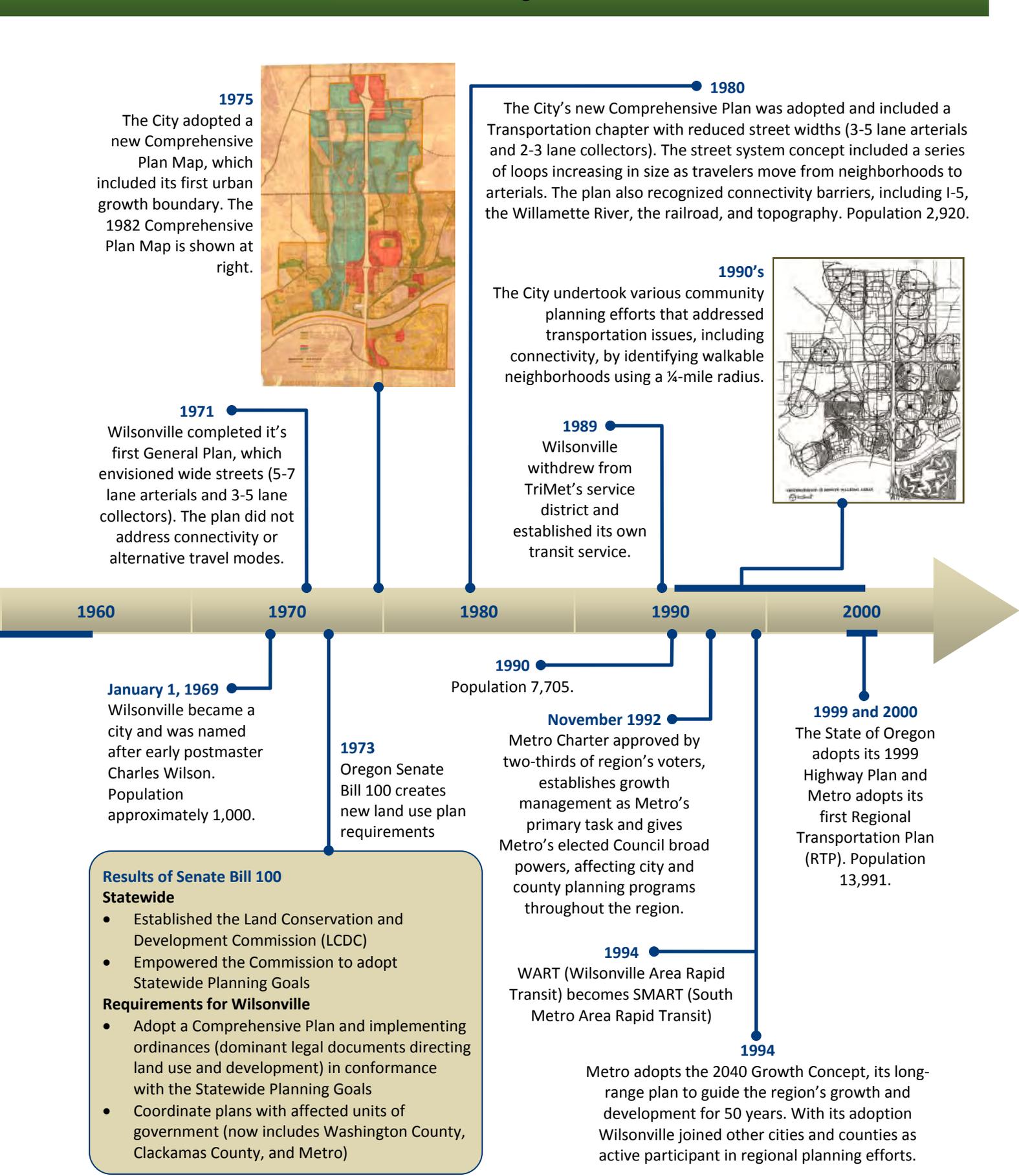
**1908**  
Railroad comes to the area.



**1950's**

The Interstate-5 freeway system was built.





**Results of Senate Bill 100**

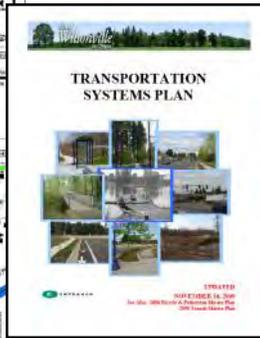
**Statewide**

- Established the Land Conservation and Development Commission (LCDC)
- Empowered the Commission to adopt Statewide Planning Goals

**Requirements for Wilsonville**

- Adopt a Comprehensive Plan and implementing ordinances (dominant legal documents directing land use and development) in conformance with the Statewide Planning Goals
- Coordinate plans with affected units of government (now includes Washington County, Clackamas County, and Metro)

## CURRENT TRANSPORTATION PLANNING FRAMEWORK



### 2003 Transportation Systems Plan (TSP)

The City replaced the transportation chapter of its Comprehensive Plan to comply with state mandates, develop transportation standards, address problem areas, revise forecasts (2020 horizon year), and provide transportation planning guidelines for all travel modes.

### 2006 Bicycle and Pedestrian Master Plan

The City replaced the bicycle and pedestrian chapters of the 2003 TSP with new prioritized project lists providing community and regional connectivity between parks, neighborhoods, schools, and commercial and industrial areas.



### 2001 Villebois Village Master Plan

A Master Plan was prepared to guide the development of a 480-acre area on the west side of the city into an urban village based on the guiding principles of connectivity, diversity, and sustainability.

### 2006 Public Works Standards

Standards were provided for constructing public facilities, including streets, trails, and related infrastructure.

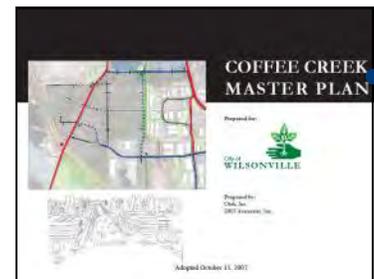
### 2007 Coffee Creek Master Plan

A Master Plan was prepared to guide development of 220-acre area on north side of city into industrial area.



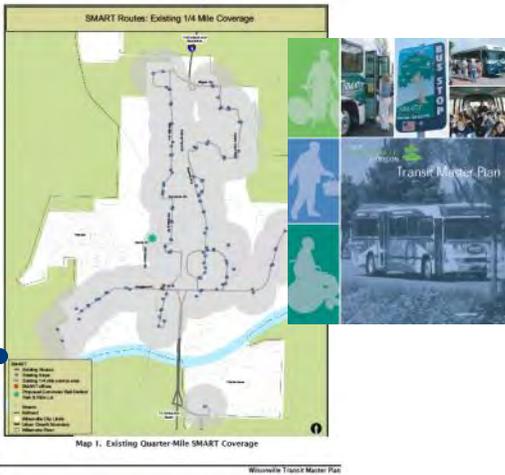
### 2007 Parks and Recreation Master Plan

The City prepared a plan for achieving a comprehensive and interrelated system of parks, recreation, and natural areas that promote connectivity throughout the city and support the 2006 Bicycle and Pedestrian Master Plan.

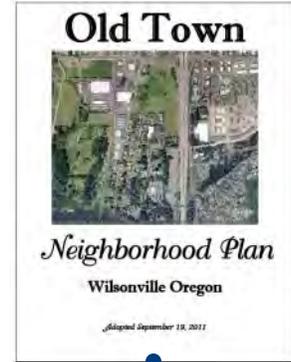


**2008 Transit Master Plan**

The City replaced the transit element of the 2003 TSP with new recommendations to increase and improve transit service and reduce the demand on roads and parking.



**2011 Old Town Neighborhood Plan**  
A plan was prepared to ensure Old Town's unique character is maintained and enhanced.



**2009 Wilsonville Road Interchange Area Management Plan (IAMP)**

A plan was prepared to identify how the City and ODOT will collaborate to improve the I-5 exit (#283) to serve planned growth. Population 17,940.

**2009**  
TriMet begins operating its Westside Express Service (WES) commuter rail line, which has its southern terminus at Wilsonville's transit center.

2008

2009

2010

2011

2012

**2010 Regional Transportation Plan (RTP) and Regional Transportation Functional Plan (RTFP)**

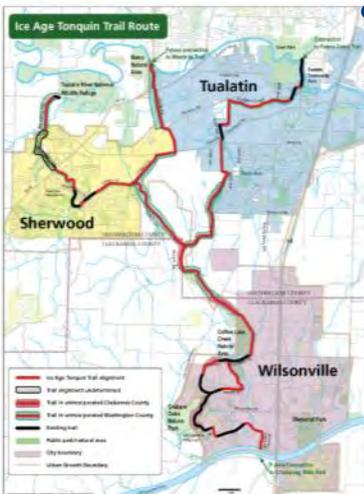
Plans were prepared to provide a long-range blueprint for all modes of transportation throughout Portland region and support Metro's 2040 Growth Concept. The plans identified improvements focused on mobility corridors (e.g., Tigard/Wilsonville) and required compliance by local jurisdictions.

**2012 Stormwater Master Plan**

The City prepares a stormwater program that supports quality of life and meets regulatory requirements. The plan also includes resources for improved street cross-sections.

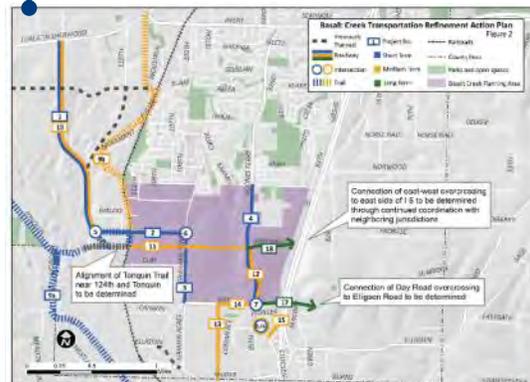
**2012 Basalt Creek Transportation Refinement Plan**

A plan was prepared to refine the major transportation improvements connecting I-5 to Tualatin-Sherwood Road through the unincorporated area to the north to support future development of the Basalt Creek area.

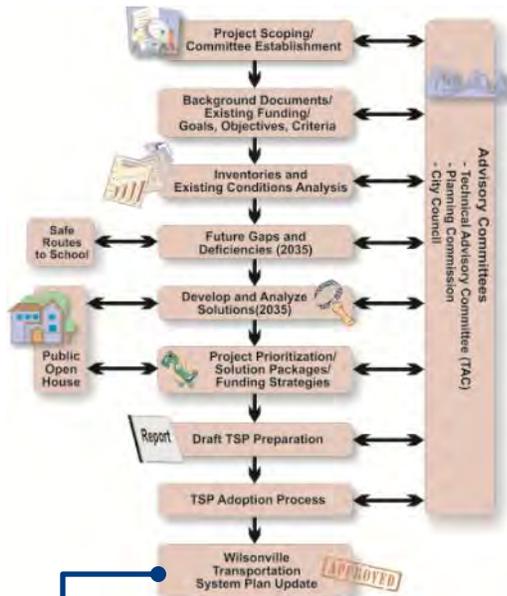


**2012 Ice Age Tonquin Trail Master Plan**

A plan was prepared to provide information needed to complete and connect 22 miles of trails within and between the cities of Wilsonville, Tualatin, and Sherwood. Approximately half of the 5 miles within Wilsonville City limits have already been completed.



## FUTURE TRANSPORTATION GROWTH AND PLANNING NEEDS



### 2013 Transportation System Plan (TSP) Update

The updated TSP envisions transportation improvements needed through 2035 horizon year for all travel modes based on revised traffic forecasts and integration of prior transportation planning efforts. It updates the transportation improvement project list, standards, programs, and performance measures to support City efforts to accomplish its vision and goals and to comply with new Regional Transportation Plan requirements.

### Next Transportation System Plan (TSP) Update (In Approximately 5-10 Years)

In the future, the City will update its TSP to respond to transportation, land use, environmental, population growth, economic, and social changes. Updates may also be triggered by regulatory changes at the state, regional, and local levels, including changes in Metro’s Regional Transportation Plan (RTP).

### 2012-2015 Climate Smart Communities Scenario Project

Wilsonville is participating with Metro and the surrounding jurisdictions in developing local strategies for reducing the region’s greenhouse gas emissions. The project will help Wilsonville define specific goals that it can work towards to reduce pollution, create a healthy and equitable community, and nurture the economy.

### 2035 Land Use Growth Assumptions

To ensure the City is prepared for local and regional growth, a 2035 horizon year was the basis of the 2012/2013 TSP update. The 2035 land use projections were based on the build-out of all vacant and underdeveloped lands within the Urban Growth Boundary (UGB) assuming Comprehensive Plan designations.

**Table 1-1. Wilsonville Growth Forecasts**

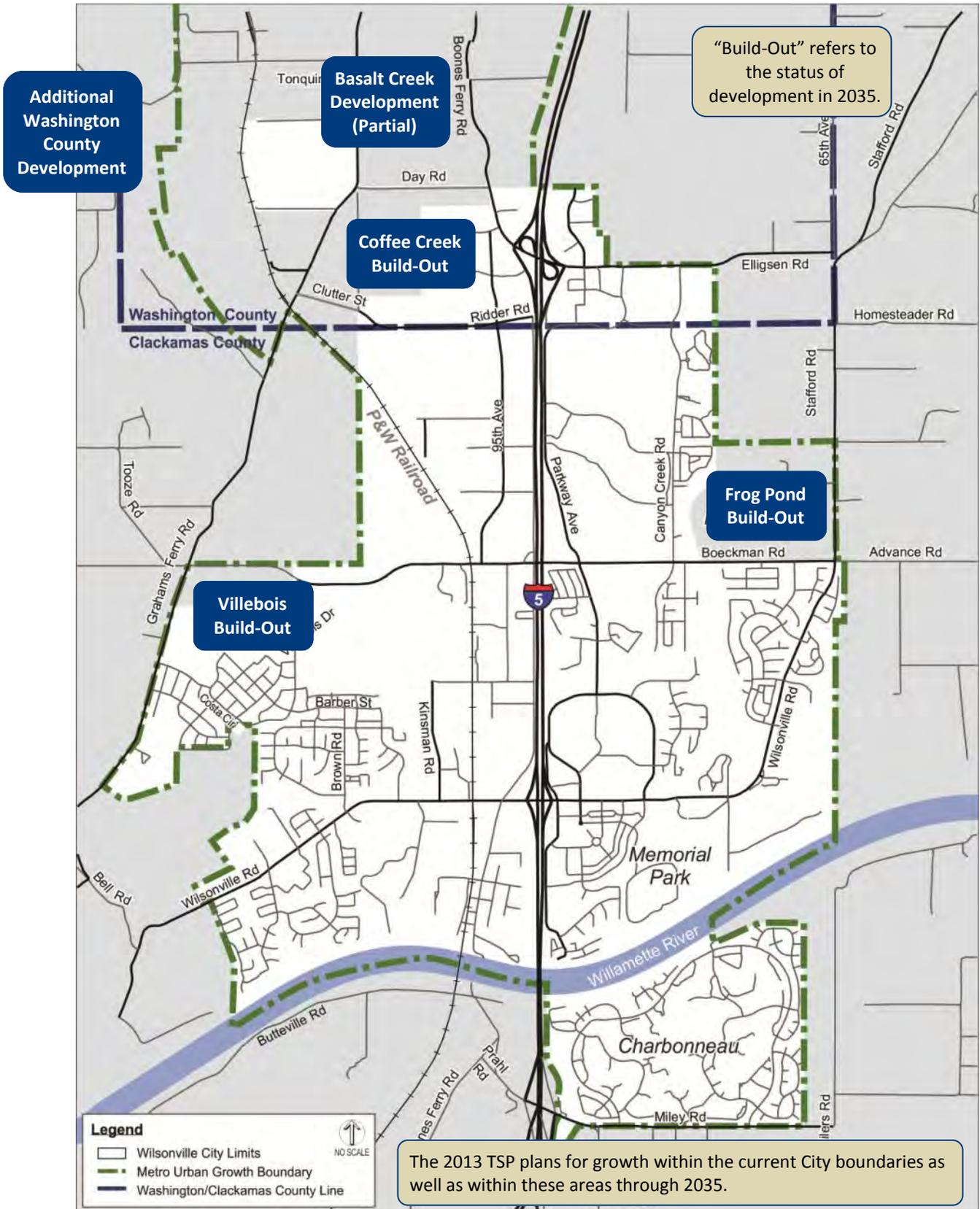
Land Use	Existing 2010 Land Use	Projected 2035 Land Use*
<b>Total Households</b>	<b>8,250</b>	<b>12,750</b>
<i>Employees</i>		
Retail Employees	2,500	3,600
Service Employees	4,900	9,200
Other Employees	11,000	19,050
<b>Total Employees</b>	<b>18,400</b>	<b>31,850</b>

\*Note: 2035 land use estimates consistent with Metro forecasts

**Wilsonville Growth From 2010 to 2035**

- 50% More Households
- 75% More Employees

FIGURE 1-1. 2035 GROWTH AREAS



### FUNDING OUTLOOK

The City draws from multiple funding sources to pay for the construction, operation, and maintenance of its transportation infrastructure and services. Table 1-2 lists the sources, how they are used, and what estimated amounts would be available.

Approximately \$104 million is estimated to be available from City sources to fund transportation-related capital improvement projects through 2035. Additional contributions are expected to be available from regional, state, and federal sources to partially fund the City projects included in the Regional

Transportation Plan (RTP). Corresponding estimates are provided in Chapter 5 for specific projects. Detailed discussion of funding sources and the City’s funding outlook by transportation expenditure are provided in the *Existing Funding* memorandum included in the Appendix.

Because the available funds will be insufficient for the City to construct all of its transportation projects (expected to cost at least \$170 million), Wilsonville must choose how to invest its available funding to best meet its needs through the year 2035.

**Table 1-2. Estimated City Funding Available through 2035 for Capital Improvements**

City Funding Source	Use	Estimated Capital Improvement Funding through 2035 <sup>a</sup>
Street System Development Charges (SDCs)	Capital improvement projects that increase transportation system capacity	\$42 million
Developer Contributions	Exactions related to development impacts, on-site facilities, and half-street frontage improvements	\$30 million
West Side Plan – Urban Renewal District (URD)	Improvements made to reduce blight and attract development within the West Side Plan URD	\$27 million
Year 2000 Plan – Urban Renewal District (URD)	Improvements made to reduce blight and attract development within the Year 2000 Plan URD	\$5 million
Park System Development Charges (SDCs)	Bicycle and pedestrian projects between and through the City parks and the off-street trail system	\$0.7 million
Road Maintenance Regulatory Fund <sup>b</sup>	Major street repairs and reconstruction (including slurry seals and overlays)	None (for maintenance only)
Road Operating Fund <sup>b</sup>	Roadway operations and minor repairs (including signal lights, striping, curbs, gutters, and potholes)	None (focused on operations)
Street Lighting Fund <sup>b</sup>	Ongoing street light maintenance, operations, and infill	None (for ongoing costs)
Transit Fund <sup>b</sup>	Transit operations and programs	None (for operations and maintenance)
Community Development Fund <sup>b</sup>	Planning, engineering, and other administration (e.g., City staff and supply costs)	None (for administration)
<b>Total City Funds</b>		<b>\$104.7 million</b>

<sup>a</sup> Estimated funding amounts are planning-level approximations based on review of past ten years of City projects and budget estimates. They assume current fee structures remain in place through 2035 as all vacant land within the City’s urban growth boundary (UGB) is developed. They also assume current urban renewal plans.

<sup>b</sup> Because roadway operations and maintenance are expected to be covered by related funds, no contributions from these funds are assumed to be available for capital improvements.

# The Vision

## Chapter 2



As Wilsonville grows, it will be essential for the community to work collaboratively toward a shared vision. Understanding the goals, and specific steps to achieve them, is the best and most cost-effective way to create a beautiful, functional transportation system.

To guide Wilsonville's transportation planning and investment decisions, the community has developed a new vision statement, transportation goals, policies, and implementation measures.

### ***WILSONVILLE'S TRANSPORTATION VISION***

*Wilsonville's coordinated multimodal transportation system is strategically designed and collaboratively built. Our system provides mode and route choices, delivering safe and convenient local accessibility to assure that Wilsonville retains its high levels of quality of life and economic health. Neighborhoods, employment centers, schools, shopping, and parks are connected by a network of streets and pathways that give residents options to easily get around town.*

*Our local accessibility is further enhanced through arterial connectivity with our neighboring communities, thereby providing excellent intercity and interstate mobility serving our residential and business needs. The system is designed, built and maintained to be cost effective and to maximize the efficient utilization of public and private funding.*

*Wilsonville envisions a transportation system that is . . .*

- *Strategically designed,*
- *Collaboratively built,*
- *Safe,*
- *Convenient, and*
- *Cost effective.*

*The result will be . . .*

- *Mode and route choices,*
- *Quality of life,*
- *Economic health,*
- *Neighborhood connectivity, and*
- *Mobility.*



### TRANSPORTATION GOALS

The City of Wilsonville is responsible for managing a transportation system that efficiently and effectively transports people and goods within the city. This system should support the quality of life of residents and the economic vitality of businesses.

The City can best fulfill its responsibilities by working collaboratively with local and regional partners in developing a transportation system that achieves its seven goals, listed in Table 2-1.



*Wilsonville Road’s landscaping and streetscape provides an attractive environment for all users.*

**Table 2-1. Wilsonville’s Transportation Goals**

Goals	Description
<b>1 Safe</b>	Follow current safety practices for design, operations, and maintenance of transportation facilities.
<b>2 Connected and Accessible</b>	Provide all users with access to integrated facilities and services that connect Wilsonville’s neighborhoods, parks, schools, employment centers, and retail areas to each other and to the surrounding region.
<b>3 Functional and Reliable</b>	Provide, manage, and maintain sufficient transportation infrastructure and services throughout Wilsonville to ensure functional and reliable multimodal and freight operations as development occurs.
<b>4 Cost Effective</b>	Utilize diverse and stable funding sources to implement transportation solutions that provide the greatest benefit to Wilsonville residents and businesses, while mitigating impacts to the city’s social, economic, and environmental resources.
<b>5 Compatible</b>	Develop and manage a transportation system that is consistent with the City’s Comprehensive Plan and coordinates with other local, regional, and state jurisdictions.
<b>6 Robust</b>	Encourage and support the availability of a variety of transportation choices for moving people and goods.
<b>7 Promotes Livability</b>	Design and construct transportation facilities in a manner that enhances the livability of Wilsonville and health of its residents.

## POLICIES AND IMPLEMENTATION MEASURES

Wilsonville's transportation policies serve as a blueprint for the City's investment in its transportation system. These policies cover a variety of areas, including how the system is designed, constructed, operated, and maintained.

The following policies all support the seven Transportation Goals. Each of the policy statements are supported by implementation measures that will guide City actions related to the development code, capital project investment, and other investments.

### System Design

Policy 1. Provide a safe, well-connected, and efficient system of streets and supporting infrastructure for all travel modes.

### POLICY AREAS

- **System Design** (Policies 1-9)
- **Connectivity** (Policy 10)
- **Transportation System Management** (Policies 11-14)
- **Land Development Coordination** (Policies 15-16)
- **Agency Coordination** (Policies 17-21)
- **Goods Movement** (Policies 22-28)
- **Public Transit** (Policies 29-36)
- **Active Transportation: Pedestrians and Bicyclists** (Policies 37-42)
- **Interchange Management Areas** (Policy 43)
- **Transportation Funding** (Policies 44-46)

## RELATIONSHIP OF POLICIES AND IMPLEMENTATION MEASURES

The City's policies support its seven Transportation Goals. Each policy statement may be supported by several implementation measures that will guide City actions relative to the development code, capital project investment, and other investments. Specific implementation measures, requirements, or standards will be included either in the TSP, the Development Code, Public Works Standards, or other implementing documents.

### Implementation Measure (Policy 1):

- 1.a. *Create a comprehensive signage and wayfinding system to assist all modes of transportation with navigating around the community.*

Policy 2. Develop and maintain a transportation system that balances land use and transportation needs in a manner that enhances the livability and economic vitality of the city.

### Implementation Measures (Policy 2):

- 2.a. *Establish and maintain design standards for each arterial and collector street, in accordance with the Functional Street Classification System.*
- 2.b. *Refine the conceptual location of proposed new major streets identified in the TSP based on detailed engineering specifications, design considerations, and consideration of local impacts.*
- 2.c. *Evaluate the alignment and design of local streets on a project-by-project basis in coordination with the overall purposes of the TSP.*
- 2.d. *Dedicate all arterial and collector streets as public streets.*

Policy 3. Support the use of alternative fuels by providing, or encouraging the provision of, needed infrastructure.

**Implementation Measure (Policy 3):**

3.a. *Facilitate private sector exploration of alternative fuel technologies, including shared use of compressed natural gas fueling stations, and electric vehicle charging stations.*

Policy 4. Provide a robust transportation system that provides all members of the community access to multiple travel mode choices.

**Implementation Measures (Policy 4):**

4.a. *Provide pedestrian and bicycle connections between residential neighborhoods and major commercial, industrial, and recreational activity centers throughout the city, as shown in the Bicycle and Pedestrian Master Plan. Coordinate the system of pathways planned by adjacent jurisdictions to allow for regional travel.*

4.b. *Fill gaps in the existing sidewalk and off-street pathway systems to create a continuous network of safe and accessible bicycle and pedestrian facilities.*

Policy 5. Design and manage the city street system to meet Level of Service (LOS) standard D. As may be approved by the City Council, possible exceptions to the LOS D standard are a change to LOS E on Boones Ferry Road and/or Elligsen Road, and on Wilsonville Road between and including the intersections with Boones Ferry Road and Town Center Loop West. Other capacity improvements intended to allow continued development without exceeding LOS E may also be approved by the City Council.

Policy 6. Evaluate, minimize, and balance the environmental impacts of new transportation projects.

Policy 7. Design the transportation system to be multifunctional by integrating stormwater management into the design of transportation facilities, as described in the Stormwater Master Plan.

Policy 8. Consider the needs of traditionally underserved citizens when planning and designing the transportation system, and identify targets and improvements to meet the specific needs of these populations.

Policy 9. Enhance transportation connections and choices in and between all parts of the city as a means for preserving the function and capacity of the existing system.



*The recent Fred Meyer near the I-5/Wilsonville Road Interchange provides two electric vehicle charging stations for patrons to use for free to charge their vehicles while shopping.*

## Connectivity

Policy 10. Add system connections for all modes throughout the city's transportation system to improve access between neighborhoods, serve new development, and manage system performance.

### **Implementation Measures (Policy 10):**

- 10.a. *Promote the concept of a "walkable neighborhood" when advising developers and other agencies to ensure that logical connections are made to activity centers (e.g., schools, retail, and parks), and that such destinations can be reached on foot or by bicycle.*
- 10.b. *Where street connections are not possible, provide bicycle and pedestrian linkages to connect neighborhoods with each other and with surrounding destinations, except if prevented by physical barriers.*
- 10.c. *Where streets lack pedestrian and bicycle facilities, explore opportunities to fill these gaps.*



*A meandering sidewalk along Barber Street adjacent to the SMART Central at Wilsonville Station transit center supports connectivity by providing a safe and comfortable pedestrian environment with connections to transit.*

## Transportation System Management

Policy 11. Manage the transportation system to improve reliability and maximize efficient use of existing facilities.

### **Implementation Measures (Policy 11):**

- 11.a. *Continue to implement Transportation Demand Management measures through South Metro Area Regional Transit's SMART Options Program.*
  - 11.b. *Manage access to improve safety and mobility in the city by applying access spacing standards, limiting access on arterials and at key identified intersections, and by preparing access management plans for interchanges.*
- Policy 12. Implement Intelligent Transportation System (ITS) improvements as identified in the Clackamas County ITS Plan.
- Policy 13. Coordinate with Clackamas County, Washington County, and the Oregon Department of Transportation to implement system management and operations strategies on arterials and highways.
- Policy 14. On- and off-street parking facilities are part of the transportation system, and will be managed and regulated to ensure sufficient parking is provided, maximize efficiency, minimize impacts to traffic in the right-of-way, and reduce environmental impacts. Over time as new development is planned in the Town Center area and the Westside Express Service (WES) commuter rail station area, the City will work with property owners to prepare parking management plans that manage supply and demand for parking areas.

**Land Development Coordination**

Policy 15. Review all land use/development proposals for consistency with the TSP.

**Implementation Measures (Policy 15):**

- 15.a. *The City may approve local private streets through the Planned Development process, provided that adequate emergency access is available and that proper maintenance by private entities is ensured.*
- 15.b. *Any proposed change to the Comprehensive Plan or Zoning Maps that would result in additional trips above that allowed under the City’s concurrency policies may be denied unless mitigation measures are identified and provided.*
- 15.c. *Consider only improvements listed in the Financially Constrained funding scenario of the Regional Transportation Plan, and/or in the City’s Capital Improvement Plan (CIP), in determining the planned capacity, function and level of service of transportation facilities and services.*
- 15.d. *The Development Review Board or City Council may approve specific street design and alignment modifications through the planned development process. Such modifications shall be made in consideration of existing traffic volumes and the cumulative traffic generation potential of the land uses being developed.*

Policy 16. Ensure new development and redevelopment provide connections to transit streets and facilities, providing protected street crossings, and bus stop amenities, if needed.

*Villebois Village is the region’s largest residential development and provides a variety of housing choices in a dense setting with wide open spaces, parks, and trails. It is located just west of the SMART Central transit center and WES Commuter Rail station .*



*Old Town Square, located near the I-5/Wilsonville Road interchange, provides a well-connected network of sidewalks and crosswalks and accommodates SMART Transit Route 4, which loops through the site.*

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*“Connectivity is something I think is important within our transportation system. Having our schools not only connected to our neighborhoods, but neighborhoods connected to neighborhoods, and neighborhoods connected to retail and employment centers.”*

*Marta McGuire  
Planning Commission*

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## Agency Coordination

Policy 17. Collaborate with the State, Metro, Clackamas and Washington Counties, and adjacent jurisdictions and transit agencies to develop and implement a Regional Transportation Plan that is complementary to and supportive of the City's Plan while addressing regional concerns. The City expects a reciprocal commitment from the other agencies. This policy recognizes that there is a need for a collective and cooperative commitment from all affected agencies to solve existing and future transportation problems. The City will do its part to minimize transportation conflicts, but it must also have the support of County, regional, State and Federal agencies to effectively implement this Plan.

### **Implementation Measure (Policy 17):**

*17.a. Advocate for the State, Metro, and Counties to improve regional transportation facilities which, due to inadequate carrying capacities, limits implementation of the City's Transportation Plan.*

Policy 18. Work with ODOT, Metro, TriMet, Cherriots, and neighboring communities to maintain the capacity of I-5 through a variety of techniques, including requirements for concurrency, transit connections, continued development of a local street network within and connecting cities along I-5, access management, and completion of targeted improvements on I-5 such as auxiliary lanes, improvements at interchanges, etc.

Policy 19. Actively encourage the Federal Highway Administration, Federal Transit Administration, Oregon Department of Transportation, Clackamas and Washington Counties, Metro, TriMet, and Cherriots to improve regional transportation facilities and services.

### **Implementation Measure (Policy 19):**

*19.a. Consistent with the City's policy that needed public facilities and services are provided in advance of or concurrently with development, proposed land use changes within the I-5/Wilsonville Road Interchange Management Area (IMA) shall be consistent with planned future transportation projects.*

*19.b. Seek support from regional partners to construct connections that improve bicycle, pedestrian, and emergency vehicle access across the Willamette River.*

*19.c. Collaborate with Metro and surrounding jurisdictions to plan, and advocate for completion of, trails that link Wilsonville with neighboring jurisdictions as identified on the Regional Trails System Plan Map.*

Policy 20. Work with neighboring jurisdictions to plan, fund, and implement a phased transportation network that serves southwest employment area growth while reserving I-5 interchange capacity for access to and from Wilsonville destinations.

Policy 21. Recognize the Aurora State Airport as a component of the state's transportation system and an economic asset to Wilsonville, while advocating that any expansion of the airport consider potential impacts (e.g., noise, pollution, and safety) to Wilsonville neighborhoods, area roadways, I-5 interchanges, agricultural operations, and the environment.

## Goods Movement

- Policy 22. Provide an adequate motor vehicle system that serves commercial vehicle/truck traffic to and from the land uses they serve.
- Policy 23. Consider the requirements for truck movement when designing all improvements in the public right of way on designated truck routes. Requirements include turn radii, sight distance, lane widths, turn pocket lengths, and pavement design.



Located along Interstate-5 just south of the Interstate-205 junction, Wilsonville is ideally situated as a freight hub in the region. The city is home to multiple distribution, manufacturing, and warehouse facilities.

- Policy 24. Ensure that the needs of other transportation users are considered in the design and construction of freight improvements. Improvements that reduce freight vehicle impacts to bicyclists and pedestrians (particularly along identified bikeways and walkways) will be considered, including buffered bike lanes, enhanced pedestrian crossings, and other safety improvements.
- Policy 25. Maintain access to the Willamette River so that the river may be used for transportation purposes in the future. Acquire or improve access to Willamette River for public docking purposes and consider the potential development of a new port or ports.
- Policy 26. Assist with efforts to improve the viability of the railroad for freight.
- Policy 27. Upgrade and/or complete the street network on the west side of I-5, including in the Coffee Creek and Basalt Creek areas, to serve the warehousing, distribution, and other industrial uses located there.
- Policy 28. Coordinate with adjacent jurisdictions and the freight community to ensure that regional freight traffic is directed only toward the city's freight routes.

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*“A number of the companies that operate here in Wilsonville export outside the United States . . . that’s why it is so important that we get to market as effectively and efficiently as possible as we can, but at the same time, our goal is to make it so transparent that the local residents are aware of it, but don’t really have to deal with it.”*

*Ray Phelps  
Planning Commission*

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## Public Transit

Policy 29. Increase public awareness of transit and other transportation options, such as walking and bicycling, so that individuals can make informed decisions.

Policy 30. Provide transit service which is coordinated, convenient, comfortable, and safe.

### **Implementation Measures (Policy 30):**

30.a. *Maintain transit service and expand as necessary to meet the demands of a growing population and employment base in Wilsonville.*

30.b. *Perform ongoing transit service updates, based on demand and available financial resources. Service updates will be considered following major roadway improvements, pedestrian and bicycle system completion, and master planned, or other major, development.*

30.c. *Construct transit stop amenities and implement technology improvements, as funding is available. Prioritize improvements in activity centers and when they can be constructed in coordination with land use development.*

Policy 31. Create a sense of community ownership of the transit system by encouraging citizen involvement in the planning and development of transit facilities and services.

Policy 32. Develop a process for responding to public feedback regarding transit services, including additional service requests, bus routing, and transit stop amenities.

Policy 33. Guided by a transit-specific public feedback process, provide transit routes throughout the city so that transit stops are located within one-quarter mile walking distance from residents and businesses .

Policy 34. Establish a Transit Advisory Board comprised of interested stakeholders, including residents and employers, to guide future planning and decision-making regarding transit service.

Policy 35. Strive to improve air quality and traffic congestion by increasing transit efficiency, promoting transportation options, and implementing transportation system management.

Policy 36. Coordinate with other transit districts, including TriMet and Cherriots, to strengthen the efficiency and performance of the Wilsonville transit network.

### **Implementation Measures (Policy 36):**

36.a. *Advocate for TriMet to provide full day and Saturday service for its Westside Express Service (WES) commuter rail.*

36.b. *Advocate for the extension of WES to Salem.*



*Wilsonville's transit center, SMART Central at Wilsonville Station, is located at the corner of Baber Street and Kinsman Road. It is SMART's main transportation hub and includes a 400-stall park-and-ride lot, twelve bus bays, an operator break room, public restrooms, shelters, and a clock tower with security cameras. It also shares the site with TriMet's Westside Express Service (WES) commuter rail station. Wilsonville is WES's southern terminus.*

## Active Transportation: Pedestrians and Bicyclists

Policy 37. Provide facilities that allow more people to walk and bike, not only as low-impact transportation choices, but also to benefit the health and economy of the community.

### **Implementation Measures (Policy 37):**

- 37.a. *Encourage a balance between housing, employment, and commercial activities within the city so more people desire to live and work within Wilsonville, thereby reducing cross-jurisdictional commuting.*
- 37.b. *Increase densities and intensities of development in or near the Town Center area and in other locations where a multimodal transportation system can meet those needs.*
- 37.c. *Continue use of the Planned Development/Master Plan process to encourage developments that make it more convenient for people to use transit, walk, bicycle, and to drive less to meet daily needs.*
- 37.d. *Provide more and better options for travel between both sides of the freeway, the railroad, and the Willamette River.*
- 37.e. *Assist with efforts to improve the viability of rail for passenger service.*



*Bike lockers at the SMART Central at Wilsonville Station transit center provide secure storage for transit riders who use their bikes to complete a leg of their trip.*



*Pedestrians enjoy a casual stroll around the Villebois Sunday Market. The market uses Villebois Drive, which functions as a street when not being used for the market.*

- 37.f. *Consider reducing parking requirements where it can be shown that transit and/or bicycle pedestrian access will reduce vehicular trips.*
- 37.g. *Require new development to include sufficient and convenient bicycle parking, and encourage improvements to bicycle parking facilities throughout the community. Allow a range of bicycle parking solutions to address the specific needs of different users.*
- 37.h. *Construct stand-alone improvements to fill key gaps in the pedestrian and bicycle network, including Safe Routes to School projects and connections to transit stops, prioritizing low-cost and safety-related projects.*
- 37.i. *Improve the quality of the pedestrian environment by ensuring new public and private development meets a pedestrian quality standard that encourages walking for short trips and is fitting for the specific location.*

Policy 38. Establish a Pedestrian and Bicycle Advisory Board comprised of interested stakeholders, including residents and employers, to guide future planning and decision-making regarding pedestrian and bicycle facilities.



*Bicyclists riding north on Brown Road approach the Barber Street roundabout as they enter Villebois Village.*

- Policy 39. Improve and expand pedestrian and bicycle facilities throughout the community, with a focus on improved connectivity within the city and with the Regional bicycle and trails systems.
- Policy 40. Ensure that pedestrian and bicycle networks provide direct connections between major activity centers (e.g., civic, recreation, employment, and retail centers) and minimize conflicts with other modes of transportation.
- Policy 41. The planning, design, and construction of transportation projects should maintain or improve the accessibility and quality of existing and planned pedestrian and bicycle facilities.
- Policy 42. Provide more enhanced pedestrian crossings (which may include pedestrian flashers, a median refuge, or other treatments) as a way to improve safety and connectivity in Wilsonville's transportation system.
- Policy 43. Develop more transportation options within the city, increasing transportation demand management programming and improving walking, biking, and transit facilities.

## Interchange Management Areas

Policy 44. Provide for an adequate system of local roads and streets for access and circulation within I-5 Interchange Management Areas (IMAs) that minimize local traffic through the interchanges and on the interchange cross roads.

### ***Implementation Measures for I-5/Wilsonville Road IMA, subject to Interchange Area Master Plan (IAMP) (Policy 43) :***

- 44.a. *Require future development to plan for and develop local roadway connections consistent with the I-5/Wilsonville Road IAMP as part of the development permit approval process.*
- 44.b. *Require bicycle and pedestrian connections within the IMA for new development consistent with the City's Bicycle and Pedestrian Plan.*
- 44.c. *Implement system operational improvements, including signal synchronization, transportation demand management measures and incident management within the vicinity of the interchange to maximize the efficiency of the local street network and minimize the impact of local traffic on the interchange.*



*The Interstate-5/Wilsonville Road interchange serves as a key regional connection while also providing connectivity between east and west Wilsonville.*

- 44.d. *The City will require future development to adhere to access management spacing standards for private and public approaches on statewide highways as adopted in the Wilsonville Road IAMP.*
- 44.e. *The City will approve development proposals in the I-5/Wilsonville Road IMA only after it is demonstrated that proposed access and local circulation are consistent with the Access Management Plan in the I-5/Wilsonville Road IAMP.*
- 44.f. *Ensure that future changes to the planned land use system are consistent with protecting the long-term function of the interchange and the surface street system.*
- 44.g. *Any proposed change to the Comprehensive Plan Map or existing zoning that would result in additional trips above that allowed under the current zoning and assumed in the I-5/Wilsonville Road IAMP must include a review of transportation impacts consistent with OAR 660-12-0060.*
- 44.h. *The City will provide notice to ODOT for any land use actions proposed within the I-5/Wilsonville Road IAMP Overlay Zone.*
- 44.i. *Eliminate or consolidate accesses on Wilsonville Road within one-quarter mile of the I-5 interchange as opportunities arise. Specific access management deficiencies were identified as part of the I-5/Wilsonville Road Interchange Area Management Plan (IAMP).*
- Implementation Measures for I-5/Elligsen Road Interchange (no adopted IAMP) (Policy 43 continued):**
- 44.j. *The City will require future development to adhere to access management spacing standards for private and public approaches on statewide highways as required by the Oregon Highway Plan.*
- 44.k. *Ensure that future changes to the planned land use system are consistent with protecting the long-term function of the interchange and the surface street system.*
- 44.l. *Bicycle and pedestrian connections within the Interchange Area will be required for new development consistent with the City's Bicycle and Pedestrian Plan.*
- 44.m. *System operational improvements, including signal synchronization, transportation demand management measures and incident management shall be implemented within the vicinity of the interchange to maximize the efficiency of the local street network and minimize the impact of local traffic on the interchange.*
- 44.n. *Eliminate or consolidate accesses on Elligsen Road and Boones Ferry Road within one-quarter mile of the I-5 interchange as opportunities arise.*

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*“One of Wilsonville’s strengths is location with it’s easy access to I-5. Almost any point in town is within easy access to one of the interchanges. Preserving the capacity of two interchange will be important for the City’s future.”*

*Katie Mangle  
Long Range Planning Manager*

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## Transportation Funding

Policy 45. Require each individual development to provide all collector and local streets, unless the benefit to the entire community warrants public participation in funding those collector streets.

Policy 46. The City will plan, schedule, and coordinate implementation of all transportation system improvements through the on-going five-year Capital Improvements Plan. A priority is given to eliminating existing gaps and deficiencies and in upgrading the structural quality of the existing arterial system.

### **Implementation Measures (Policy 45):**

- 46.a. *The City shall coordinate routine and necessary maintenance with the appropriate State or County agencies.*
- 46.b. *The City shall pursue grants and other funding resources to assist the City with constructing infrastructure improvements, buying new transit buses, and making other transportation investments.*



*SMART Transit's 21-passenger compressed natural gas (CNG) buses offer a clean burning fuel alternative to traditional diesel buses.*

46.c. *To ensure development of an adequate transportation system, the City shall collect a System Development Charge as development occurs. Funds collected shall be allocated through the Capital Improvements Plan as needed to provide capacity service.*

Policy 47. Maintain a transportation financing program for the construction and implementation of transportation facilities, improvements, and services necessary to support the TSP, the Transit Master Plan, and the Bicycle and Pedestrian Plan. This program should be resourceful and innovative to ensure the City can make key transportation investments. Revenue sources may include public/private partnerships, Local Improvement Districts (LIDs), grants, etc.



*A family rides bikes together on Canyon Creek Road.*



*Looking southwest towards farmland and forests beyond Metro’s urban growth boundary as Interstate 5’s Boone Bridge and Portland and Western’s Oregon Electric line railroad bridge cross the Willamette River. Wilsonville is Metro’s southernmost city and provides an important connection to the rest of the Willamette Valley.*

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*“Our city is great. We have done an excellent job in planning this community and being thoughtful, and maintaining that. But it is also important to look into the future and how we may grow and plan for that and find out what things continue to be a priority for our community.”*

*Marta McGuire  
Planning Commission*

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# The Standards

## Chapter 3



Wilsonville's transportation standards ensure the city develops consistent with its vision of supporting a multimodal transportation system that is strategically designed for optimum community function and benefit. A street's design determines how it will look and function. How a street looks and functions is ultimately dependent upon which street elements are included, their dimensions, and how they relate to each other.

The standards are intended to ensure appropriate design and create a consistent approach throughout the city as development and redevelopment occurs. Since the design of a street is so closely tied to how it performs and how people experience the city, it is important for Wilsonville to carefully consider how it wants its streets to look and function and then to design them accordingly.

### OTHER CITY DOCUMENTS WITH TRANSPORTATION STANDARDS

The transportation standards in this chapter cover a variety of areas that help inform other City documents:

- Standard Detail Drawings
- Public Works Standards
- Planning and Land Development Ordinance

*Standards support the vision of a multimodal transportation system that is . . .*

- *Strategically designed and*
- *Collaboratively built,*

*Resulting in . . .*

- *Mode and route choices,*
- *Safe and convenient local accessibility, and*
- *Quality of life and economic health.*



## HOW STANDARDS BENEFIT THE TRANSPORTATION SYSTEM

The transportation standards included in this chapter support the City's management of an effective multimodal transportation system:

- **Functional Classifications** provide a hierarchy for managing public roadways practically and cost effectively. They provide a framework for identifying which street elements to include in a street's design.
- **Connectivity and Facility Spacing Standards** ensure that direct routes and travel options are available for all transportation users.
- **Freight Routes** connect the city's industrial and commercial sites with I-5 and other regional facilities and improve the coordination between freight and other travel modes.
- **Bicycle Routes** connect neighborhoods, schools, parks, community centers, business districts, and natural resource areas to support bicycle travel by residents of varying physical capabilities, ages, and skill levels.
- **Cross-Section Standards** provide guidance for selecting and sizing various design elements to serve intended users' needs.
- **Access Management** balances the transportation system's need to provide safe, efficient, and timely travel with the need to allow access to individual properties.

*Looking north at Boones Ferry Road north of Day Road. Washington County recently received jurisdiction of this roadway from ODOT and will be constructing improvements that include roadway widening, bike lanes, and sidewalks.*

## ROADWAY JURISDICTION

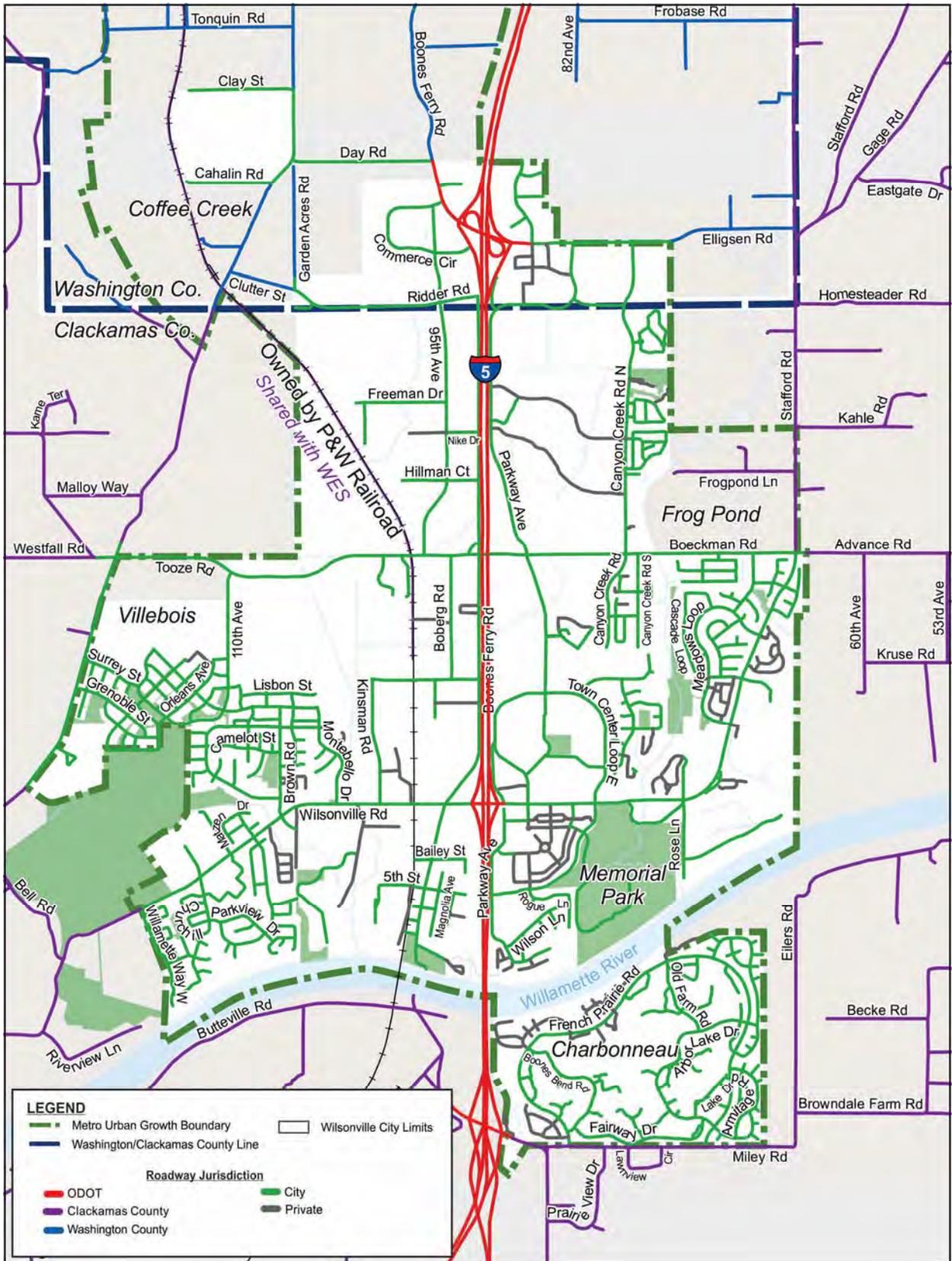
A roadway's jurisdiction affects who will have the ultimate authority over improvements and what standards apply. In the Wilsonville vicinity, there are four agencies with jurisdiction:

- **City of Wilsonville** has the majority of roadways within City limits.
- **Washington County** roadways are on the outskirts to the north of the city.
- **Clackamas County** roadways are on the outskirts to the east, west, and south of the city.
- **ODOT** has jurisdiction of Interstate-5, the corresponding interchange ramps, the portions of Elligsen Road and Boones Ferry Road between the Parkway Avenue and Day Road, and Wilsonville Road between Town Center Loop West and Boones Ferry Road.

As the City expands, it is expected that the county roadways in the immediate vicinity of the city will transfer jurisdictions to the City of Wilsonville. These roadways include Stafford Road, Advance Road, Elligsen Road, Frog Pond Lane, Clutter Street, and Grahams Ferry Road.



**FIGURE 3-1. ROADWAY JURISDICTION**



## FUNCTIONAL CLASSIFICATION

The City's street functional classification system is an important tool for managing public roadways. It is based on a hierarchical system of roads (see diagram at right) where streets with a higher classification, such as arterial streets, emphasize a higher level of mobility for through-movement. They look and function very differently than a street with a lower classification, such as local streets, which emphasize the land access function.

Wilsonville has four functional classes:

- **Major Arterials** primarily connect the I-5 interchanges with major activity centers (i.e., Town Center and Argyle Square) but also include the key connections requiring additional travel lanes (i.e., Boeckman Road bridge over I-5 and Stafford Road). They generally have four or more travel lanes, bicycle lanes, and limited access (preferably connecting with minor arterials).
- **Minor Arterials** serve as the direct connections through town and usually do not penetrate identifiable neighborhoods. They generally have two or three travel lanes, bicycle lanes, and consolidated access to larger developed areas and neighborhoods.
- **Collectors** provide traffic circulation within residential, commercial, and industrial areas and serve to funnel traffic from neighborhoods to the arterial street network. They have two or three travel lanes, bicycle lanes, optional on-street parking, and minor access restrictions.
- **Local Streets** are located within residential, commercial, and industrial areas and discourage through movement. They allow on-street parking and ensure that every parcel is accessible for all modes.

The roadway classifications throughout the city are shown in Figure 3-2. These classifications provide a vision of how these roadways should be designed and constructed as improvements are made.

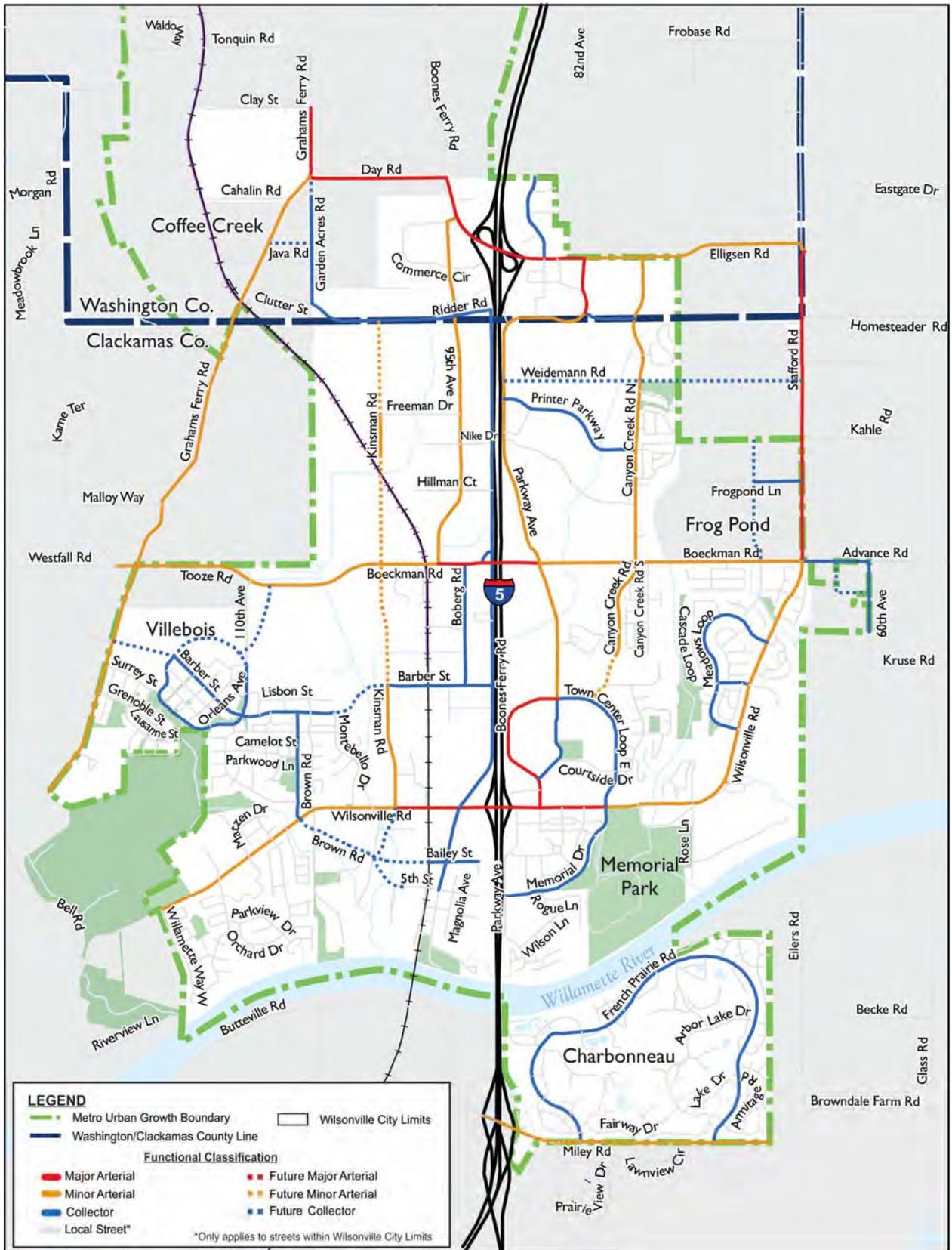


### FUNCTIONAL CLASSIFICATION AS A FRAMEWORK FOR STANDARDS

Functional classification provides a helpful framework for managing the City's transportation system and supporting the following standards:

- **Connectivity and Spacing Standards** indicate how far apart roadways of different functional classifications should be spaced to ensure a balanced approach to mobility and land access throughout the city.
- **Freight Routes and Transit Streets** primarily use higher classification roads to serve freight and/or transit vehicles due to the wider cross-sections and greater focus on mobility.
- **Cross-Section Standards** vary by functional classification to meet user needs. However, functional class is not the only factor in determining street design.
- **Access Management Standards** are more stringent for higher class roadways, which are intended to emphasize mobility.

**FIGURE 3-2. FUNCTIONAL CLASS DESIGNATIONS**



### CONNECTIVITY AND SPACING

One of Wilsonville’s goals is to improve connectivity by constructing parallel facilities spaced at regular intervals throughout the city. These facilities provide multiple alternatives and more direct routes between both local and regional destinations, including neighborhoods, parks, schools, employment centers, and retail areas.

Table 3-1 lists the desired spacing of each facility type throughout Wilsonville to ensure a high level of connectivity. Figure 3-3 illustrates the desired spacing for the arterial and collector street network. Deviations to these guidelines may be needed in locations where there are significant barriers, such as topography, rail lines, freeways, existing development, and the presence of natural areas.

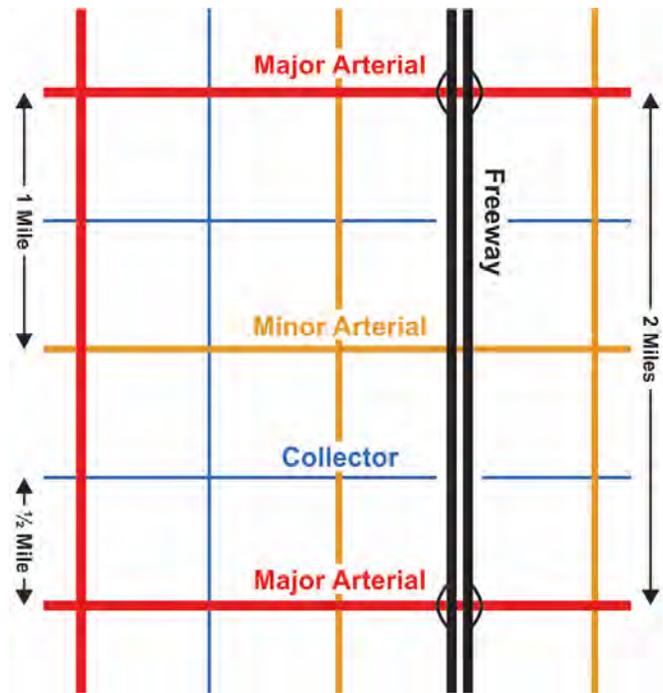
Bicyclists and pedestrians benefit the most from closely spaced facilities because they are the most affected by distance. By providing walking and biking facilities spaced less than 300 feet apart, Wilsonville will support walking and biking use within and between its neighborhoods. In addition, these connections can improve access to transit.

**Table 3-1. Facility Spacing Guidelines**

Facility Type	Desired Spacing <sup>a</sup>
Major Arterial	1 - 2 mi
Minor Arterial	1 mi
Collector	1/4 - 1/2 mi
Local Street	300 - 500 ft
Bicycle and Pedestrian Facilities	300 ft

<sup>a</sup> Desired Spacing refers to distance between facilities with same or higher functional classification.

**FIGURE 3-3. DESIRED FACILITY**



### BENEFITS OF CONNECTIVITY

Connectivity provides all transportation system users with multiple benefits:

- Increased mobility by distributing traffic over multiple connected streets rather than forcing all traffic onto the City’s arterial street system
- More equitable access for all businesses and neighborhoods throughout the city

- Improved walking, biking, and transit use due to more direct connections and less out of direction travel between neighborhoods, schools, transit stops, retail centers, employment centers, and recreational areas
- Reduction in short auto trips between adjacent neighborhoods and land uses



*Villebois Village Master Plan was designed to provide a high level of connectivity for all travel modes using short blocks arranged in a grid pattern, numerous pathways, and a diversity of land use.*

---

*“Connectivity is important because you want to be able to have options for how you move through your community. I don’t personally always want to drive my car places, especially when I have my children with me. I want us to get out and be active and to be able to bike to the store. We have stores that are really close to us, but it’s not always safe and convenient for us to ride our bike there. Which is why having bike lanes and sidewalks that are designed to accommodate these other options are critical to enhance our livability.”*

*Marta McGuire  
Planning Commission*

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## FREIGHT ROUTES

Wilsonville’s freight routes connect the city’s industrial and commercial sites with I-5 and other regional facilities. Figure 3-4 identifies the City’s freight routes, which include truck routes, railroads, and waterways. Improvement projects should be coordinated to facilitate freight needs while balancing the needs of other users.

Some of the key truck routes that provide important truck connections to Washington County include Boones Ferry Road, Kinsman Road, and Tonquin Road. In addition, the Portland and Western Railroad runs through Wilsonville and serves freight traffic, and the Willamette River has the potential for handling barge traffic. These routes are identified in *Metro’s Regional Freight Plan* (June 2010).

As a major employment center and industry hub along I-5, Wilsonville will benefit from ensuring that its freight routes are designed to accommodate the needs of its industrial and commercial sites. At the same time, Wilsonville’s residential neighborhoods should be protected from freight traffic. The call-out box at right lists multiple freight coordination improvements resulting from having freight routes.

## IMPROVED FREIGHT COORDINATION

By having designated freight routes, various City efforts regarding freight and non-freight users will be improved:

- **Roadway and Intersection Improvements** can be designed for freight vehicles with adjustments for turn radii, sight distance, lane widths, turn pocket lengths, and pavement design.
- **Bicycle and Pedestrian Improvements**—such as buffered bike lanes, enhanced pedestrian crossings, and other safety improvements—can be identified to reduce freight impacts to other users (particularly along bikeways and walkways).
- **Roadway Durability** can be increased by using concrete instead of asphalt.
- **Railroad Connections** can be coordinated to support businesses that ship goods by rail, particularly in areas where railroad sidings can be provided along the Portland and Western Railroad track.
- **Willamette River Port** can be considered to support businesses that ship goods using barges on the Willamette River.
- **Coordination with Businesses and Adjacent Jurisdictions** can ensure that local and regional freight traffic uses the City’s freight routes to travel

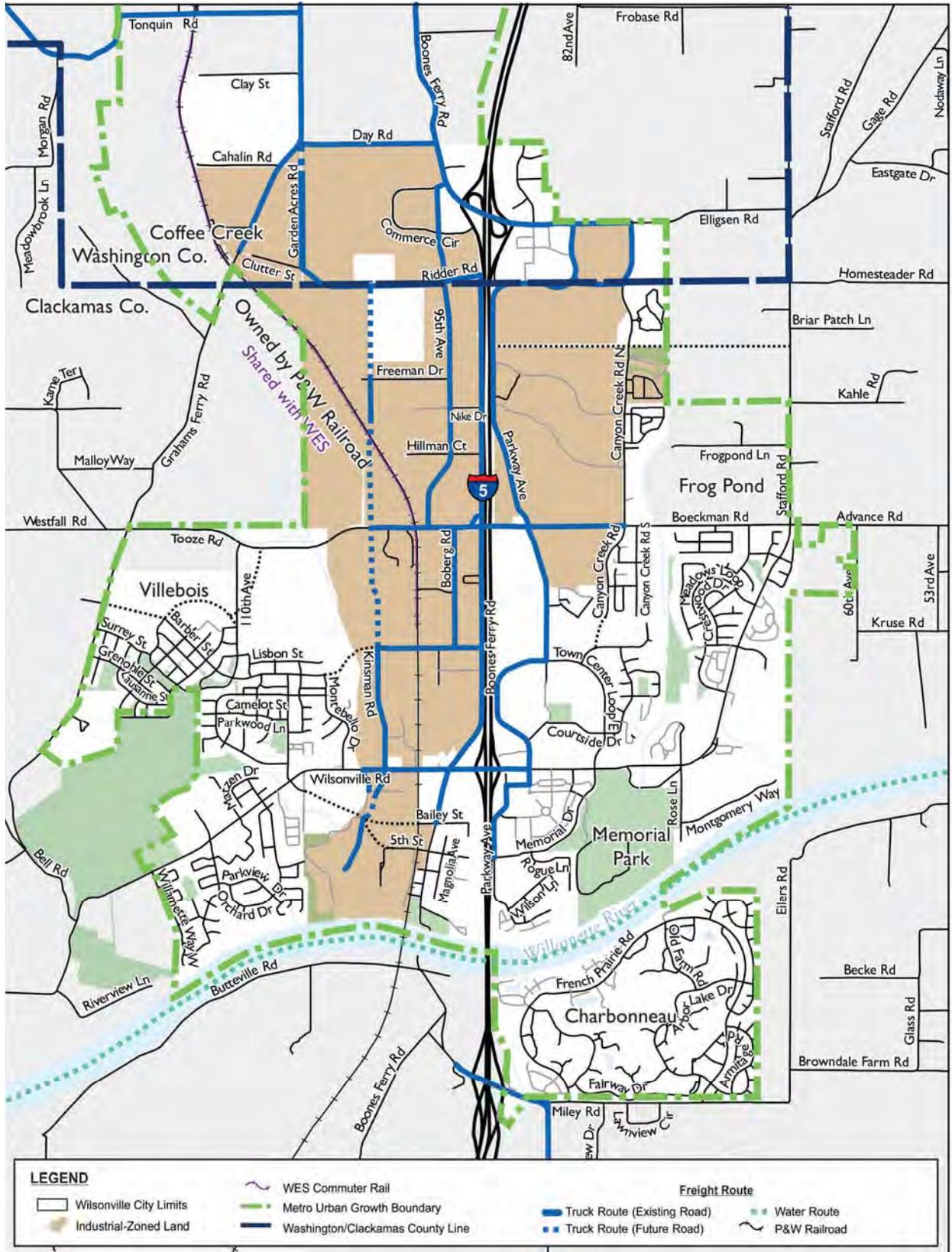
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*“We have a significant number of large manufacturing companies because we have an efficient freight mobility process where our trucks can get in and out of town with the least amount of interference from local traffic. For the part of the transporter, that’s very important in as much as it costs money for these trucks, even when they are not moving. Secondly, the local resident doesn’t want to have to be disrupted by freight transportation.”*

*Ray Phelps  
Planning Commission*

---

FIGURE 3-4. FREIGHT ROUTES



## BICYCLE ROUTES

Bicycle routes are provided throughout Wilsonville and connect to neighborhoods, schools, parks, community centers, business districts, and natural resource areas. The City's bicycle network serves multiple users of varying physical capabilities, ages, and skill levels.

Figure 3-5 identifies the City's bicycle routes, which include three facility types:

- **Shared-Use Paths** are 10-foot to 12-foot wide pathways that have minimal conflicts with automobile traffic and may have their own right-of-way (cross-section standards shown in Figure 3-11). Shared-use paths serve multiple non-motorized users: bicyclists, pedestrians, wheelchair users, skaters, and others. Many of the shared-use paths throughout Wilsonville are part of the regional trail network, which traverses large sections of the city and connects to neighboring jurisdictions and regionally significant destinations. These regional trails are designed to meet state and federal guidelines, which make them eligible for state and federal transportation funding.
- **Bike Lanes** are provided on Arterial and Collector streets throughout Wilsonville. They are usually 6-feet wide and adjacent to motor vehicle travel lanes (cross-section standards shown in Figures 3-6, 3-7, and 3-8). Buffered bike lanes and one-way or two-way cycle tracks may be used instead of bike lanes and include buffers between the bike and motor vehicle travel lanes (cross-section standards shown in Figure 3-12).
- **Local Street Bikeways** are streets designated as important bicycle connections where bicyclists share the travel lane with motor vehicle traffic. Even though all Local Streets allow bicyclists to share the travel lane (cross-section standards shown in Figures 3-9 and 3-10), Local Street Bikeways are intended to serve a greater number

of bicyclists. They typically are provided on low-volume, low-speed residential streets that serve as important connections to nearby bike lanes, shared-use paths, and key destinations.

Modifications—such as sharrows, traffic calming devices, or wayfinding signage—may be made to these streets to emphasize their use as bicycling facilities and increase the comfort and confidence of bicyclists.

### KEY BICYCLE FACILITIES

The following existing and future bicycle facilities (which are included in Figure 3-5) provide important connections throughout the city:

#### Regional Trails

- Ice Age Tonquin Trail (through West Wilsonville with connections to Tualatin and Sherwood)
- Waterfront Trail (along the Willamette River)
- Boeckman Creek Trail (along Boeckman Creek in East Wilsonville)
- Stafford Spur Trail (connecting to regional destinations in Northeast Wilsonville)

#### Shared-Use Paths

- Primarily near schools, parks, transit hubs, retail centers, and other pedestrian areas

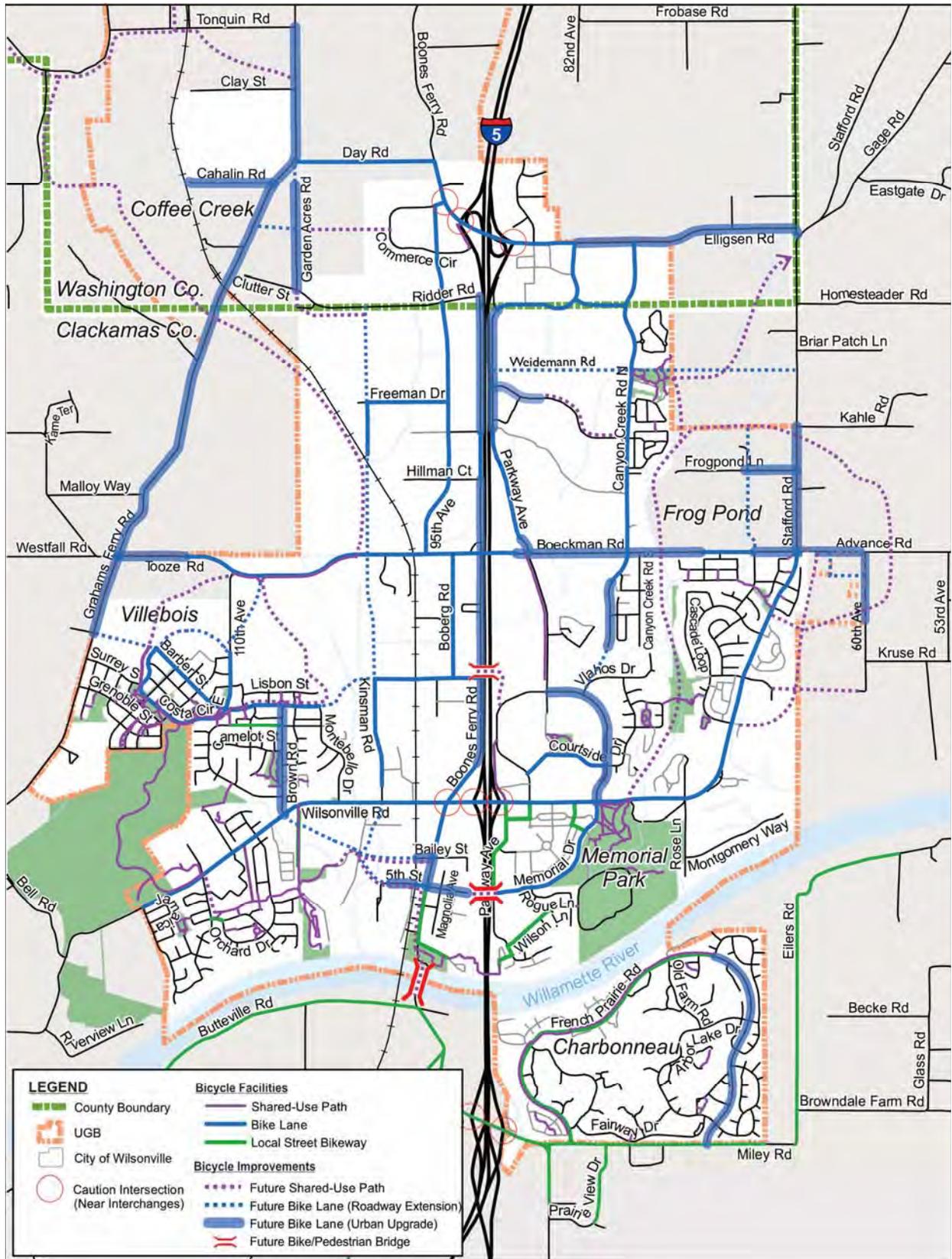
#### Bike Lanes

- On Arterial and Collector streets

#### Local Street Bikeways

- Boones Ferry Road south of 5th Street to connect to future Willamette River bridge
- Parkway Avenue connecting to Wilsonville Road to the nearby neighborhood
- Wilson Lane, Metolius Lane, and Kalyca Drive connecting Memorial Park to the Waterfront Trail near where it passes underneath the I-5 Boone Bridge

**FIGURE 3-5. BICYCLE ROUTES**



## STREET CROSS-SECTION DESIGN

Since different streets serve different purposes, a functional classification system—which is a hierarchy of street designations—provides a framework for identifying the size and type of various street elements to consider including in a street's design. Not all elements are included on all streets and so they must be carefully selected based on multimodal needs.

While a street's functional classification does not dictate which street elements to include, it does facilitate the selection of multimodal facilities and widths that will help ensure the roadway can meet its intended multimodal function. Adjacent land uses and available right-of-way width also influence which elements are included in a specific segment.

Roadway cross-section design elements include travel lanes, curbs, planter strips, sidewalks on both sides of the road, and bicycle facilities consistent with designated bikeways, walkways, and shared-use trails. Low impact development (LID) standards may also be used throughout the City at the City's discretion.

### FACILITY TYPES

Cross-section standards are provided for the following facilities:

- Major Arterials
- Minor Arterials
- Collectors
- Local Streets
- Low Impact Development (LID) Local Streets (similar modifications may be made to other streets regardless of classification)
- Shared-Use Paths and Trails
- Bicycle Facility Design Options



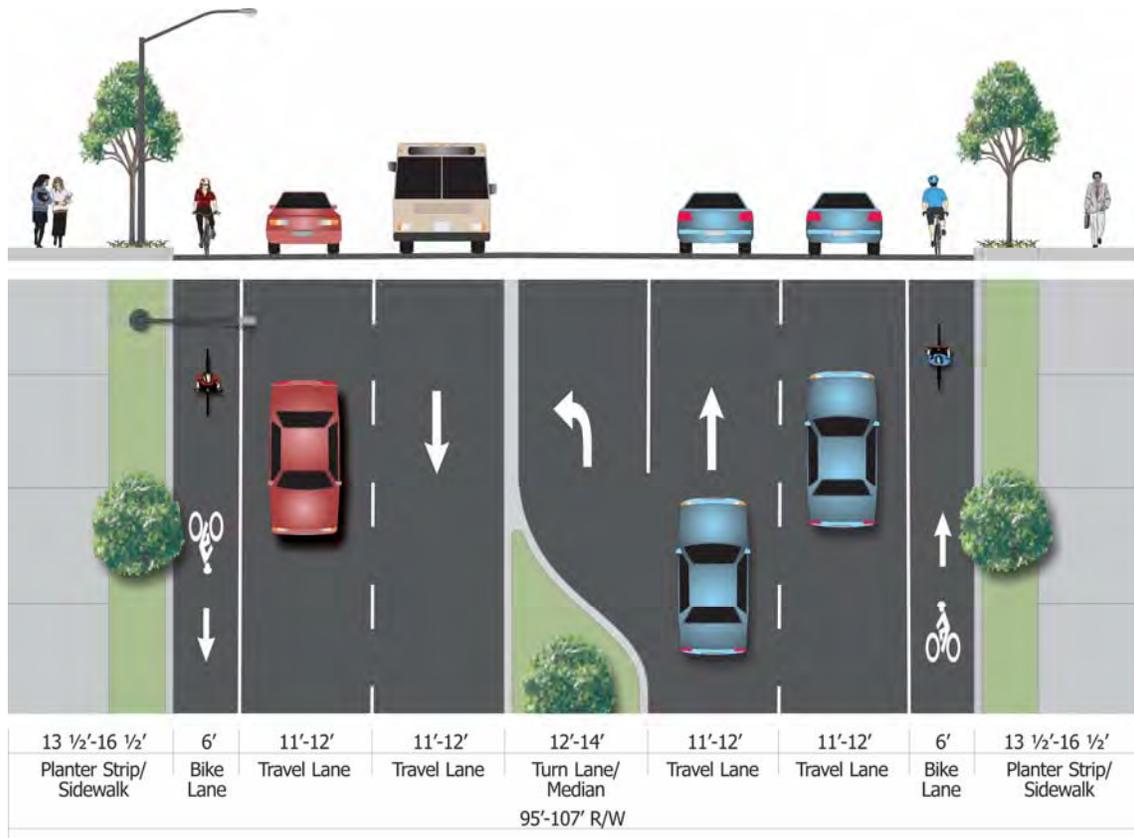
*Example of a Major Arterial - Boeckman Road looking west towards Boberg Road and 95th Avenue*



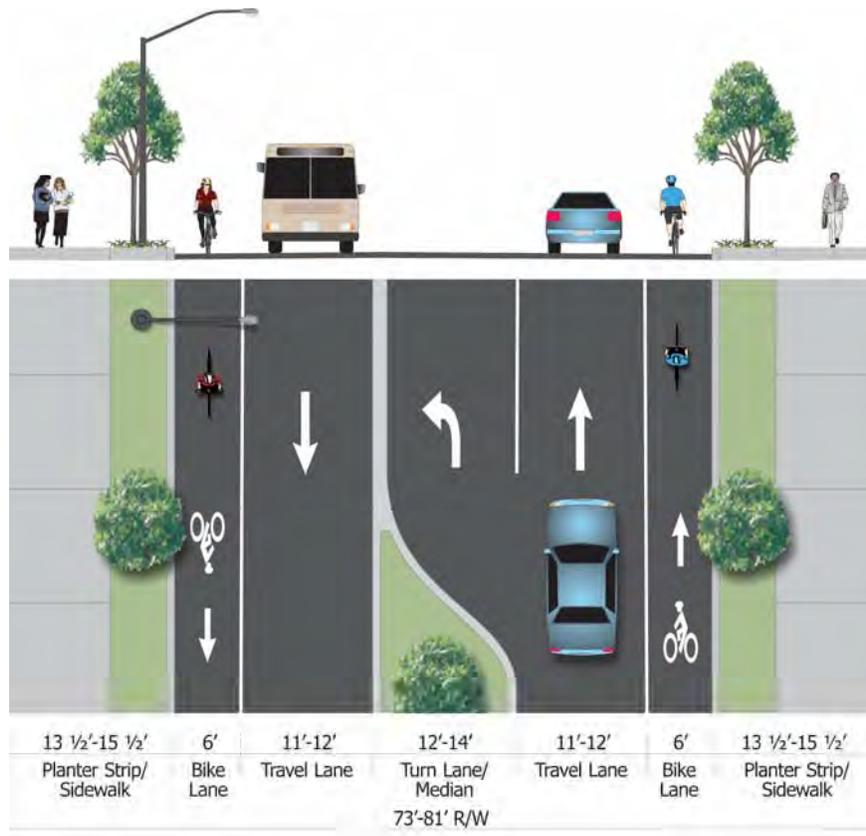
*Example of a Collector - Barber Street looking east near SMART Central at Wilsonville Station transit center*



*Example of a Local Street - Rogue Lane looking east near Memorial Park*

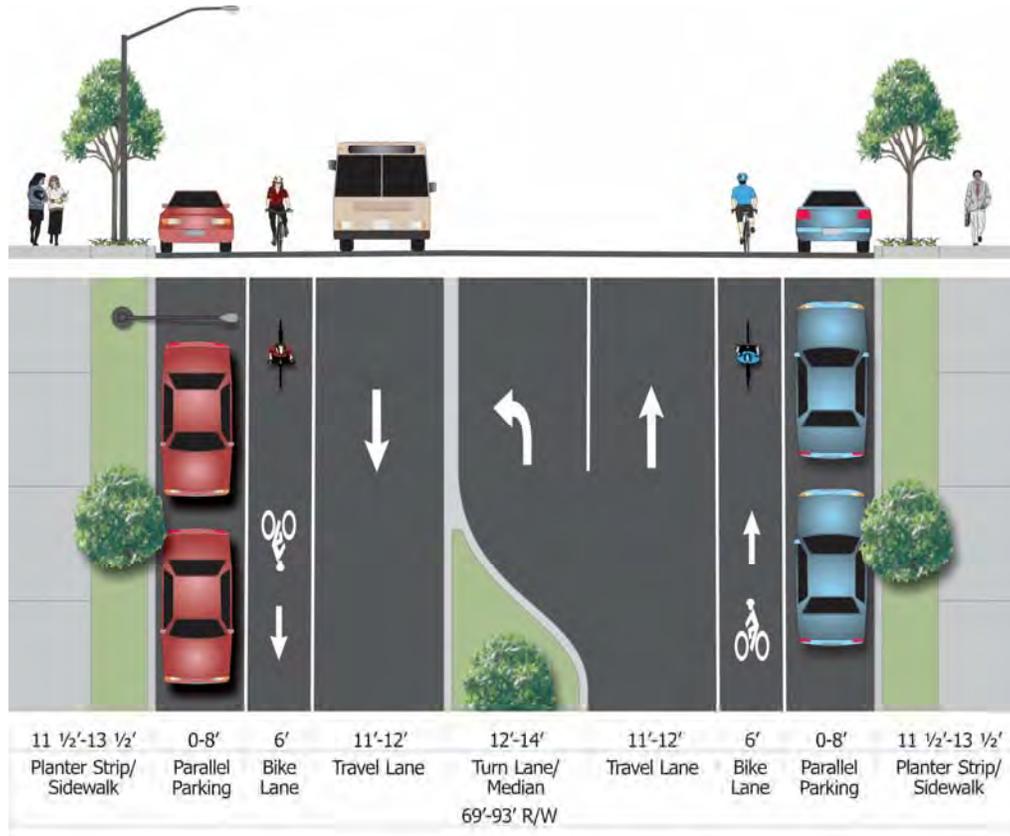
**FIGURE 3-6. MAJOR ARTERIAL CROSS-SECTION****Notes:**

1. Travel lane and turn lane/median widths as determined by Community Development Director.
2. Minimum sidewalk width is 5 feet; actual sidewalk width as determined by Community Development Director. Width of sidewalk/planting strip may be combined in commercial/retail areas for a total width of 13½ to 16½ feet; street trees shall be located in minimum 4-foot tree wells.
3. Curb width of ½-foot is included in the sidewalk/planter strip width.
4. Street lights shall be located within the planter strip, center landscape median, or sidewalk as determined by Community Development Director.
5. Striping and signage as required in the PW Standards.
6. On-street parking is not allowed.
7. Transit stop locations to be determined by Transit Director.
8. When not needed as a left-turn lane, median may be provided to serve safety, stormwater, or aesthetic objectives.
9. New streets shall incorporate low impact development design as practicable.
10. Allow for separation for bikes on major arterials (especially freight routes).

**FIGURE 3-7. MINOR ARTERIAL CROSS-SECTION****Notes:**

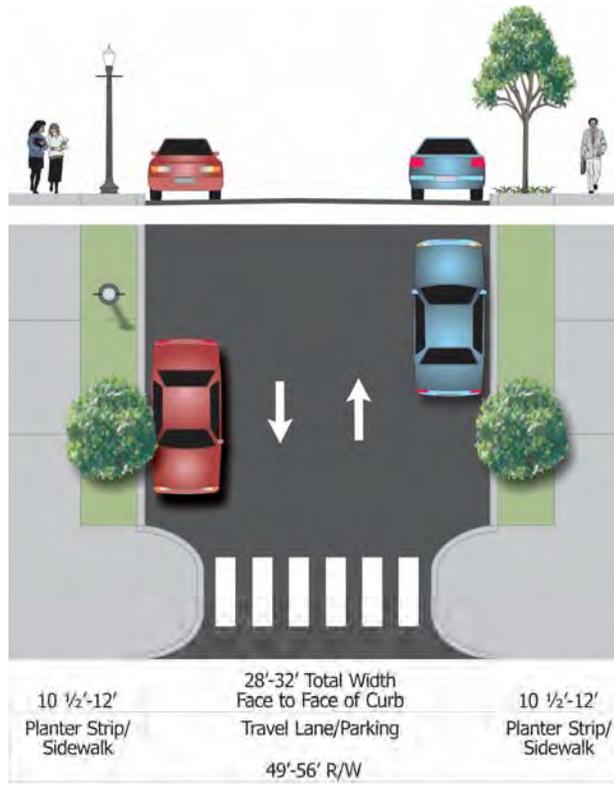
1. Travel lane and turn lane/median widths as determined by Community Development Director.
2. Minimum sidewalk width is 5 feet; actual sidewalk width as determined by Community Development Director. Width of sidewalk/planting strip may be combined in commercial/retail areas for a total width of 13½ to 15½ feet; street trees shall be located in minimum 4-foot tree wells.
3. Curb width of ½ foot is included in the sidewalk/planter strip width.
4. Street lights shall be located within the planter strip, center landscape median, or sidewalk as determined by Community Development Director.
5. Striping and signage as required in the PW Standards.
6. On-street parking is not allowed.
7. Transit stop locations to be determined by Transit Director.
8. When not needed as a left-turn lane, median may be provided to serve safety, stormwater, or aesthetic objectives.
9. New streets shall incorporate low impact development design as practicable.
10. Allow for separation for bikes on minor arterials (especially freight routes).

FIGURE 3-8. COLLECTOR CROSS-SECTION

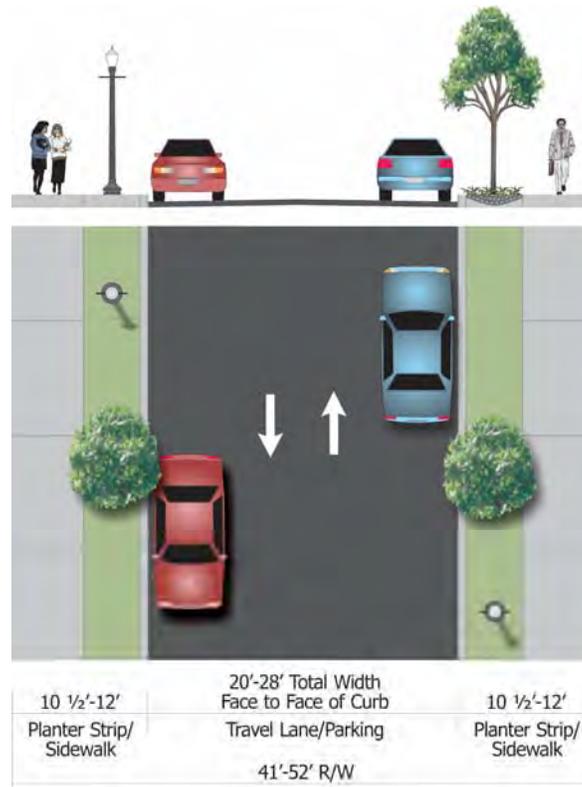


## Notes:

1. Collector right-of-way varies between 59 to 89 feet as determined by Community Development Director based on surrounding planned development of residential, commercial or industrial and need for on-street parking and/or turn lane/median.
2. Minimum sidewalk width is 5 feet; actual sidewalk width as determined by Community Development Director. Width of sidewalk/planting strip may be combined in commercial/retail areas for a total width of 11½ to 13½ feet; street trees shall be located in minimum 4-foot tree wells.
3. Curb and sidewalk bulb-outs at crosswalks or street intersections as determined by Community Development Director.
4. Curb width of ½ foot is included in the sidewalk/planter strip width.
5. Street lights shall be located within the planter strip, center landscape median, or sidewalk as determined by Community Development Director.
6. Travel lane and turn lane/median widths as determined by Community Development Director. Turn lane/median may be eliminated.
7. Striping and signage as required in the PW Standards.
8. On-street parking on one or both sides is allowed.
9. Transit stop locations to be determined by Transit Director.
10. When not needed as a left-turn lane, median may be provided to serve safety, stormwater, or aesthetic objectives.
11. New streets shall incorporate low impact development design as practicable.

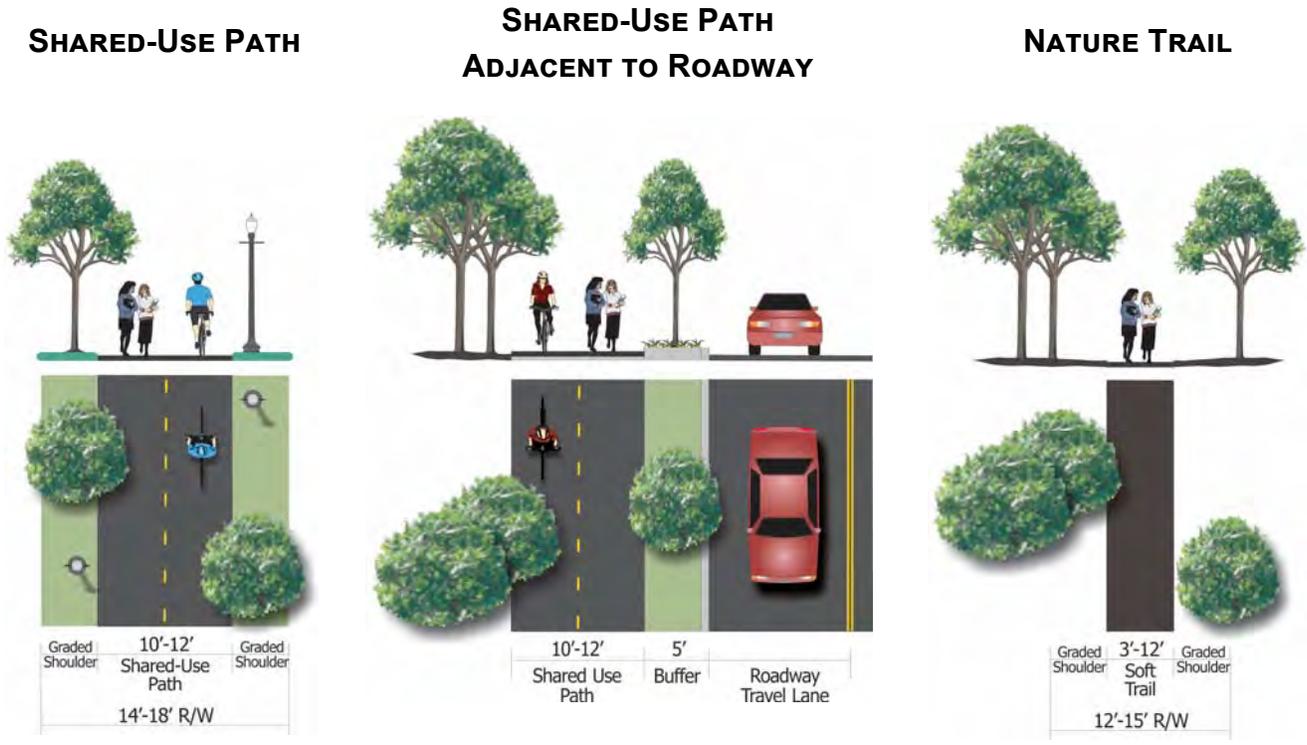
**FIGURE 3-9. LOCAL STREET CROSS-SECTION****Notes:**

1. Minimum right-of-way width of 47 feet (parking on one side) and 51 feet (parking on both sides). Providing parking on both sides is preferred unless constraints exist.
2. Minimum sidewalk width is 5 feet; minimum planter strip width is 5 feet.
3. Curb width of 1/2 foot is included in the planter strip width.
4. Curb and sidewalk bulb-outs at crosswalks or street intersections as determined by Community Development Director.
5. Street lights shall be located within the planter strip as required in the PW Standards.
6. No lane striping on street. Signage as required.
7. New streets shall incorporate low impact development design as practicable.

**FIGURE 3-10. LOW IMPACT DEVELOPMENT (LID) LOCAL STREET CROSS-****Notes:**

1. LID streets located as approved by Community Development Director.
2. Minimum sidewalk width is 5 feet; actual sidewalk width as determined by Community Development Director.
3. Minimum landscape width of 6½ feet where a water quality swale is proposed.
4. Curb width of ½ foot is included in the planter strip width.
5. Stormwater control as required in the PW Standards.
6. Use of pervious surfaces as determined by Community Development Director.
7. Narrower streets as approved by Community Development Director and as permitted in the PW Standards.
8. 28-foot curb-to-curb street is intended to allow on-street parking on both sides.
9. 24-foot curb-to-curb street is intended to allow on-street parking on one side.
10. 20-foot curb-to-curb street would not allow on-street parking on either side.

**FIGURE 3-11. SHARED-USE PATH AND TRAIL CROSS-SECTIONS**



**Notes:**

1. Trail types and widths as approved by Community Development Director.
2. Typical cross section of shared-use path is 12 feet wide with 2-foot-wide compacted crushed stone shoulders.
3. Vertical separation between shared-use path and roadway may be used instead of 5' buffer as approved by Community Development Director.
4. Cross-section standards identified in the Ice Age Tonquin Trail Master Plan are required along the Ice Age Tonquin Trail.
5. Additional design standards are available in the Bicycle and Pedestrian Master Plan.

**FIGURE 3-12. BICYCLE FACILITY DESIGN OPTIONS**

**BUFFERED BIKE LANES AND CYCLE TRACKS**

Buffered bike lanes (buffer between travel lane and bike lane) and cycle tracks (parking and/or other buffer between travel lane and one- or two-way bike facility) are two alternate bicycle facility options that are gaining popularity throughout the United States and have been implemented in other parts of the Portland Metro area. Therefore, the design options shown below have been provided to allow the City flexibility to consider these bicycle treatments on their Arterial and Collector streets in place of typical bike lanes.

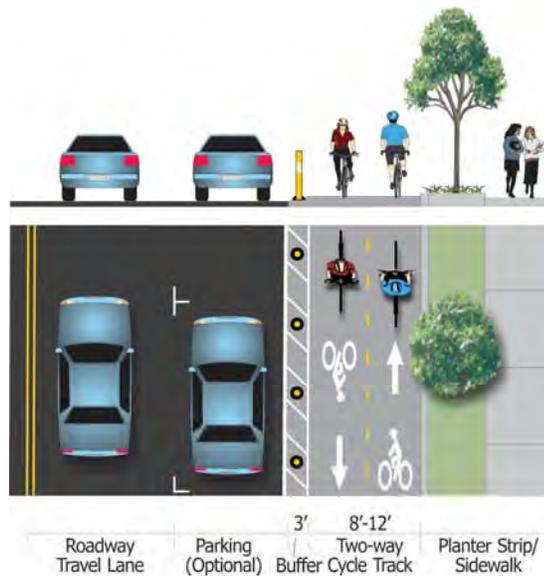


*One-Way Cycle Track on Cully Boulevard in Northeast Portland. Cycle tracks are typically protected from motor vehicle traffic by parked cars, raised curbs, or other physical buffers.*

**BUFFERED BIKE LANE OR ONE-WAY CYCLE TRACK**



**TWO-WAY CYCLE TRACK**



Notes:

1. Design option locations, widths, separation buffer features, and adjacent parking as approved by Community Development Director.
2. Additional design guidance can be obtained from the National Association of City Transportation Officials (NACTO) Urban Bikeway Design Guide

### ACCESS MANAGEMENT

Access management refers to the broad set of techniques that are used to balance safe, efficient, and timely travel with the ability to allow access to individual properties. Access is an important component of the city’s transportation infrastructure and significantly affects system operations and safety.

Wilsonville should continue to manage roadway access to improve traffic flow and safety. By limiting access to higher classification roadways (especially Major and Minor Arterials), conflicts between vehicles entering and exiting driveways and vehicles on the roadway are reduced. Pedestrians and bicyclists also benefit from reduced conflicts with vehicles entering and exiting the roadway.

Table 3-2 lists the City’s access spacing standards. Because there are existing non-conforming accesses, these standards will primarily guide access layout of future development consistent with the strategies listed in the call-out box at right. ODOT also has access spacing standards that apply to the I-5 interchange areas and to the section of Boones Ferry Road that is under ODOT jurisdiction (i.e., between Parkway Avenue and Day Road). The I-5/Wilsonville Road Interchange Area Management Plan (IAMP) should also be consulted when considering access needs near the Wilsonville Road interchange.



Looking east to the I-5/Wilsonville Road interchange. Interchange areas have the most restrictive access spacing standards to ensure safety and mobility.

### ACCESS MANAGEMENT STRATEGIES

The City can use various access management strategies to help improve mobility and safety:

- **Interchange Areas:** Eliminate or consolidate accesses within one-quarter mile of the I-5 interchanges as opportunities arise.
- **Adjacent to High Volume Intersections:** Pursue appropriate treatments at accesses adjacent to high volume intersections, particularly when queues block access.
- **Existing Driveways:** Evaluate accesses that do not conform to the City’s access spacing standard and consider modifications as practicable, while maintaining reasonable access to each property.
- **Ongoing Development Review:** Manage new driveway locations and spacing on a case-by-case basis. Where driveways do not meet spacing standards, consider mitigation treatments, such as consolidating accesses or restricting turn movements to right-in/right-out.

Table 3-2. Access Spacing Standards

Functional Classification	Access Spacing Standards <sup>a</sup>	
	Desired <sup>b</sup>	Minimum
Near Interchanges	ODOT Requires 1,320 ft	
Major Arterial	1,320 ft	1,000 ft
Minor Arterial	1,000 ft	600 ft
Collector	300 ft	100 ft
Local Street	Access Permitted to Each Lot	

<sup>a</sup> Spacing is measured from centerline to centerline on Major Arterials and Minor Arterials and between adjacent curb returns on Collectors and Local Streets

<sup>b</sup> Desired Access Spacing shall be adhered to unless otherwise approved by the City Engineer. Reasons for deviating from Desired Access Spacing include aligning with existing driveways, topography, property limitations, and other safety related issues as identified in a transportation study.





*A colorful row of street trees along Wilsonville Road near Boones Ferry Primary School during a fall day. Street trees can provide both aesthetic and safety benefits. They improve the walking environment by creating a pleasing buffer between the motor vehicle and pedestrian facilities. They also provide visual cues to drivers that can result in reduced traffic speeds.*

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*“The City needs to have a Transportation System Plan to make sure we are prepared for how we get around the city in the future. This includes automobiles, freight, bikes, and pedestrians.”*

*Nancy Kraushaar  
Community Development Director*

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As a growing community, Wilsonville faces the challenge of addressing new and ongoing transportation system needs. These needs are categorized as either gaps (missing connections or barriers in the transportation network) or deficiencies (shortcomings of the existing system). The City's transportation policies (see Chapter 2) and standards (see Chapter 3) serve as a framework for determining what gaps and deficiencies currently exist or are anticipated to arise through the 2035 horizon year as additional development occurs throughout the city and the region. The City's transportation improvement projects (see Chapter 5) and programs (see Chapter 6) address these needs and ensure Wilsonville's continued growth and prosperity.

### GAPS AND DEFICIENCIES

- **System Gaps** are missing connections or barriers in the urban transportation system that functionally prohibit travel for a given mode. While a gap generally means a connection does not exist, it could also be the result of a physical barrier (such as I-5, the Willamette River, other natural feature, or existing development) or a social barrier (including lack of information, language, education, and/or limited resources).
- **System Deficiencies** are performance, design, or operational constraints that limit travel by a given mode. Examples may include unsafe designs, bicycle and pedestrian connections that contain obstacles, inadequate intersection or roadway capacity, insufficient bus frequency, and congestion.

*Wilsonville's transportation needs include . . .*

- *Gaps (missing connections or barriers)*
- *Deficiencies (shortcomings)*

*These needs will be addressed by . . .*

- *Improvement projects (Chapter 5)*
- *Programs (Chapter 6)*



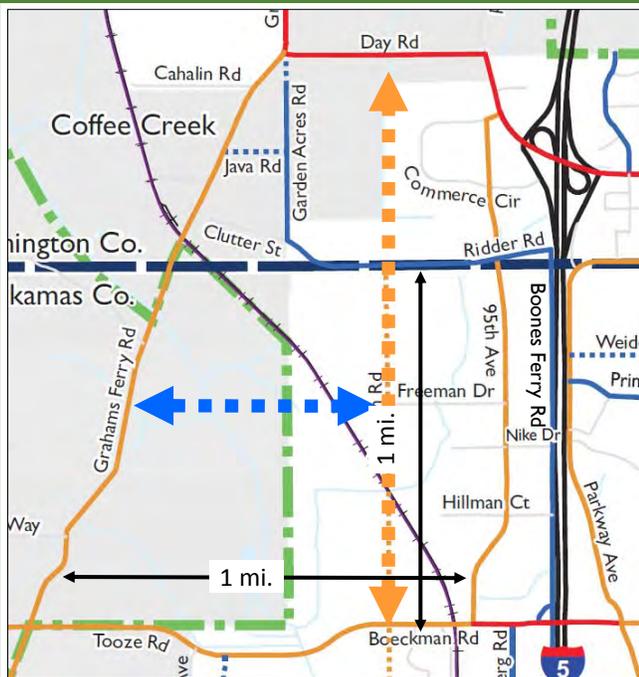
Header Photo Source: OBEC

### MULTIMODAL CONNECTIVITY GAPS

Providing a well connected transportation system is one of the City’s goals. In order to ensure this goal is achieved, the City has developed facility spacing standards to provide direct routes and travel options

for system users. Based on the street connectivity guidelines set forth in Chapter 3, there are system gaps in each of the city’s four quadrants. However, there are also constraints and barriers that may make some connections infeasible.

#### Northwest Quadrant Connectivity



Two connectivity gaps exist in this quadrant:

- A north-south gap exists between Day Road and Boeckman Road that increases congestion at the 95th Avenue/Elligsen Road intersection and the nearby I-5 interchange.
- An east-west gap exists between 95th Avenue and Grahams Ferry Road.

**North/south Minor Arterial and east/west Collector** would be needed as future development occurs to fill these gaps, provide additional travel options, and allow access to future development. However, these roads will be difficult to construct due to the P&W railroad track and Metro green space in this quadrant that are barriers. The new north/south roadway should be considered after 95th Avenue between Boeckman Road and Ridder Road no longer sufficiently serves this function.

#### Northeast Quadrant Connectivity



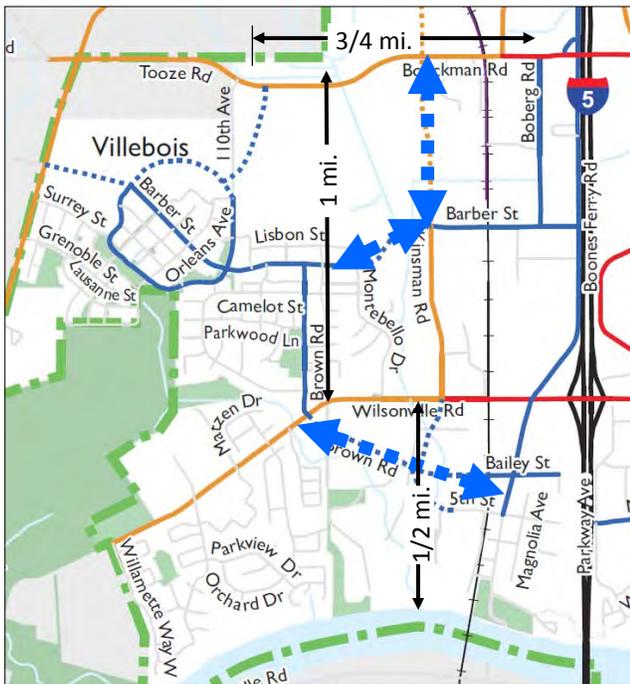
There is a gap in the east west connectivity between Elligsen Road and Boeckman Road.

**An east/west Collector** from Parkway Avenue to Stafford Road would be needed to fill this gap. The City currently owns partial right-of-way along the west end of Wiedemann Road, which is a single-lane gravel road that runs east/west for a short distance east of Parkway Avenue.

The following legend applies to each of the four quadrant images.

LEGEND	
Functional Classification	New Connection Needed
Major Arterial	Minor Arterial
Minor Arterial	Collector
Collector	
Local Street*	

### Southwest Quadrant Connectivity



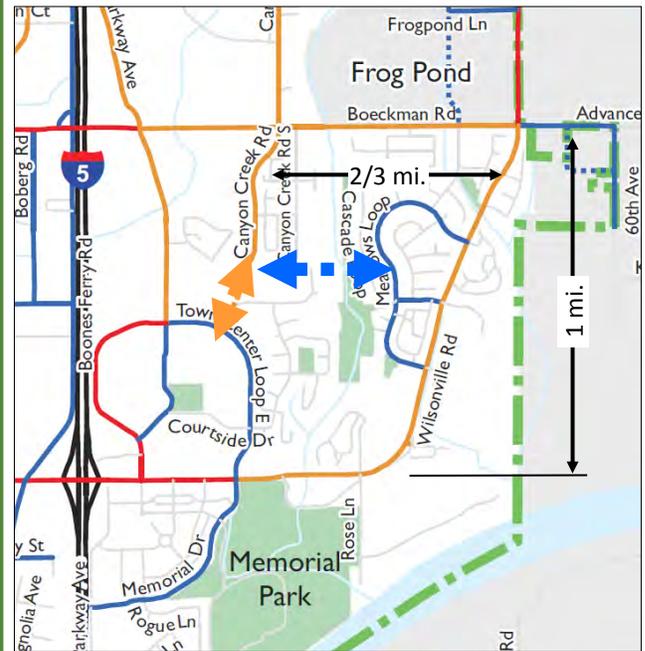
There are several gaps in east-west and north-south connectivity as follows:

- North/south and east-west gap exists between Wilsonville Road and Boeckman Road and between the Villebois development and the WES station.
- An east-west gap exists between the Willamette River and Wilsonville Road.

**North/south Minor Arterial and east/west Collector (north of Wilsonville Road)** streets are needed to fill these gaps. The Barber Street and Kinsman Road extensions are currently in the design phase that would satisfy these needs.

**An east/west Collector (south of Wilsonville Road)** would be needed as development occurs to provide the necessary connectivity. This roadway would also provide a secondary access option to and from Old Town (that is needed today), and the likely connection options are either 5th Street or Bailey Street.

### Southeast Quadrant Connectivity



There are two existing gaps in this quadrant as follows:

- A north-south gap exists between Boeckman Road and Town Center Loop that leads to additional traffic on Parkway Avenue and Wilsonville Road.
- An east-west gap exists between Canyon Creek Road and Meadows Loop.

**North/south Minor Arterial** extension of Canyon Creek Road is needed as soon as funding is available and would provide the connection to Town Center Loop. A major portion of this connection has already been constructed by adjacent development.

**An east/west Collector** from Canyon Creek Road to Meadows Loop would provide the connectivity needed. However, there are topographical, environmental, and development constraints that make this connection difficult. An existing trail and bridge provide pedestrian and bicycle connectivity.

## CROSS-SECTION DEFICIENCIES

To ensure Wilsonville’s roadways adequately serve all modes, the City has cross-section standards that guide roadway design based on the street’s functional classification with the acknowledgement that design elements shall be matched with the adjacent land use to provide safe transportation choices for users. The functional classifications and cross-section standards include number of motor vehicle travel lanes, sidewalks on both sides of the street, planter strips, and curbs (see Chapter 3: The Standards). In addition, the higher classification roadways also include bicycle facilities.

Building roads that provide facilities for all travel modes and meet applicable cross-section standards is critical to assure a safe and well connected transportation system. If bike lanes and sidewalks are

missing, the users of these facilities are likely using other portions of the roadway (motor vehicle travel lanes or shoulders) that may be unsafe.

Figure 4-1 shows which City roadways do not meet their applicable cross-section standards. In some instances, all that is needed are sidewalks for improved pedestrian connectivity. In other instances, roadways may need to be widened to include center turn lanes or bike lanes. Many of these roads are adjacent to rural areas and will be brought up to meet standards as adjacent parcels develop. Others will require standalone improvement projects. Depending on the situation, these roadway sections will require urban upgrades, sidewalk infill, or bike lane infill improvements.

*Freeman Drive between 95th Avenue and businesses lacks sidewalks on the south side.*



*Parkway Avenue near the Xerox campus is a Minor Arterial but does not include bike lanes. There is a sidewalk on the east side, but it ends at the boundary with the vacant parcel to the north.*



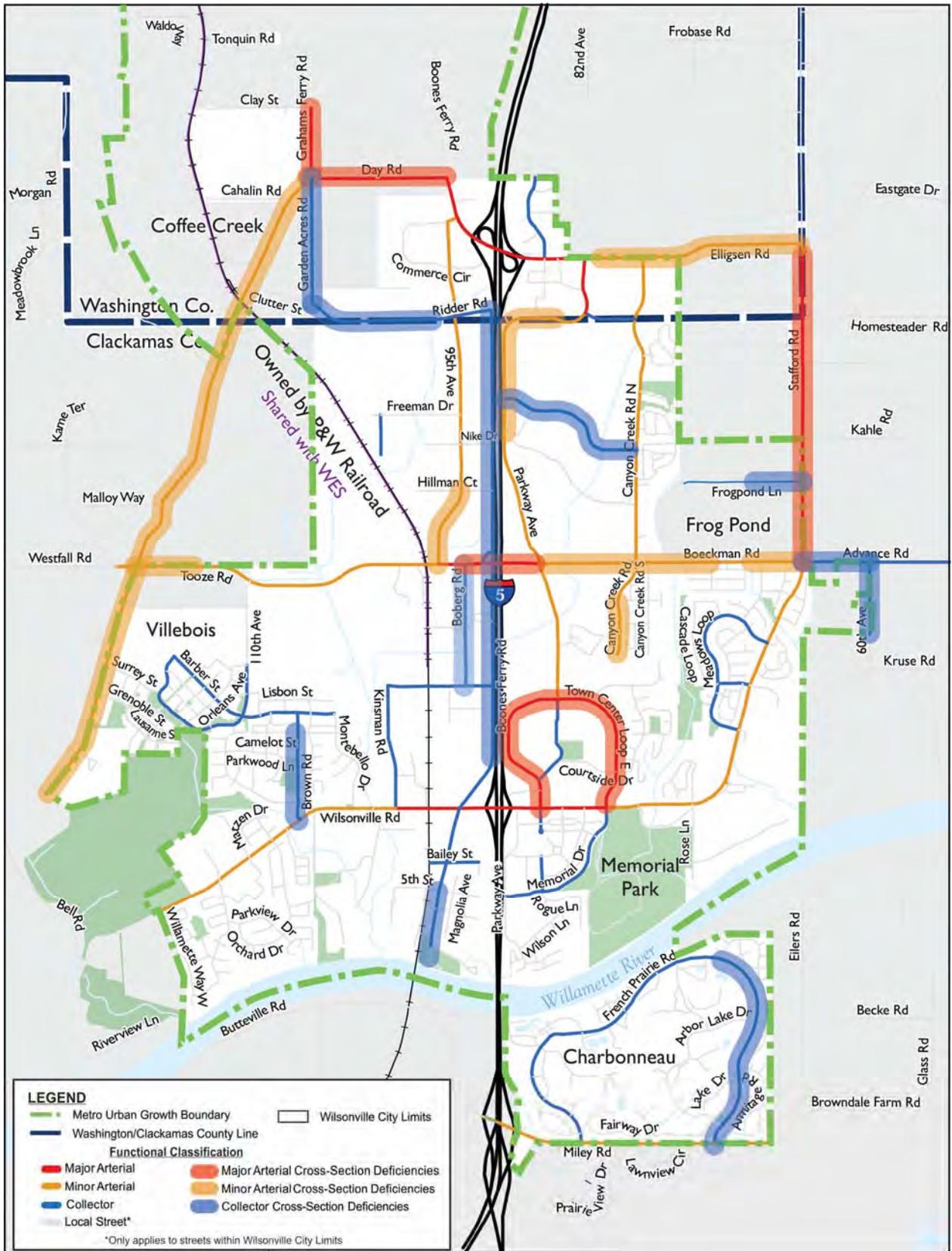

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*“I-5 poses some challenges because it serves as a barrier in between the east and west sides of town. This puts a lot of pressure on the few existing connections that make it harder for people to walk between one place and another.”*

*Katie Mangle  
Long Range Planning Manager*

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**FIGURE 4-1. ROADWAY CROSS-SECTION DEFICIENCIES**



## CAPACITY DEFICIENCIES

Capacity deficiencies for motor vehicles were identified throughout Wilsonville by evaluating traffic operations for a 2035 future scenario. The traffic forecasts were performed using a travel demand model based on Metro regional land use with the transportation network refined specifically for Wilsonville.

Due to the high level of detail, the Wilsonville travel demand model was able to more accurately represent local routing choices while also forecasting traffic pattern changes resulting from varying levels of congestion and delay expected for 2035. The model also assumed the completion of seven key roadway extensions (listed in the call-out box at right), as well as land use growth based on regional population and employment forecasts for the 2035 horizon year.

Figure 4-2 shows the 20 study intersections and five roadway segments that would not meet adopted mobility standards under the 2035 baseline scenario. These roadway capacity improvements would primarily be needed when the vacant land in their vicinity is developed.

The majority of the intersection and roadway deficiencies were identified in prior planning efforts and already included associated improvement projects. Therefore, many of the City's planned projects only required minor revisions, refinements, and prioritization adjustments. Along with minor changes to existing projects, a few new projects are also needed to meet the city's long term capacity needs.

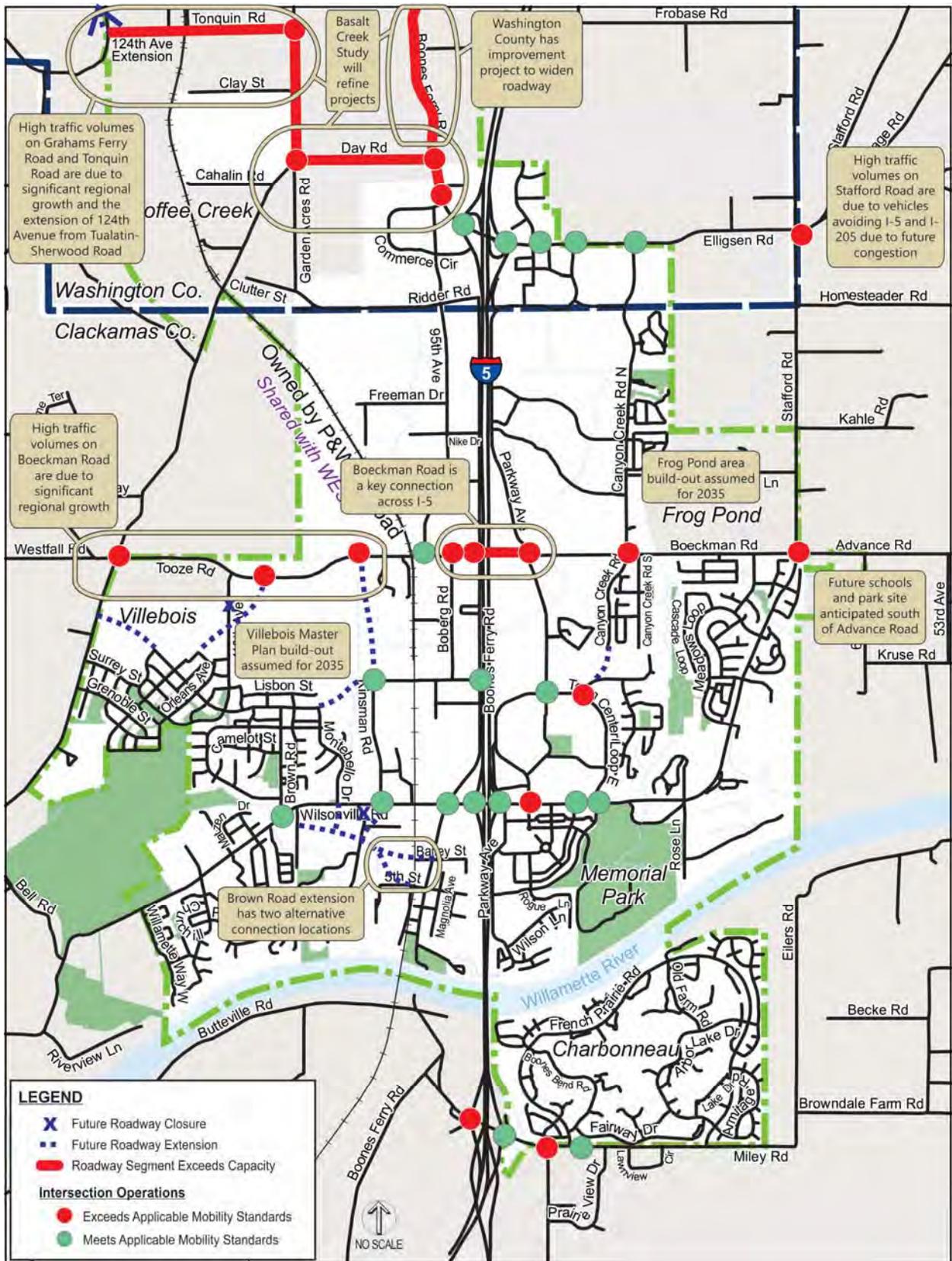
## 2035 BASELINE ROADWAY EXTENSION ASSUMPTIONS

Various roadway extensions throughout the city satisfy critical connectivity needs and would be constructed as development occurs. To account for the resulting traffic patterns, the 2035 baseline capacity analysis assumed the completion of these projects:

- **Barber Street Extension** from Kinsman Road to Montebello Drive, connecting the WES Station to Villebois (Regional Transportation Plan Project 10153, design plans are currently in process)
- **Barber Street Extension** to Grahams Ferry Road (Key roadway in Villebois Master Plan Area)
- **Villebois Drive Extension** to Boeckman Road (Key roadway in Villebois Master Plan Area to replace existing 110th connection)
- **Kinsman Road Extension** from Barber Street to Boeckman Road (Regional Transportation Plan Project 10130; design plans are currently in process)
- **Kinsman Road Extension** from Ridder Road to Day Road (Regional Transportation Plan Project 10853; key roadway in Coffee Creek Master Plan Area)
- **Brown Road Extension** (Currently has partial preliminary design plans for two alternatives)
- **Canyon Creek Road Extension** to Town Center Loop East (Small segment remains to finish connection; eligible as one of final projects using East Side Urban Renewal funding)

These roadway improvements are included in Figure 4-2, which also shows with the 2035 capacity

**FIGURE 4-2. FUTURE 2035 CAPACITY DEFICIENCIES**



## FREIGHT-RELATED DEFICIENCIES

In the past, Wilsonville relied on county and Metro designated freight routes. As a major employment center and industry hub along Interstate-5 (I-5), the city and its freight community will benefit from adopting a local freight plan and freight routes. Wilsonville's residential areas will also benefit from designating freight routes that avoid neighborhoods. The community would also benefit from increased marine freight traffic on the Willamette River.

The plan is a result of outreach to identify the city roadways used by freight carriers, as well as the freight-related deficiencies and problem locations on these roadways. This outreach included distribution of surveys to the city's major freight carriers, and a meeting with the Allied Waste commercial and

residential drivers, who service the entire city and have a particularly extensive understanding of the city's freight needs.

Figure 4-3 identifies the key gaps and deficiencies that were identified based on the feedback received. It also identifies the streets where freight vehicles are present, though not all of these should become designated freight routes.

The following feedback, which is more general in nature, was also provided by the freight carriers:

- Flashing yellow left-turn arrows at traffic signals are the preferable design treatment for protective/permissive phasing.
- Where possible, it is important to separate trucks from pedestrians and bicycles (especially on roadways and at tight intersection corners).
- There are inconsistent speeds on similar functioning roadways (for example, Boones Ferry Road versus Parkway Avenue).
- Trucks block traffic when they must wait off-site to access busy on-site loading docks.
- Improved loading areas and site access at retail establishments would aid delivery.
- There are limited direct routes for freight that exist between north and south Wilsonville.

### FREIGHT CARRIER OUTREACH

Multiple freight carriers provided feedback on freight routes and deficiencies:

- Allied Waste Services of Wilsonville
- Coca-Cola Bottling of Oregon
- Eaton Corporation
- FLIR Systems, Inc.
- Mentor Graphics Corp
- OrePac Building Products
- Owens & Minor Distribution Inc
- Parker Johnstone's Wilsonville Honda
- Rite Aid Distribution Center
- Rockwell Collins Head-Up Guidance Systems
- SYSCO Food Services of Portland
- Tyco Electronics Medical Products/Precision Interconnect Corp.
- US Crane & Hoist, Inc.
- Vision Plastics, Inc.
- Wilsonville Concrete
- Wilsonville Toyota
- Xerox Corporation



*Roadway congestion and queuing on Elligsen Road leads to increased delay to freight movement.*



## BICYCLE AND PEDESTRIAN NEEDS

Bicycle and pedestrian facilities support complete community connectivity and opportunities for work, play, shopping, and exercise. They also help reduce traffic congestion, vehicle-miles traveled, and greenhouse gas emissions, while increasing the vibrancy and connectedness of communities and improving the health of city residents.

Figure 4-4 shows the major bicycle and pedestrian gaps and deficiencies in Wilsonville. These needs are due to the various barriers in the system relating to natural areas, topography, and existing development.

There is also a need for improved street cleaning and related maintenance to remove debris from the I-5 interchange areas on Wilsonville Road and Elligsen Road, which are under ODOT jurisdiction. These facilities serve as primary connections over the city's



*The lack of continuous bike lanes on Brown Road north of Wilsonville Road requires cyclists to use the travel lane.*

## SAFE ROUTES TO SCHOOL

Additional bicycle and pedestrian gaps and deficiencies were identified as part of the Safe Routes to School assessment that the City performed in collaboration with the West Linn-Wilsonville School District and each of the city's primary and middle school. These needs are identified in Chapter 6: The Programs.

two most significant barriers (i.e., Interstate-5 and the Willamette River).

Another pedestrian and bicycle need that affects Wilsonville is regional access to the nearby communities. The Ice Age Tonquin Trail and Boones Ferry Road improvements north of Day Road are two examples of facilities that will provide regional connectivity. In addition, Clackamas County has identified the need to provide bicycle facilities on Stafford Road and 65th Avenue to the north and east of Wilsonville. A connection to the south over the Willamette River is also a critical need to link to Charbonneau and the Willamette River Heritage Area (including Champoeg State Park and the Willamette Valley Scenic Bikeway).

To further enhance regional connectivity, the City should continue to coordinate with Clackamas County and Washington County to ensure that bicycle and pedestrian improvements on county roadways are identified in their county TSP updates and that these facilities connect to the city's bicycle and pedestrian systems.

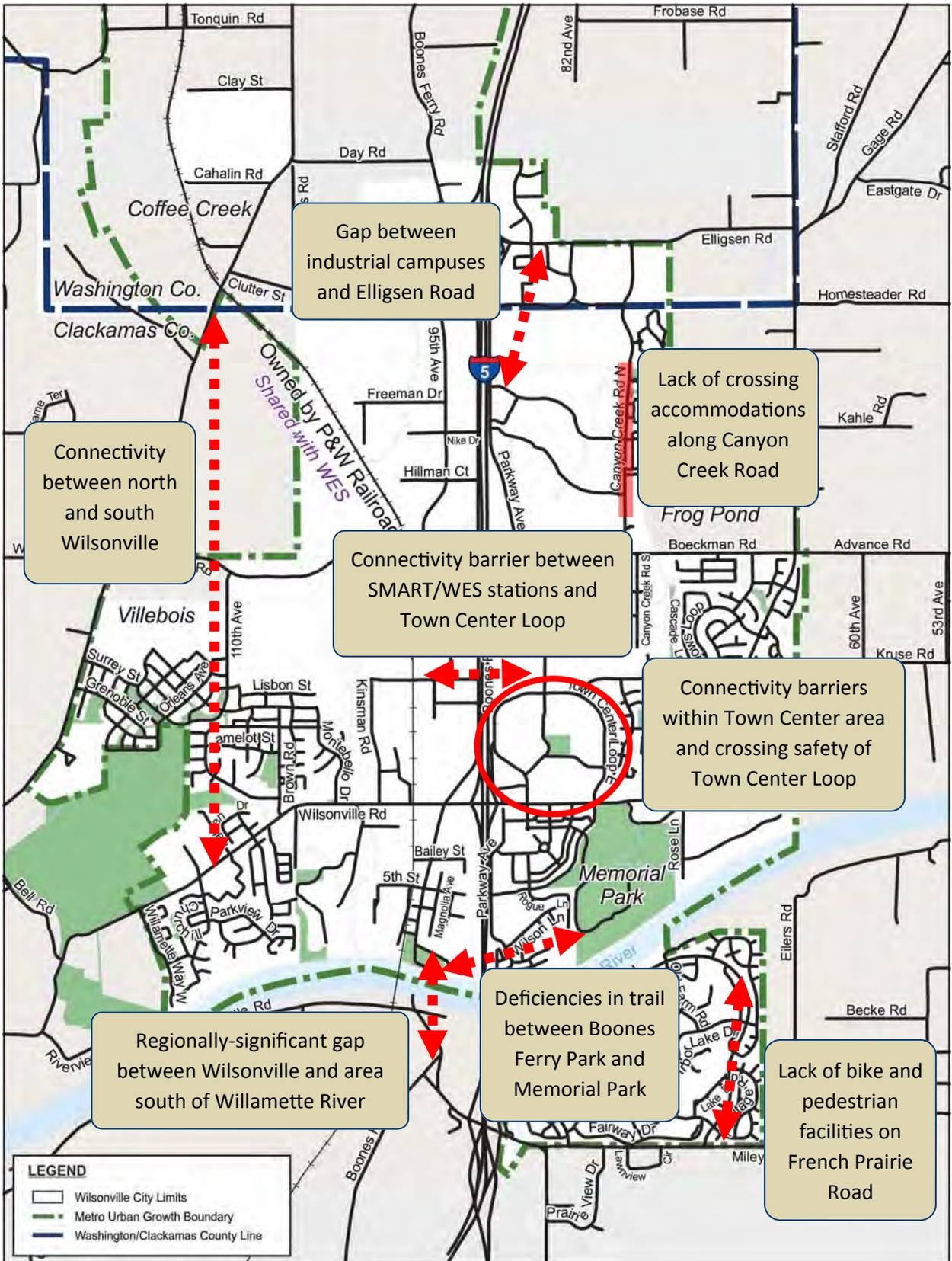
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*“Right now there are many gaps where sidewalks end or cross into areas where there are no receiving facilities for them. So, the transportation system plan is looking at those gaps and will be trying to fill them.”*

*Al Levit  
Planning Commission*

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FIGURE 4-4. MAJOR BICYCLE AND PEDESTRIAN NEEDS



## TRANSIT NEEDS

Wilsonville is unique among the cities within the Portland Metro area because it has its own transit system. While the rest of Metro is served by TriMet, Wilsonville has been operating South Metro Area Regional Transit (SMART) since it withdrew from TriMet's service district in 1988.

A locally run transit system provides many benefits for Wilsonville's residents and employees. Because it is not dependent upon another agency, SMART is able to determine its own bus routes, frequencies, and fares. It currently provides fare-free service within Wilsonville and supports other programs unique to Wilsonville, such as the SMART Options program. SMART is financially supported by payroll taxes from its strong employment base.

SMART also experiences various challenges, including six key transit needs:

- **Regional Transit Connections** are important for SMART due to Wilsonville's central location between two metropolitan areas (Portland Metro and Salem-Keizer) and its large employment base. While it has existing connections to TriMet (Portland Metro) and Cherriots (Salem-Keizer), these connections should be improved as opportunities arise. For example, expanded service hours and express service to downtown Portland would benefit a larger population of employees and residents of Wilsonville.
- **Service Coverage and Bus Frequency** require ongoing adjustments as demand and resources change. SMART should provide transit service within 1/4-mile of land uses throughout the city. Currently, there are only a few areas that do not fall within the 1/4-mile coverage radius, including Wilson Lane on the east, Willamette Way and Orchard Drive on the west, and the majority of Charbonneau. SMART will need to be responsive

to the desires of the public and all affected neighbors before providing or removing service from a given neighborhood. SMART will also need to expand its service as new development occurs in the areas of Coffee Creek, Villebois, and Frog Pond. To expand coverage and service, SMART may require additional buses.

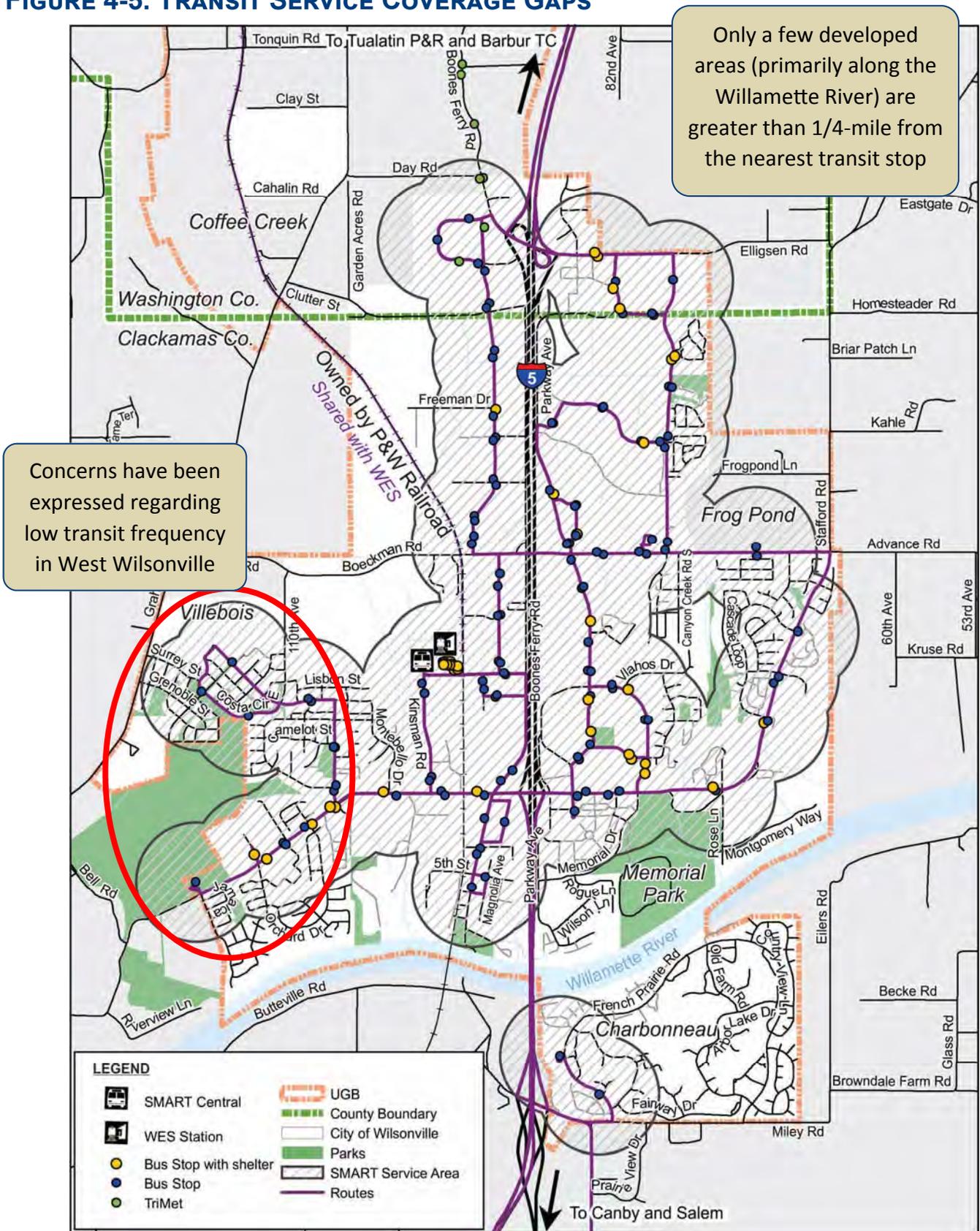
- **Pedestrian and Bicycle Access to Transit** can help improve transit service by providing safe and convenient connections at either end of transit trips. Pedestrian and bicycle networks that provide access to transit stops and good connectivity to all destinations throughout the city are important. They encourage increased use of transit, walking, and bicycling, which are

## RECENT TRANSIT IMPROVEMENTS

Since the prior 2008 Transit Master Plan was adopted, three major transit system improvements have been implemented that provide a backbone to the city's transit service:

- **SMART Central at Wilsonville Station** was constructed to act as SMART's main transportation hub and includes a 400 space park and ride lot, twelve bus bays, a new facility with an operator break room and public restrooms, shelters, and a clock tower with security cameras.
- **TriMet's Westside Express Service (WES) Commuter Rail** service began operating out of its new station located adjacent to the SMART Central at Wilsonville Station transit center.
- **SMART Bus Routes** changed to coordinate with WES train departures and arrivals.
- **SMART Operations Center** was built to house fleet and operations facilities, including administration offices, maintenance bays, and a bus parking area.

**FIGURE 4-5. TRANSIT SERVICE COVERAGE GAPS**



complementary travel modes and often used as part of the same trip. Some of the most important locations for access improvements include the Town Center Loop area and the Barber Street connection between Villebois Village and the SMART Central transit center. Other needs throughout the city should be addressed on an ongoing basis.

- **New Buses** are needed for SMART to maintain a quality transit fleet. Many of its buses are aging and require a greater amount of maintenance to keep them in operation. SMART can lower the amount of its budget that it spends on maintenance costs by replacing these buses. Additional buses will also be needed as growth occurs throughout the city. When possible, new buses should use alternative fuels, such as compressed natural gas. This will help SMART to reduce fuel costs and help meet regional and statewide goals for reducing greenhouse gas emissions.
- **Development Review** should address transit needs to ensure that transit users are accommodated as new development occurs in the city. SMART should be involved in the development review process to ensure that existing transit stops are improved and new stops, amenities or routes are provided as needed. In addition, when a new employment or commercial development occurs near a major transit stop, it should locate its building close to the transit stop.
- **Rider Education and Outreach** are ongoing needs that support and encourage transit ridership. One particular area where improvement is needed is adapting to new technology. This includes passenger access to ‘real time’ transit data and improved on-board amenities. Rider safety education is also an ongoing need.

## ENVIRONMENTAL JUSTICE

As stated by the Environmental Protection Agency, “Environmental Justice is the fair treatment and meaningful involvement of all people regardless of race, color, national origin, or income with respect to the development, implementation, and enforcement of environmental laws, regulations, and policies” (U.S. EPA, Environmental Justice, Compliance and Enforcement, Website, 2007).

Within the context of the TSP, Environmental Justice is an effort to identify underserved and vulnerable populations so the City can improve transportation services while reduce future inequalities. Two areas of particular need are Charbonneau (due to the higher proportion of elderly residents) and a small area on the southern edge of Villebois (due to lower income housing).

## SAFETY NEEDS

While there are no high-collision locations within Wilsonville, various safety-related deficiencies exist. Figure 4-6 shows five primary locations where there are existing safety concerns. Topography, roadway curvature, and nearby barriers (including I-5 and the railroad track) are key contributors.



*The railroad bridge over Grahams Ferry Road has limited horizontal and vertical clearance. This creates a safety hazard, particularly for bicyclists, pedestrians, and freight traffic.*



## RAIL NEEDS

The primary rail-related deficiency in Wilsonville is the limited vertical and horizontal clearance that the railroad bridge over Grahams Ferry Road causes for trucks. This is also a safety deficiency.

ODOT Rail has a policy of not granting new at-grade crossings. Crossings may be relocated (i.e., a new one is provided but only if an old one is removed). Therefore, railroad tracks can pose a significant barrier to the transportation system due to the high cost of grade separated crossings. The primary location in Wilsonville where the railroad contributes to a roadway system gap is the potential Kinsman Road extension in the northwest quadrant (see the prior Multimodal Connectivity Gaps discussion in this chapter).

Another future item that may affect Wilsonville is that ODOT Rail is studying the feasibility of improving intercity rail service between Eugene and Portland (with the potential for developing a high-speed rail line). Portland and Western's Oregon Electric rail



*Portland and Western's Oregon Electric rail line runs north/south through Wilsonville and serves as an important freight and commuter rail corridor. However, it also creates a barrier to travel for other modes due to limited crossing locations.*

line, which runs through Wilsonville, is one of the existing rail alignments being studied. Depending on the outcome of this study, there may be additional passenger rail trains traveling through Wilsonville that would increase gate down time and rail related congestion for all modes of travel.

## AIR NEEDS

The City of Wilsonville has no direct jurisdictional control or responsibility for managing the Aurora Airport. However, the City, concerned citizens, and local businesses have participated in the Oregon Department of Aviation's (ODA) development of an updated Master Plan for the airport. The City acknowledges the adoption of the Master Plan by ODA and will continue to monitor planned improvements at the airport and coordinate with ODA and Marion County, who have jurisdictional responsibilities.

The City also has two, potentially conflicting interests that must be balanced related to the airport. These include noise sensitivity for city residents and the reliance local businesses have on the airport for corporate travel.

## WATER NEEDS

The City of Wilsonville has no direct jurisdictional control or responsibility for managing activities on the Willamette River. However, it supports efforts by Corps of Engineers to maintain the following two activities, which are essential for the river to function over time as a viable transportation facility:

- Periodic dredging to maintain channel depth to support applicable river traffic
- Maintenance of the Locks at Oregon City

## PIPELINE SYSTEM

A high-pressure natural gas mainline pipe exists in the vicinity of the Interstate-5 corridor. The location of this pipeline may impact a project's feasibility or limit available improvement options in its vicinity.

## TRANSPORTATION SYSTEM MANAGEMENT AND OPERATIONS NEEDS

Transportation System Management and Operations (TSMO) improvements include integrated operations solutions that incorporate advanced technologies.

Due to the regional significance of TSMO improvements, Clackamas County and Metro have prepared their own plans. Some key needs include:

- **Arterial Corridor Management** for Boones Ferry Road, Elligsen Road, 65<sup>th</sup> Avenue, Wilsonville Road, and Stafford Road to improve reliability and traveler information along the corridors. Arterial Corridor Management includes installing fiber optic cable to allow communication with the ODOT/County Transportation Management and Operations Center as well as other intelligent transportation devices such as variable message signs, CCTV cameras, traveler information and adaptive traffic signal systems.
- **Transportation Demand Management (TDM)** by supporting the SMART Options Program, which works with Wilsonville area employers and residents to promote transit and other transportation options that reduce traffic congestion, such as carpool, vanpool, bike, walk, and telecommute.
- **Regional Fiber Network Connections** between Wilsonville's traffic signals and Clackamas County's fiber network (Clackamas County currently maintains and operates the City's traffic signals on its behalf).

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*"We have a new beautiful interchange with much more capacity, but we don't want to use up the capacity just to get from one side of town to the other."*

*Ben Altman, Chair  
Planning Commission*

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- **Adaptive Signal Timing** and associated video monitoring cameras and vehicle detection equipment (to collect traffic counts and speeds) on Wilsonville Road from Brown Road to Town Center Loop East.
- **Closed Circuit Television Cameras** at the key locations along Wilsonville Road and I-5.
- **Video Monitoring Cameras and Vehicle Detection Equipment** (to collect traffic counts and speeds) on Elligsen Road from Day Road to Canyon Creek Road.
- **Railroad Crossing Alert System** at Portland and Western at-grade railroad crossings.

### RECENT TSMO PROJECTS

Through a collaborative effort by Wilsonville, Clackamas County, and ODOT, the following TSMO projects have already been implemented:

- **Wilsonville Road Traffic Signal Communications** were improved as part of the Wilsonville Road Interchange Improvements to help manage traffic operations.
- **I-5 Interchange Area CCTV Cameras** were installed by ODOT and linked to the ODOT Trip Check website to provide real time information to drivers traveling within and through Wilsonville.
- **Discover Wilsonville** was a one-year program to make sure every Wilsonville resident has all the information they need to use whatever travel options interest them.
- **Sunday Streets** was a special event focusing on connecting neighborhoods, parks, and people. Bicyclists, walkers, runners, seniors, adults, and children enjoyed traffic-free streets filled with physical activities, fun and

## ALTERNATIVE FUEL NEEDS

Within Wilsonville and throughout the Portland Metro area, there is an increasing need to provide infrastructure to support vehicles that use alternative fuels (i.e., electrical and compressed natural gas vehicles). These vehicles help to reduce greenhouse gas emissions and are becoming more popular and affordable. SMART already has a compressed natural gas fueling station that it will use for its bus fleet.

The City could consider identifying various electrical vehicle stations at strategic locations that serve both residential and business users. Level II charging stations (input voltage of 240 volts, which requires two to four hours for charging) already exist at City Hall (2 stations) and the Fred Meyer parking lot (2 stations). Additional locations that may be considered for Level II charging stations are the SMART Central transit center and Town Center Loop.

The City of Wilsonville could also take advantage of its location at the southern tip of the Portland Metropolitan area to install (or coordinate with a willing business to install) a Level III (480 volt) fast charging station, which require only 20 to 40 minutes to complete the charge. An ideal location would be near one of the I-5 interchanges.

Another option to be ready for the transition to electric transportation would be to include provisions in residential, commercial, and industrial building codes for supporting the required infrastructure. It would be less expensive to require new buildings and parking lots to have the required electrical wiring and outlets to support future electric vehicle charging stations than it would be to retrofit older buildings and parking lots. By taking this preliminary step in preparing its infrastructure, a smoother transition could be made to alternative fuels for vehicles.



*Electric vehicle charging stations, such as those located at Fred Meyer (shown above) and Wilsonville City Hall (shown below), allow patrons, employees, and visitors to charge their vehicles while working, shopping, and visiting Wilsonville.*





Wilsonville is responsible for managing an efficient and effective transportation system that supports the quality of life of its residents and the economic vitality of its businesses. This is no easy task, but the City can succeed by implementing programs and projects that provide three primary benefits:

- Reduce rush hour traffic
- Improve operations and safety
- Make strategic investments in new and expanded facilities to serve all modes.

Wilsonville should be engaged in these three activities simultaneously through a balanced effort of programs and projects to receive the greatest value from its infrastructure expenditures. This balanced approach can also guard against over-building roadway capacity.

The list of transportation projects that will repair or complete the transportation system through 2035 is based largely on past plans, but includes updated solutions. Constructing all of the identified transportation solutions would cost approximately \$218.2 million, which exceeds \$123.4 million, which is forecasted to be available through 2035 from both City and other funding sources. Therefore, Wilsonville must choose how to invest its limited resources to provide the greatest benefit to Wilsonville residents and businesses. The highest priority solutions to meet the most important transportation system needs are included in the “Higher Priority” project list, while all other projects are included in the “Planned” project list.

*Wilsonville will . . .*

- *Improve system efficiency,*
- *Reduce congestion, and*
- *Save money*

*By implementing programs and projects that . . .*

1. *Reduce rush hour traffic,*
2. *Improve operations and safety, and*
3. *Make strategic investments in new and expanded facilities to serve all modes*



## SYSTEM IMPROVEMENT PRIORITIES

Most of the transportation system improvement projects needed to address gaps and deficiencies in the system were identified in prior City plans, including its 2003 Transportation Systems Plan, 2006 Bicycle and Pedestrian Master Plan, 2008 Transit Master Plan, and multiple development master plans (see Chapter 1: The Context). The City's prior transportation projects were reconsidered, integrated, and revised to address updated information and prepare for the 2035 planning horizon.

Because transportation funding is limited, Wilsonville recognizes the importance of being fiscally responsible in managing and improving its transportation system. The diagram at right illustrates cost-effective steps and associated solution areas to resolving transportation needs by following a multimodal, network-wide approach. These five steps were considered from top to bottom when evaluating Wilsonville's transportation projects:

- **Manage** the performance of congested locations with strategies that reduce traffic conflicts, increase safety, and encourage more efficient usage of the transportation system. Intersection operational improvements are considered to fall under this category.
- **Reduce** the driving demand at congested locations by ensuring safe and available walking, biking, and transit options.
- **Revisit** land use decisions and congestion thresholds to support shorter driving trips or modified travel decisions.
- **Extend** streets to increase connectivity and create parallel routes that reduce the driving demand on congested facilities.
- **Expand** existing streets or intersections to increase the driving capacity of congested facilities.

## COST-EFFECTIVE STEPS TO RESOLVING TRANSPORTATION NEEDS

FIGURE 5-1. IMPROVEMENT PRIORI-



*“We want to create a transportation system that has multiple choices . . . That way we are not heavily reliant on the car, which will still stay a key element to the system. But we want to make sure we are providing options for bicycles, pedestrians, and transit.”*

*Ben Altman, Chair  
Planning Commission*

## PRIORITIZED SOLUTION AREAS

As illustrated in Figure 5-1, the City can best manage its transportation system by having plans, programs, and/or projects that address each of the following solution areas:

1. **Transportation System Management and Operations (TSMO)** strategies that improve the safety and efficiency of the current system, including Transportation Demand Management (TDM)
2. **Bicycle, Pedestrian, and Transit** system improvements that target key system gaps and safely accommodate all transportation users
3. **Land Use and Development Strategies** that (1) provide equal accessibility and connectivity to those users who choose to travel by transit, bicycle, and pedestrian modes and (2) utilize the City's functional classification hierarchy to reduce out-of-direction travel and manage congestion on arterials
4. **Connectivity** improvements that include motor vehicle, pedestrian, bicycle, and transit facilities to provide more direct routes for all transportation users between neighborhoods, schools, parks, and retail/industrial areas
5. **Motor Vehicle Capacity** improvements upon a demonstration that the other strategies are not appropriate or cannot adequately address identified transportation needs

General preference should be given to those listed first, but only to the degree to which they are more cost-effective at supporting the City's vision and goals (i.e., a transportation system that is safe, connected and accessible, functional and reliable, cost effective, compatible, robust, and promotes livability). Many of the City's projects include elements that address multiple solutions.

## PROJECT EVALUATION PROCESS

Wilsonville's transportation improvement projects were also evaluated and prioritized to help select which projects to include in the Higher Priority project list. Many projects had been evaluated and prioritized in recently adopted mode-specific transportation plans. As a result, the TSP evaluation process varied for the different modes:

- **Motor Vehicle Projects:** The projects were ranked according to a point-based technical scoring methodology using evaluation criteria consistent with the City's transportation goals. This allowed for a consistent method to understand how well the projects would meet the City's transportation goals and policies. In addition, community input was considered when prioritizing the projects.
- **Bicycle, Pedestrian, and Transit Projects:** The project priorities in the 2006 Bicycle and Pedestrian Master Plan and 2008 Transit Master Plan were reviewed, and a few changes were made based on City staff and public input. The majority of the higher priority bicycle and pedestrian projects were included in the Higher Priority project list, even if it would require them to be constructed separately from associated motor vehicle projects.

Prioritizing the projects in this way allowed for them to be separated into two lists: the "Higher Priority" project list includes the highest priority solutions to meet the City's most important transportation system needs, while the "Additional Planned" project list includes all of the other projects.

## HIGHER PRIORITY PROJECTS

The “Higher Priority” project list includes the recommended projects reasonably expected to be funded through 2035. These are the highest priority solutions to meet the City’s most important needs. These projects will inform the City’s yearly budget and 5-year Capital Improvement Plan (CIP). As shown in Table 5-1, the Higher Priority projects would cost a total of \$118.0 million, which is consistent with forecast available funding through 2035.

Figures 5-2 through 5-6 show locations of the projects, and corresponding project details are included in Tables 5-1 through 5-5 (project numbering is alphabetical). Some of the City’s Higher Priority projects are not associated with a specific location but instead will be applied citywide as needed. These projects are listed in Table 5-6. Additional project details are included in the appendix (where they are sorted by project type).

**Table 5-1. Higher Priority Project Costs<sup>a</sup>**

Project Type	2011 Cost Estimate
Roadway Extensions	\$55,255,000
Roadway Widening	\$20,000,000
Urban Upgrades	\$58,355,000
Spot Improvements	\$3,950,000
Standalone Bicycle and Pedestrian Improvements	\$16,520,000
Transit Improvements	\$500,000
<b>Total Higher Priority Project Costs</b>	<b>\$154,580,000</b>

<sup>a</sup> See Tables 5-2, 5-3, 5-4, 5-5, and 5-6 for individual project costs.

### PROJECT TYPES

**RE – Roadway Extensions (Multimodal Connectivity):**

New transportation facilities in Wilsonville will connect neighborhoods to one another and to other important destinations. Many of the bicycle and pedestrian improvements related to roadway extensions will fill important system gaps so that neighborhoods have improved non-motorized connectivity, while roadway extension projects are the key motor vehicle improvements that provide increased connectivity in Wilsonville. The roadway extensions help the City to meet the one-mile arterial and half-mile collector spacing standards, consistent with City and regional policy.

**RW – Roadway Widening (Capacity):** The roadway widening projects increase roadway capacity.

**UU – Urban Upgrades (Multimodal Connectivity and Safety):** The urban upgrade projects complete existing roadways, and often improve connectivity by adding bike lanes, sidewalks, and turn lanes that accommodate access to adjacent neighborhoods.

These projects improve the roadways to meet the City’s cross-section standards.

**SI – Spot Improvements (Transportation System Management and Operations):** Spot improvements consist of isolated intersection improvements and safety improvements throughout the city.

**BW, SR, LT, and RT – Standalone Bicycle and Pedestrian Improvements (Multimodal Connectivity and Safety):** While many bicycle and pedestrian facilities will be constructed as elements of roadway extension and widening projects, there are a number of projects that the City should construct separately or as part of future development. These include the highest priority bikeways/walkways (**BW**), Safe Routes to School projects (**SR**), local trails (**LT**), and regional trails (**RT**).

**TI – Transit Improvements:** Transit projects are needed throughout the city to provide bus stop amenities and improve bicycle and pedestrian access to

**FIGURE 5-2. HIGHER PRIORITY PROJECTS**

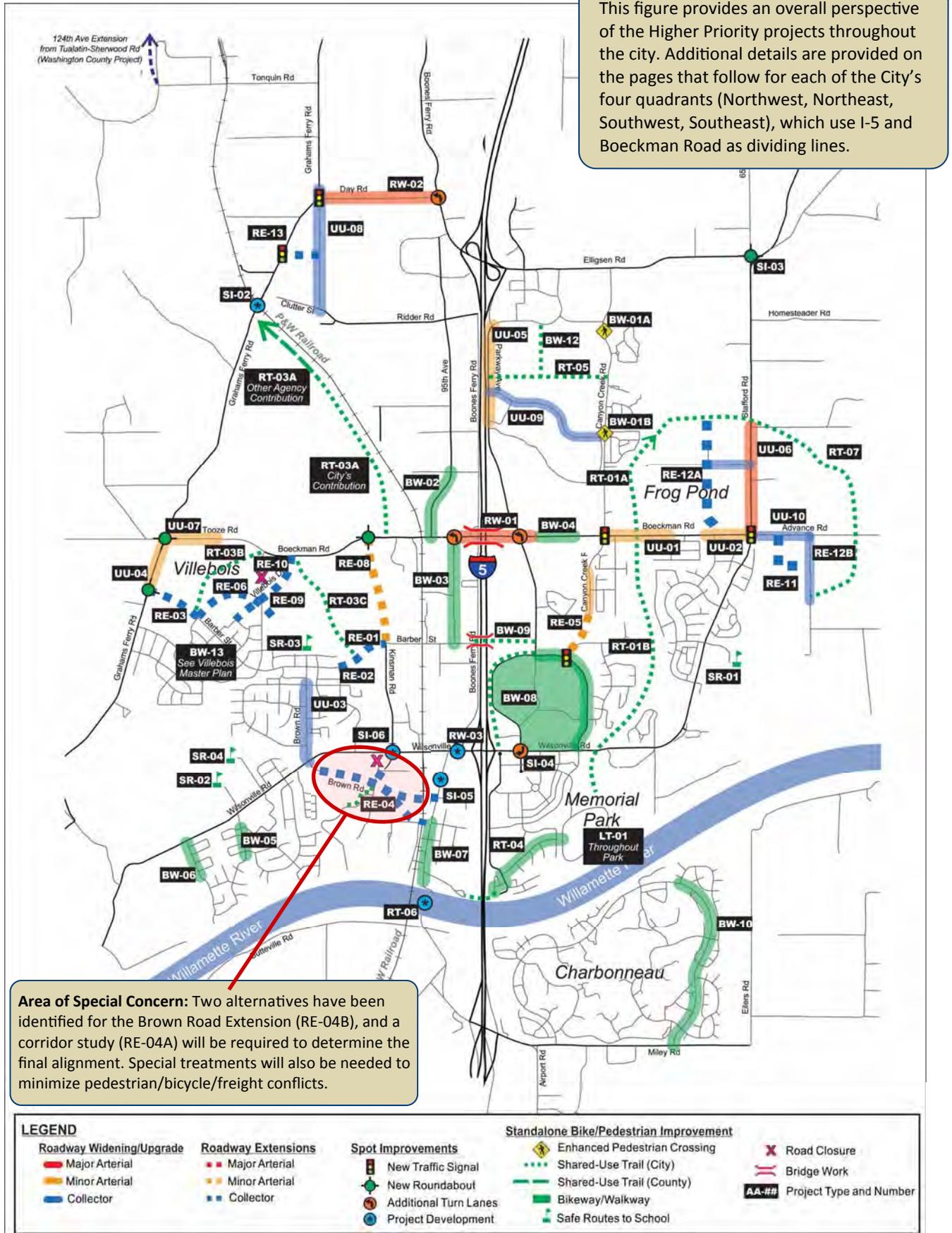


Table 5-2. Higher Priority Projects (Northwest Quadrant)

Project	Description	Cost
<b>Roadway Extensions</b>		
RE-13 Java Road Connection and Signal	Construct Java Road from Boones Ferry Road to Grahams Ferry Road and Garden Acres Road with a signal at the Java Road/Grahams Ferry Road intersection and disconnect Clutter Street from Grahams Ferry Road.	\$1,500,000
<b>Urban Upgrades</b>		
UU-08 Garden Acres Road Urban Upgrade	Upgrade Garden Acres Road to a three-lane collector with bicycle lanes and upgrade the Garden Acres Road/Day Road intersection to either a signal or a roundabout. Realign Ridder Road to Garden Acres Road. Close the existing Clutter Road connection to Grahams Ferry Road after completion of Project RE-13. Close the existing Coffee Creek Correctional Facility driveway to Grahams Ferry Road and relocate the driveway to Cahalin Road.	\$14,260,000
<b>Roadway Widening</b>		
RW-02 Day Road Widening	Widen Day Road from Boones Ferry Road to Grahams Ferry Road to include additional travel lanes in both directions along with bike lanes and sidewalks; project includes improvements at the Day Road/Boones Ferry Road intersection.	\$5,900,000
<b>Spot Improvements</b>		
SI-02 Grahams Ferry Railroad Undercrossing Project Development	Perform preliminary analysis to determine needs, feasibility, etc.	\$500,000
<b>Standalone Pedestrian and Bicycle Improvements (Bikeways and Walkways)</b>		
BW-02 95th Avenue Sidewalk Infill	Fill in gaps in the sidewalk network on the east side of 95th Avenue from Boeckman Road to Hillman Court, and construct transit stop improvements.	\$85,000
<b>Standalone Pedestrian and Bicycle Improvements (Regional Trails)</b>		
RT-03A Ice Age Tonquin Trail (North)	Construct sections of the Ice Age Tonquin Trail north of Boeckman Road; City to construct portion within City limits (approximately \$750,000) and coordinate portion farther north with Washington County and neighboring cities.	\$2,040,000 (Partial Regional funding)

**FIGURE 5-3. HIGHER PRIORITY PROJECTS (NORTHWEST QUADRANT)**

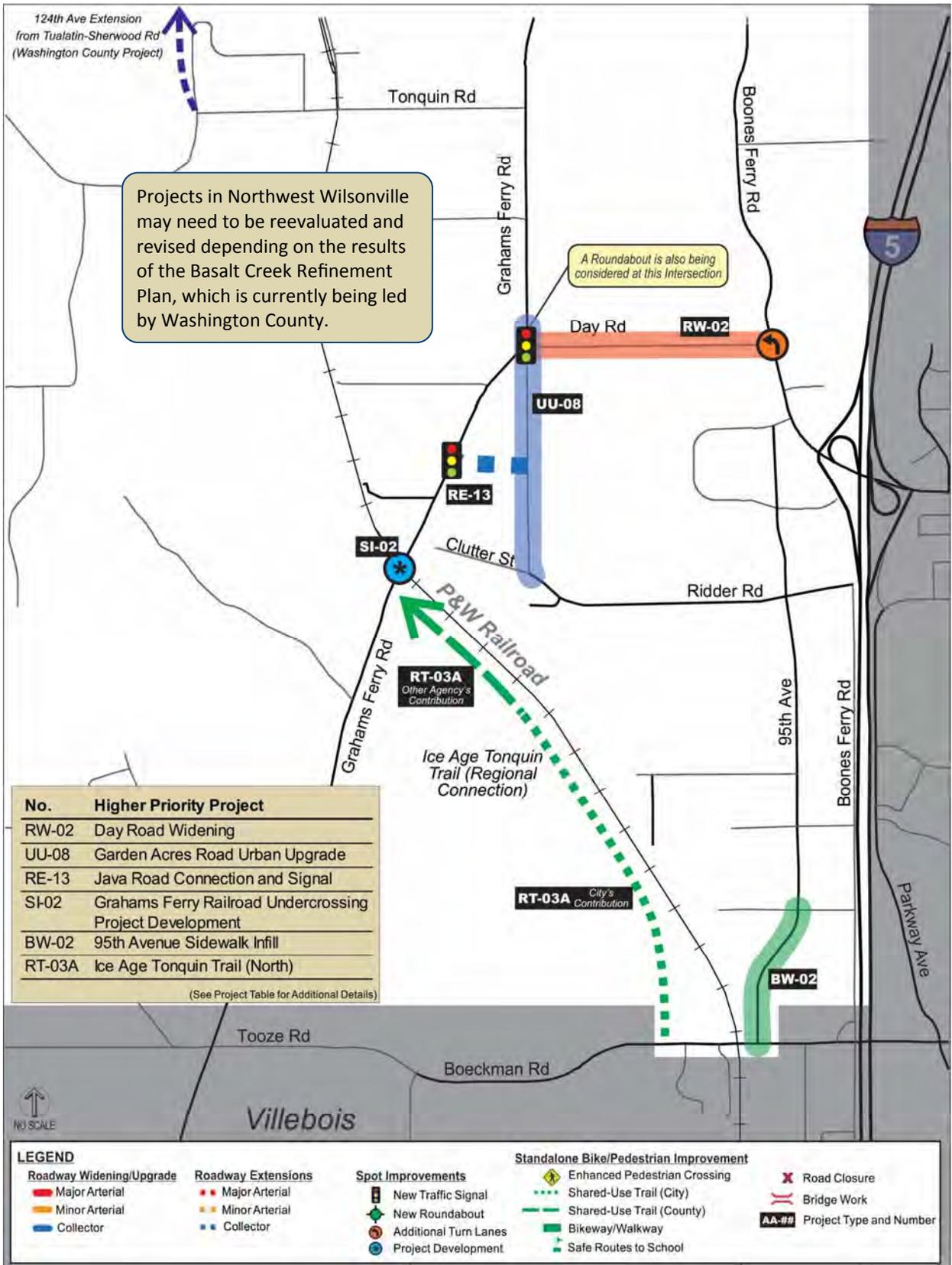


Table 5-3. Higher Priority Projects (Northeast Quadrant)

Project	Description	Cost
<b>Roadway Extensions</b>		
RE-11 Meridian Creek Middle School Site Improvements	Construct the collector roadways and site improvements associated with the proposed Meridian Creek Middle School site	\$1,600,000
RE-12A Frog Pond West Neighborhood Collector Roads	Construct the collector roadways within the west neighborhood as identified in the Frog Pond Area Plan	\$9,510,000
RE-12B Frog Pond South Neighborhood Collector Roads	Construct the collector roadways within the south neighborhood as identified in the Frog Pond Area Plan	\$2,650,000
<b>Roadway Widening</b>		
RW-01 Boeckman Road Bridge and Corridor Improvements	Widen Boeckman Road from Boberg Road to 500 feet east of Parkway Avenue to include additional travel lanes in both directions along with bike lanes and sidewalks; project includes reconstruction of the bridge over I-5 and improvements at Boeckman Road/Boberg Road and Boeckman Road/Parkway Avenue intersections and adjacent transit stops	\$13,600,000
<b>Urban Upgrades</b>		
UU-01 Boeckman Road Dip Improvements	Upgrade at vertical curve east of Canyon Creek Road to meet applicable cross-section standards (i.e., 3 lanes with bike lanes, sidewalks, and transit stop improvements); options should also be considered to make connections to the regional trail system and to remove the culvert and install a bridge	\$12,220,000
UU-02 Boeckman Road Urban Upgrade	Upgrade to meet applicable cross-section standards (i.e., 3 lanes with bike lanes, sidewalks, and transit stop improvements); project includes a traffic signal or roundabout at the Boeckman Road-Advance Road/Stafford Road-Wilsonville Road Intersection	\$2,100,000
UU-05 Parkway Avenue Urban Upgrade	Upgrade to meet applicable cross-section standards (i.e., 3 lanes with bike lanes, sidewalks, and transit stop improvements)	\$5,000,000
UU-06 Stafford Road Urban Upgrade	Upgrade to meet applicable cross-section standards (i.e., 3 lanes with bike lanes, sidewalks, and transit stop improvements)	\$4,200,000
UU-09 Printer Parkway Urban Upgrade	Upgrade Printer Parkway to a three-lane collector with bicycle lanes and multiuse path	\$3,600,000
UU-10 Advance Road Urban Upgrade	Upgrade Advance Road to collector standards starting at Stafford Road to the proposed 63 <sup>rd</sup> Avenue (entrance to proposed Meridian Creek Middle School)	\$3,175,000
<b>Spot Improvements</b>		
SI-03 Stafford Road/65th Avenue Intersection Improvements	Improve turn radii, sight distance and grade differential by combining intersections as either a roundabout or traffic signal	\$2,000,000 (Partial County funding)
<b>Standalone Pedestrian and Bicycle Improvements (Bikeways and Walkways)</b>		
BW-01 Canyon Creek Road Enhanced A/B Pedestrian Crossings	Install two new pedestrian crossings of Canyon Creek Road that include rectangular rapid flashing beacons (RRFBs), center pedestrian median island, signage, etc. (final locations to be determined)	\$130,000
BW-04 Boeckman Road Bike Lanes and Sidewalk Infill	Construct bike lanes (both sides of street) and sidewalks (south side of street) from Parkway Avenue to Canyon Creek Road	\$515,000
BW-12 Parkway Center Trail Connector	Construct shared-use path as development occurs; with connection to proposed regional trail (Wiedeman Road Trail) on the south	\$120,000
<b>Standalone Pedestrian and Bicycle Improvements (Regional Trails)</b>		
RT-01A Boeckman Creek Trail (North)	Construct north-south trail through east Wilsonville following Boeckman Creek, with connections to neighborhoods, parks, and intersecting roads (may need a boardwalk for various sections and would require a comprehensive public process)	\$850,000
RT-05 Wiedeman Road Trail	Construct east-west trail in north Wilsonville near the Xerox campus with City responsible for portion through developed land and future developer responsible for portion on future development site	\$340,000
RT-07 Revised Frog Pond Regional Trail	Construct the regional trail identified in the Frog Pond Area Plan	\$700,000

**FIGURE 5-4. HIGHER PRIORITY PROJECTS (NORTHEAST QUADRANT)**

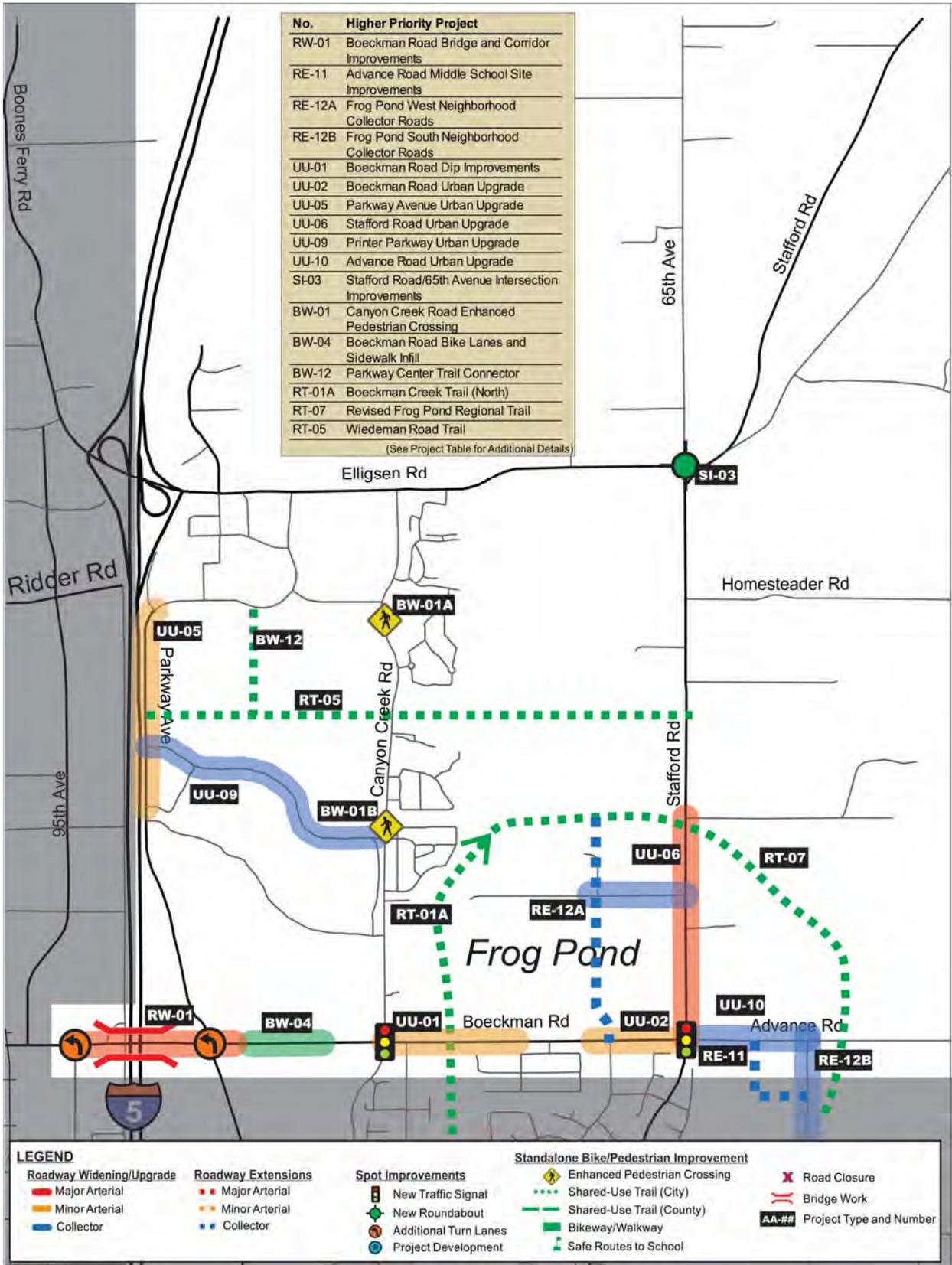


Table 5-4. Higher Priority Projects (Southwest Quadrant)

Project	Description	Cost
<b>Roadway Extensions</b>		
RE-01 Barber Street Extension	Construct 2-lane roadway with bridge, bike lanes, sidewalks, and transit stop improvements from Kinsman Road to Coffee Lake Drive to facilitate access and circulation to WES Station and Villebois	\$8,315,000
RE-02 Barber Street Extension (Part 2)	Construct remaining 2-lane roadway with bike lanes, sidewalks, and transit stop improvements from Coffee Lake Drive to Montebello Drive to facilitate access and circulation to WES Station and Villebois	\$400,000
RE-03 Barber Street through Villebois	Construct remaining 2-lane roadway with bike lanes, sidewalks, and transit stop improvements from Monte Carlo Avenue to Grahams Ferry Road	\$520,000
RE-04A Corridor Study for Brown Road Extension	Perform a corridor study to determine the recommended Brown Road extension alignment (i.e., connection at either Bailey Street or 5th Street)	\$20,000
RE-04B Brown Road Extension	Construct remaining 2-lane roadway with bike lanes, sidewalks, and transit stop improvements from Wilsonville Road to Boones Ferry Road (connect at either Bailey Street or 5th Street); includes roadway connection to Kinsman Road (with bike lanes and sidewalks), portion of Ice Age Tonquin Trail connecting to trial terminus on Arrowhead Creek Lane, and Brown Road/Kinsman Road intersection	\$15,200,000
RE-06 Costa Circle Loop Extension	Construct remaining 2-lane roadway with bike lanes, sidewalks, and transit stop improvements from Barber Street to Villebois Drive to Mont Blanc Street	\$3,000,000
RE-08 Kinsman Road Extension (South)	Construct 2-lane roadway with bike lanes, sidewalks, and transit stop improvements from Barber Street to Boeckman Road; project also includes a roundabout at Kinsman Road/Boeckman Road intersection	\$8,400,000
RE-09 Villebois Drive Extension	Construct 2-lane roadway with bike lanes, sidewalks, and transit stop improvements from Costa Circle to Coffee Lake Drive	\$390,000
RE-10 Villebois Drive Extension (Part 2)	Construct 2-lane roadway with bike lanes, sidewalks, and transit stop improvements from Coffee Lake Drive to Boeckman Road	\$250,000
<b>Roadway Widening</b>		
RW-03 Widen Wilsonville Road East of Boones Ferry Road	Widen eastbound SW Wilsonville Road east of SW Boones Ferry Road by removing the center median. This project involves lane configuration analysis to best address congestion.	\$500,000
<b>Urban Upgrades</b>		
UU-03 Brown Road Upgrades	Upgrade to meet cross-section standards (i.e., 3 lanes with bike lanes, sidewalks, and transit stops)	\$3,500,000
UU-04 Grahams Ferry Urban Upgrade	Upgrade to meet cross-section standards (i.e., 3 lanes with bike lanes, sidewalks, and transit stop improvements); includes roundabout at Grahams Ferry Road/Barber Street intersection	\$2,400,000
UU-07 Tooze Road Urban Upgrade	Upgrade to meet cross-section standards (i.e., 3 lanes with bike lanes, sidewalks, and transit stop improvements); includes roundabout at Grahams Ferry Road/Tooze Road intersection	\$7,900,000
<b>Spot Improvements</b>		
SI-05 Curb Extension Removal on Boones Ferry Road	Remove curb extension and add an additional northbound through lane on SW Boones Ferry Road starting at the southern SW Boones Ferry Road/Fred Meyer access and ending at the SW Boones Ferry Road/SW Wilsonville Road intersection where the curbside through lane will terminate into the existing right turn lane.	\$200,000
SI-06 Truck Turning Improvements SW Kinsman Road	Rebuild the northwest corner of the Wilsonville Road/Kinsman Road intersection to accommodate truck turning movements and improve pedestrian safety. Requires right-of-way acquisition, widening, pedestrian ramp replacement, and traffic signal pole relocation.	\$750,000
<b>Standalone Pedestrian and Bicycle Improvements (Bikeways and Walkways)</b>		
BW-03 Boberg Road Sidewalk Infill	Fill in gaps in the sidewalk network on the east side of the roadway from Boeckman Road to Barber Street, and construct transit stop improvements	\$375,000
BW-05 Willamette Way East Sidewalk Infill	Fill in gaps in the sidewalk network on the west side of the roadway from Chantilly to south of Churchill (part of Ice Age Tonquin Trail)	\$50,000
BW-06 Willamette Way West Sidewalk Infill	Construct a new sidewalk on west side of the roadway from Wilsonville Road to Paulina Drive	\$50,000
BW-07 Boones Ferry Road Sharrows	Stripe sharrows (shared travel lanes) from 5th Street to Boones Ferry Park; this will connect Ice Age Tonquin Trail (once the portion along the Brown Road Extension is completed) to Waterfront Trail	\$5,000
BW-13 Villebois Loop Trail	Construct shared-use path as part of Villebois development; include connections to Villebois Greenway, the Ice Age Tonquin Trail, and the Village Center	\$180,000
<b>Standalone Pedestrian and Bicycle Improvements (Safe Routes to School)</b>		
SR-02 Boones Ferry Primary Safe Routes to School Improvements	Construct shared-use path between Boones Ferry Primary and Wood Middle School, a bicycle parking shelter near the school, and a shared-use path connecting the bicycle shelter to the sidewalks along Wilsonville Road	\$200,000
SR-03 Lowrie Primary Safe Routes to School Improvements	Construct shared-use path from existing connection of Lowrie Primary School to Barber Street as part of Villebois development; include connections to new school, Ice Age Tonquin Trail, and Barber Street To future connections	\$150,000
SR-04 Wood Middle School Safe Routes to School Improvements	Construct a bicycle parking shelter near the school and a shared-use path connecting the bicycle shelter to the sidewalks along Wilsonville Road; also widen and stripe the Park at Merryfield Trail, which connects Wood Middle School to Camelot Street to the north	\$150,000
<b>Standalone Pedestrian and Bicycle Improvements (Regional Trails)</b>		
RT-03 Ice Age Tonquin Trail B/C (Villebois)	Construct the remaining sections of the Ice Age Tonquin Trail within Villebois Village in conjunction with development and adjacent roadway improvements	\$560,000
RT-06 Willamette River Bike/Pedestrian and Emergency Bridge Project Development	Perform feasibility study and project development for bike/pedestrian/emergency bridge over the Willamette River to provide a non-motorized alternative to the I-5 freeway deck	\$1,380,000 (Partial Regional funding)

FIGURE 5-5. HIGHER PRIORITY PROJECTS (SOUTHWEST QUADRANT)

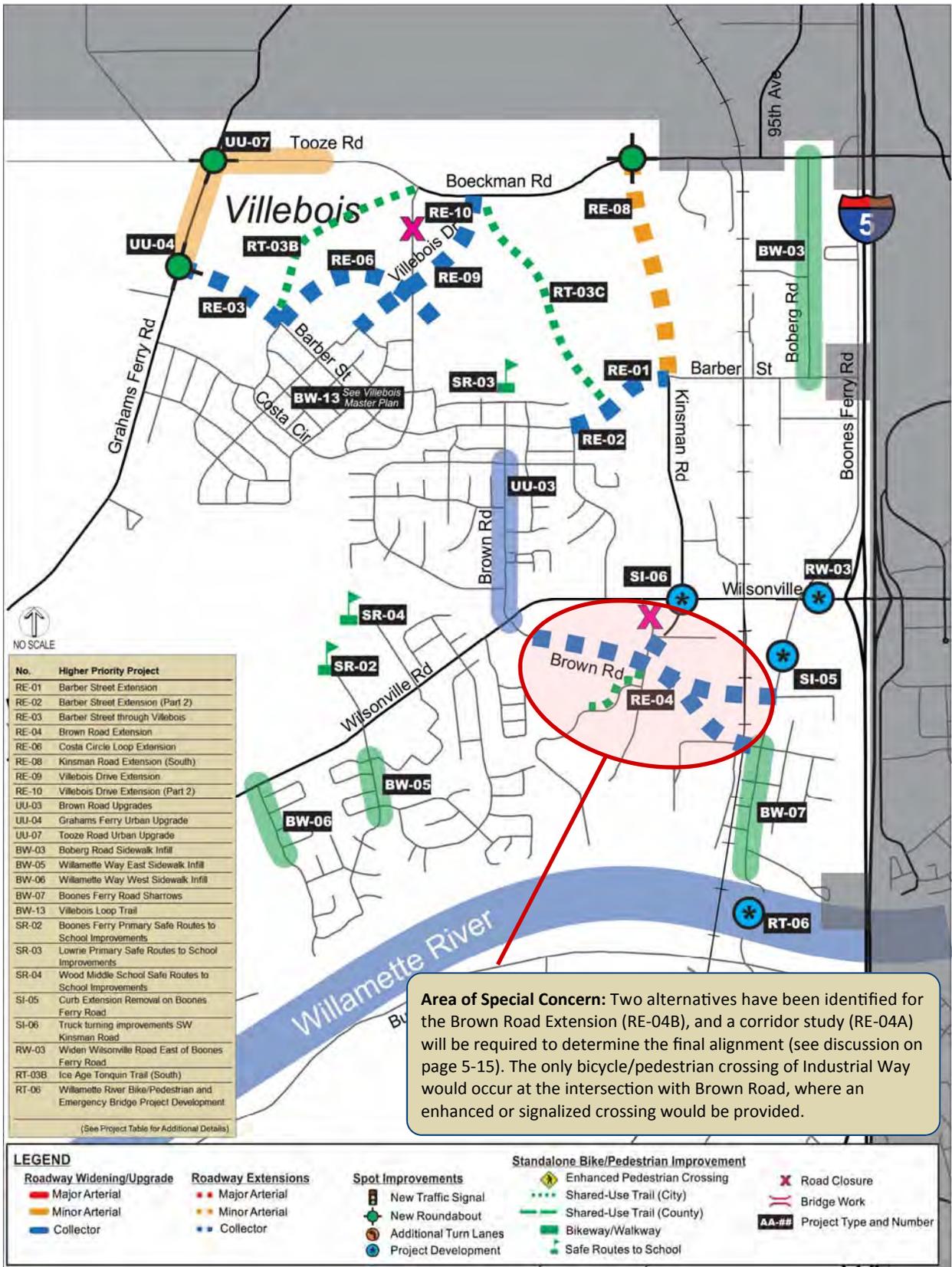
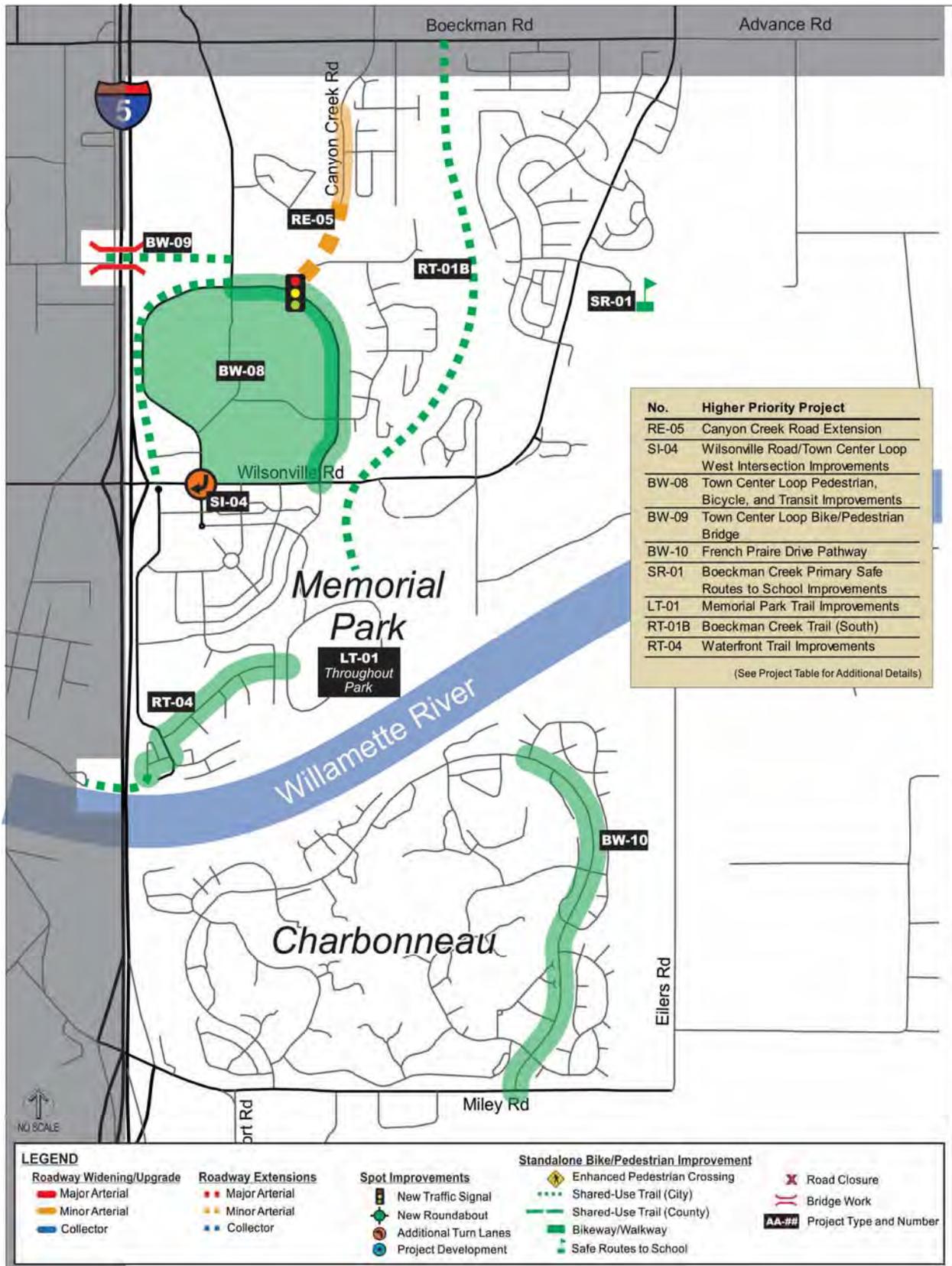


Table 5-5. Higher Priority Projects (Southeast Quadrant)

Project	Description	Cost
<b>Roadway Extensions</b>		
RE-05 Canyon Creek Road Extension	Construct remaining 3-lane roadway with bike lanes, sidewalks, and transit stop improvements from existing terminus to Town Center Loop East; project also includes realigning a portion of Vlahos Drive (so it intersects Canyon Creek Road) and installing a traffic signal at the Town Center Loop East/Canyon Creek Road intersection	\$3,500,000
<b>Spot Improvements</b>		
SI-04 Wilsonville Road/Town Center Loop West Intersection Improvements	Widen the north leg of the intersection and install a second southbound right-turn lane (dual lanes)	\$500,000
<b>Standalone Pedestrian and Bicycle Improvements (Bikeways and Walkways)</b>		
BW-08 Town Center Loop Pedestrian, Bicycle, and Transit Improvements	Create more direct connections between destinations within Town Center area, improve accessibility to civic uses and transit stops, retrofit sidewalks with curb ramps, highlight crosswalks with colored pavement, and construct other similar treatments that support pedestrian, bicycle, and transit access and circulation; also construct shared-use path along Town Center Loop West from Wilsonville Road to Parkway Avenue and restripe Town Center Loop East from Wilsonville Road to Parkway Avenue to a three-lane cross-section with bike facilities	\$500,000
BW-09 Town Center Loop Bike/Pedestrian Bridge	Construct bike/pedestrian bridge over I-5 approximately aligned with Barber Street to improve connectivity of Town Center area with businesses and neighborhoods on west side of I-5; include aesthetic design treatments	\$4,000,000
BW-10 French Prairie Drive Pathway	Construct 10-foot wide shared-use path along French Prairie Drive from Country View Lane to Miley Road or reconfigure existing roadway to remove a travel lane in each direction and add bicycle and pedestrian facilities	\$1,140,000
<b>Standalone Pedestrian and Bicycle Improvements (Safe Routes to School)</b>		
SR-01 Boeckman Creek Primary Safe Routes to School Improvements	Construct a bicycle parking shelter near the school and a new 10 to 12-foot bike path on the south side of the existing sidewalk that meanders south of the tree line and connects to the existing marked crosswalk near the school parking lot	\$65,000
<b>Standalone Pedestrian and Bicycle Improvements (Local Trails)</b>		
LT-01 Memorial Park Trail Improvements	Construct trails throughout Memorial Park, including the Memorial Park Center Loop Trail, the River Trail, Kolbe Homestead Trail, and Klein Homestead Trail	\$595,000
<b>Standalone Pedestrian and Bicycle Improvements (Regional Trails)</b>		
RT-01B Boeckman Creek Trail (South)	Construct north-south trail through east Wilsonville following Boeckman Creek, with connections to neighborhoods, parks, and intersecting roads (may need a boardwalk for various sections and would require a comprehensive public process)	\$1,150,000 (Partial Regional funding)
RT-04 Waterfront Trail Improvements	Improve the condition of the shared-use path as it passes underneath the I-5 Boone Bridge by removing the Jersey barriers, installing bollards, widening the trail, adding appropriate pedestrian features such as benches and lighting, and altering the grade of the path underneath the underpass to make it more easily accessible	\$125,000

**FIGURE 5-6. HIGHER PRIORITY PROJECTS (SOUTHEAST QUADRANT)**



**Table 5-6. Higher Priority Projects (Citywide)**

Project	Description	Cost
<b>Standalone Pedestrian and Bicycle Improvements (Bikeways and Walkways)</b>		
BW-14 Wayfinding Signage	Provide bicycle, pedestrian, and transit wayfinding signage directing users to/from the Ice Age Tonquin Trail, the SMART and WES transit center, and other points of interest throughout the city	\$65,000
BW-15 Property Acquisitions for Bike/Ped Connectivity	Provide set-aside funds to allow purchase of strategically located properties that can facilitate bicycle and pedestrian connections as these properties become available.	\$1,000,000
<b>Transit Improvements</b>		
TI-01 Pedestrian Access to Transit	Construct sidewalk and curb ramp improvements at SMART stops throughout the city to meet ADA requirements, create safe street crossings, and connect new development with transit (includes retrofits at substandard stops)	\$200,000
TI-02 Transit Street Improvements	Widen roadways or construct sidewalk extensions on a case-by-case basis to improve transit on-time performance and passenger/pedestrian safety; may involve on-site bus turnarounds with project owner approval	\$300,000

Table 5-7 provides a side-by-side comparison of the estimated funding sources available and how much they would contribute to the Higher Priority projects. Additional cost information is provided in the

appendix. The planning level project costs are intended to cover a moderate level of unanticipated costs that may arise at the time the projects are constructed.

**Table 5-7. Higher Priority Project Funding Sources and Contributions**

Project Type	Capital Improvement Funding Estimates through 2035	
	Approximate Funding Available	Contributions to Higher Priority Projects
Street System Development Charges (SDCs)	\$72 million	\$68.6 million
West Side Plan – Urban Renewal District	\$27 million	\$26.6 million
Year 2000 Plan – Urban Renewal District	\$5 million	\$3.5 million
Park System Development Charges (SDCs)	\$0.7 million <sup>a</sup>	\$0.7 million
Local/Regional Partnerships	\$2.9 million <sup>a</sup>	\$2.9 million
Grants	\$3.2 million <sup>a</sup>	\$3.2 million
State and Federal Funding	\$12.6 million <sup>a</sup>	\$12.6 million
<b>Total</b>	<b>\$123.4 million<sup>a</sup></b>	<b>\$118.1 million</b>

<sup>a</sup> The approximate funding levels estimated for various sources were considered to be equal to the contributions due to the prior experience of how the City has been able to fund transportation projects. If the City is unable to obtain local/regional partnerships, grants, and/or state and federal funding, then the associated projects that assume these funding sources may have to be put on hold until other funding becomes available.

## BROWN ROAD EXTENSION ALTERNATIVES

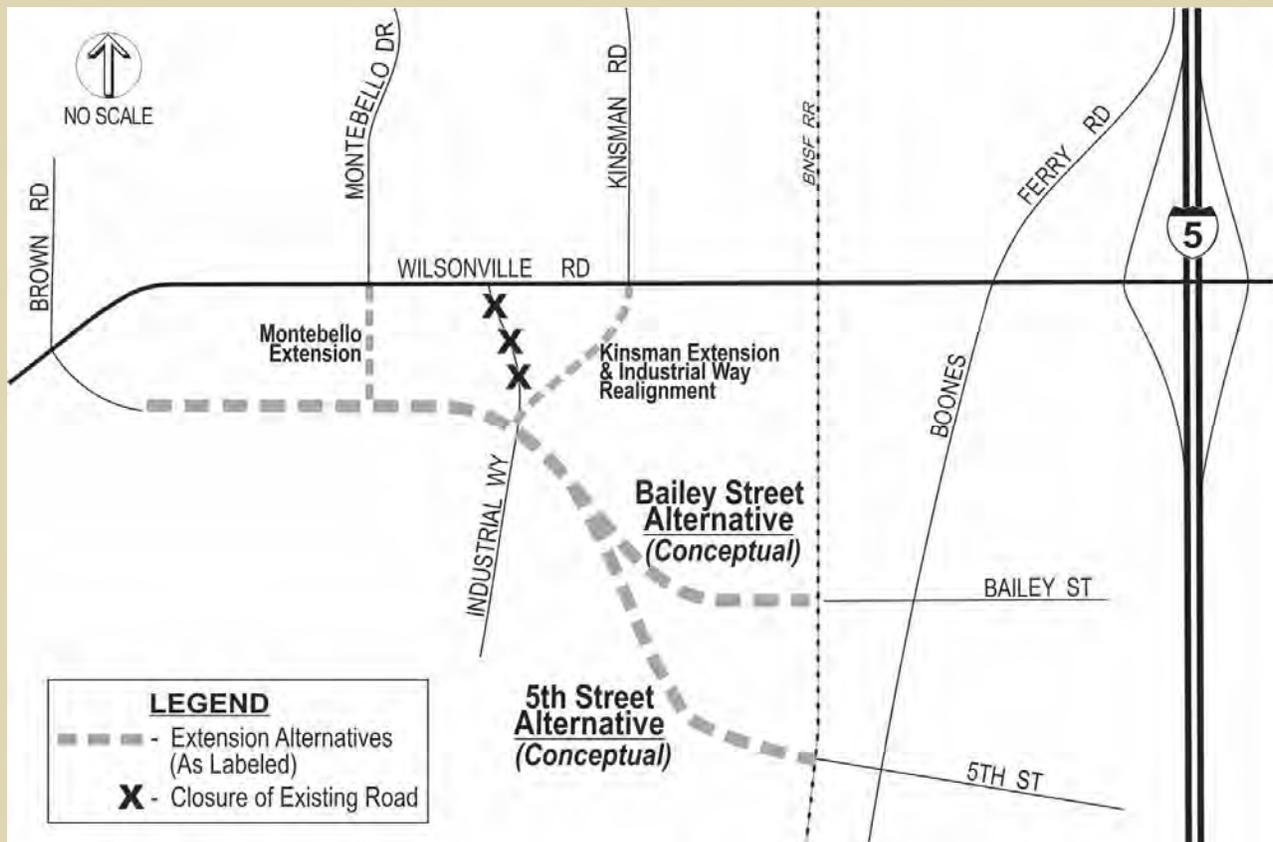
From a transportation planning standpoint, both Brown Road extension alternatives would provide comparable benefits to the transportation network. Selection of an alignment should be made during or prior to the master planning process for the large area south of Wilsonville Road and west of the railroad tracks.

The following factors should be considered as part of selecting a future alignment:

- Access
- Bicycle and pedestrian network connections
- Environmental impacts
- Freight benefits/impacts
- Future development plans and land use changes in the two areas most impacted by

the roadway extension: (1) west of the railroad tracks south of Wilsonville Road and (2) in Old Town, specifically along Boones Ferry Road

- Motor vehicle capacity
- Neighborhood/commercial connectivity
- Private property impacts
- Project costs
- Public input
- Railroad crossings
- Small business impacts
- Timing
- Traffic diversion
- Water and sewer utility issues



### ADDITIONAL PLANNED PROJECTS

The “Additional Planned” project list includes those projects that would contribute to the City’s desired transportation system through 2035 but that were not included as “Higher Priority” projects due to estimated funding limitations. This list represents a coordinated transportation network and adequate facilities to serve the community through 2035.

The State stipulates that projects listed in the TSP form the legal basis for exacting developer-provided improvements. Together, the “Higher Priority” and “Additional Planned” project lists document all the City’s desired projects so that it is clear what improvements are needed to ensure that the City’s transportation network fully supports its continued growth.

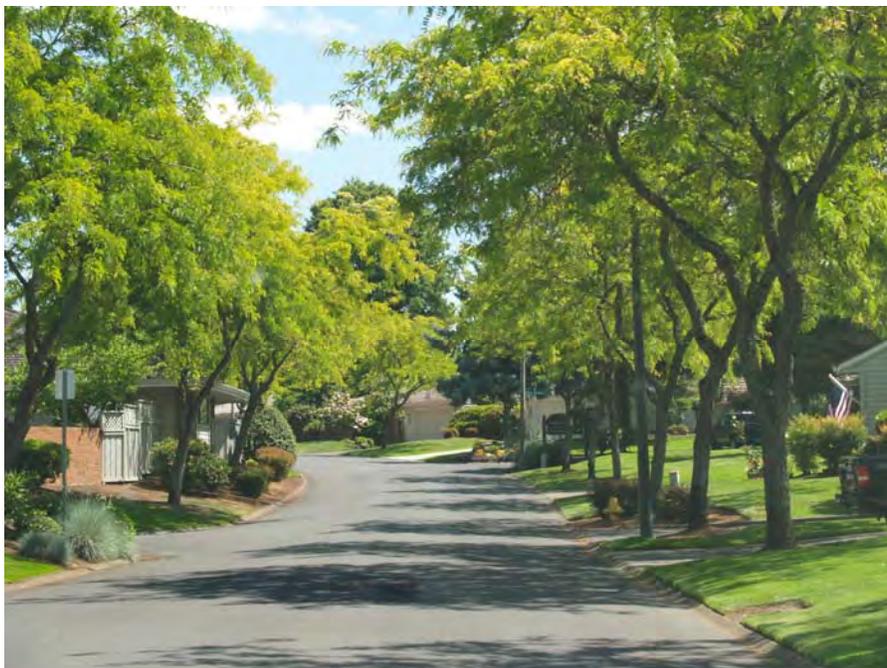
Even though the City should primarily focus on the projects included in the Higher Priority Solutions Package, it should look for opportunities to pursue these remaining projects as funding opportunities become available, including grant funding.

As shown in Table 5-8, the “Additional Planned” projects would cost a total of \$100.1 million. Figures 5-7 through 5-11 show locations of the projects, and corresponding project details are included in Tables 5-8 through 5-12. Some of the City’s Additional Planned projects are not associated with a specific location but instead will be applied citywide as needed. These projects are listed in Table 5-13.

**Table 5-8. Additional Planned Project Costs<sup>a</sup>**

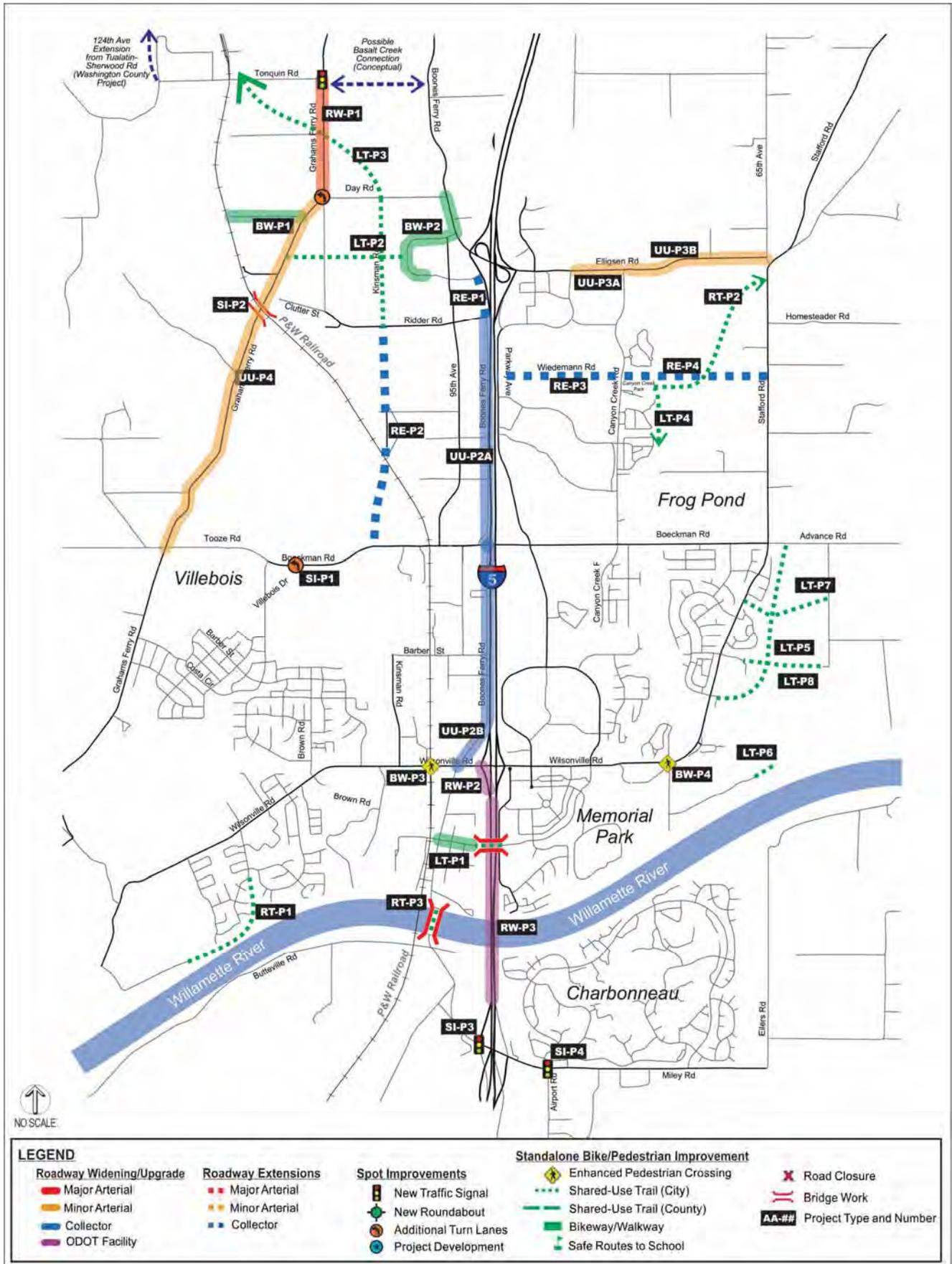
Project Type	2011 Cost Estimate
Roadway Extensions	\$27,200,000
Roadway Widening	\$8,280,000
Urban Upgrades	\$19,800,000
Spot Improvements	\$6,500,000
Standalone Bicycle and Pedestrian Improvements	\$25,610,000
Transit Improvements	\$14,450,000
<b>Total Additional Planned Project Costs</b>	<b>\$101,840,000</b>

<sup>a</sup> See Tables 5-9, 5-10, 5-11, 5-12, and 5-13 for individual project costs.



*Trees provide an aesthetically pleasing environment and shade along a street in Charbonneau, a private planned community in Wilsonville surrounding a 27-hole golf course. Because Charbonneau is on the southern bank of the Willamette River, it is separated from the remainder of the city and would benefit from a dedicated bicycle and pedestrian bridge.*

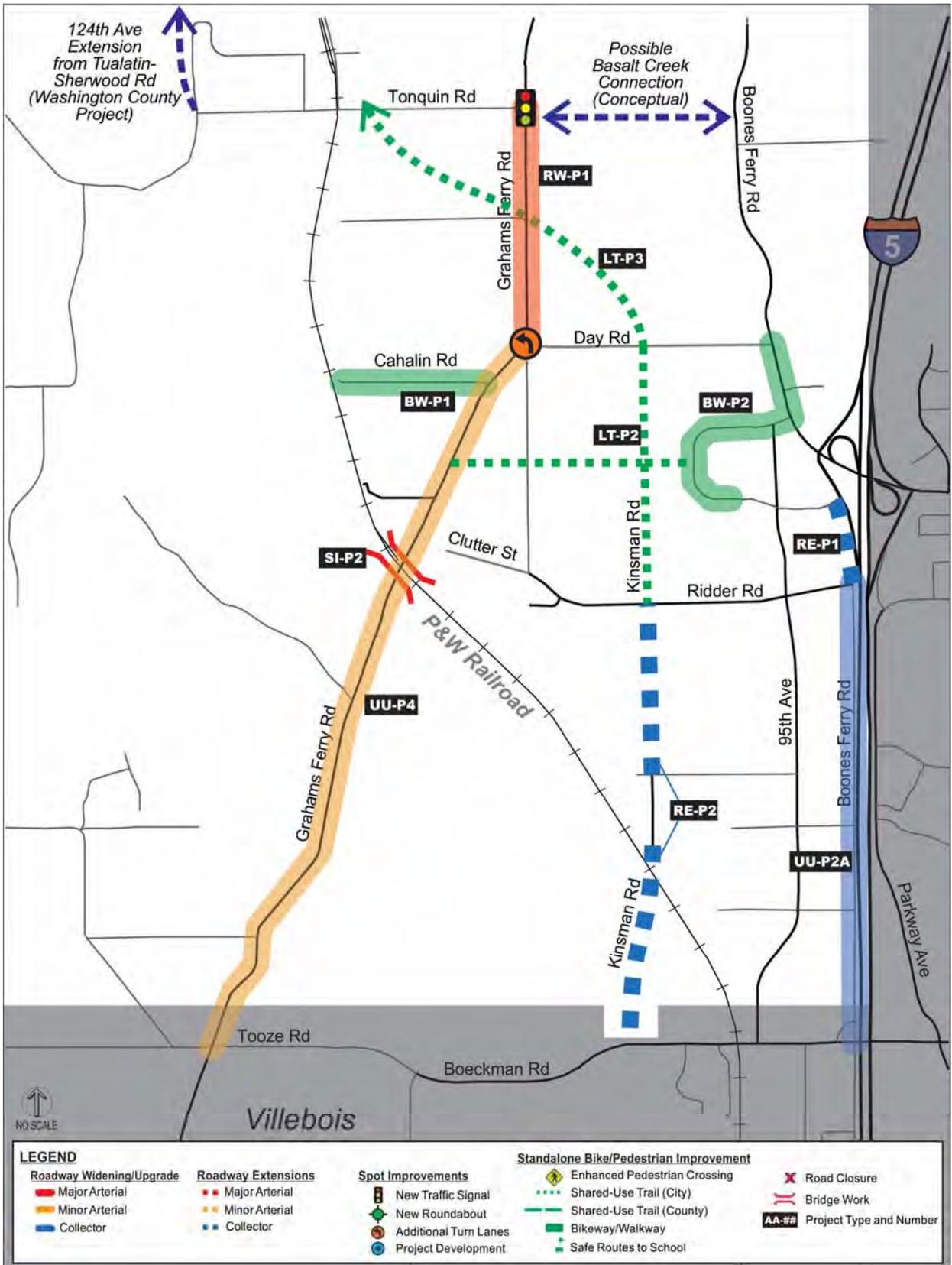
FIGURE 5-7. ADDITIONAL PLANNED PROJECTS



**Table 5-9. Additional Planned Projects (Northwest Quadrant)**

Project	Description	Why Not Higher Priority?	Cost
<b>Roadway Extensions</b>			
RE-P1 Boones Ferry Road Extension	Construct 2-lane roadway from Ridder Road to Commerce Circle with bike lanes, sidewalks, and transit improvements to facilitate access and circulation in the area surrounding Ridder Road and 95th Avenue	Identified as potentially helpful freight connection, but not a critical need at this time	\$2,100,000
RE-P2 Kinsman Road Extension (Central)	Construct 2/3-lane roadway from Boeckman Road to Ridder Road with bike lanes and sidewalks	High cost due to grade-separated RR crossing and construction across Metro lands; alternative route (95th Avenue) is available	\$12,000,000
<b>Roadway Widening</b>			
RW-P1 Grahams Ferry Road Widening	Widen Grahams Ferry Road from Tonquin Road to Day Road to four lanes with bike lanes, sidewalks, and transit improvements; acquire the full five-lane right-of-way width to accommodate future left-turn lanes; also provide additional left-turn lanes at Tonquin Road and Day Road intersections	Located within Washington County and is only needed under certain scenarios of the pending Basalt Creek Refinement Plan	\$7,000,000
<b>Urban Upgrades</b>			
UU-P2A Boones Ferry Road Urban Upgrade	Upgrade Boones Ferry Road from Wilsonville Road to Ridder Road with bike lanes on both sides and sidewalks on west side only	High cost with limited connectivity benefit alternative parallel routes exist	\$5,900,000
UU-P4 Grahams Ferry Road Urban Upgrade	Upgrade Grahams Ferry Road from Day Road to Tooze Road to meet applicable cross-section standards (i.e., 3 lanes with bike lanes, sidewalks, and transit improvements)	Grahams Ferry Road is primarily a rural road and Ice Age Tonquin Trail is a preferred option for providing north-south connection through this part of Wilsonville	\$2,000,000
<b>Spot Improvements</b>			
SI-P2 Grahams Ferry Road Undercrossing Improvements at Railroad Bridge	Reconstruct existing railroad under-crossing to City of Wilsonville Minor Arterial standards; Higher Priority project list includes project development portion of this project (costs are separate)	Located within Washington County jurisdiction, and it is an important safety-related project with particular benefits for freight travel; however, it comes with high cost and freight traffic has alternate travel routes	\$4,500,000
<b>Standalone Pedestrian and Bicycle Improvements (Bikeways and Walkways)</b>			
BW-P1 Cahalin Road Bike Lanes and Sidewalks	Construct bike lanes and sidewalks from Kinsman Road extension to Ice Age Tonquin Trail	High cost due to railroad crossing barrier	\$700,000
BW-P2 Commerce Circle Loop and Boones Ferry Road Sidewalk Infill	Fill in gaps in the sidewalk network on Commerce Circle Loop and Boones Ferry Road	Industrial area with no connectivity to other facilities	\$150,000
<b>Standalone Pedestrian and Bicycle Improvements (Local Trails)</b>			
LT-P2 Area 42 Trail	Shared Use Path from Kinsman Road to Day Road	To be constructed as Coffee Lake Creek Master Plan Area Redevelops	\$220,000
LT-P3 BPA Power Line Trail	Shared Use Path from Day Road to Ice Age Tonquin Trail providing trail users to City's northern industrial area	Ice Age Tonquin Trail provides key connection to north (more critical when Coffee Lake Creek develops)	\$500,000

**FIGURE 5-8. ADDITIONAL PLANNED PROJECTS (NORTHWEST QUADRANT)**



**Table 5-10. Additional Planned Projects (Northeast Quadrant)**

Project		Description	Why Not Higher Priority?	Cost
<b>Roadway Extensions</b>				
RE-P3	Wiedeman Road Extension (West)	Construct 2/3-lane roadway from Parkway Avenue to Canyon Creek Road with bike lanes and sidewalks	Limited impact on system capacity; money better spent upgrading Boeckman Road and Elligsen Road	\$4,300,000
RE-P4	Wiedeman Road Extension (East)	Construct 2/3-lane roadway from Canyon Creek Road to Stafford Road with bike lanes and sidewalks; would require construction over Boeckman Creek	Only needed with future development on land east of Canyon Creek Road; costly (especially over wetlands) and has limited impact on system capacity; and money better spent upgrading Boeckman Road and Elligsen Road	\$8,800,000
<b>Urban Upgrades</b>				
UU-P3 A/B	Elligsen Road Urban Upgrade	Upgrade Elligsen Road from Parkway Center to Stafford Road to meet applicable cross-section standards including bike lanes, sidewalks, and transit improvements	Much of the land is in Clackamas County; significant slopes from Parkway Center Drive to Canyon Creek Road would likely require retaining walls (higher costs) and large oak trees would be impacted	\$6,000,000 (Partial Federal funding)
<b>Standalone Pedestrian and Bicycle Improvements (Local Trails)</b>				
LT-P4	Canyon Creek Trail	Shared Use Path from Canyon Creek Park to Boeckman Creek Trail providing connectivity to neighborhoods to the south	Low priority as it needed after the Boeckman Creek Trail is constructed	\$200,000
<b>Standalone Pedestrian and Bicycle Improvements (Regional Trails)</b>				
RT-P2	Stafford Spur Trail	Shared-Use Path from Canyon Creek Park to Stafford Road	High cost project that provides limited connectivity to land uses in Clackamas County	\$1,640,000

**FIGURE 5-9. ADDITIONAL PLANNED PROJECTS (NORTHEAST QUADRANT)**

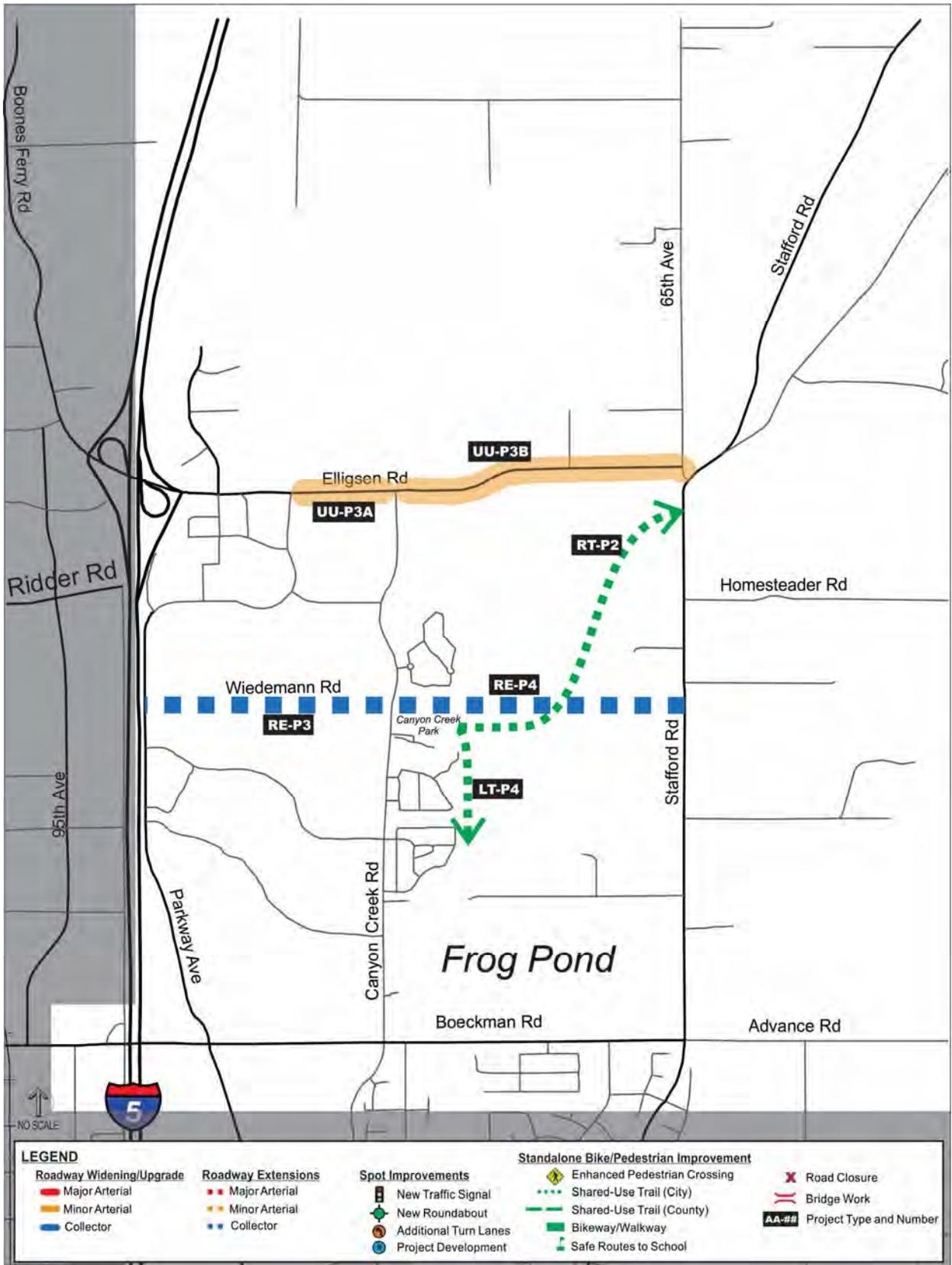
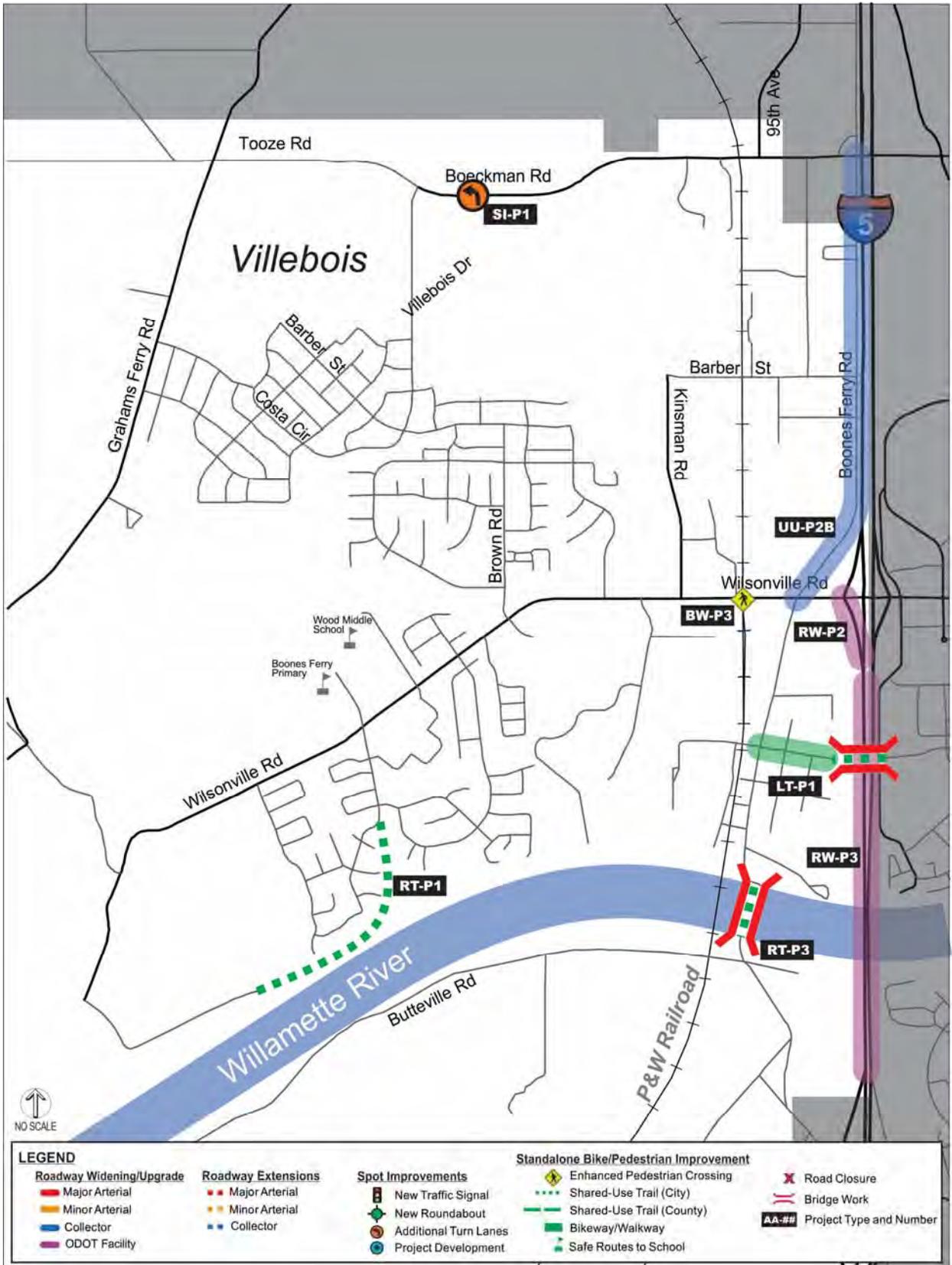


Table 5-11. Additional Planned Projects (Southwest Quadrant)

Project	Description	Why Not Higher Priority?	Cost
<b>Urban Upgrades</b>			
UU-P2B	Boones Ferry Road Urban Upgrade	Upgrade Boones Ferry Road from Wilsonville Road to Ridder Road with bike lanes on both sides and sidewalks on west side only	\$5,900,000
<b>Spot Improvements</b>			
SI-P1	Boeckman Road/Villebois Drive Roundabout Widening	Expand roundabout by adding a westbound slip lane to accommodate two westbound travel lanes on Boeckman Road	\$500,000
<b>Standalone Pedestrian and Bicycle Improvements (Bikeways and Walkways)</b>			
BW-P3	Wilsonville Road Enhanced Pedestrian Crossing at Railroad Track	Install new pedestrian crossing adjacent to the railroad tracks that includes rectangular rapid flashing beacons (RRFBs), center pedestrian median island, signage, etc.	\$70,000
<b>Standalone Pedestrian and Bicycle Improvements (Local Trails)</b>			
LT-P1	5th Street Bike/Pedestrian Bridge and Connections	Construct bike/pedestrian bridge over I-5 approximately aligned with 5 <sup>th</sup> Street; also construct bike lanes and sidewalks on 5 <sup>th</sup> Street connecting the new bridge to Boones Ferry Road	\$6,400,000
<b>Standalone Pedestrian and Bicycle Improvements (Regional Trails)</b>			
RT-P1	Rivergreen Trail	Natural Trail from Ice Age Tonquin Trail/SW Willamette Way to Waterfront Trail	\$260,000
RT-P3	Willamette River Bike/Pedestrian and Emergency Bridge	Construct bridge over Willamette River for bike, pedestrian, and emergency access to provide an alternative to the I-5 freeway deck; Higher Priority project list includes project development portion of this project (costs are separate)	\$14,000,000
<b>Roadway Widening</b>			
RW-P2	Additional Queuing Lane on Southbound I-5 Ramp	Construct a third queuing lane on the southbound I-5 ramp at the I-5/Wilsonville Road interchange.	\$1,280,000
RW-P3	Auxiliary Lane Across Boone Bridge	Construct a northbound auxiliary lane on I-5 beginning at the Charbonneau northbound entrance ramp and terminating just north of the Wilsonville Road Interchange.	N/A

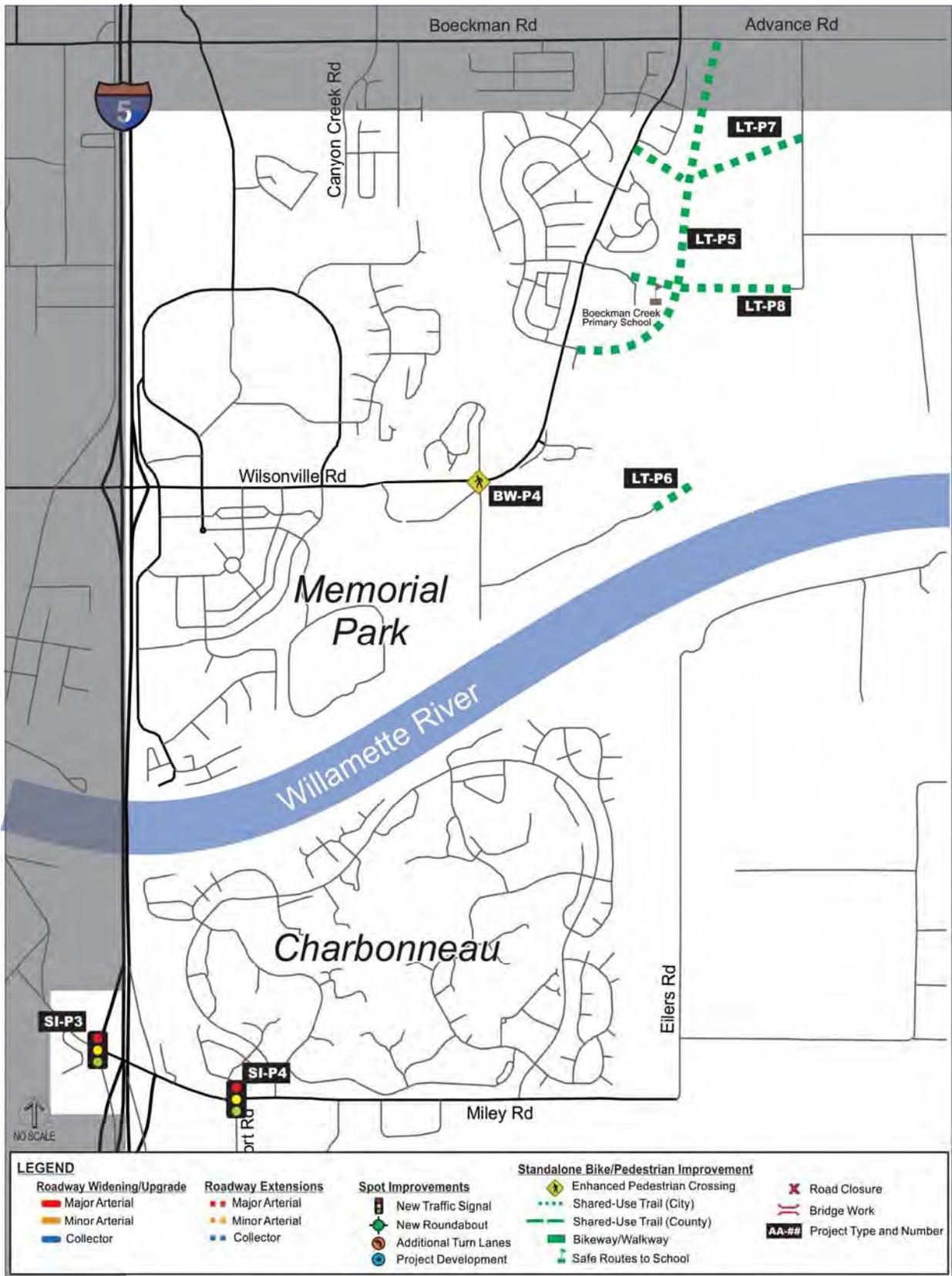
**FIGURE 5-10. ADDITIONAL PLANNED PROJECTS (SOUTHWEST QUADRANT)**



**Table 5-12. Additional Planned Projects (Southeast Quadrant)**

Project	Description	Why Not Higher Priority?	Cost	
<b>Spot Improvements</b>				
SI-P3	Miley Road/I-5 Southbound Ramp Improvements	Install traffic signal and southbound left-turn lane	Outside City's jurisdiction (ODOT facility) and no future Wilsonville growth expected; improvement needs would be triggered primarily by regional traffic	\$750,000
SI-P4	Miley Road/Airport Road Intersection Improvements	Install traffic signal and northbound left-turn lane	Outside City's jurisdiction (Clackamas County facility) and no future Wilsonville growth expected; improvement needs would be triggered primarily by regional traffic	\$750,000
<b>Standalone Pedestrian and Bicycle Improvements (Bikeways and Walkways)</b>				
BW-P4	Wilsonville Road Enhanced Pedestrian Crossing at Rose Lane	Install new pedestrian crossing adjacent to Rose Lane and nearby transit stops; potential crossing treatments include, but are not limited to, rectangular rapid flashing beacons (RRFBs), signage, etc.	Crossing need at this location is considered low at this time, and there is an existing pedestrian crossing and flasher to the west at Kolbe Lane that provides more direct access to Memorial Park and the Boeckman Creek Trail	\$50,000
<b>Standalone Pedestrian and Bicycle Improvements (Local Trails)</b>				
LT-P5	New School Site Trail	Shared Use Path from Boeckman Creek Elementary School to planned school and park site, with possible connections to adjacent neighborhoods	Medium priority due to existing connections; will become important when school and park are constructed	\$700,000
LT-P6	Park Access Trail	Low Volume Roadway accessed from Montgomery Way; would require extensive public process	Lower priority until after other critical trail and pathway connections are completed	\$20,000
LT-P7	School Connection Trail	Construct the School Connection Trail identified in the Frog Pond Area Plan	Medium priority due to existing connections; will become important when school and park are constructed	\$460,000
LT-P8	60 <sup>th</sup> Avenue Trail	Construct the 60 <sup>th</sup> Avenue Trail identified in the Frog Pond Area Plan	Medium priority due to existing connections; will become important when school and park are constructed	\$240,000

**FIGURE 5-11. ADDITIONAL PLANNED PROJECTS (SOUTHEAST QUADRANT)**



**Table 5-13. Additional Planned Projects (Citywide)**

Project		Description	Why Not Higher Priority?	Cost
<b>Spot Improvements</b>				
TI-P1	Bus Stop Amenities	Install bus shelters, benches, and bus seat poles on a case-by-case basis as needs are identified and funds are available	Funding has not been identified	\$450,000
TI-P2	SMART Buses	Replace old buses; also outfit each bus with a tracking system and provide real-time display boards at the SMART Central station and other key routes	Funding has not been identified	\$14,000,000

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*“It is very important we prepare now so that we don’t have congestion in the future—or can at least manage the congestion. We can also prepare for connectivity so we can get places conveniently.”*

*Nancy Kraushaar  
Community Development Director*

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# The Programs

## Chapter 6



Wilsonville's transportation programs play an important role in the City's ongoing efforts to provide a coordinated, cost-effective, multimodal transportation system. Well-run programs help extend the service life of infrastructure improvements and increase the value of transportation investments. The City's Community Development and SMART Transit departments are responsible for managing the majority of its transportation programs.

### TRANSPORTATION PROGRAMS

Wilsonville has various transportation programs that support ongoing operations and services:

- Capital Improvement Program (CIP)
- Safety (Proposed)
- Safe Routes to School
- ADA Comprehensive Access (Proposed)
- SMART Transit
- SMART Options and Transportation Demand Management (TDM)
- Intelligent Transportation System (ITS)
- Bike Smart and Walk Smart

*Instead of trying to . . .*

- *Build its way out of congestion*

*Wilsonville's programs help the City . . .*

- *Extend the service life of infrastructure improvements and*
- *Increase the value of transportation investments.*



## CAPITAL IMPROVEMENT PROGRAM

Wilsonville's Capital Improvement Program (CIP) is a short-range 5-year plan that identifies upcoming capital projects and equipment purchases, provides a planning schedule, and identifies financing options. It provides an important link between the projects identified in the City's master plans and its annual budget, which enables the City to manage and use public dollars in the most efficient and productive manner possible.

Through its annual CIP efforts, the City considers which capital investments enable it to manage growth to boost the economy, protect the environment and public health, and enhance community vitality while working to preserve the special qualities of life in Wilsonville.

Wilsonville uses its Capital Improvement Program (CIP) to plan and prioritize its infrastructure investments in eight categories:

- Water
- Sewer
- Streets
- Streetscape/Bicycle
- Stormwater
- Transit
- Buildings
- Parks

The CIP program includes a 5-year project list, which provides a short-range plan of upcoming infrastructure improvement needs. These projects include new facilities, major repairs, replacement and improvements of roads, buildings, water systems (sanitary, drinking, storm), and parks. The City regularly packages multiple capital projects together (such as roads, sewer, and water) to maximize the cost effectiveness of City funds.

## PUBLIC INVESTMENT BENEFITS

From clean, safe drinking water to convenient transportation options, the City's public investment funds an improved quality of life. Benefits of investment into the City's Capital improvement Program include:

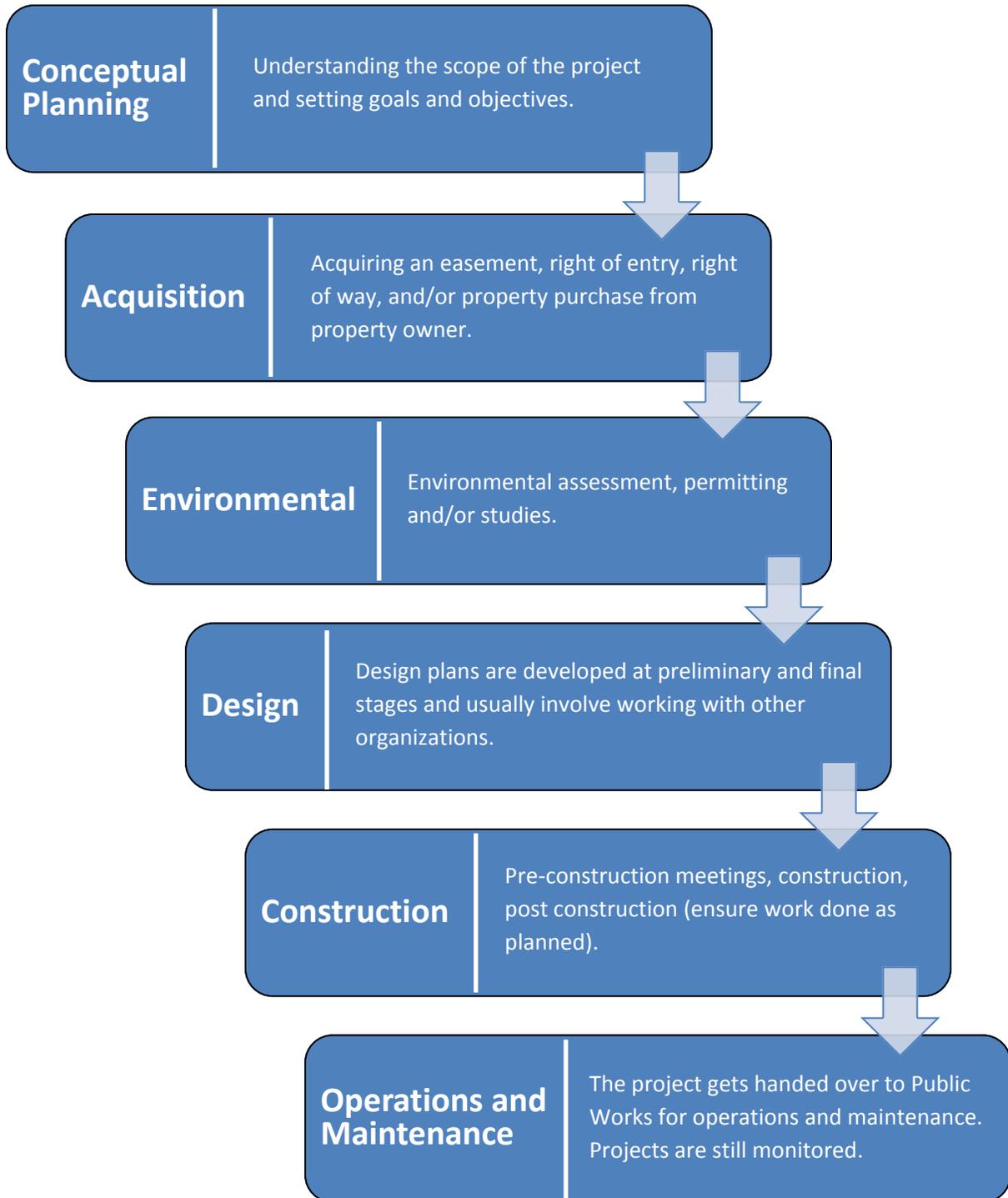
- Transportation facilities that provide capacity to support economic development
- Streets that are maintained and constructed to ensure safety and comfort for all users
- A multimodal transportation system that provides options to commuters and travelers
- Trails and green spaces that are maintained and enhanced, providing both wildlife habitat and a place for outdoor recreation
- Water and sewer maintenance and expansion for increased water quality, convenience and sanitation
- Stormwater improvements for safety and efficiency

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*“A city thrives when the vision for the community includes designing attractive, safe neighborhoods, protecting natural resources, stimulating economic growth, and maintaining existing infrastructure.”*

*Tim Knapp  
Mayor*

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**FIGURE 6-1. MULTIPLE STAGES OF CAPITAL IMPROVEMENT PROJECT PROCESS****Notes:**

- Stages of the project often occur simultaneously and include engagement of surrounding property owners.
- Projects are reviewed by other City departments, regional partners (such as ODOT and Metro), and consultants.
- Staff is held accountable to City Council throughout the life of the project.
- The City's website is a helpful tool for sharing project information with the public.

## SAFETY

Transportation safety is an important goal of Wilsonville's transportation system. To ensure the well being of residents, employees, and visitors, the City follows the most current safety practices for the design, construction, operation, and maintenance of its transportation facilities.

Many of the City's transportation standards and improvement projects provide safety benefits. Access management, multimodal connectivity, cross-section and other design standards, and capacity improvements all contribute to improve safety.

Wilsonville will also benefit from a safety program founded on the five E's, listed at right. Specific actions of the safety program would include the following:

- **Construct Safety-Related Infrastructure Improvements** as identified in Chapter 4: The Projects, including Safe Routes to School projects.
- **Prepare and Distribute Education Materials** that effectively convey the best safety practices for all travel modes.
- **Coordinate Education Efforts with Local Partners** including West Linn-Wilsonville School District (Safe Routes to School programs for each school), local businesses, and neighborhood groups. Particular benefits will be realized from educating youth, new users, and those who express interest.
- **Collaborate with Regional and State Partners** by (1) developing relationships with the ODOT, Clackamas County, Washington County, and Metro staff members who oversee their agencies' safety efforts; (2) communicating the City's needs and limitations to these agencies as applicable; and (3) seeking ways to benefit from

## FIVE E'S (SAFETY PROGRAM)

Wilsonville's Safety Program will be most effective by addressing the five E's identified by the Metro Regional Transportation Safety Plan:

- **Educate** transportation users of all ages about bicycle, pedestrian, transit, and traffic safety skills and laws
- **Emergency Medical Service (EMS)** providers are supported by a highly organized transportation and information system that ensures prompt notification of the location and severity of a crash, timely dispatch of trained emergency care providers, use of evidence-based treatment protocols, and triage to an appropriate health care facility
- **Engineer** a safe and efficient multimodal transportation system that meets the needs of all users
- **Enforce** traffic laws, particularly those relating to safety, such as speeding and cell phone use while driving
- **Evaluate** program periodically to measure performance and adjust efforts as needed

These five E's encompass a broad group of solutions administered by a wide variety of stakeholders responsible for making the transportation system safe for all users. There is a similar set of five E's for Safe Routes to School programs, but "EMS" is replaced with "Encouragement."

regional and state resources, information, training, and publicity campaigns.

- **Coordinate with Law Enforcement Officers** regarding the enforcement and reporting of traffic safety issues.

## REGIONAL, STATE, AND NATIONAL SAFETY PLANS

Regional, state, and national safety plans serve as a helpful resource for Wilsonville's safety program:

- **Toward Zero Deaths: A National Strategy on Highway Safety** is a data-driven effort by the Federal Highway Administration (FHWA) to enhance national, state, and local safety planning and implementation efforts in identifying and creating opportunities for changing American culture as it relates to highway safety
- **ODOT's 2011 Transportation Safety Action Plan (TSAP)** is the safety element of the Oregon Transportation Plan (OTP) and provides guidance for safety-related investment decisions, including helpful information for local agencies, such as Wilsonville
- **Metro's 2012 Regional Transportation Safety Plan (RTSP)** is a data-driven framework and urban-focused safety plan intended to help the region reduce fatalities and serious injury crashes by 50 percent by 2035 (as compared to 2005)
- **Clackamas County Transportation Safety Action Plan (TSAP)** outlines a strategy for the county to build and implement a county-wide safety culture with the goal of reducing transportation-related fatalities and serious injuries by 50 percent over the next ten years

These plans are helpful resources that support the City's efforts to improve transportation safety.

*Wilsonville residents take to the streets during the City's Sunday Streets event in August 2012.*

*This special event focused on connecting neighborhoods, parks, and people. Bicyclists, walkers, runners, seniors, adults, and children enjoyed traffic-free streets filled with fun and interactive educational demonstrations, entertainment, music, physical activities, and food.*



## SAFE ROUTES TO SCHOOL

Wilsonville is helping to facilitate Safe Routes to School (SRTS) programs to improve the transportation system in the neighborhoods around its each of its public schools, whose locations are shown in Figure 6-2. These programs also incorporate five E's (shown at right), which include a combination of ongoing educational and outreach efforts as well as pedestrian and bicycle infrastructure improvements along routes used by school children. Federal funding is available for these programs and is administered by the Oregon Department of Transportation (ODOT).

The SRTS programs are intended to reduce school-related traffic congestion and provide numerous additional benefits, including improved safety, increased physical activity and related health benefits, increased sense of community, and reductions in transportation-related air pollution. To be successful, these programs require the coordinated effort and support of school officials, parents, residents, City planning and engineering staff, and law enforcement agencies.

*Students use the crosswalk on Wilsonville Road at the Willamette Way East traffic signal to walk and bike to Boones Ferry Primary School.*



## FIVE E'S (SAFE ROUTES TO SCHOOL)

The most successful Safe Routes to School programs incorporate five E's (which are similar to the five E's identified for Wilsonville's Safety Program but the "EMS" is replaced by "Encourage"):

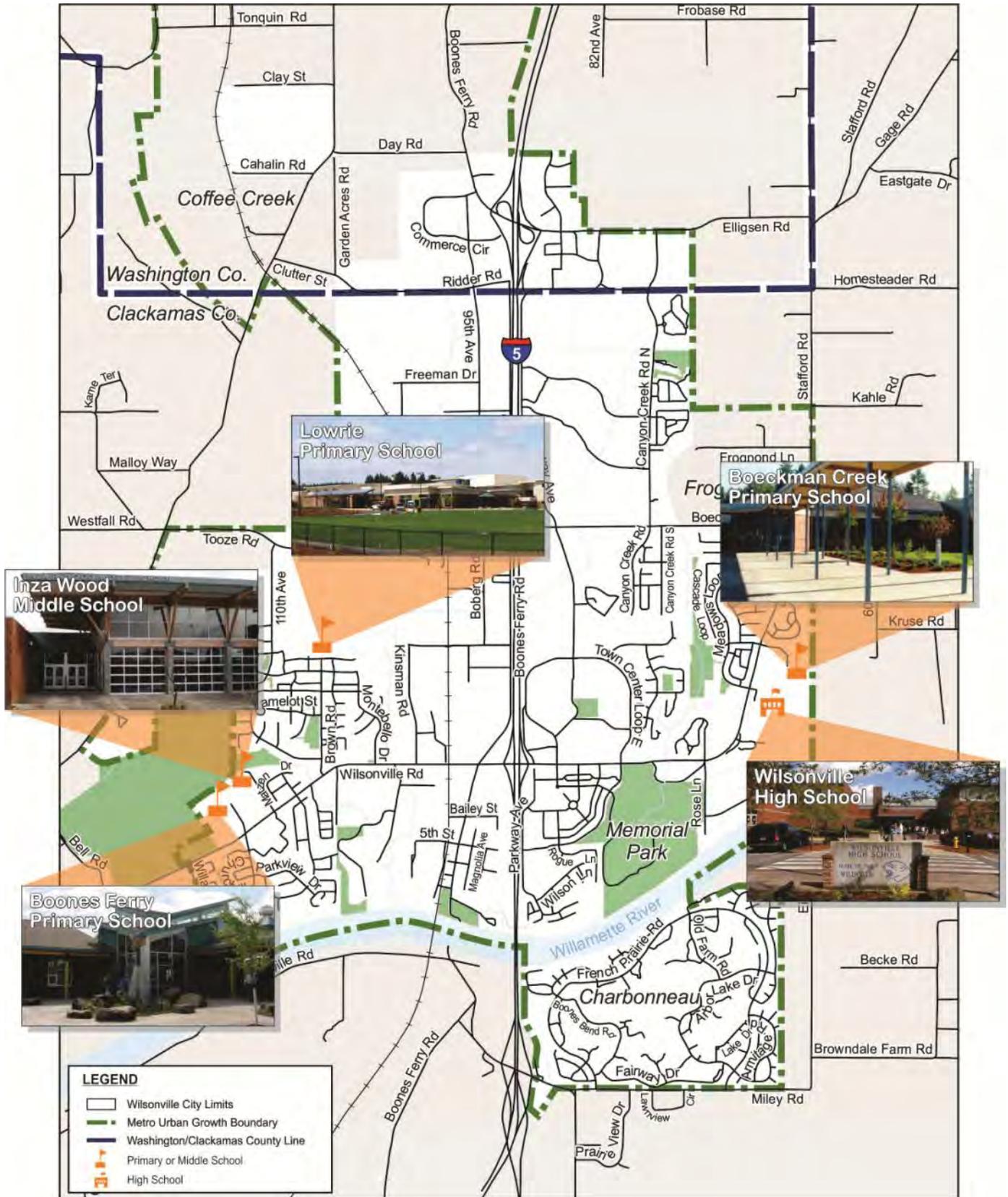
- **Educate** students, parents, and drivers about bicycle, pedestrian, and traffic safety skills, laws, and educational programs
- **Encourage** participation through fun events and contests such as walk-to-school days
- **Engineer** walking and biking infrastructure improvements along school routes
- **Enforce** traffic laws, particularly relating to speeding and pedestrian safety
- **Evaluate** program periodically to measure performance and adjust efforts as needed

Each of the five E's has a range of possible interventions and must be tailored to suit each school's unique needs and challenges.



*Students use the bike lanes on Wilsonville Road to bike to Inza Wood Middle School.*

**FIGURE 6-2. WILSONVILLE SCHOOLS**



## ADA COMPREHENSIVE ACCESS

Wilsonville has a goal to provide all users with access to integrated facilities and services that connect Wilsonville's neighborhoods, parks, schools, employment centers, and retail areas to each other and to the surrounding region. The City can achieve this goal by addressing the needs of those with limited mobility, consistent with the federal Americans with Disabilities Act (ADA).

Identifying and improving existing ADA-related deficiencies will be an ongoing effort to ensure that new facilities account for the needs of all users.

There are four specific areas of focus:

- Providing ADA-compliant curb ramps and pedestrian push buttons at intersection and roadway crossings.
- Maintaining sidewalks and curb ramps to meet ADA accessibility guidelines, including slopes and accessible area.
- Providing sidewalk connectivity between neighborhoods, businesses, transit stops, and other destinations.
- Providing sufficient on-street and off-street disabled parking stalls.



*Curb ramps with gradual slopes and large transit pads at the SMART Central transit center can accommodate users in wheel chairs or with other special needs.*

## SMART TRANSIT

The City's transit service plays an important role in providing mobility for residents, employees, and students who travel to, from, and within Wilsonville. It provides an important connection to the region, particularly due to Wilsonville's strong employment base and central location between Portland and Salem.

South Metro Area Regional Transit (SMART) is a City department and operates several fixed bus routes that serve Wilsonville and make connections to TriMet in Portland, Cherriots in Salem, and Canby Area Transit. SMART also manages various programs, including Dial-a-Ride (door-to-door service for elderly and disabled residents) and SMART Options (programs that support, educate, and encourage the use of active transportation modes and rideshare). SMART also provides Spanish language assistance regarding its services and on its website.

The primary transit hub in Wilsonville is the SMART Central at Wilsonville Station transit center, which provides connections to all SMART bus routes and TriMet's Westside Express Service (WES) commuter rail station. Wilsonville Station includes a 400-space park-and-ride lot and 48 bicycle lockers.

In the immediate future, SMART will benefit from focusing its efforts in five key improvement areas:

- **Transit Hubs** are key multimodal activity centers within the community that can most effectively provide efficient access and connections for transit users. Hubs include SMART Central/WES Commuter Rail station, Town Center Loop, Villebois Village Center, and other community and employment centers. By ensuring a high level of transit service is provided at these hubs, SMART can serve a greater number of transit riders most efficiently.



*A bus for Route 1X (servicing the Salem Transit Center) waits at its designated space in the SMART Central at Wilsonville Station transit center.*

- **Information Technology** is an important way for SMART to enhance transit efficiency and enhance customer service. Key investments in innovative technology will provide new venues to communicate with passengers, coordinate service in real-time with regional providers, and provide an enhanced understanding of operational metrics and measures.
- **Service Innovation** is an important way for Wilsonville to explore new transit service options or adjustments that can better meet the needs of its growing community. Possibilities include express service to downtown Portland and earlier peak commuter services for industrial and office uses that operate with an early morning shift. In addition, other service models can be considered, particularly relating to the integration of its various programs and services.

## OTHER TRANSIT REFERENCES

Wilsonville's transit system is also addressed in the following chapters:

- **Transit-Related Policies** (see Chapter 2: The Vision) are provided for land development coordination, transit services and facilities, pedestrian and bicycle access, and funding.
  - **Transit Needs** (see Chapter 4: The Needs) include regional transit connections, service coverage and bus frequency, pedestrian and bike access, new buses, developer coordination, and rider education and outreach.
  - **Transit Projects** (see Chapter 5: The Projects) include pedestrian access to transit, transit street improvements, bus stop amenities, and new buses.
- **Public Feedback Process** refinement would help SMART improve its efforts to respond to residents and employees regarding transit services, including bus routing and transit stop amenity decisions. This process should address both complaints and additional service requests while allowing an equal opportunity for input from those with opposing viewpoints. It should also give consideration to the needs of youth, seniors, people with disabilities, and environmental justice populations (including minorities and low-income families) due to the greater dependence that these citizens have on transit services for basic mobility.



## SMART OPTIONS AND TRANSPORTATION DEMAND MANAGEMENT (TDM)

SMART Options is a program administered by SMART to help residents and employees in Wilsonville find the best way to get to work. By using other options besides traveling alone in personal automobiles during peak congestion times, Wilsonville will extend the service life of its infrastructure improvements. These efforts are referred to as Transportation Demand Management (TDM) and are an important component of a well-managed transportation system.

SMART Options can help individuals determine whether to take transit (bus, train, or commuter rail), carpool/vanpool, walk, or bike. SMART Options also can provide information about car sharing, park and rides, close-to-home commuting, teleworking, and creative work schedules to help individuals make informed decisions regarding their travel needs.

SMART Options also provides free assistance to Wilsonville businesses that set up transportation programs. They can organize vanpools, write articles

### DEQ EMPLOYEE COMMUTE OPTIONS RULES

The Oregon Department of Environmental Quality (DEQ) Employee Commute Options Rules apply to all businesses within the Portland-metro area having more than 100 employees reporting to one work site. These businesses are required to:

- Receive approval from DEQ for a site specific trip reduction plan to reduce motor vehicle trips to their work site
- Survey and monitor progress at least every two years

SMART Options helps business comply with these rules.



*SMART Options staff participate in an information fair in the Town Center parking lot with education materials and a bus bike rack display.*

for employee newsletters, and hold transportation fairs. In addition, they are able to help with commuter surveys, trip reduction plan creation, and monitoring and compliance of the DEQ Employee Commute Options Rules, which apply to businesses with more than 100 employees.

The following additional TDM efforts will benefit the SMART Options program:

- **Mode Choice Surveys** performed on a consistent basis for residents and employees in each of the city's neighborhoods and commercial/industrial areas would allow the City to better understand what transportation choices are being made. This information would also allow the City to determine the impacts that its bicycle, pedestrian, and transit infrastructure improvements are having on the use of these facilities so that it can make improved decisions in the future.
- **Car Sharing Demand Monitoring** will be helpful for determining when sufficient interest is shown by residents and businesses to support a car sharing system.

## OTHER TRANSPORTATION DEMAND MANAGEMENT (TDM)

Transportation Demand Management (TDM) is the general term for implementing strategies that either reduce or shift the number of vehicles on the roadway (i.e., the “demand”). By managing transportation demand, Wilsonville will ensure more efficient use of the system’s available capacity and also support members of the community who may otherwise be increasingly burdened by the rising fuel prices.

The two primary methods for managing demand are to (1) reduce the overall number of vehicles on the roadway and (2) shift demand to less congested (i.e., off-peak) periods. These methods are best achieved by a combination of educational and outreach programs as well as supporting infrastructure and services (i.e., bicycle and pedestrian facilities and transit services).

In the past, the City has coordinated with large employers to schedule off-peak shift changes. This coordination was beneficial to both the City and the employers because it allowed development to occur even though there were capacity limitations at the Wilsonville Road interchange and the 95th Avenue/ Boones Ferry Road intersection. Traffic counts and observations suggest that the majority of these large employers still operate with off-peak shifts, but the City can improve its tracking and management.

There are three TDM improvements (in addition to the SMART Options program) that will benefit Wilsonville:

- **Off-Peak Shift Change Policies and Practices:** Develop consistent policies and practices to encourage, document, track, and manage off-peak shift changes, starting with employers who have already agreed to operate off-peak shifts. These efforts could be performed in conjunction with the SMART Options program. Because businesses that enact TDM measures may have lower traffic volumes (and associated system impacts) during peak congestion periods, these businesses may be

## PARKING MANAGEMENT PLANS

Parking management plans are a helpful way to inventory bicycle and motor vehicle parking supply in high demand locations (for example, park-and-ride lots, transit stations, and commercial areas). They do not require parking limitations but instead ensure that deliberate decisions are being made regarding parking provision and management.

There are two key locations that would benefit from parking management plans:

- Town Center
- WES Station

eligible for reduced Transportation System Development Charges (SDCs). Efforts should be made to provide these employers with public transit options that accommodate their schedules.

- **Town Center Parking Management Plan:** Prepare and adopt a parking management plan that includes an inventory of parking supply and usage, an evaluation of bicycle parking needs, the identification of desired improvement strategies and policies, and car sharing considerations (additional explanation provided in the call-out box above). This parking management plan will be an important component of an overall concept plan, which would benefit the Town Center area by ensuring the highest and best uses are provided to support the nearby businesses and residents and to formulize the City’s vision for this area.
- **WES Station Parking Management Plan:** Prepare and adopt a parking management plan that includes an inventory of parking supply and usage, an evaluation of bicycle parking needs, and the identification of desired strategies and policies (additional explanation provided in the call-out box above). These considerations should support future park-and-ride demand increases to avoid impacts resulting from inadequate capacity.

## TRANSPORTATION SYSTEM MANAGEMENT AND OPERATIONS

Transportation System Management and Operations (TSMO) is the general term for implementing various solutions that enhance the performance of existing and programmed transportation infrastructure. The focus of TSMO is to reduce congestion and save money by improving the transportation system's efficiency before expanding infrastructure. Improving efficiency requires a collaborative effort by system managers, operators, and users both prior to and during travel.

Four of the primary TSMO strategies include:

- **Access Management** strategies reduce traffic conflicts at intersections and driveways in order to improve traffic flow and safety (Addressed in Chapter 5: The Standards).
- **Safety Improvements** support the efficient use of existing infrastructure by reducing safety-related incidents.
- **Transportation Demand Management (TDM)** strategies encourage users to choose other transportation modes besides traveling alone in their vehicles or to travel at off-peak periods of the day.
- **Intelligent Transportation System (ITS)** strategies involve the deployment and management of advanced technologies that collect and distribute information to both users and operator staff so they can most effectively use and manage the transportation system.

## INTELLIGENT TRANSPORTATION SYSTEM

The development and management of intelligent transportation system (ITS) solutions is one of the most important areas of recent transportation-related technological advancement. ITS strategies are a type of Transportation System Management and Operation (TSMO) strategy (additional explanation provided in the call-out box at left).

ODOT currently manages and operates the ITS infrastructure along the I-5 corridor. In addition, Clackamas County manages and operates the ITS infrastructure in and around Wilsonville. One of the basic ITS strategies is to effectively operate the City's traffic signals. Two of the signalized roadway corridors currently have coordinated signals that allow improved traffic flow:

- Wilsonville Road from Kinsman Road to Town Center Loop East
- Boones Ferry Road/Elligsen Road from Day Road to Parkway Center Drive

Additional ITS solutions will benefit Wilsonville:

- **Coordinate with Clackamas County** to ensure that projects include improvements consistent with those identified in the Clackamas County Intelligent Transportation System (ITS) Plan, particularly on Wilsonville Road and Elligsen Road near the two I-5 interchanges. Clackamas County is one of the agencies that is part of the Transport ITS working group made up of ITS professionals within the Metro boundary.
- **Install 3-Inch Conduit** as part of all Arterial and Collector roadway improvement projects to prepare the City for future fiber communications. This conduit can be used for fiber, traffic counters, and other ITS equipment. By connecting Clackamas County's fiber network to the City's traffic signals and traffic control cameras, Clackamas County will be able to transfer

information back to their operations center in order to more effectively monitor and operate the City's traffic signal system. This infrastructure will also support emergency responders in performing rapid incident detection and response. SMART would also benefit from improved integration with traffic operations by connecting its new service and operations center to Clackamas County's fiber.

- **Deploy Adaptive Signal Timing on Wilsonville Road** from Brown Road to Town Center Loop East consistent with Clackamas County's ITS Plan,

including the installation of video monitoring cameras and vehicle detection equipment to collect traffic counts and speeds.

- **Collect and Manage Transportation Data** to help the City evaluate the performance of its transportation system and to help travelers make more informed decisions regarding their choice of mode, departure time, and routing. The City will first need to evaluate ways to collect and distribute information in coordination with Clackamas County.

*The Clackamas County Traffic Management Center is located in Oregon City and is connected to Wilsonville via State, County, and City communication links. These links allow County staff to remotely manage and operate Wilsonville's traffic signals and ITS infrastructure.*




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*“Transportation is important for all of us whether you ride your bike around town, whether you walk, or whether you drive a car, take transit, or for that matter, drive a truck through town. It is very important for you to be able to get where you want to go and not have a lot of trouble doing so.”*

*Nancy Kraushaar  
Community Development Director*

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## BIKE SMART AND WALK SMART

Wilsonville benefits from focusing staff resources on coordinating bicycle and pedestrian outreach and infrastructure planning, which it does primarily through its Bike Smart and Walk Smart programs. SMART and Community Development staff collaborate to lead the City's efforts.

Four ongoing efforts will help improve walking and biking in Wilsonville:

- Maintain an updated **bike and pedestrian map** that provides the current bicycle and pedestrian facilities that are available to Wilsonville residents for these mode choices.
- Expand **bike and pedestrian safety education and outreach** to the general public, focusing on clinics and workshops that communicate safety messages to particular audiences like children, motorists, and older pedestrians.
- Coordinate **group rides and walking tours** to identify street, trail, art and natural amenities that are available to residents.
- Staff an **Active Transportation Planner** that works for both Community Development and SMART and is tasked with development review, plan implementation and updates, safety education and outreach, and program support (Bike SMART, Walk SMART, and Safe Routes to Schools). This planner could also continue **regional coordination** efforts with other agency Active Transportation Plans and Metro.

*In 2011, Wilsonville was awarded the designation of being a Walk Friendly Community due to its commitment to improving walkability and pedestrian safety through comprehensive programs, plans, and policies. The Bronze Level designation indicates the City is "on the right track" but has several areas where it can continue to improve.*

## NATIONAL RECOGNITION AVAILABLE AS WALK FRIENDLY AND BIKE FRIENDLY COMMUNITY

Two national recognition programs have been developed in recent years to encourage towns and cities across the U.S. to establish or recommit to a high priority for supporting safer walking and bicycling environments. These programs evaluate current efforts and provide recommendations for improvement:

- **Walk Friendly Communities** designation is awarded at one of five levels (from lowest to highest): honorable mention, bronze, silver, gold, and platinum. Wilsonville was awarded a bronze designation in 2011. As additional pedestrian improvements are made throughout the city, Wilsonville may consider reapplying for a higher designation.
- **Bicycle Friendly Community (BFC) Campaign** is administered by the League of American Bicyclists and awards one of four designations (from lowest to highest): bronze, silver, gold, and platinum. Wilsonville has not yet applied for a BFC designation, but doing so will provide the City with recognition while also providing helpful recommendations for how it can continue to improve its bicycle network.





Wilsonville's transportation system plan (TSP) provides standards, projects, and programs that, when put into action, will improve the City's transportation system. By tracking specific performance measures with each successive TSP update, the City will learn if its planning efforts are leading to the desired outcomes and if additional improvements are needed. In this way, Wilsonville will make continued progress towards its transportation system vision and goals.

To be most effective, the City's transportation performance measures should provide its decision-makers with metrics that reflect what progress is being made towards Wilsonville's goals and policies. They should also include a combination of system-wide and facility-level performance measures so that incremental progress can be determined for the entire system as well as on a project-by-project basis.

Performance measurement is an approach to transportation planning that has been receiving increased national and regional attention. The new federal transportation legislation, Moving Ahead for Progress in the 21st Century (MAP-21), transitions the nation towards performance-based, outcome-driven planning processes. In doing so, this law is not prescriptive regarding what the standards should be, but instead requires that states and metropolitan planning organizations (MPOs) establish their own targets and measures. This encourages the framework of performance measurement throughout the nation without requiring a one-size-fits-all approach.

*Performance measures allow Wilsonville to . . .*

- *Track the benefits of its efforts and*
- *Identify areas where additional improvements are needed*

*So that it can . . .*

- *Make more informed investment decisions and*
- *Best achieve its vision and goals.*



## PERFORMANCE MEASURES

Though it preceded MAP-21, Metro’s Regional Transportation Plan (RTP) also focuses on performance targets and standards. While there are some performance targets specified by Metro, Metro requires each city to identify its own performance measures for five areas and then to evaluate them with each successive transportation system plan (TSP) update to check its progress.

Table 7-1 lists Wilsonville’s performance measures, including the 2035 targets and how they will be

measured. The majority of these performance measures were selected because they are recommended by Metro and can be relatively easily measured using Metro’s travel demand model, which is also the basis for Wilsonville’s future travel demand forecasting. The one performance target that differs is safety. Because the City has such a low number of collisions, its target is to keep the collision rate below the statewide average.

**Table 7-1. Wilsonville Performance Measures**

Performance Area	2035 Performance Target <sup>a</sup>	How Measured
Safety	Maintain collision rates below the statewide average and zero fatalities	Analysis of ODOT, Washington County, and Clackamas County collision data
Vehicle Miles Traveled (VMT) Per Capita	Reduce VMT/capita by 10% compared to 2005 <sup>b</sup>	Estimate using travel demand model
Freight Reliability	Reduce vehicle hours of delay <sup>c</sup> for truck trips by 10% from 2005	Estimate using travel demand model for roadways on City’s freight network
Congestion	Reduce vehicle hours of delay <sup>c</sup> (VHD) per person by 10% from 2005	Estimate using travel demand model
Walking, Biking, and Transit Mode Shares	Triple walking, biking and transit mode share from 2005	Use Metro mode split forecasts and provide qualitative assessment; supplement with SMART data

<sup>a</sup> Performance targets are for the 2035 horizon year. Performance tracking during intermediate years should be compared against interpolated values.

<sup>b</sup> Oregon House Bill 3543 codifies greenhouse gas emission reductions, and the Portland Metro area has set this regional target.

<sup>c</sup> Delay is defined in the 2035 RTP as the amount of time spent in congestion > than .9 V/C (see p.5-7 of RTP)

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*“The TSP is doing an excellent job addressing bicycle and pedestrian issues. Once the TSP is adopted, it is going to be a matter of following through to make these things happen.”*

*Al Levit  
Planning Commission*

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